California High-Speed Train Project

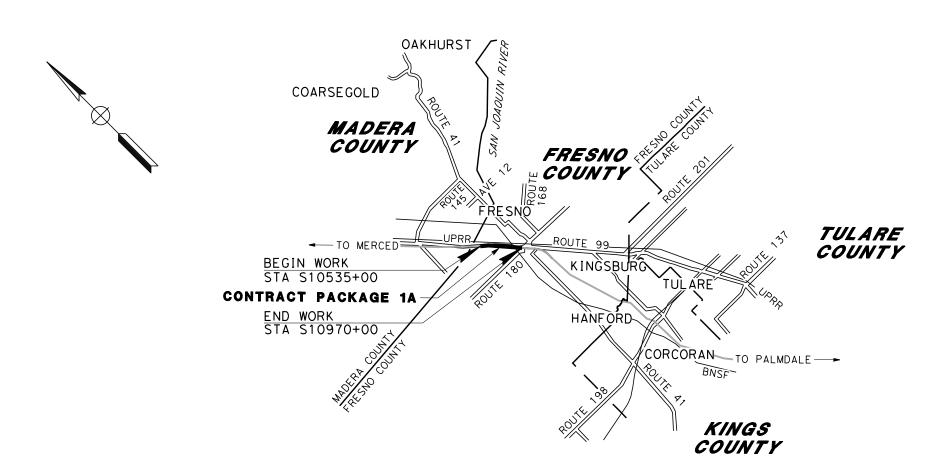


Request for Proposal for Design-Build Services

RFP No.: HSR 11-16
Contract Package 1A
North of Veterans Blvd to North of Stanislaus St



PROPOSED PRELIMINARY DESIGN CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION **CONTRACT PACKAGE 1A**











AECOM 2020 L Street, Suite 300 Socramenta, CA 9581 CH2MHILL

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129

VOLUME 1 - GENERAL & TRACK ALIGNMENT

DRAWING No.	REV No.	DRAWING DESCRIPTION
		VOLUME 1A - GENERAL
GE-D0001		PACKAGE 1A - TITLE SHEET
GE - A0001		PACKAGE 1A - GENERAL - INDEX OF DRAWINGS
TT-B0001		PACKAGE 1A - GENERAL - NOTES
VS-C0001		PACKAGE 1A - GENERAL - SURVEY CONTROL DATA - SHEET 1 OF 4
VS-C0002		PACKAGE 1A - GENERAL - SURVEY CONTROL DATA - SHEET 2 OF 4
VS-C0003		PACKAGE 1A - GENERAL - SURVEY CONTROL DATA - SHEET 3 OF 4
VS-C0004		PACKAGE 1A - GENERAL - SURVEY CONTROL DATA - SHEET 4 OF 4
		VOLUME 1B - TRACK ALIGNMENT
TT-B0001		PACKAGE 1A - TRACK GUIDEWAY - KEY MAP
TT-D3001		PACKAGE 1A - TRACK GUIDEWAY - TYPICAL SECTIONS
TT-D3002		PACKAGE 1A - TRACK GUIDEWAY - TYPICAL SECTIONS
TT-D3003		PACKAGE 1A - TRACK GUIDEWAY - TYPICAL SECTIONS
TT-D3004		PACKAGE 1A - TRACK GUIDEWAY - TYPICAL SECTIONS
TT-D3005		PACKAGE 1A - TRACK GUIDEWAY - TYPICAL SECTIONS
TT-D3006		PACKAGE 1A - TRACK GUIDEWAY - TYPICAL SECTIONS
TT-D3007		PACKAGE 1A - TRACK GUIDEWAY - TYPICAL SECTIONS
TT-D3008		PACKAGE 1A - TRACK GUIDEWAY - TYPICAL SECTIONS
TT-D3009		PACKAGE 1A - TRACK GUIDEWAY - TYPICAL SECTIONS
TT-D0001		PACKAGE 1A - TRACK GUIDEWAY - HORIZONTAL ALIGNMENT DATA
TT-D1000		PACKAGE 1A - TRACK GUIDEWAY - PLAN AND PROFILE - STA. 10535+00 TO 10554+00
TT-D1001		PACKAGE 1A - TRACK GUIDEWAY - PLAN AND PROFILE - STA. 10554+00 TO 10582+00
TT-D1002		PACKAGE 1A - TRACK GUIDEWAY - PLAN AND PROFILE - STA. 10582+00 TO 10610+00
TT-D1003		PACKAGE 1A - TRACK GUIDEWAY - PLAN AND PROFILE - STA. 10610+00 TO 10638+00
TT-D1004		PACKAGE 1A - TRACK GUIDEWAY - PLAN AND PROFILE - STA. 10638+00 TO 10666+00
TT-D1005		PACKAGE 1A - TRACK GUIDEWAY - PLAN AND PROFILE - STA. 10666+00 TO 10694+00
TT-D1006		PACKAGE 1A - TRACK GUIDEWAY - PLAN AND PROFILE - STA. 10694+00 TO 10722+00
TT-D1007		PACKAGE 1A - TRACK GUIDEWAY - PLAN AND PROFILE - STA. 10722+00 TO 10750+00
TT-D1008		PACKAGE 1A - TRACK GUIDEWAY - PLAN AND PROFILE - STA. 10750+00 TO 10778+00
TT-D1009		PACKAGE 1A - TRACK GUIDEWAY - PLAN AND PROFILE - STA. 10778+00 TO 10806+00
TT-D1010		PACKAGE 1A - TRACK GUIDEWAY - PLAN AND PROFILE - STA. 10806+00 TO 10834+00
TT-D1011		PACKAGE 1A - TRACK GUIDEWAY - PLAN AND PROFILE - STA. 10834+00 TO 10862+00
TT-D1012		PACKAGE 1A - TRACK GUIDEWAY - PLAN AND PROFILE - STA. 10862+00 TO 10890+00
TT-D1013		PACKAGE 1A - TRACK GUIDEWAY - PLAN AND PROFILE - STA. 10890+00 TO 10918+00
TT-D1014		PACKAGE 1A - TRACK GUIDEWAY - PLAN AND PROFILE - STA. 10918+00 TO 10946+00
TT-D1015		PACKAGE 1A - TRACK GUIDEWAY - PLAN AND PROFILE - STA. 10946+00 TO 10974+00
		TT-D0016 TO TT-D1029 NOT USED
TT-D1030		PACKAGE 1A - SJVRR SOUTH SPUR - PLAN AND PROFILE - STA. 0+00 TO STA. 8+68.74

						DESIGNED BY A. SHIELDS
						DRAWN BY A. SHIELDS
						CHECKED BY A. BOONE
						IN CHARGE A. BOONE
REV	DATE	BY	СНК	APP	DESCRIPTION	DATE 02/01/2012

PROPOSED PRELIMINARY DESIGN NOT FOR

CONSTRUCTION

AECOM Technical Services, Inc. 2020 L Street, Suite 300 Sacramento, CA 958II **CH2MHILL**



CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A GENERAL INDEX OF DRAWINGS

CONTRACT NO.
DRAWING NO.
GE-A0001
SCALE
NONE
SHEET NO.
SHEET NO.

- 2. REFER TO SCOPE OF WORK DOCUMENTATION IN THE PROCUREMENT PACKAGE FOR SCOPE REQUIREMENTS. FINAL DESIGN AND DIMENSIONS SHALL BE BASED ON DESIGN CRITERIA, DIRECTIVE DRAWINGS AND STANDARD DRAWING REQUIREMENTS.
- B. CONTROL SURVEY AND MAPPING:
- 1. NAD 83 (NSRS2007), (1991.35)CALIFORNIA COORDINATE SYSTEM DATUM SHALL BE USED AS HORIZONTAL COORDINATE VALUES.
- 2. NAVD 88 VERTICAL DATUM SHALL TO BE USED AS VERTICAL DATUM.
- 3. MAPPING SHALL BE BASED ON CALIFORNIA COORDINATE SYSTEM (CCS 83) IN US CUSTOMARY
- 4. NETWORK OF CONTROL SURVEY MONUMENTS WAS ESTABLISHED AT TWO MILE INTERVALS ALONG THE CHSTP ALIGNMENT. THESE CONTROL MONUMENTS SHALL BE USED IN FUTURE SURVEYS TO ENSURE THAT SURVEY ACTIVITIES ARE BASED ON THE SAME ADJUSTMENT POSITIONS AND EPOCH THAT TRANSITIONS BETWEEN ZONES OF THE CALIFORNIA COORDINATE SYSTEM ARE UNIFORM AND CORRECT.

TRACK GENERAL NOTES:

GENERAL TRACK NOTES:

- 1. THE GENERAL BASIS FOR TRACK STANDARDS AND MATERIALS SHALL BE THE AREMA MANUAL.
- 2. TRACK AND TRACK COMPONENTS SHALL BE DESIGNED AND FABIRICATED TO PERFORM UNDER THE PREVAILING AND EXTREME CLIMATIC AND ENVIRONMENTAL CONDITIONS OCCURING WITHIN THE GEOGRAPHIC EXTENT OF THE SYSTEM.
- 3. THE PROFILE IS CARRIED ON THE TOP OF LOWER RAIL THROUGH HORIZONTAL CURVES AND SPIRALS FOR THE DESIGNATED TRACK.
- 4. THE LENGTHS OF LINES ARE BASED ON CENTER OF TRACK ALIGNMENT.
- 5. UNLESS SEPARATE TRACK PROFILES ARE GIVEN, TRACK PARALLEL TO THE DESIGNATED TRACK ARE AT THE SAME TOP OF RAIL ELEVATIONS PROJECTED ON EITHER PERPENDICULAR OR RADIAL LINES FROM THE DESIGNATED TRACK CENTERLINES.
- 6. BALLASTED TRACKS ARE GENERALLY PREFERRED FOR YARD TRACKS. DESIGNERS SHALL FOLLOW THE REQUIREMENTS ASSOCIATED WITH CONSTRUCTION OF BALLASTED TRACK IN THE CALIFORNIA HIGH SPEED TRAIN DESIGN MANUAL.
- 7. DIMENSIONS SHOWN ON THE DESIGN DRAWINGS WERE DEVELOPED BY PRELIMINARY ENGINEERING AND FINAL DIMINSIONS WILL BE DETERMINED ON THE FINAL TRACK PLANS BY THE DESIGN
- 8. "ORIGINAL GROUND" SHOWN ON CROSS SECTIONS REFERS TO THE APPROXIMATE EXISTING GROUND LINE AT THE DESIGNATED CENTERLINE, BASELINE, LAYOUT LINE OR SECTION LINE.

						DESIGNED BY A. SHIELDS	
						DRAWN BY A. SHIELDS	
						CHECKED BY A. BOONE	
						IN CHARGE A. BOONE	
REV	DATE	BY	СНК	APP	DESCRIPTION	02/01/2012	

PROPOSED PRELIMINARY DESIGN

NOT FOR CONSTRUCTION

2020 L Street, Suite 300 Sacramento, CA 95811 **CH2MHILL**



CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A TRACK GUIDEWAY GENERAL NOTES

CONTRACT NO.
DRAWING NO.
TT-B0001

NONE SHEET NO.

NOTES:

1. HORIZONTAL CONTROL DATUM:

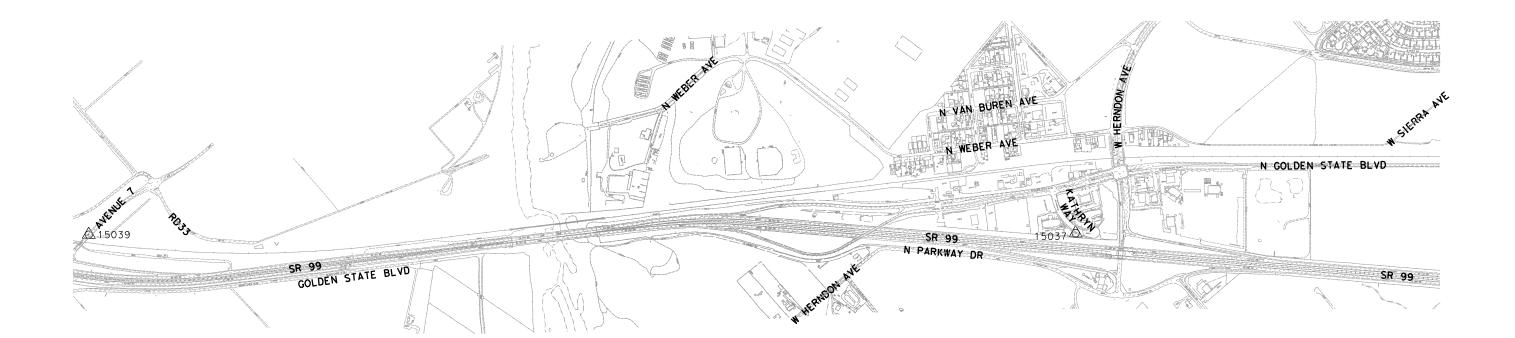
THE HORIZONTAL GRID COORDINATES SHOWN HEREIN ARE BASED UPON THE NORTH AMERICAN DATUM OF 1983 (NAD 83). CALIFORNIA STATE PLANE COORDINATE SYSTEM, ZONE IV.

2. VERTICAL CONTROL DATUM:

THE VERTICAL DATUM FOR THE EVEVATIONS SHOWN HEREIN ARE BASED UPON THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88).

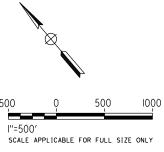
3. PROJECTION:

LAMBERT CONIC PROJECTION



HORIZONTAL AND VERTICAL CONTROL INFORMATION

POINT #	DESCRIPTION	NORTHING	EASTING	ELEVATION
15037	PB CONTROL: S182-P 2½" BRONZE DISK IN 1" IRON PIPE IN 8" CONCRETE PIER, STAMPED "CHSRA S182P LS 4430 2010"	2187855.88	6292923.69	300.64
15039	NGS CONTROL: DG9697	2194533.009	6285098.921	318.89



V								
/07/							DESIGNED BY A. SHIELDS	
-							DRAWN BY F. DeMARCO	PROPOSED Preliminary
_							CHECKED BY	DESIGN
Nan							IN CHARGE A. BOONE	NOT FOR
SUL	REV	DATE	ВΥ	СНК	APP	DESCRIPTION	01/09/2012	CONSTRUCTION

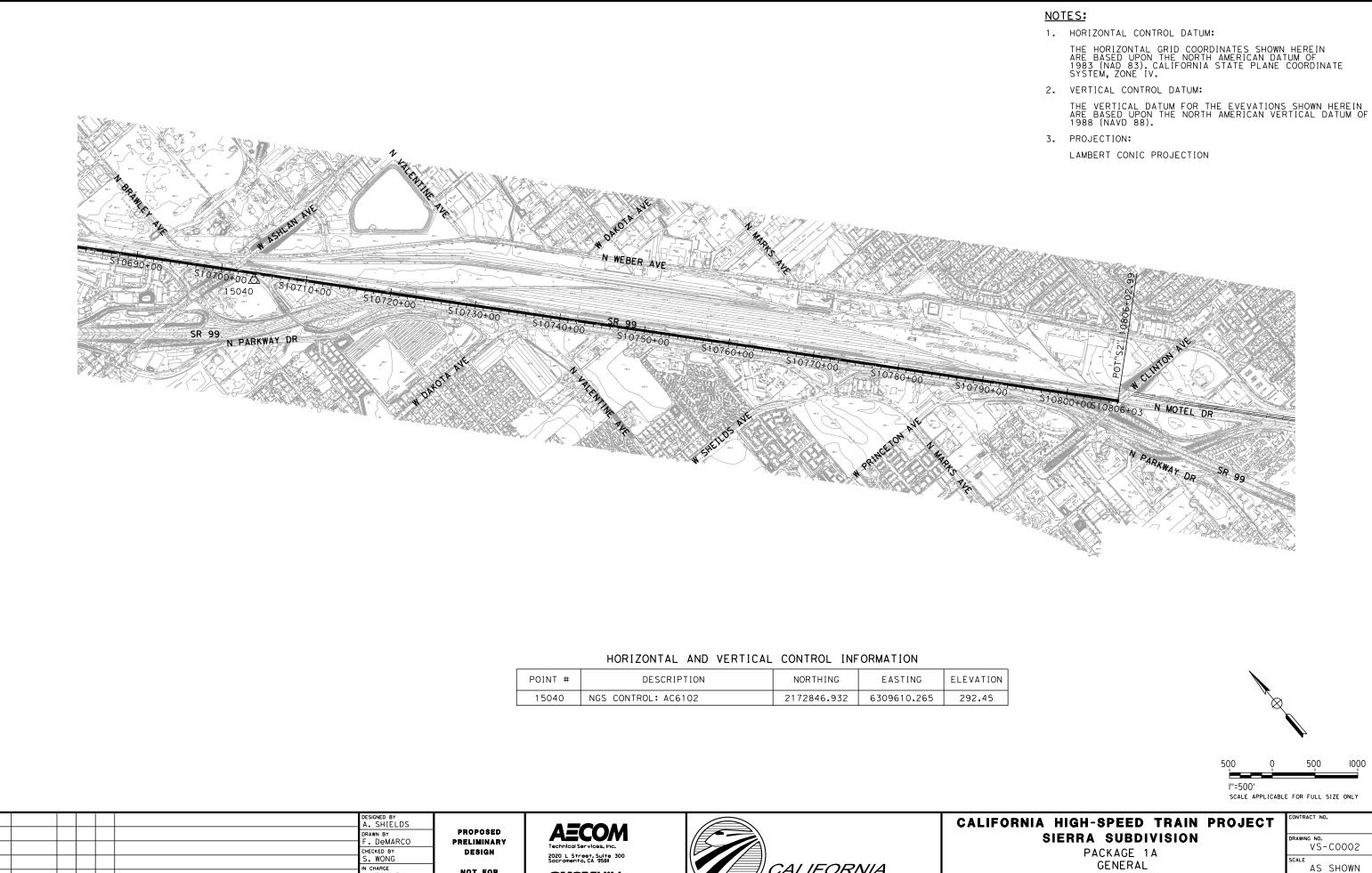
2020 L Street, Suite 300 Sacramento, CA 95811 **CH2MHILL**



CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A GENERAL SURVEY CONTROL DATA SHEET 1 OF 4

CONTRACT NO.
DRAWING NO.
VS-C0001
SCALE
AS SHOWN
SHEET NO.



REV DATE

BY CHK APP

CHARGE A. BOONE 02/01/2012 DESCRIPTION

NOT FOR

CONSTRUCTION

CH2MHILL



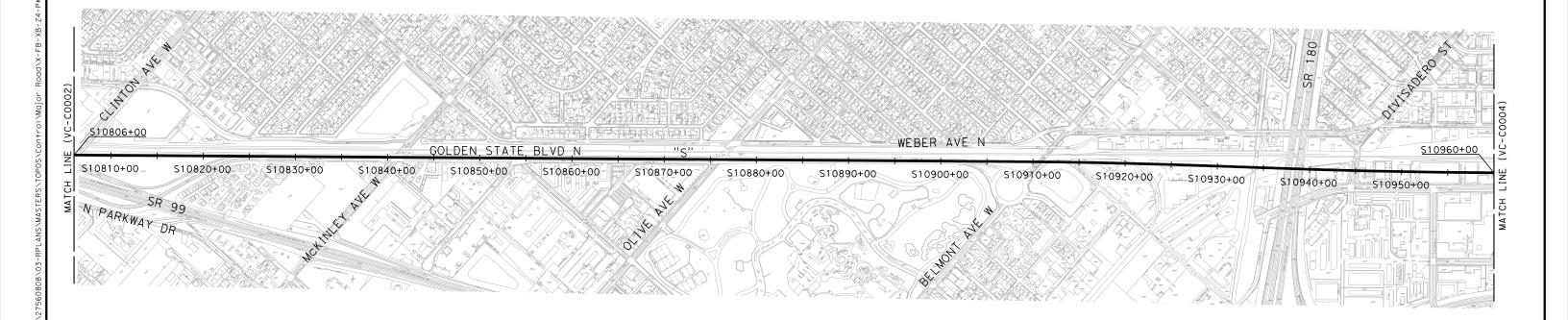
SURVEY CONTROL DATA SHEET 2 OF 4

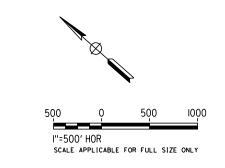
CONTRA	CT NO		
DRAWIN		-C0002	
SCALE	AS	SHOWN	
SHEET	NO.		

NOTES:

1. FOR LOCATION OF CONTROL POINT # 15037 (S182P) SEE SHEET VS-COO1

FOR LOCATION OF CONTROL POINT # S193P SEE SHEET VS-C0004





cariton 8					
_				G. HORTON	NOT FOR CONSTRUCTION
<u>-</u>				J. LABANOWSKI IN CHARGE	
le.				CHECKED BY	DESIGN
- L				DRAWN BY	PROPOSED Preliminary
7/1/2				DESIGNED BY S. CHRISTENSEN	



CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A GENERAL SURVEY CONTROL DATA SHEET 3 OF 4

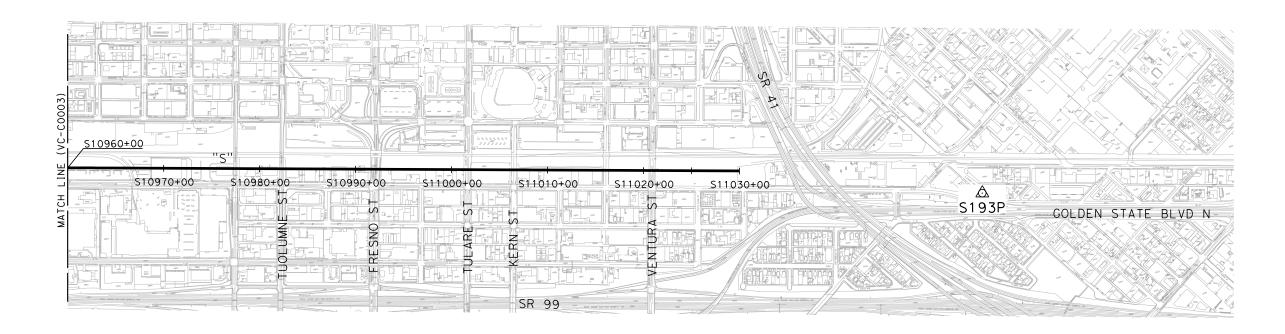
CONTR	ACT NO).	
DRAWIN	IG NO.		
		C0003	
SCALE			
	AS	SHOWN	
SHEET	NO.		

- 1. DATA SHOWN HEREON IS BASED ON SURVEY CONTROL MONUMENT DATA SHEETS AND MAP ON FILE IN THE OFFICE OF THE CHSRA
- 2. HORIZONTAL CONTROL DATUM:

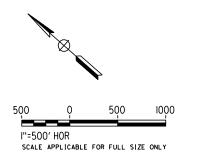
THE HORIZONTAL GRID COORDINATES SHOWN HEREIN ARE BASED UPON THE NORTH AMERICAN DATUM OF 1983 (NAD83). CALIFORNIA STATE PLANE COORDINATE SYSTEM, ZONE IV, US SURVEY FEET

- 3. VERTICAL CONTROL DATUM:
 THE VERTICAL CONTROL DATUM FOR THE ELEVATIONS SHOWN HEREIN
 ARE BASED UPON THE NORTH AMERICAN VERTICAL DATUM OF
 1988 (NAVD88), US SURVEY FEET
- 4. PROJECTION:

LAMBERT CONIC PROJECTION



[POINT #	DESCRIPTION	N	E	ELEV	LOCATION				
		FOUND STANDARD OUSDA	ZON	E 4		IN THE CITY OF FRESNO, COUNTY OF FRESNO, 450 FEET				
	S193P	FOUND STANDARD CHSRA 2-1/2" BRONZE DISK IN 1" IRON PIPE IN 8" CONCRETE PIER STAMPED "S193P LS4430 2010"	2146202.84	6332518.62	286.04	SOUTHERLY ALONG CHERRY AVENUE FROM THE INTERSECTION OF CALIFORNIA AVENUE, 7.8 FEET WESTERLY OF THE WEST CURB FACE OF CHERRY AVENUE, 11.8 FEET NORTHEASTERLY OF THE NORTHEASTERLY CURB FACE OF G STREET.				



2/1/2							DESIGNED BY S. CHRISTENSEN	
len 15							DRAWN BY S. AVILES	
ō.							CHECKED BY J. LABANOWSKI	
ton_							IN CHARGE G. HORTON	
car	REV	DATE	BY	СНК	APP	DESCRIPTION	12/08/11	

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DESIGN
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CONSTRUCTION



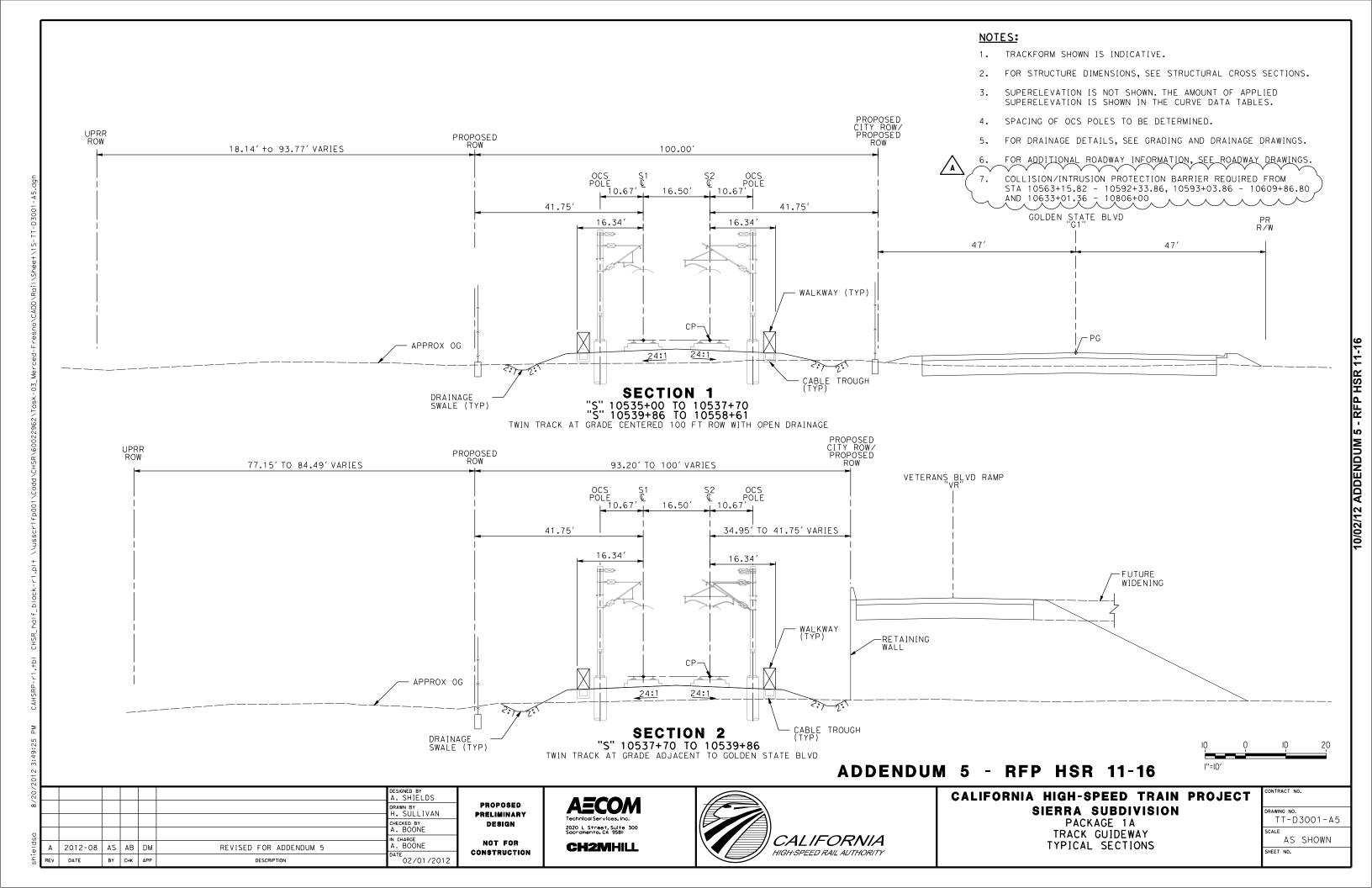


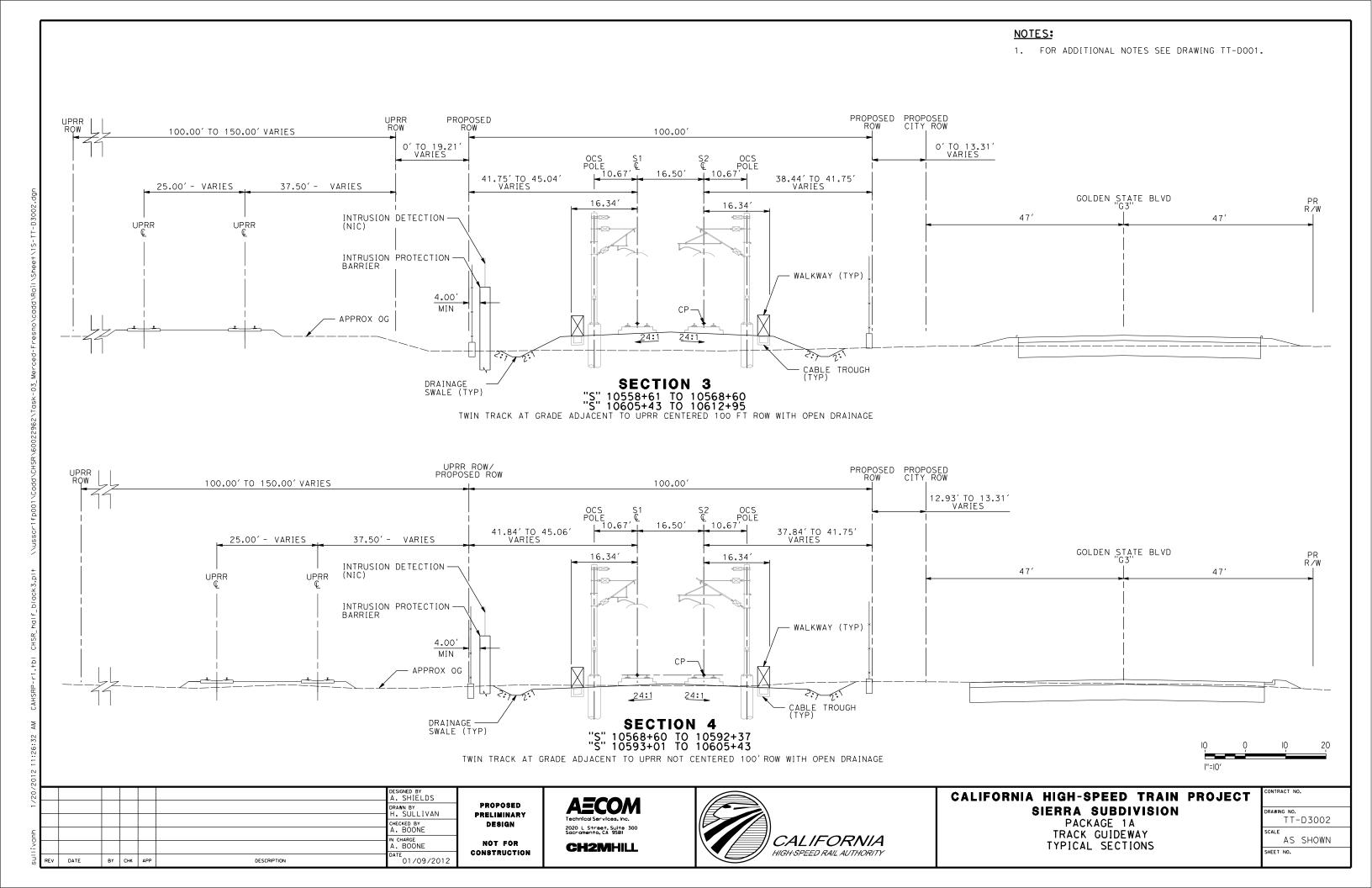
CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A GENERAL SURVEY CONTROL DATA SHEET 4 OF 4

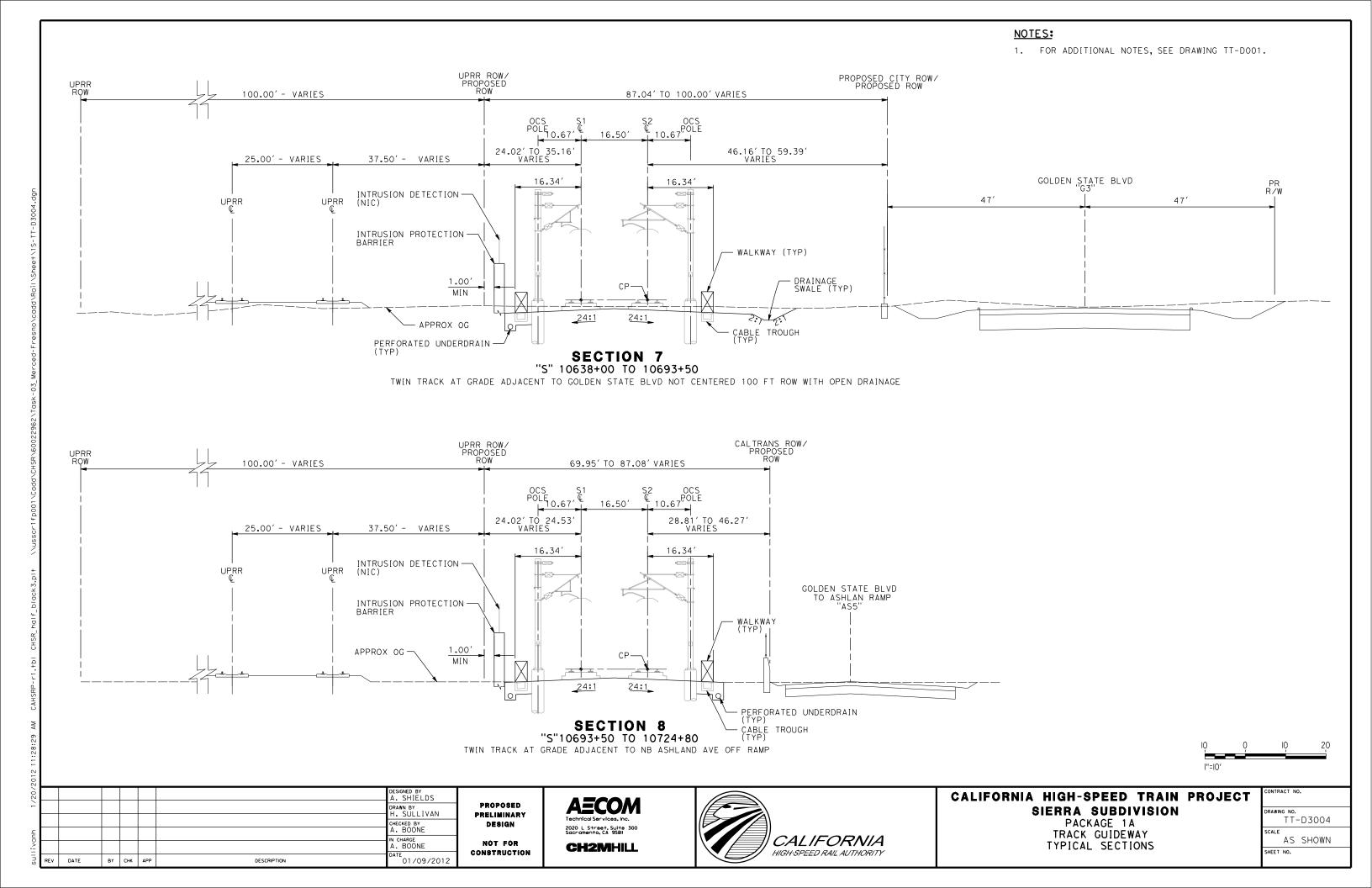
Γ	CONTR	ACT NO).
	DRAWIN		-C0004
	SCALE	AS	SHOWN
	CHEET	NO	

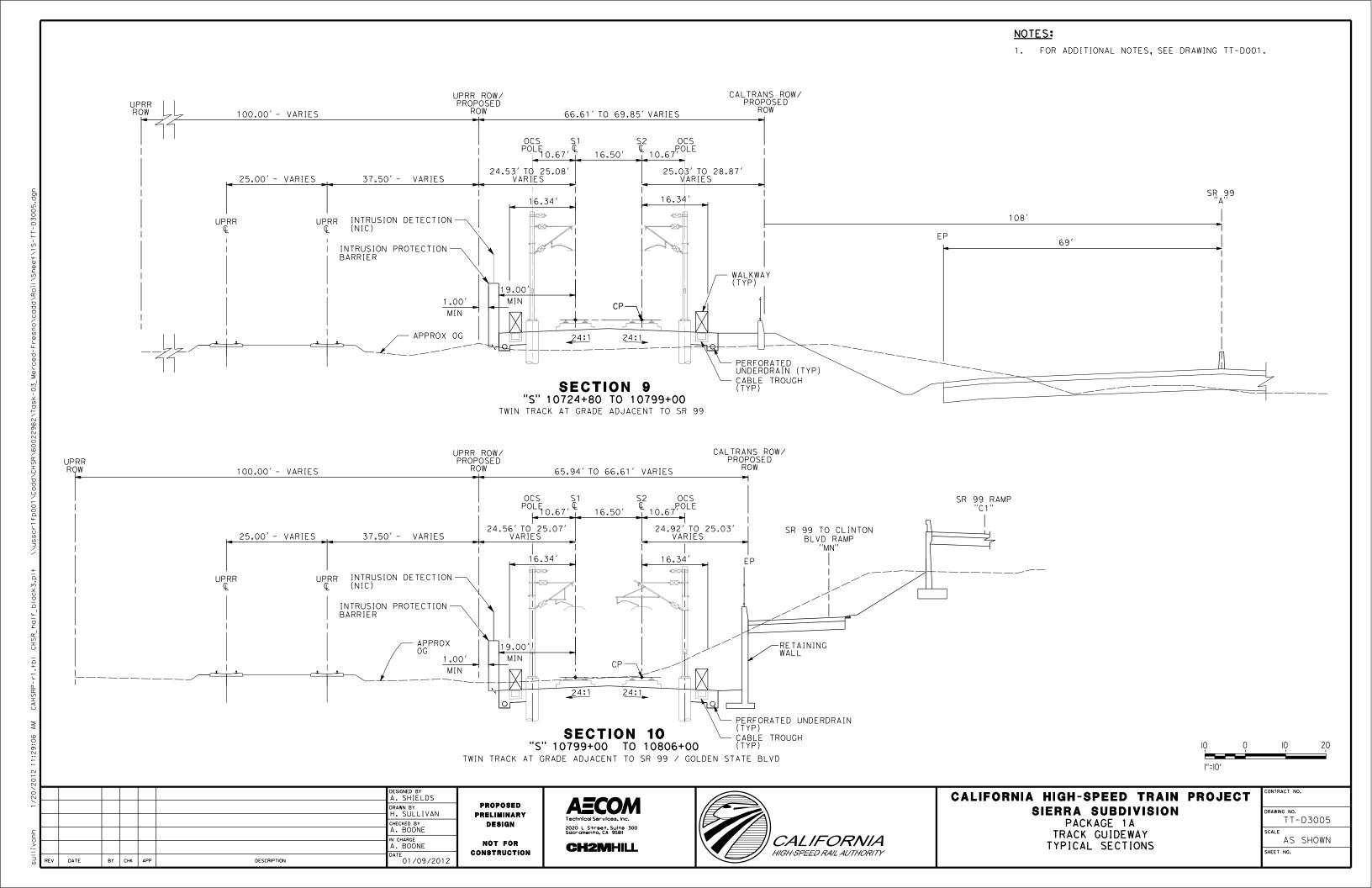
REFER TO SCOPE OF WORK DOCUMENTATION IN THE PROCUREMENT PACKAGE FOR SCOPE REQUIREMENTS. FINAL DESIGN AND DIMENSIONS SHALL BE BASED ON DESIGN CRITERIA MANUAL, DIRECTIVE DRAWINGS, AND STANDARD DRAWINGS REQUIREMENTS. START STA S10535+00 (CP1A) END STA 1801+43.63 (HYBRID) **FRESNO** UPRR TT-D1004 TT-D1003 STATE ROUTE 99 TT-D1002 TT-D1005 TT-D1006 TT-D1001 TT-D1007 STATE ROUTE 99 TT-D1000 STATE ROUTE 99 PACKAGE 1A CROSSOVERS FRESNO UPRR TT-D1014 TT-D1008 TT-D1009 TT-D1010 STATE ROUTE 99 TT-D1011 TT-D1012 TT-D1013 TT-D1015 ROEDING PARK TURNOUTS STATE ROUTE 99 STATE ROUTE 99 2000 SCALE APPLICABLE FOR FULL SIZE ONLY DESIGNED BY K. SEYMOUR CALIFORNIA HIGH-SPEED TRAIN PROJECT PROPOSED DRAWN BY P. TONKIN SIERRA SUBDIVISION PRELIMINARY URS HMM ARUP TT-B0001 PACKAGE 1A TRACK GUIDEWAY CHECKED BY DESIGN CALIFORNIA AS SHOWN N CHARGE R. COFFIN NOT FOR KEY MAP CONSTRUCTION SHEET NO. 3/01/12 BY CHK APP DESCRIPTION





NOTES: 1. FOR ADDITIONAL NOTES, SEE DRAWING TT-D001. OCS \$1 \$2 OCS POLE & POLE 10.67' 16.50' 10.67' SECTION 5 "S" 10592+37 TO 10593+01 TWIN TRACK OVER HERNDON CANAL PROPOSED CITY ROW/ PROPOSED ROW PROPOSED ROW UPRR ROW 100.00' 100.00' TO 150.00' VARIES 9.21' TO 38.58' VARIES OCS POLE 10.67 16.50 35.16' TO 41.75' VARIES 41.75' TO 48.32' VARIES 37.47' - VARIES 25.00' - VARIES 16.34 16.34 INTRUSION DETECTION — (NIC) PR R∕W GOLDEN STATE BLVD UPRR ¢ UPRR 47′ 47′ INTRUSION PROTECTION -WALKWAY (TYP) 4.00 MIN APPROX OG 24:1 24:1 DRAINAGE SWALE (TYP) CABLE TROUGH SECTION 6 "S" 10612+95 TO 10638+00 TWIN TRACK AT GRADE ADJACENT TO GOLDEN STATE BLVD CENTERED 100 FT ROW WITH OPEN DRAINAGE DESIGNED BY A. SHIELDS CALIFORNIA HIGH-SPEED TRAIN PROJECT **AECOM** PROPOSED DRAWN BY H. SULLIVAN SIERRA SUBDIVISION PRAWING NO. PRELIMINARY PACKAGE 1A TRACK GUIDEWAY CHECKED BY A. BOONE DESIGN 2020 L Street, Suite 300 Sacramento, CA 958II CALIFORNIA AS SHOWN CHARGE A. BOONE NOT FOR TYPICAL SECTIONS **CH2MHILL** CONSTRUCTION HIGH-SPEED RAIL AUTHORITY SHEET NO. 01/09/2012 REV DATE BY CHK APP DESCRIPTION

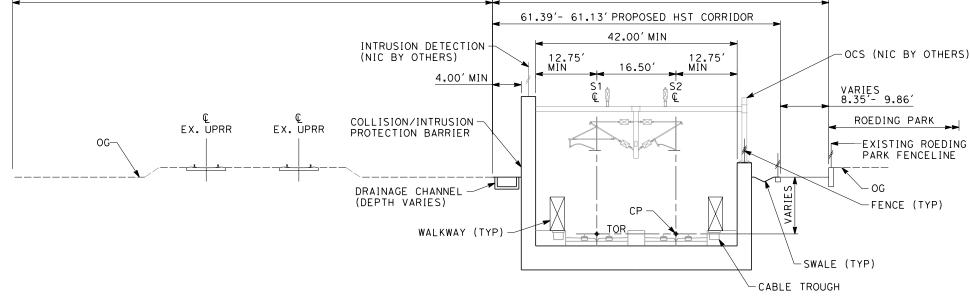




61.39' - 68.30' VARIES PROPOSED HST CORRIDOR 100.00' - VARIES - UPRR CORRIDOR 24.35' - 27.34' VARIES WITHIN STA 10806+00 TO 10828+00 23.35' - 24.56' VARIES 37,50' - VARIES 25.00' - VARIES 37.50' - VARIES OCS (NIC BY OTHERS) INTRUSION DETECTION -(NIC BY OTHERS) - WALKWAY -FENCE (STA 10828+00 TO 10885+00) EX. UPRR EX. UPRR 8.33′ MIN -FENCE MOUNTED ON TRAFFIC BARRIER (STA 10806+00 TO 10828+00) 10.67′ 16.50 10.67 COLLISION/INTRUSION - PROTECTION BARRIER TRAFFIC BARRIER GOLDEN STATE TOR - OG OG-BLVD ROADWAY 24:1 24:1 DRAINAGE (TYP) AT STA 10828+00 21.25' - 25.74' VARIES WITHIN STA 10828+00 TO 10885+00 HST ROW TRANSITIONS
TO EXCLUDE TRAFFIC CABLE TROUGH (TYP) BARRIER SECTION 11 "S" 10806+00 THROUGH 10885+00 TWIN TRACK AT GRADE ADJACENT TO UPRR 100.00' - VARIES - UPRR CORRIDOR EX. GOLDEN STATE BLVD 61.39'- 61.13' PROPOSED HST CORRIDOR 42.00' MIN INTRUSION DETECTION -OCS (NIC BY OTHERS) (NIC BY OTHERS)

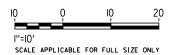
NOTES:

- TRACKFORM SHOWN FOR INFORMATION ONLY (NIC BY OTHERS).
- 2. FOR STRUCTURE DIMENSIONS SEE STRUCTURAL TYPICAL SECTIONS.
- 3. SUPERELEVATION IS NOT SHOWN.
 THE AMOUNT OF APPLIED
 SUPERELEVATION IS SHOWN IN
 THE CURVE DATA TABLES.
- 4. COLLISION/INTRUSION PROTECTION BARRIER REQUIRED FROM STA 10806+00 - 10950+30



SECTION 12

"S" 10885+00 THROUGH 10902+50
TWIN TRACK IN GRADE SEPARATION ADJACENT
TO UPRR AND ROEDING PARK



- - - -	EV	DATE	BY	СНК	APP	DESCRIPTION	R. COFFIN DATE 02/17/12	C
2 K							D. HUNT	
۔ ا							CHECKED BY	· [
2							DRAWN BY P. TONKIN	1.
$\frac{1}{2}$							DESIGNED BY K. SEYMOUR	

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PRELIMINARY
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NOT FOR
CONSTRUCTION

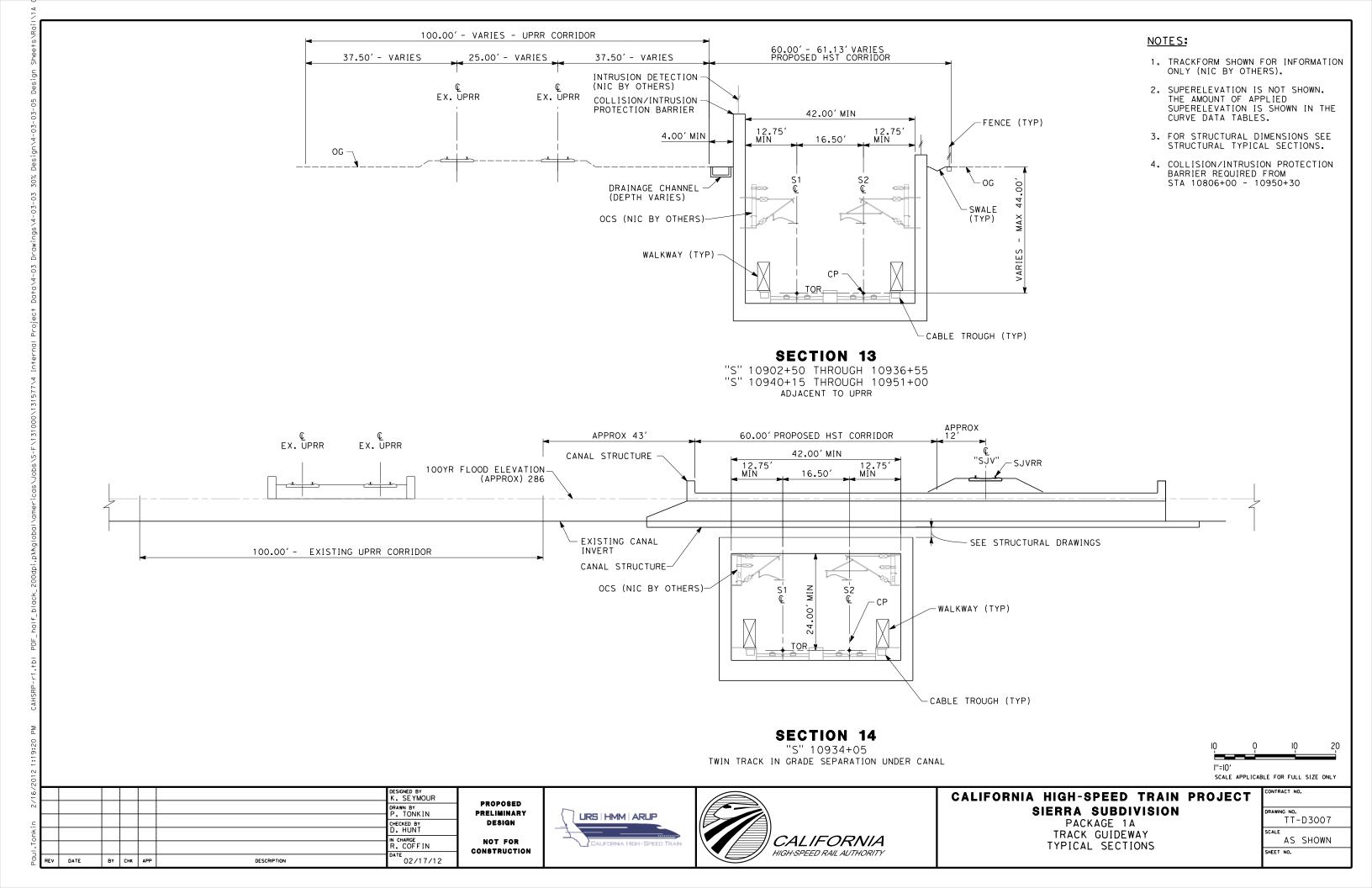


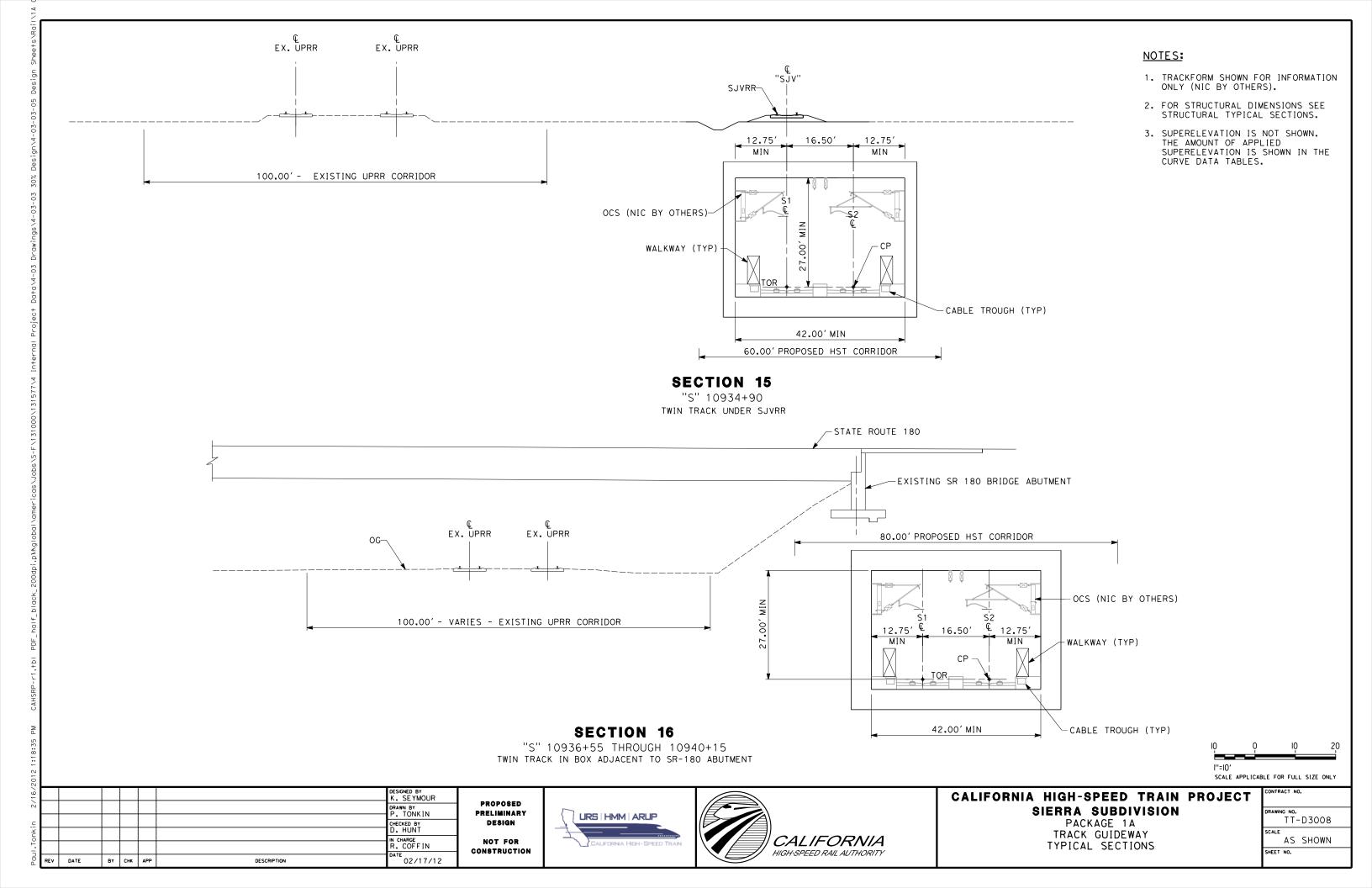


CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION PACKAGE 1A

PACKAGE 1A TRACK GUIDEWAY TYPICAL SECTIONS

CONTRA	CT NO		
DRAWING	. NO		
		D300	_
	11-	.D200	О
SCALE			
	ΔS	SHOW	N





DESIGNED BY K. SEYMOUR P. TONKIN HECKED BY N CHARGE R. COFFIN 02/17/12 BY CHK APP DESCRIPTION

PROPOSED PRELIMINARY DESIGN NOT FOR

CONSTRUCTION





NOTES:

- 1. TRACKFORM SHOWN FOR INFORMATION ONLY (NIC BY OTHERS).
- 2. FOR STRUCTURAL DIMENSIONS SEE STRUCTURAL TYPICAL SECTIONS.
- 3. DISTANCE BETWEEN TRACKS VARIES FROM 0.00'-25.00' THROUGH TURNOUTS AND REVERSE TO PARALLEL.
- 4. COLLISION/INTRUSION PROTECTION BARRIER REQUIRED FROM STA 10806+00 - 10950+30 AND STA 10990+70 - 11030+00
- 5. SUPERELEVATION IS NOT SHOWN. THE AMOUNT OF APPLIED
 SUPERELEVATION IS SHOWN IN THE CURVE DATA TABLES.

SECTION 17

60.00' - 100.00 - PROPOSED HST CORRIDOR

42.00' MIN

16.50'

12.75

MIN

-FENCE

WALKWAY CABLE TROUGH

-SWALE (TYP)

12.75

MIN

OCS (NIC BY OTHERS)-

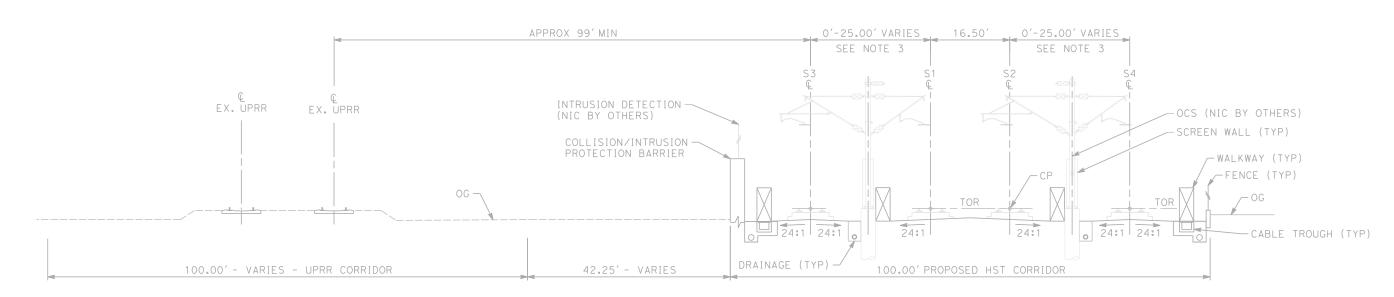
FENCE (TYP)

WALKWAY

OG -

DRAINAGE CHANNEL (DEPTH VARIES)

> (PACKAGE 1A) "S" 10951+00 THROUGH 10970+00 TWIN TRACK IN GRADE SEPARATION



SECTION 18

(PACKAGE 1B)

"S" 10970+00 THROUGH 10991+52 "S" 10992+67 THROUGH 10992+95

"S" 11007+05 THROUGH 11007+80

"S" 11026+00 THROUGH 11030+00

4 TRACK AT GRADE STATION APPROACH

CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A TRACK GUIDEWAY TYPICAL SECTIONS

SCALE APPLICAE	BLE FOR FULL SIZE ONLY
DJECT	CONTRACT NO.
	DRAWING NO.
	TT-D3009
	SCALE AS SHOWN

SHEET NO.

l"=10'

1. THE S1 LINE IS OFFSET TO S2 LINE BY 16.50 FEET.

		TRACK (GEOMETRY [ATA ALONG	SOUTHBOU	ND(S2) T	RACK CE	NTERLII	ΝE				
CURVE NO.	DESCRIPTION	STATION	NORTHING	EASTING	BEARING	DISTANCE (FT)	R (FT)	Lc (FT)	SPIRAL TYPE	Ls (FT)	Ea (in)	Eu (in)	V (MPH)
	POB	S10535+00.00	2184645.09	6297590.88	S51°24′12"E	919.91							250
	TS	S10544+19.91	2184071.22	6298309.85	331 24 12 6	919.91			HALF SINE	1200,00			250
101	SC	S10556+19.91	2183320.23	6299245.81			70000.00	1202 30	HALF SINE	1200.00	3.00	0.57	250
	CS	S10568+22.31	2182554.13	6300172.52			70000.00	1202.39	HALF SINE	1200,00		0.51	250
	ST	S10580+22.31	2181776.12	6301086.14	S49°26′13"E	1333.20			HALF SINE	1200.00			250
	TS	S10593+55.51	2180909.17	6302098.96	349 20 13 E	1333.20			HALF SINE	2000,00			250
102	SC	S10613+55.51	2179592.62	6303604.36			28000.00	2100 91	HALF SINE	2000.00	6.00	2.93	250
102	CS	S10634+65.32	2178107.16	6305101.90			28000.00	2109.01	HALF SINE	2000,00		2.93	250
	ST	S10654+65.32	2176612.48	6306430.60	S41°01′38″E	24843.38			HALF SINE	2000.00			250
	TS	S10903+08.70	2157870.69	6322738.24	341 UI 38 E	24043.30			HALF SINE	1000,00			220
103	SC	S10913+08.70	2157114.93	6323393.08			71400.00	1002.81	HALF SINE	1000.00	1.50	1.21	220
103	CS	S10923+11.52	2156349.26	6324040.66			71400.00	1002.01	HALF SINE	1000.00		1.21	220
	SS	S10933+11.52	2155578.08	6324677.27						1000.00			220
	SC	S10943+11.52	2154806.89	6325313.88			71416.50	1166 50	HALF SINE	1000.00	1.50	1.21	220
104	CS	S10954+78.01	2153917.10	6326068.18			71416.50	1166.50	HALF SINE	1000.00		1.21	220
	ST	S10964+78.01	2153162.84	6326724.75	C 4 4 9 0 0 / 0 0 !! E	17272 60			HALF SINE	1000.00			
	TS	S11097+50.62	2154806.89	6335459.98	S41°09′29"E	13272.60			LIALE CINE	2500 00			220
105	SC	S11122+50.62	2141259.73	6337072.14			21400.00	17072 00		2500.00	6.75	2.70	220
103	CS	S11253+22.70	2129190.24	6341539.67			21400.00	13012.08		2500.00		2.30	
	ST	S11278+22.70	2126690.71	6341559.78	00070/07!!	0177 70			HALF SINE	2500.00			220
	POT	S11300+00.00	2124513.50	6341539.49	S0°32′03"W	2177.30							250

DESIGNED BY A. SHIELDS DRAWN BY H. SULLIVAN CHECKED BY A.BOONE N CHARGE A. BOONE CONSTRUCTION 12/08/11 REV DATE BY CHK APP DESCRIPTION

PROPOSED PRELIMINARY DESIGN NOT FOR

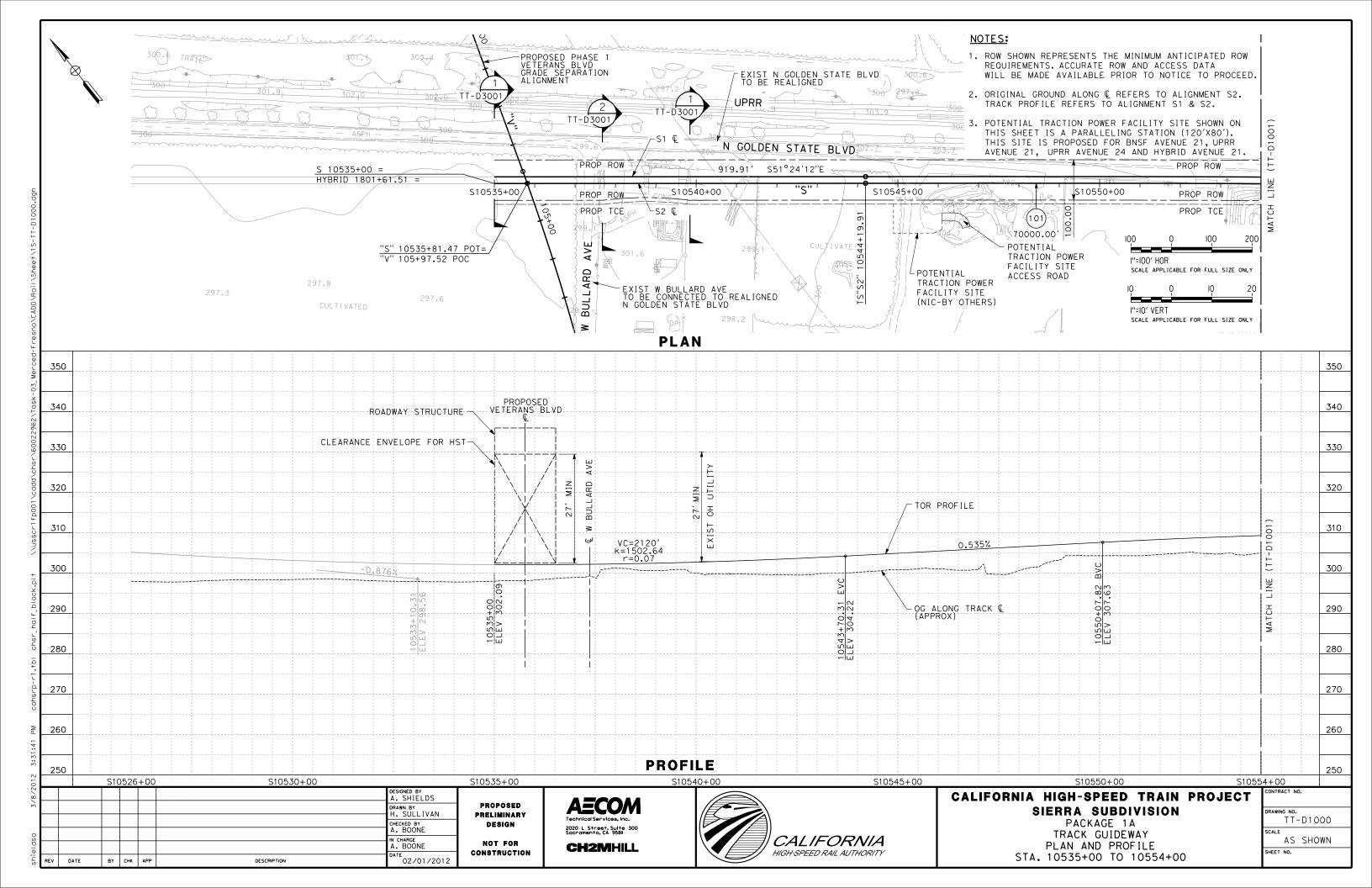
Technical Services, Inc. 2020 L Street, Suite 300 Sacramento, CA 958II **CH2MHILL**

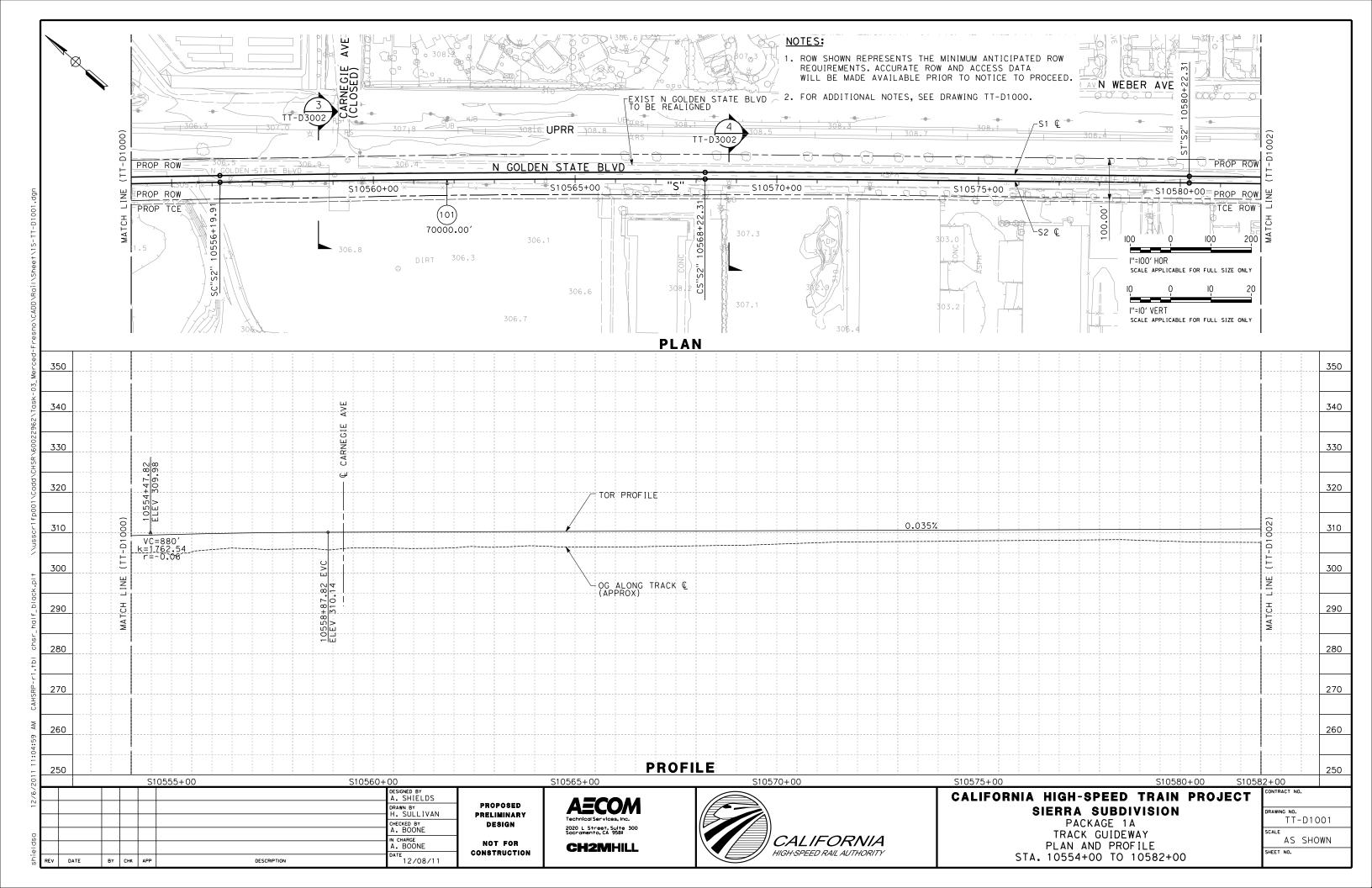


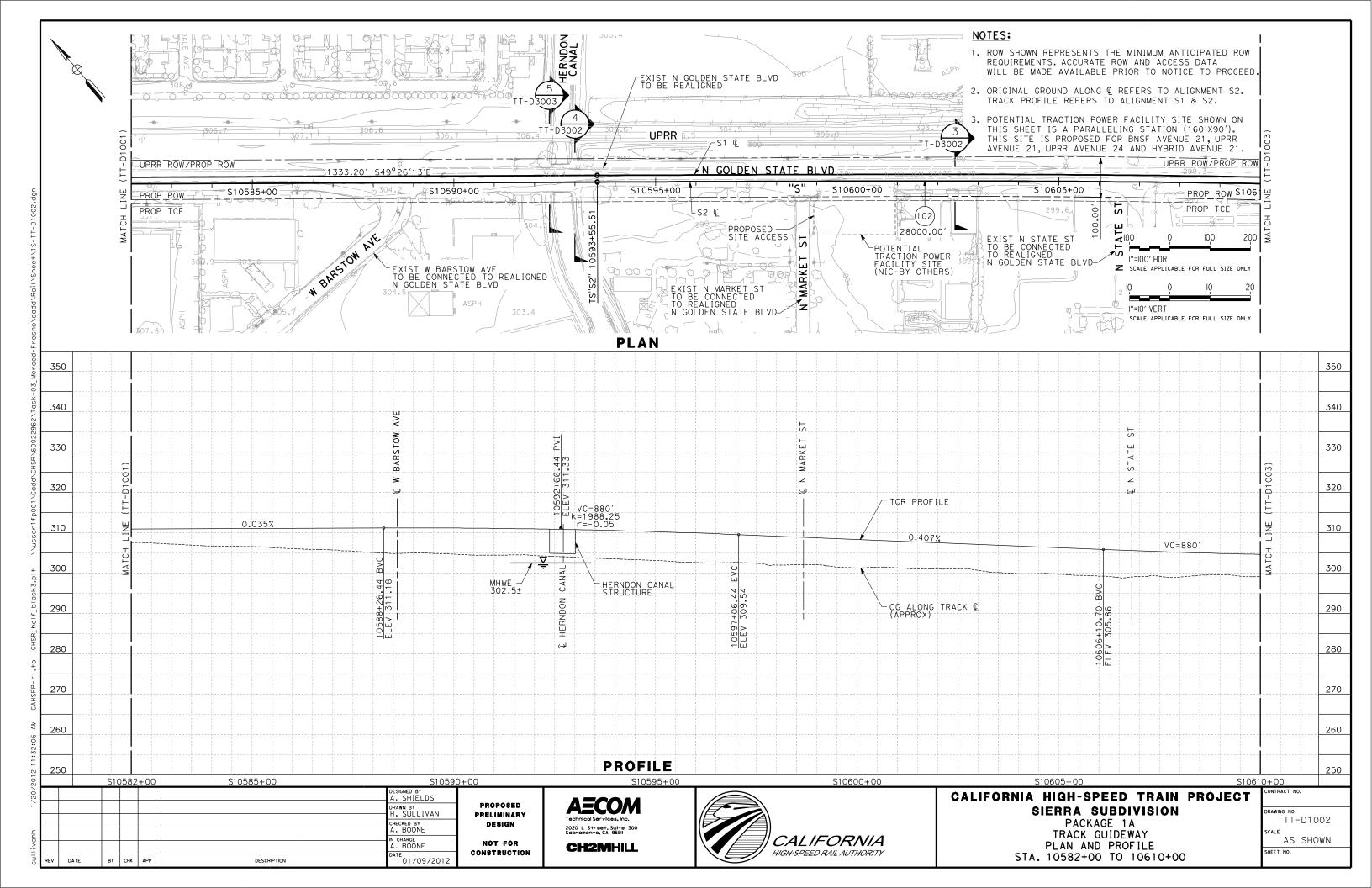
CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

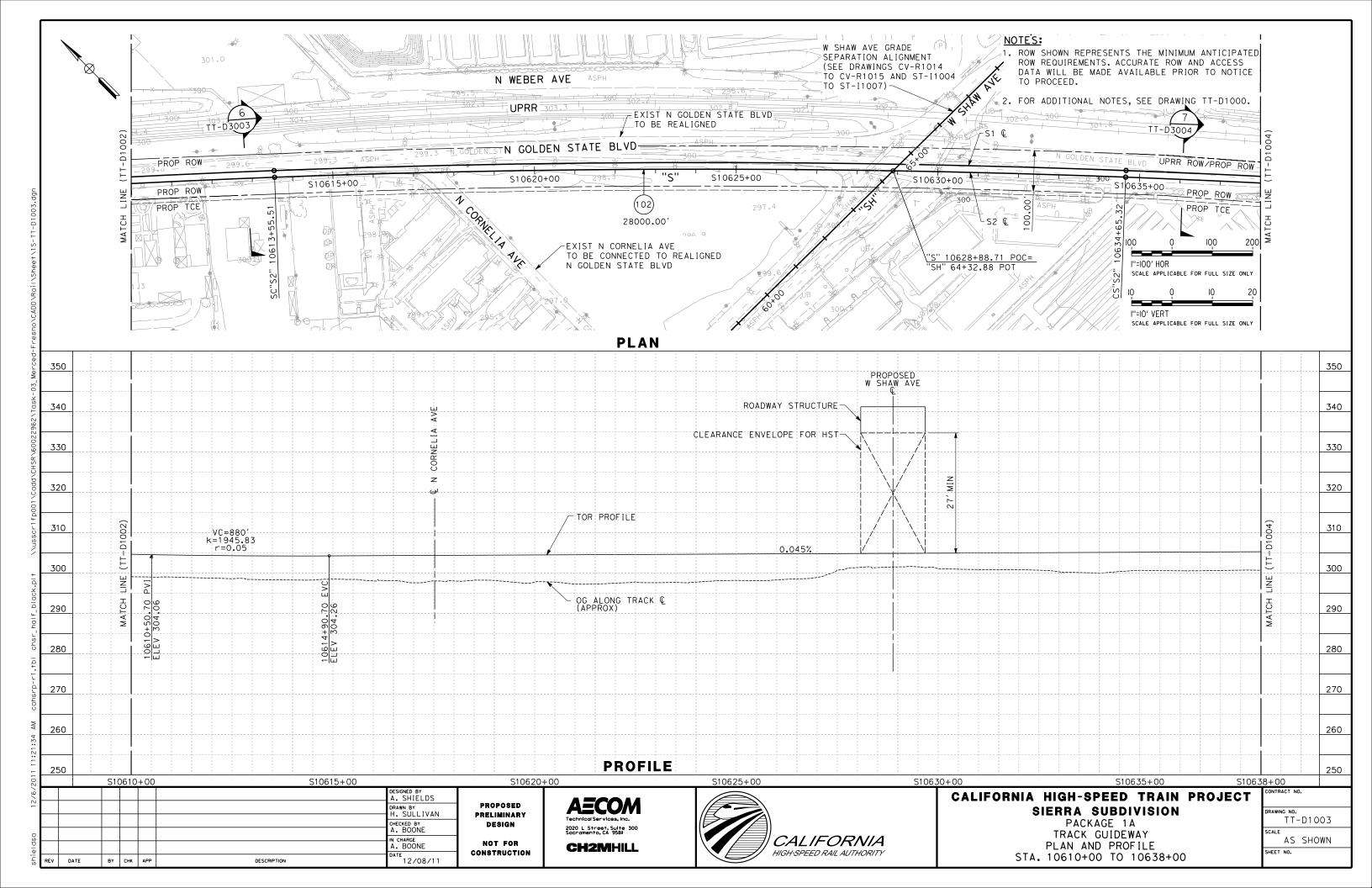
PACKAGE 1A TRACK GUIDEWAY HORIZONTAL ALIGNMENT DATA

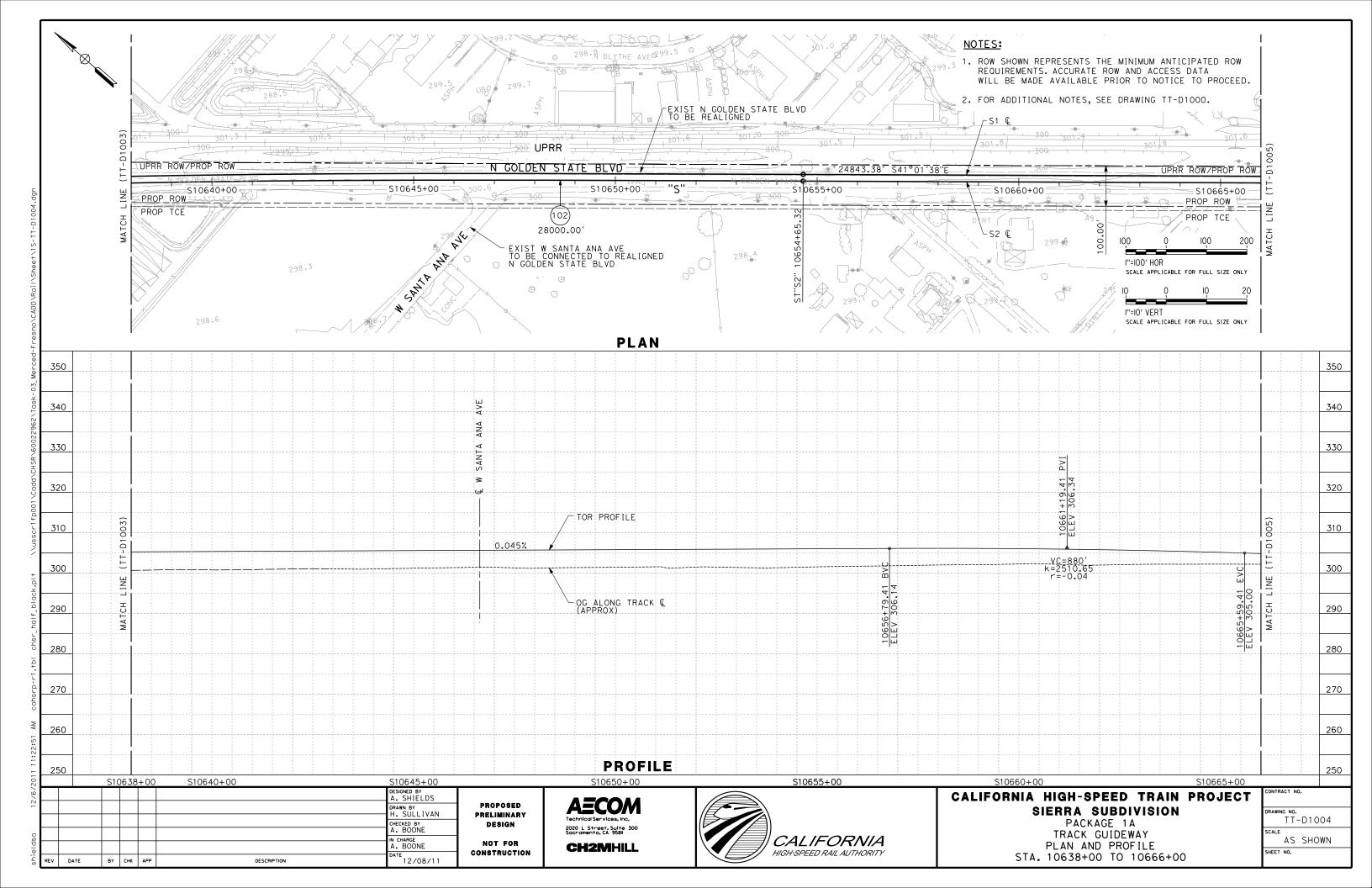
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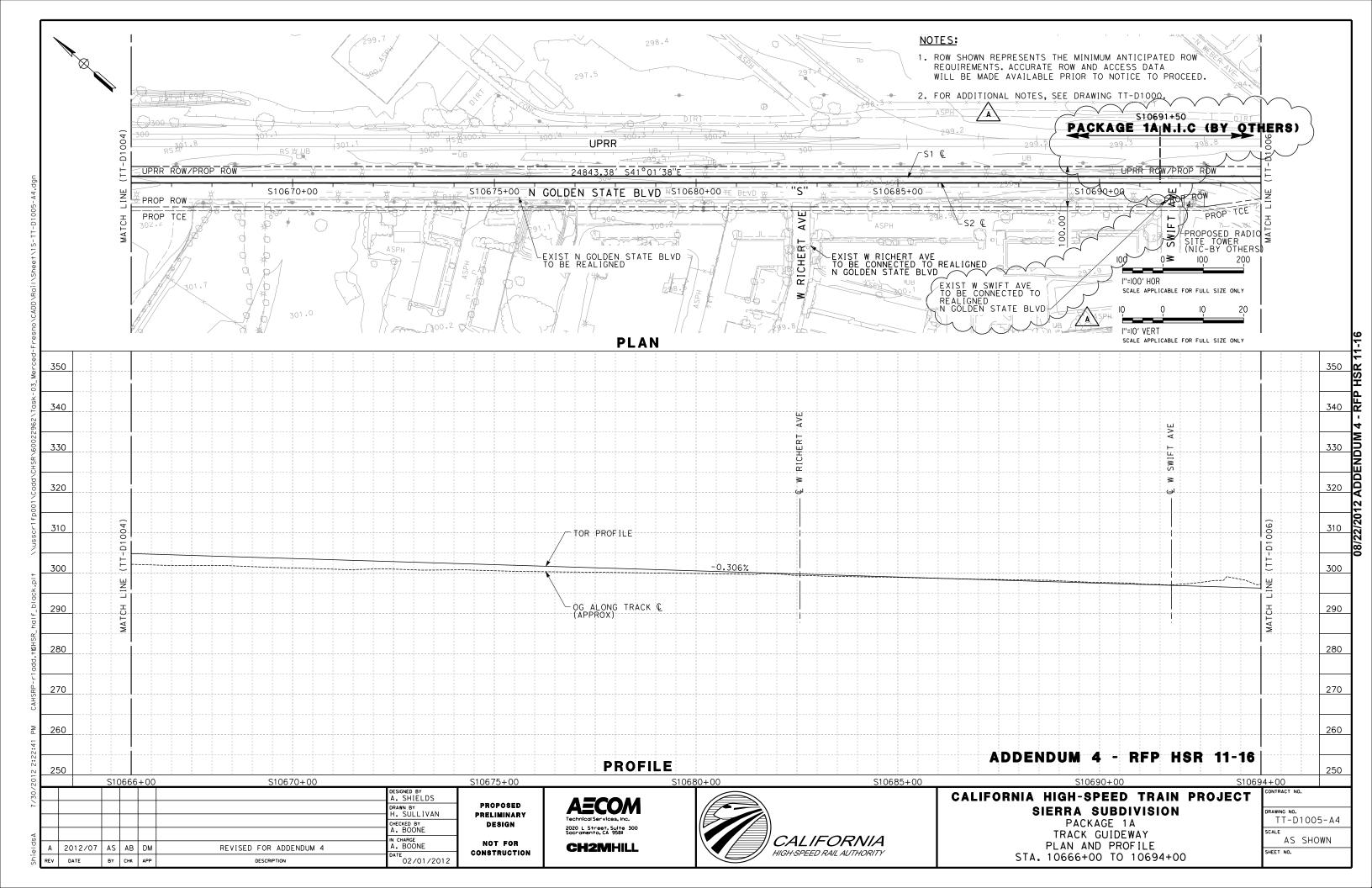


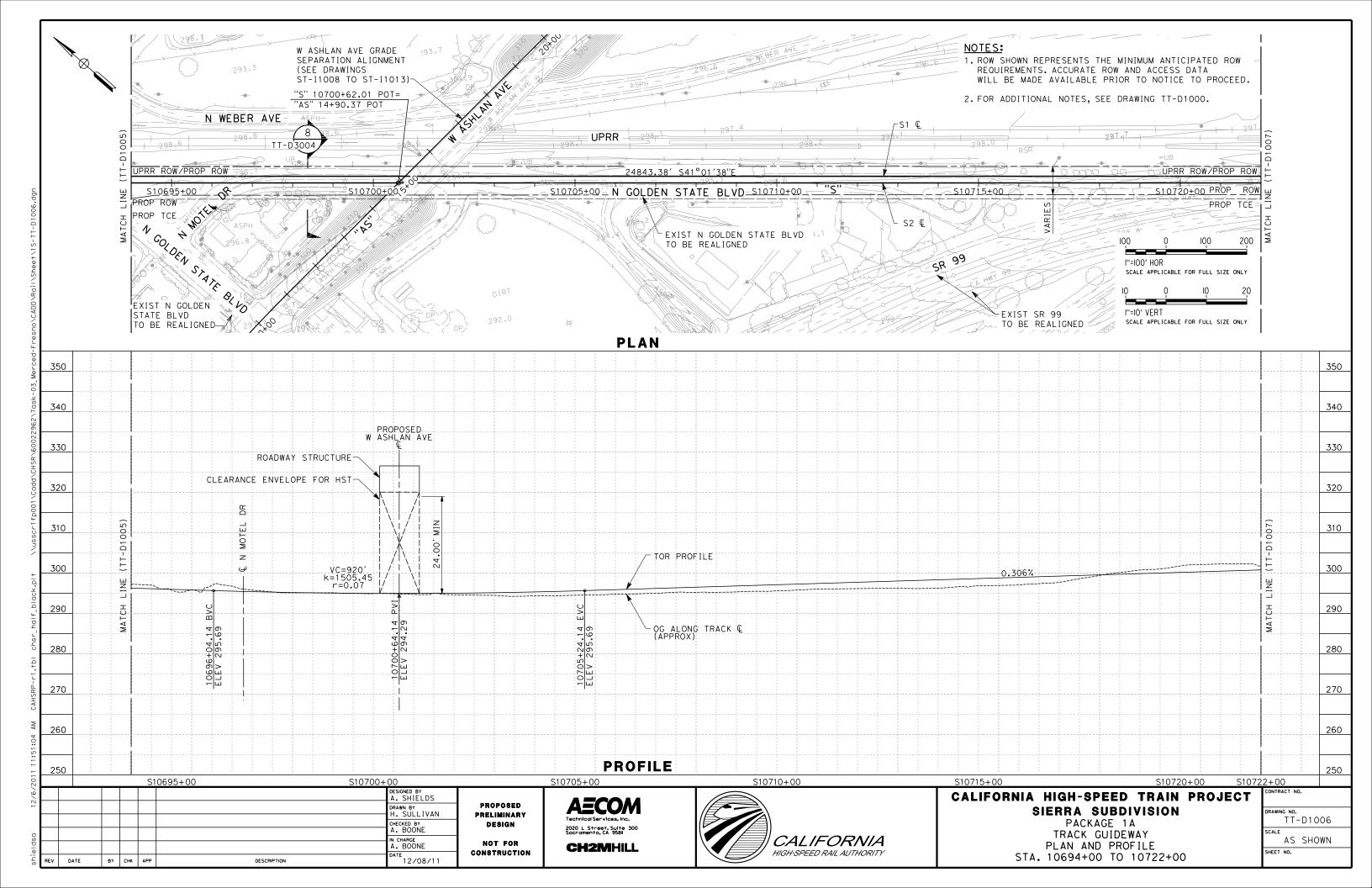


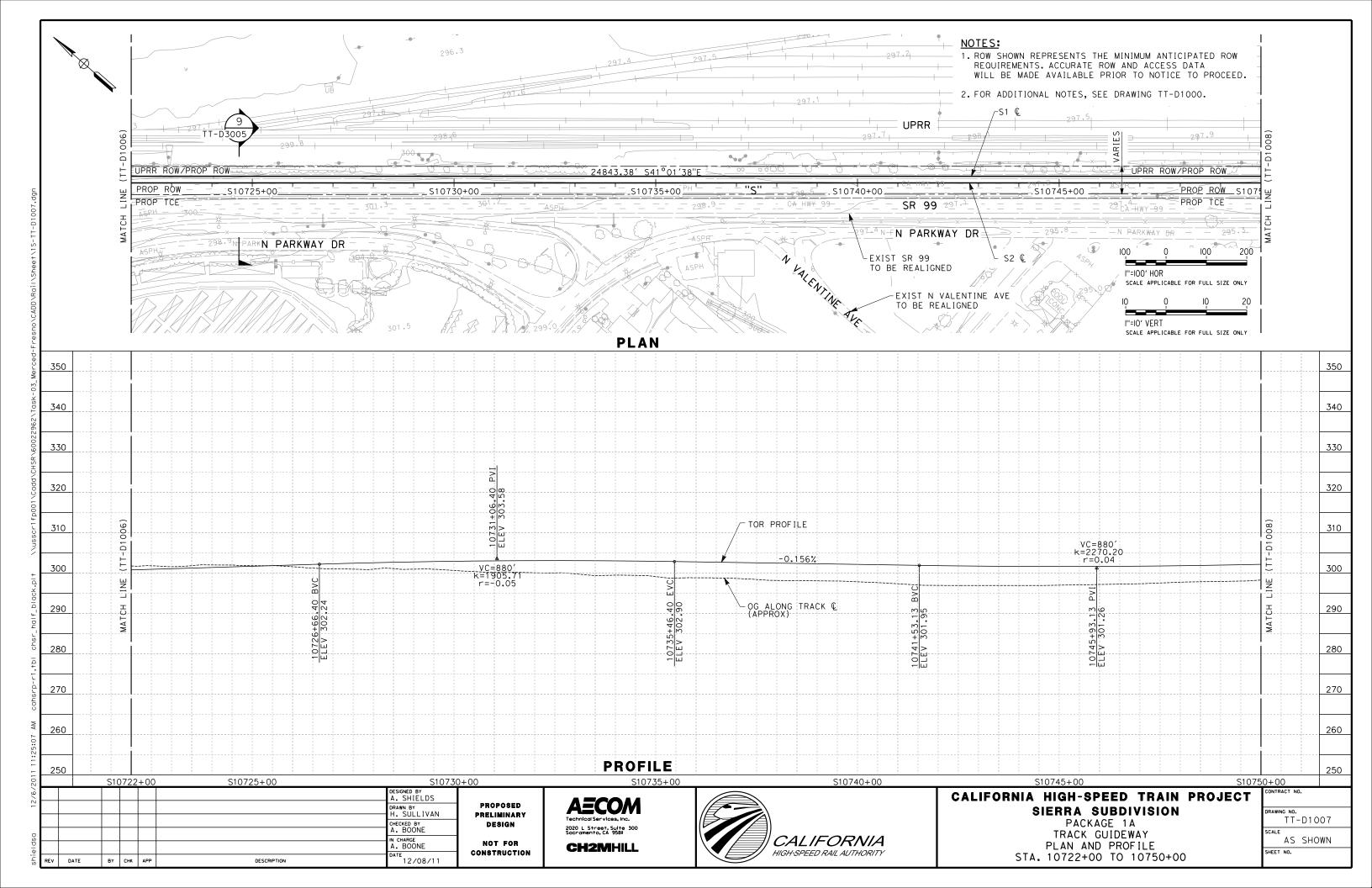


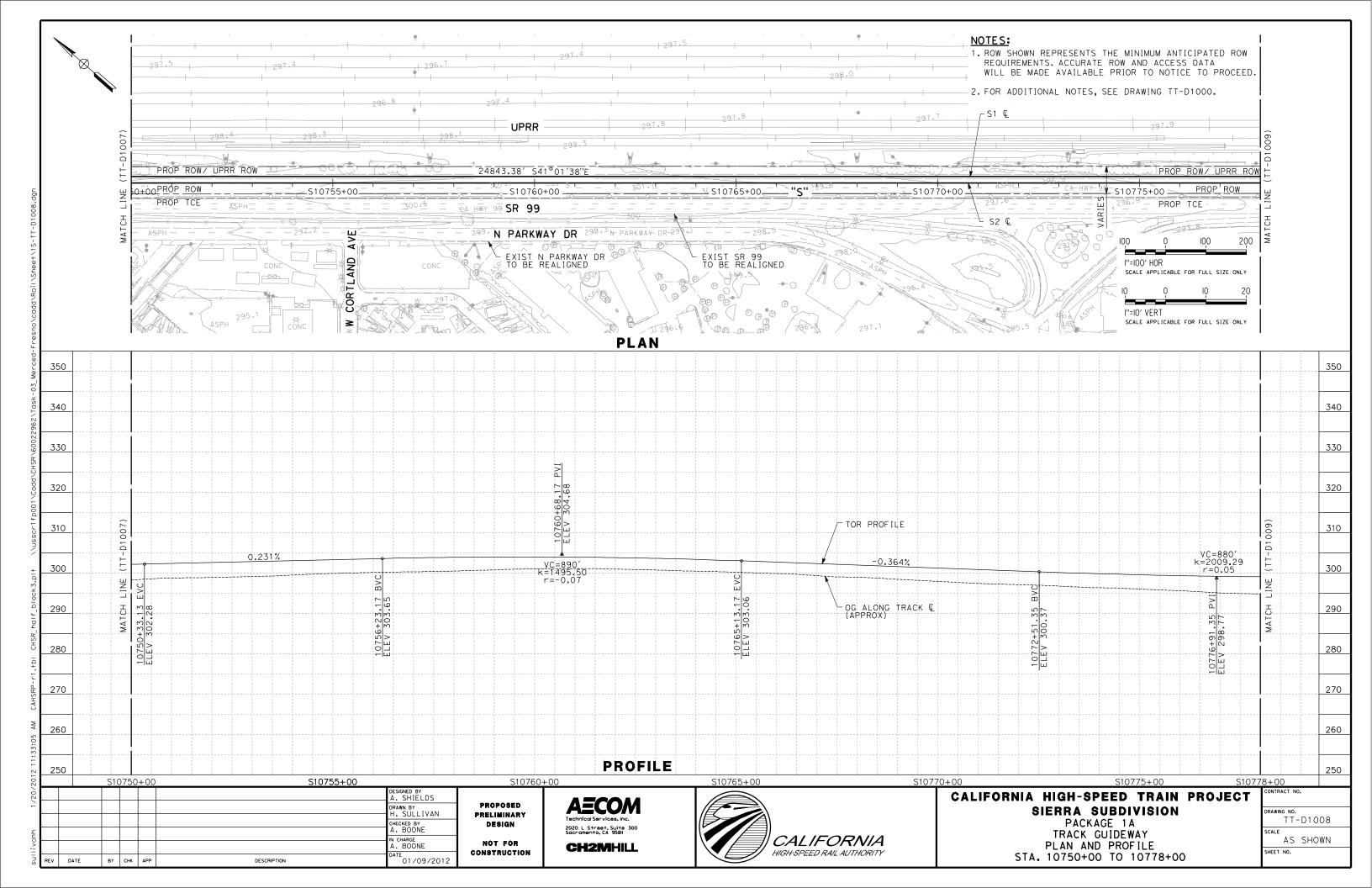


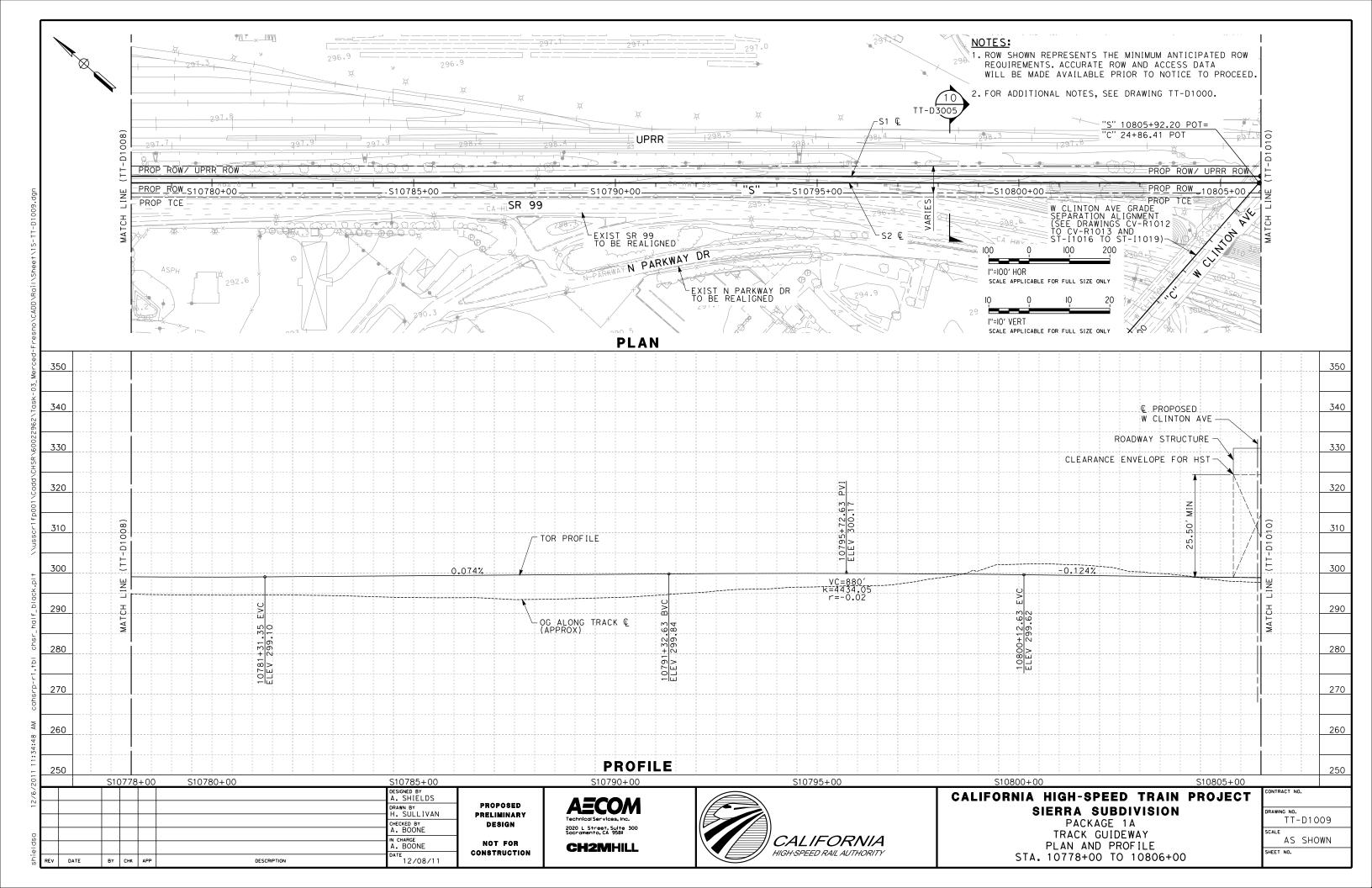


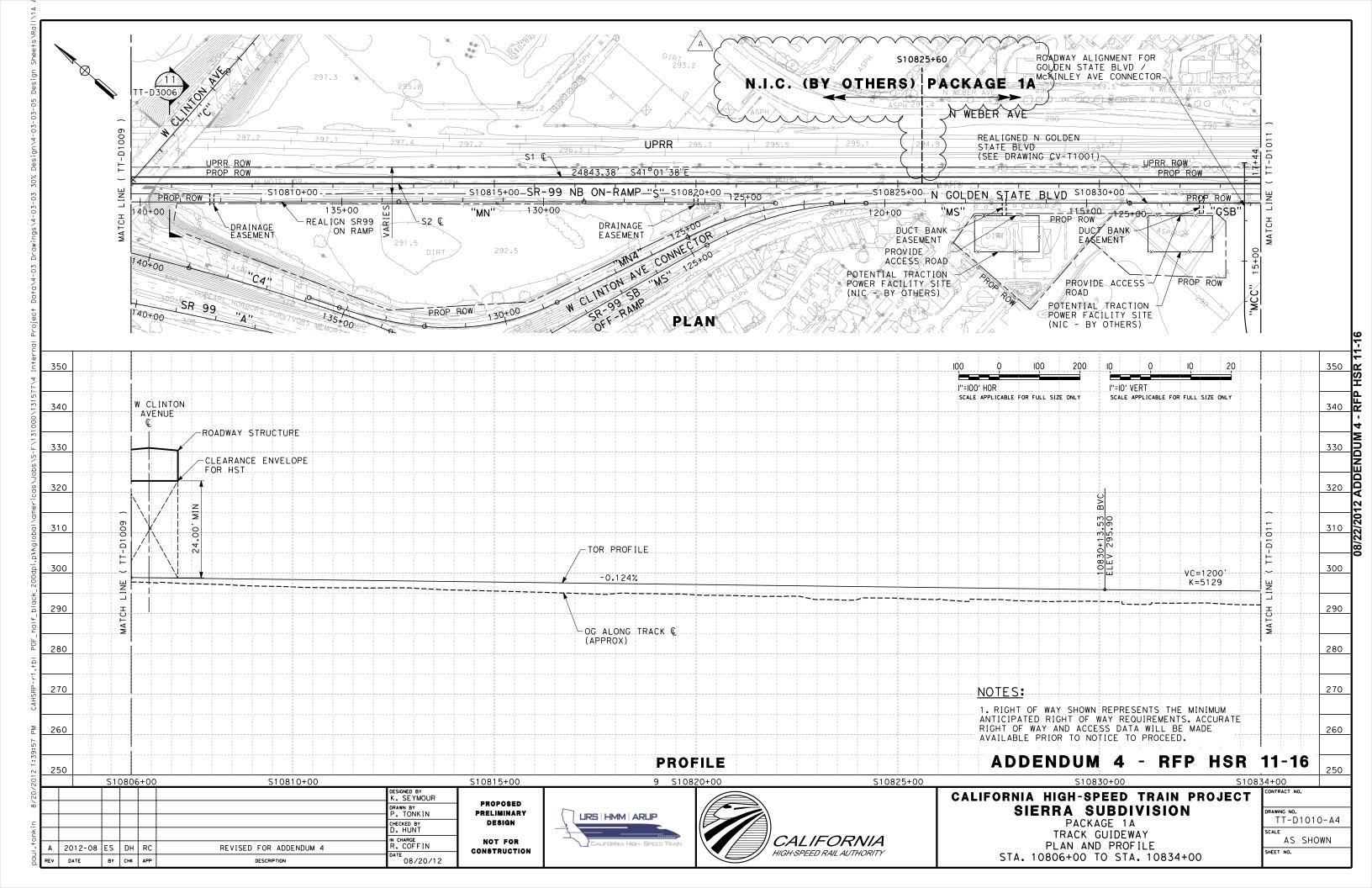


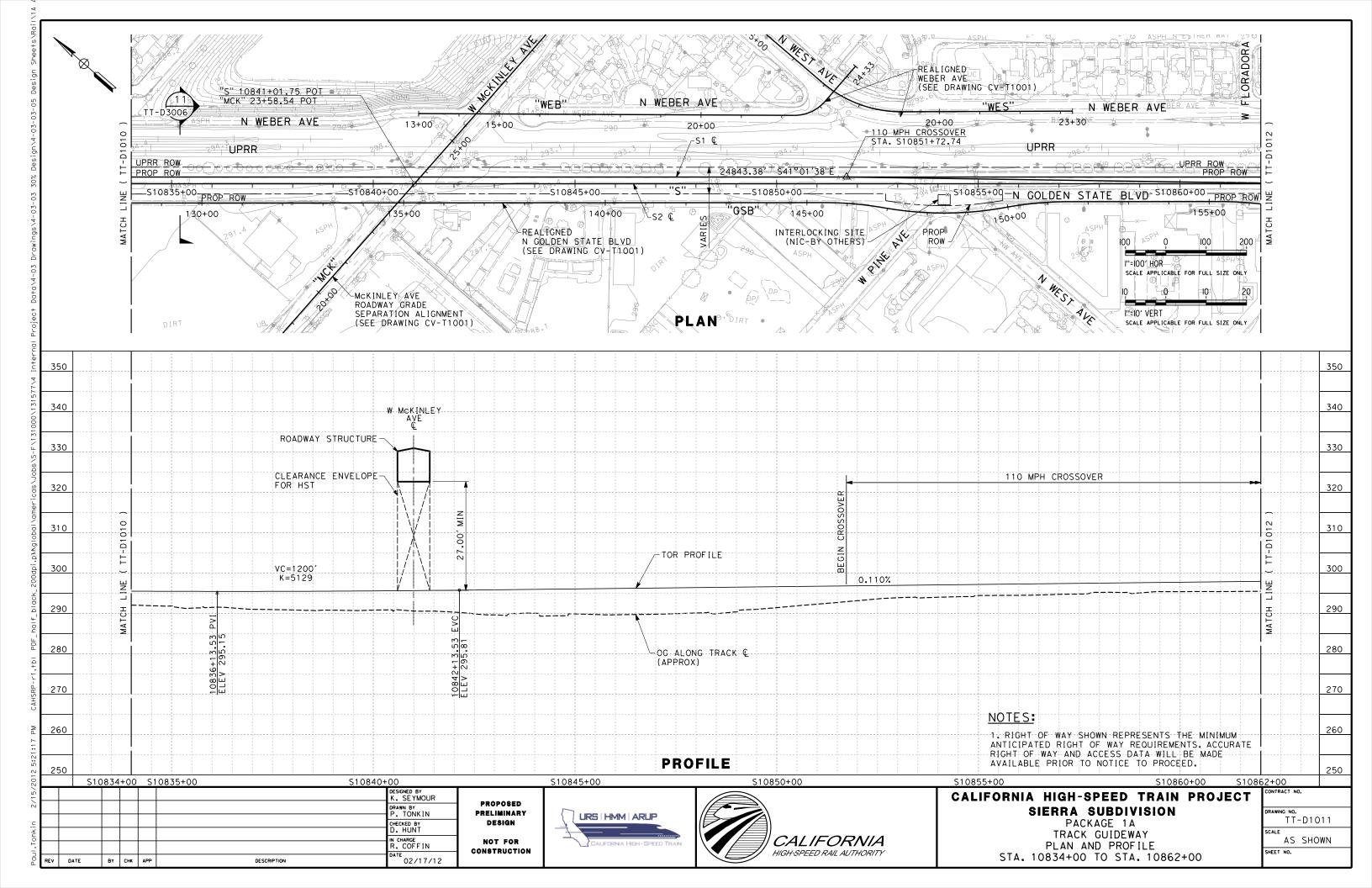


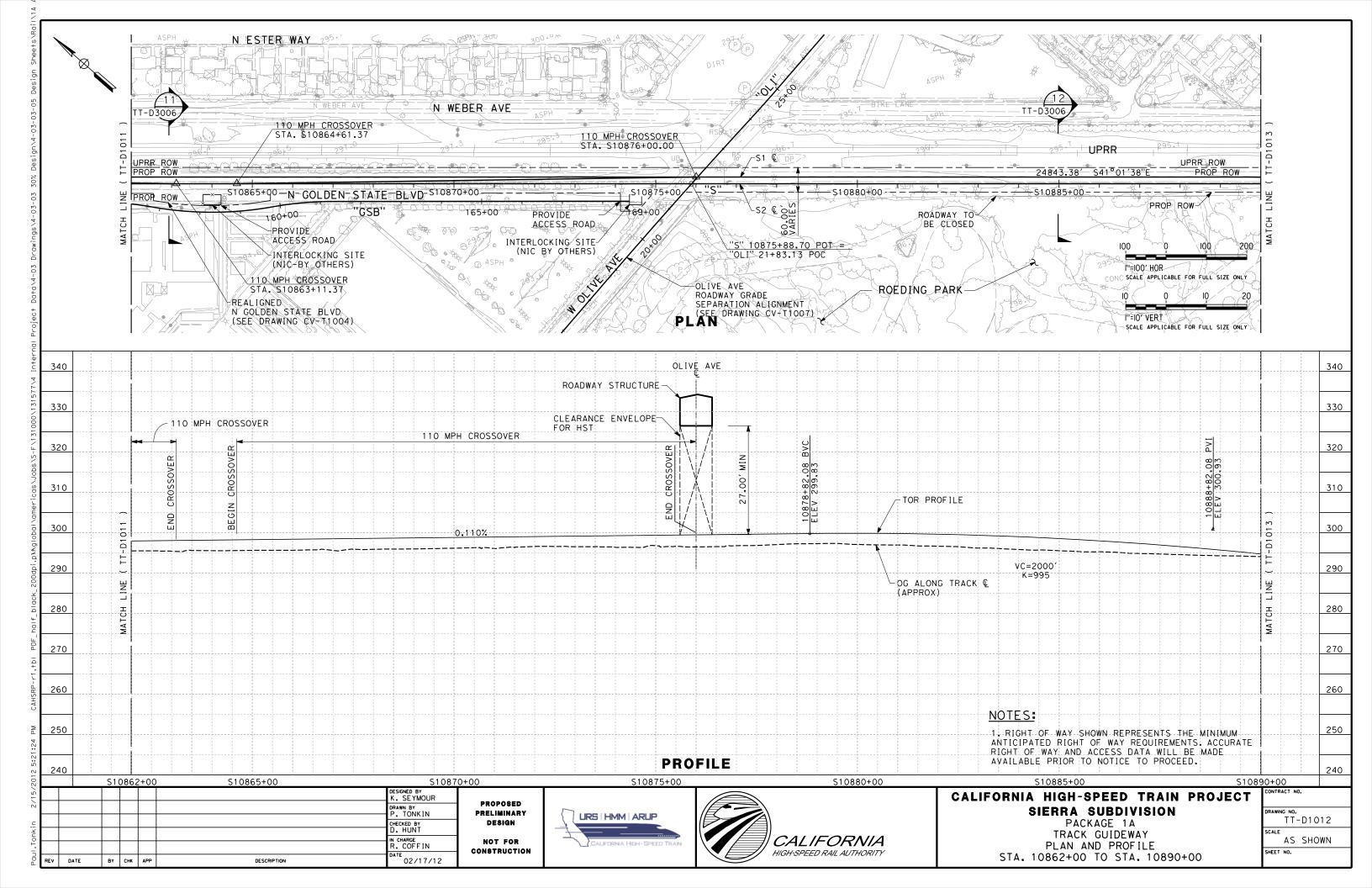


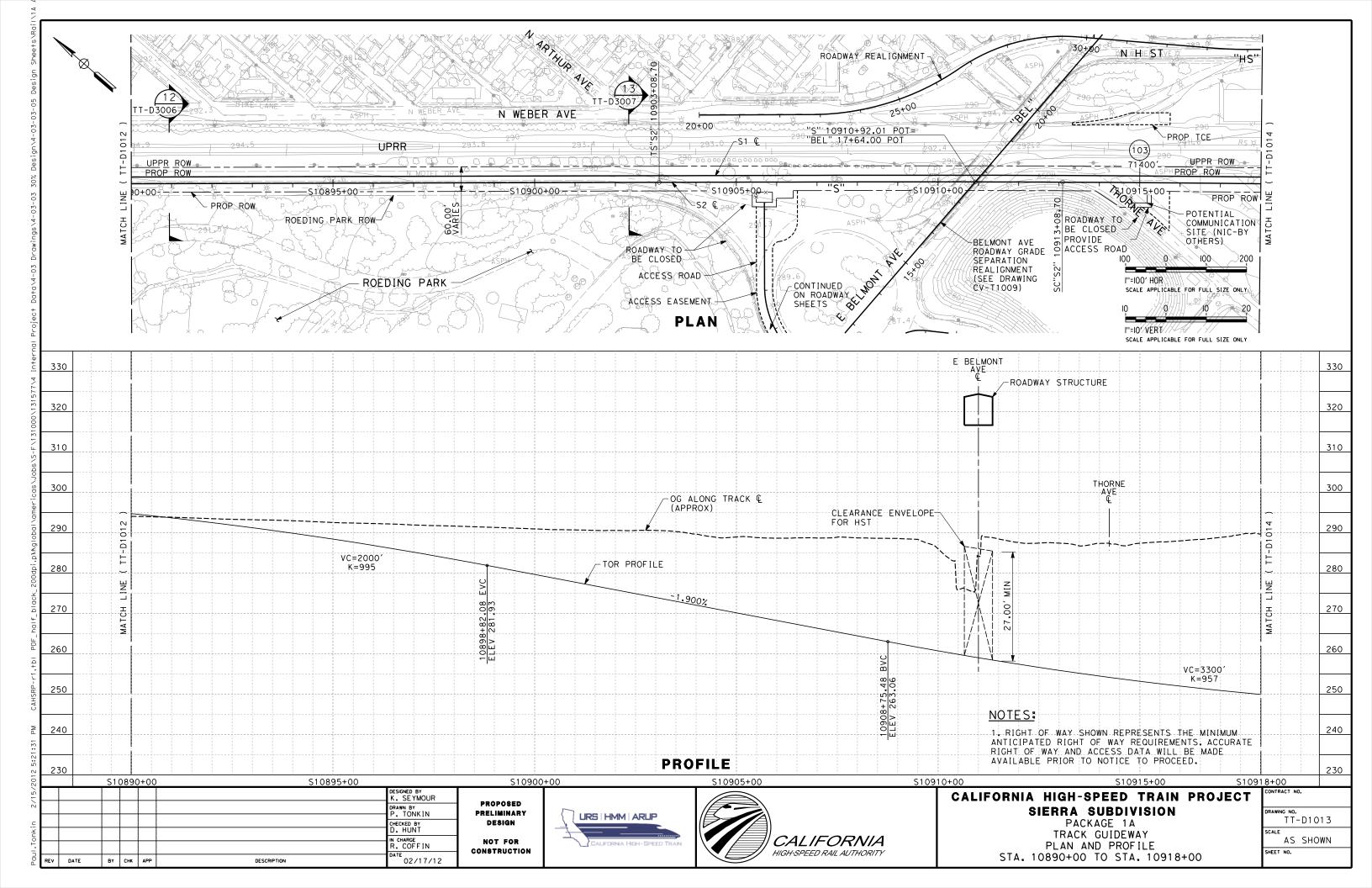


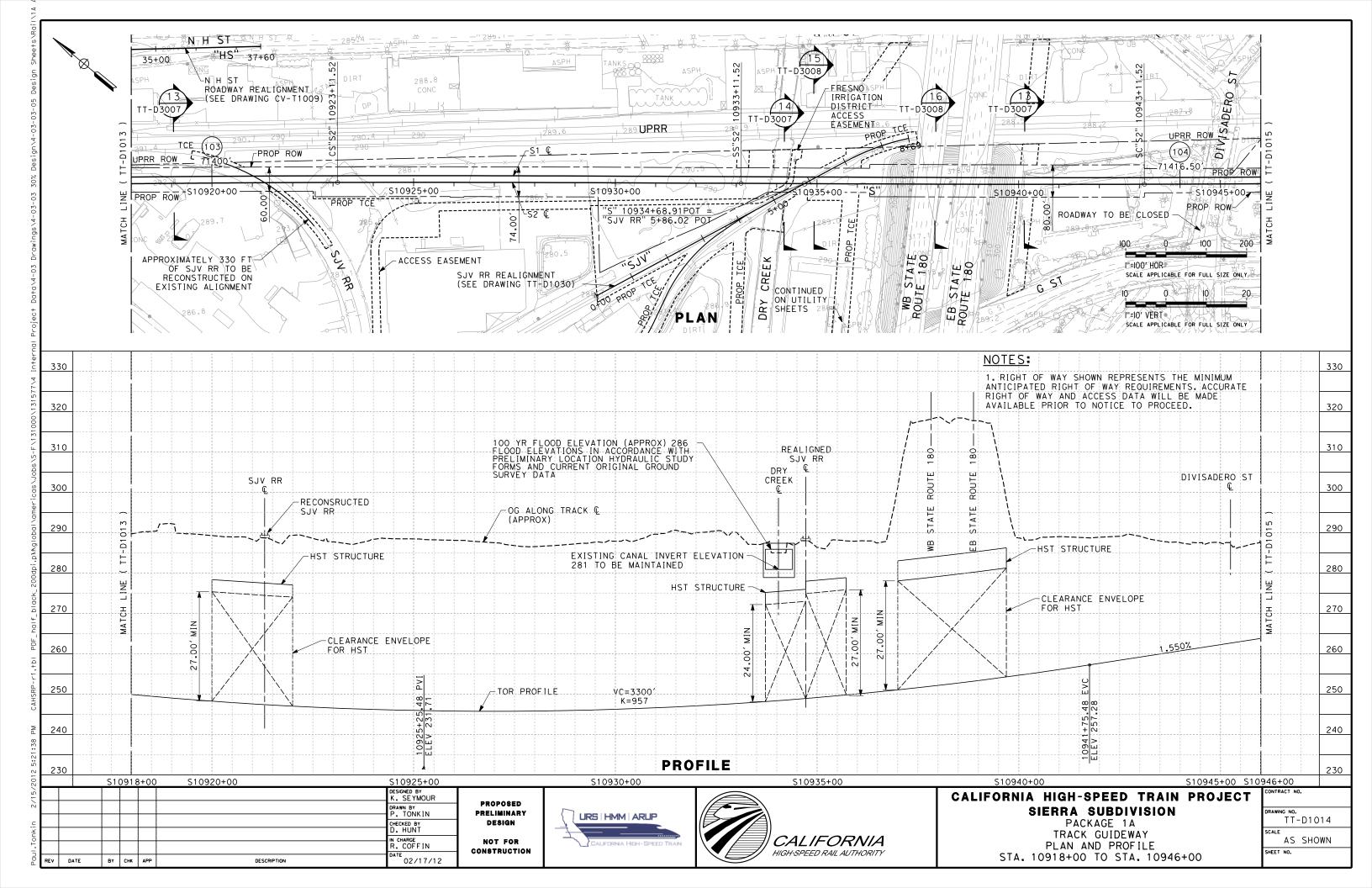


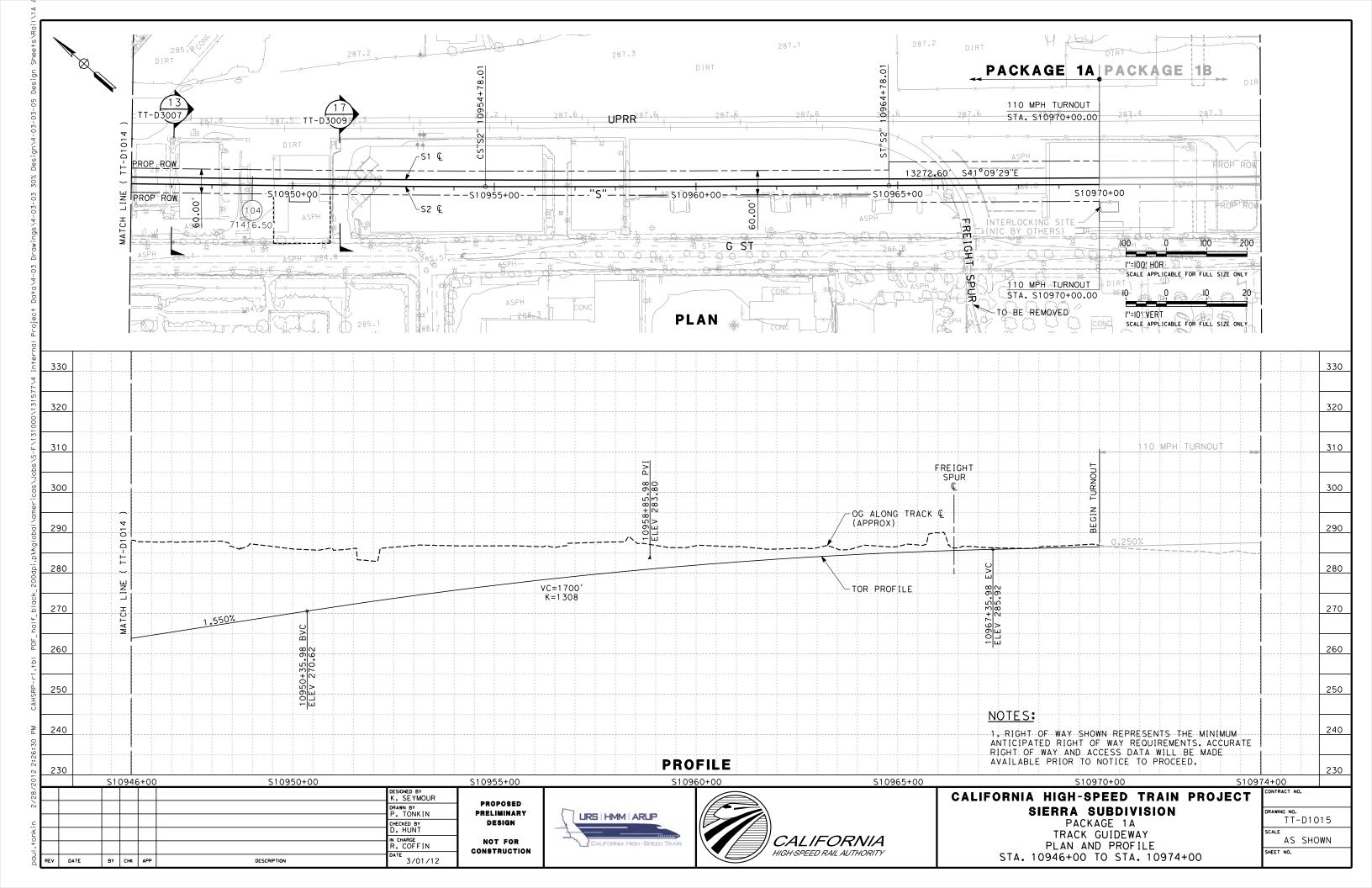


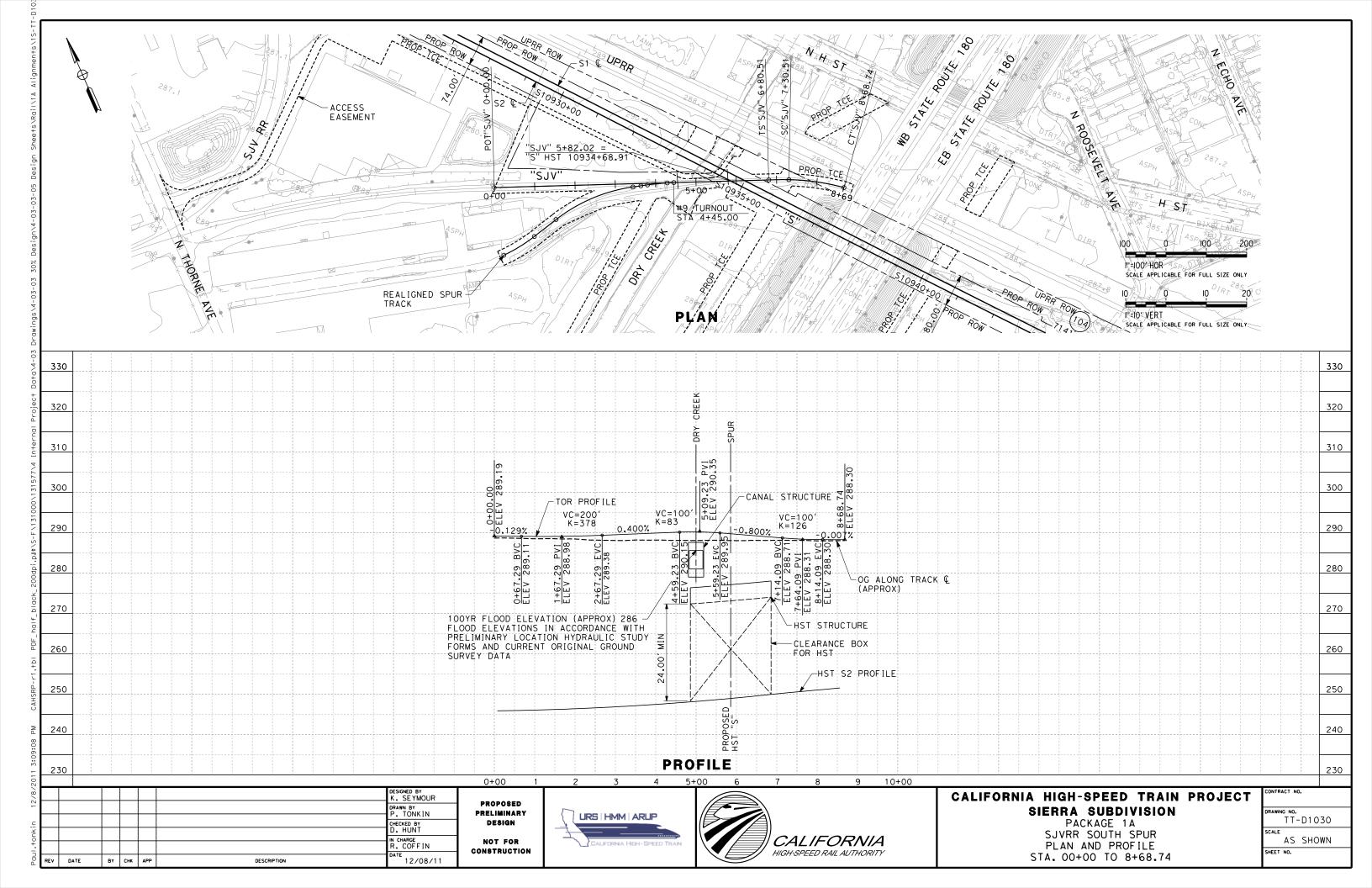












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VOLUME 2 - TRACK STRUCTURES

DRAWING No.	REV No.	DRAWING DESCRIPTION
DRAWING NO.	REV NO.	
		GENERAL
GE-A0002		PACKAGE 1A - TRACK STRUCTURES - INDEX OF DRAWINGS
ST-B0001		PACKAGE 1A - TRACK STRUCTURES - GENERAL NOTES - SHEET 1 OF 2
ST-B0002		PACKAGE 1A - TRACK STRUCTURES - GENERAL NOTES - SHEET 2 OF 2
ST-B0003		PACKAGE 1A - TRACK STRUCTURES - KEY MAP
		TRACK STRUCTURES
ST-K1001		PACKAGE 1A - TRACK STRUCTURES - HST BRIDGE AT HERNDON CANAL - GENERAL PLAN
ST-K1002		PACKAGE 1A - TRACK STRUCTURES - HST BRIDGE AT HERNDON CANAL - TYPICAL SECTIONS
ST-K1003		PACKAGE 1A - TRACK STRUCTURES - HST BRIDGE AT HERNDON CANAL - FOUNDATION PLAN
ST-Y3001		PACKAGE 1A - TRACK STRUCTURES - FRESNO GRADE SEPARATION - TYPICAL SECTIONS
ST-Y3002		PACKAGE 1A - TRACK STRUCTURES - FRESNO GRADE SEPARATION - TYPICAL SECTIONS
ST-Y3003		PACKAGE 1A - TRACK STRUCTURES - FRESNO GRADE SEPARATION - TYPICAL SECTIONS
ST-Y3004		PACKAGE 1A - TRACK STRUCTURES - FRESNO GRADE SEPARATION - TYPICAL SECTIONS
ST-Y1001		PACKAGE 1A - TRACK STRUCTURES - FRESNO GRADE SEPARATION - PLAN AND ELEVATION
ST-Y1002		PACKAGE 1A - TRACK STRUCTURES - FRESNO GRADE SEPARATION - PLAN AND ELEVATION
ST-Y1003		PACKAGE 1A - TRACK STRUCTURES - FRESNO GRADE SEPARATION - PLAN AND ELEVATION
ST-Y1004		PACKAGE 1A - TRACK STRUCTURES - FRESNO GRADE SEPARATION - PLAN AND ELEVATION
ST-Y1005		PACKAGE 1A - TRACK STRUCTURES - FRESNO GRADE SEPARATION - PLAN AND ELEVATION
ST-Y1006		PACKAGE 1A - TRACK STRUCTURES - FRESNO GRADE SEPARATION - PLAN AND ELEVATION
ST-Y1007		PACKAGE 1A - TRACK STRUCTURES - FRESNO GRADE SEPARATION - PLAN AND ELEVATION
ST-Y1008		PACKAGE 1A - TRACK STRUCTURES - FRESNO GRADE SEPARATION - PLAN AND ELEVATION
ST-Y1009		PACKAGE 1A - TRACK STRUCTURES - FRESNO GRADE SEPARATION - PLAN AND ELEVATION
ST-Y5001		PACKAGE 1A - TRACK STRUCTURES - FRESNO GRADE SEPARATION - DRY CREEK CANAL CULVERT
ST-Y5002		PACKAGE 1A - TRACK STRUCTURES - FRESNO GRADE SEPARATION - SR 180 BRIDGE

	REV	DATE	BY	СНК	APP	DESCRIPTION	02/01/2012	
ieldsa							IN CHARGE A. BOONE DATE	١.
\cdot							CHECKED BY A. BOONE	
²							DRAWN BY A. SHIELDS	
/22/							DESIGNED BY A. SHIELDS	

PROPOSED PRELIMINARY DESIGN

NOT FOR CONSTRUCTION





CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A TRACK STRUCTURES INDEX OF DRAWINGS

i	CONTRACT NO.
	DRAWING NO. GE-A0002
	scale NONE
	SHEET NO.

- DESIGN-BUILD STANDARD SPECIFICATION, CALIFORNIA HIGH SPEED TRAIN, JUNE 2011.
- 2. THE STRUCTURAL DESIGN OF STRUCTURES SUPPORTING HIGH SPEED TRAINS SHALL BE BASED ON THE REQUIREMENTS OF THE CALIFORNIA HIGH SPEED TRAIN AUTHORITY.
- 3. DESIGN CRITERIA FOR HIGHWAY BRIDGES SHALL BE THE CALIFORNIA BRIDGE DESIGN SPECIFICATION. FOR HIGHWAY BRIDGES PASSING OVER THE HIGH SPEED TRAIN THE BRIDGE DESIGN SPECIFICATION SHALL BE SUPPLEMENTED BY THE CALIFORNIA HIGH SPEED TRAIN REQUIREMENTS FOR SEISMIC DESIGN.
- 4. DESIGN CRITERIA FOR RAILROAD STRUCTURES NOT SUPPORTING HIGH SPEED TRAINS SHALL BE THE AMERICAN RAILWAY ENGINEERING AND MAINTENANCE-OF-WAY ASSOCIATION (AREMA) MANUAL FOR RAILWAY ENGINEERING (APRIL 2008). FOR RAILROAD BRIDGES PASSING OVER THE HIGH SPEED TRAIN THE BRIDGE DESIGN SPECIFICATION SHALL BE SUPPLEMENTED BY THE CALIFORNIA HIGH SPEED TRAIN REQUIREMENTS FOR SEISMIC DESIGN.
- B. DESIGN METHOD
 - DESIGN SHALL BE PERFORMED TO THE LOAD AND RESISTANCE FACTOR (LRFD) DESIGN METHOD.
- C. GENERAL
 - 1. FOR ACRONYMS AND ABBREVIATIONS SEE DRAWING STD-SB-001 THROUGH STD-SB-002.
 - ALL STRUCTURAL DRAWINGS SHALL BE READ IN CONJUNCTION WITH THE SPECIFICATIONS AND ALL OTHER DRAWINGS RELATED TO THE WORK.
 - 3. EMBEDDED ITEMS SUCH AS PIPES, INSERTS, SLEEVES AND CONDUITS, AND ANY RECESSES, NICHES OR OPENINGS REQUIRED FOR UTILITY, ARCHITECTURAL, MECHANICAL AND ELECTRICAL INSTALLATIONS ARE NOT SHOWN ON THE STRUCTURAL DRAWINGS. CONTRACTOR SHALL REFER TO THE UTILITY, ARCHITECTURAL, MECHANICAL AND ELECTRICAL DRAWINGS FOR THE LOCATIONS AND DETAILS OF THESE ITEMS. CONTRACTOR SHALL REVIEW AND APPROVE ALL PENETRATIONS PRIOR TO CONSTRUCTION. PENETRATIONS WHICH LOCAL THICKENING OF CONCRETE OR STEEL MEMBERS AND YOR SUPPLEMENTAL REINFORCING SHALL BE SHOWN ON THE STRUCTURAL DRAWINGS.
 - 4. THE VERTICAL CONTROL OF ALL TRACK STRUCTURES IS BASED ON THE CONTROL POINT AS DEFINED ON THE CROSS SECTION DRAWINGS.
 - 5. CONTRACTORS ATTENTION IS DIRECTED TO THE AREAS OF SAG VERTICAL CURVES. IN SUCH AREAS CAUTION SHOULD BE EXERCISED THAT THE DIMENSION TO THE INVERT OF CONCRETE OF GUIDEWAY IS NEVER LESS THAN THAT SHOWN FOR INVERT DETAILS.

- 6. HORIZONTAL CONTROL FOR TRACK STRUCTURES SHOULD BE BASED ON THE VEHICLE CLEARANCE AND EQUIPMENT OPERATION AND MAINTENANCE REQUIREMENTS. CONTRACTOR SHALL EXERCISE NECESSARY CARE TO ASSURE THAT THE STRUCTURES ARE DESIGNED AND CONSTRUCTED TO THE INDICATED ALIGNMENT AND WITHIN THE SPECIFIED CLEARANCE CRITERIA AND TOLFRANCES.
- 7. ALL CONSTRUCTION JOINTS IN EARTH RETAINING STRUCTURES AND IN STRUCTURES BELOW THE FINISH GRADE SHALL CONTAIN CONTINUOUS WATERSTOPS, AND SHALL HAVE REINFORCEMENT CONTINUOUS ACROSS ALL JOINTS. HYDROSWELLING STRIPS SHALL BE INSTALLED ON ALL JOINT SURFACES WHICH WILL BE EXPOSED TO EARTH.
- 8. ALL WATERSTOPS SHALL BE INSTALLED SECURELY IN ACCORDANCE WITH THE SPECIFICATIONS. THE WATERSTOPS SHALL BE PLACED CONTINUOUSLY THROUGHOUT THE LENGTH OF THE CONSTRUCTION JOINT. LAPPING OF WATERSTOPS SHALL NOT BE PERMITTED. SPLICING SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS.
- 9. UNLESS INDICATED OTHERWISE, CONCRETE SURFACES LEADING TO DRAINS SHALL BE SLOPED A MINIMUM OF 1/8 INCH PER FOOT TOWARD THE DRAIN AND THE ADJACENT SURFACES WARPED AS REQUIRED TO SATISFY AN ADEQUATE DRAINAGE FLOW.
- D. MATERIAL PROPERTIES
 - 1. CONCRETE 28 DAY COMPRESSIVE STRENGTH (MINIMUM) UNLESS NOTED OTHERWISE
 - a) DRILLED SHAFTS: f'c=4500 PSI
 - b) PRECAST-PRESTRESSED PILES: f'c=6,500 PSI
 - c) FORMED CAST-IN-PLACE STRUCTURAL CONCRETE: f'c=5,000 PSI
 - d) PRECAST GIRDERS OR SEGMENTS OF GIRDERS: f'c=6,000 PSI
 - e) UNLESS NOTED OTHERWISE ON THE DRAWINGS, OR SPECIFIED, MINIMUM STRUCTURAL CONCRETE SHALL HAVE A MINIMUM 28 DAY COMPRESSIVE STRENGTH OF 4,000 PSI.
 - f) ALL EXPOSED CONCRETE EDGES AND CORNERS SHALL BE CHAMFERED WITH A ¾ INCH, 45 DEGREE CHAMFER UNLESS NOTED OTHERWISE.
 - 2. REINFORCING STEEL SHALL CONFORM TO THE SPECIFICATIONS OF ASTM A 706 GRADE 60.
 - 3. PRESTRESSING STEEL
 - ASTRAND:
 ASTM A416/AASHTO M203, GRADE 270, LOW RELAXATION
 FRICTION COEFFICIENT: 0.25
 WOBBLE COEFFICIENT: 0.0002 PER FT ANCHOR SET: 0.375"
 APPARENT MODULUS: 28,500 KSI MINIMUM JACKING STRESS: 216 KSI (80% ULTIMATE)
 MAXIMUM ANCHORING STRESS: 189 KSI (70% ULTIMATE)
 MAXIMUM STRESS AFTER ANCHOR SET: 202
 KSI (75% ULTIMATE)
 STRAND DIAMETER: 0.6" (AREA=0.216 SQ IN)

- b) POST TENSIONING BARS:
 ASTM A722/AASHTO M275, GRADE 150, TYPE II
 ANCHOR SET: 0.0625"
 APPARENT MODULUS: 30,000 KSI
 MAXIMUM JACKING STRESS: 113 KSI
 MAXIMUM ANCHORING STRESS: 105 KSI
 MAXIMUM STRESS AFTER LOSSES: 96 KSI
- 4. STRUCTURAL STEEL SHAPES SHALL CONFORM TO ASTM A6 WITH A YIELD STRENGTH OF FY = 50 KSI UNLESS NOTED OTHERWISE. THE FOLLOWING MATERIAL PROPERTIES SHALL APPLY:
 - a) WIDE FLANGE SHAPES: ASTM A992
 - b) M-SHAPES, S-SHAPES, HP SHAPES: ASTM A572
 - c) ANGLES, CHANNELS: ASTM A572
 - d) RECTANGULAR AND SQUARE HSS: ASTM A500 GR B (46 KSI)
 - e) ROUND HSS: ASTM A500 GR B (42 KSI)
 - f) STEEL PIPE: ASTM A53 GR B (35 KSI)
 - g) PLATES, BARS: ASTM A36 (36 KSI)
 - h) BOLTS: ASTM A325
 - i) NUTS: ASTM A563
 - j) WASHERS: ASTM F436
- 5. STEEL FABRICATIONS
 - a) WELDING OF BUILT UP MEMBERS AND STEEL FABRICATIONS SHALL COMPLY WITH AASHTO/AWS D 1.5
 - b) WELDING OF HSS SECTIONS AND PIPES SHALL COMPLY WITH AWS D 1.1
 - c) MISCELLANEOUS STEEL ITEMS SHALL BE HOT-DIP GALVANIZED AFTER FABRICATION UNLESS COMPLETELY EMBEDDED IN CONCRETE AND UNLESS NOTED OTHERWISE.
- 6. FASTENERS
 - a) ALL HIGH STRENGTH BOLTS NUTS AND WASHERS SHALL BE ZINC COATED
 - b) ALL BOLTED CONNECTIONS SHALL COMPLY WITH RESEARCH COUNCIL ON STRUCTURAL CONNECTIONS (RCSC) "SPECIFICATION FOR STRUCTURAL JOINTS USING ASTM A325 OR A490 BOLTS".
 - c) ALL BOLTS ARE ASTM A325 HIGH STRENGTH SLIP CRITICAL WITH THREADS EXCLUDED FROM THE SHEAR PLANE
- 7. CONCRETE COVER

UNLESS OTHERWISE NOTED, MINIMUM CONCRETE COVER SHALL BE:

- a) DRILLED SHAFTS: 6"
- b) PIERS, COLUMNS, WALLS, BENT CAPS: 2"
- c) FOOTINGS AND CONCRETE CAST AGAINST EARTH: 3"

- d) SUPERSTRUCTURE (CAST-IN-PLACE, PRECAST) TOP OF DECK: 2.5"
- e) SUPERSTRUCTURE (CAST-IN-PLACE, PRECAST) EXTERIOR SURFACES: 2"
- f) SUPERSTRUCTURE (CAST-IN-PLACE, PRECAST) INTERIOR SURFACES: 1.5"
- g) CONCRETE SURFACES NOT EXPOSED TO WEATHER, SOIL OR WATER: PRINCIPAL REINFORCEMENT: 2" STIRRUPS, TIES AND SPIRALS: 1.5"
- 8. ALLOWABLE STRESSES IN POST-TENSIONED CONCRETE
 - a) SUPERSTRUCTURE CONCRETE STRESSES AT
 THE SERVICE LIMIT STATE
 MAXIMUM COMPRESSION:
 0.45 f'c (POST-TENSIONING AND PERMANENT
 LOADS)
 0.60 f'c (PERMANENT LOADS AND TRANSIENT
 LOADS)
 - b) LONGITUDINAL TENSION NO TENSION STRESSES ARE ALLOWED IN PRECOMPRESSED TENSILE ZONE AFTER ALL LOSSES HAVE OCCURRED.
 - c) PRINCIPAL TENSION STRESS: 0.1106 SQUARE ROOT f'c (KSI)
 - d) THE SUPERSTRUCTURE SHALL BE DESIGNED FOR APPLICABLE SERVICE, STRENGTH AND EXTREME LIMIT STATES AS DEFINED BY THE LOAD GROUPS IN THE DESIGN MANUAL.
- E. SEISMIC LOADING AND DESIGN

THERE ARE TWO LEVELS OF DESIGN EARTHOUAKES: THE MAXIMUM CONSIDERED EARTHOUAKE (MCE) AND THE OPERATING BASIS EARTHOUAKE (OBE) DEFINED AS:

- 1. MAXIMUM CONSIDERED EARTHQUAKE (MCE):
 GROUND MOTIONS CORRESPONDING TO GREATER OF
 (1) A PROBABILISTIC SPECTRUM BASED UPON A
 10% PROBABILITY OF EXCEEDANCE IN 100 YEARS
 (i.e., A RETURN PERIOD OF 950 YEARS) AND (2)
 A DETERMINISTIC SPECTRUM BASED UPON THE
 LARGEST MEDIAN RESPONSE RESULTING FROM THE
 MAXIMUM RUPTURE (CORRESPONDING TO M...) OF
 ANY FAULT IN THE VICINITY OF THE STRUCTURE.
- 2. OPERATING BASIS EARTHQUAKE (OBE): GROUND MOTIONS CORRESPONDING TO A PROBABILISTIC SPECTRUM BASED UPON AN 86% PROBABILITY OF EXCEEDANCE IN 100 YEARS (i.e., A RETURN PERIOD OF 50 YEARS).

PROPOSED Preliminary Design

NOT FOR CONSTRUCTION





CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A TRACK STRUCTURES GENERAL NOTES SHEET 1 OF 2 _____

ST-B0001

NO SCALE

DESIGN: HIGH SPEED RAIL DESIGN CRITERIA, AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS 4TH EDITION AND

CALTRANS AMENDMENTS, ACI 350 (WATER

CONVEYING STRUCTURES)

HIGH SPEED RAIL SPECIFICATIONS, STANDARD

CALTRANS STANDARD SPECIFICATIONS DATED MAY SPECIFICATIONS:

SEISMIC HIGH SPEED RAIL DESIGN CRITERIA, DESIGN:

CALTRANS SEISMIC DESIGN CRITERIA (SDC)

VERSION 1.6, NOVEMBER 2010

LIVE LOADING: COOPER E50 - HST ROUTE

COOPER E80 - UPRR ROUTE

WIND LOAD: 100 MPH WIND VELOCITY

REINFORCED fy = 60 KSI

CONCRETE: f'c = SEE CONCRETE STRENGTH AND TYPE LIMITS

LOW RELAXATION STRANDS PRESTRESSED

fpy = 243 KSI CONCRETE:

fpu = 270 KSI

f'ci = 5 KSI AT TIME OF RELEASE

f'c = 7 KSI AT 28 DAYS

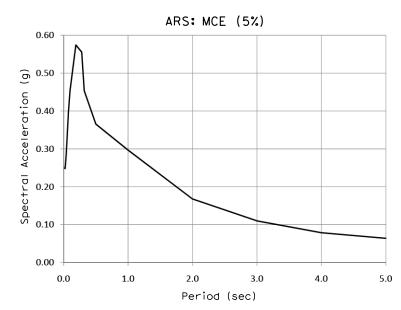
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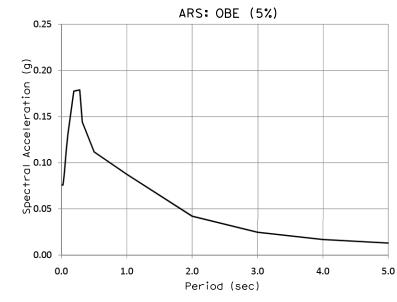
CLASS 200 PILES



CLASS 140 PILES CLASS 90 PILES

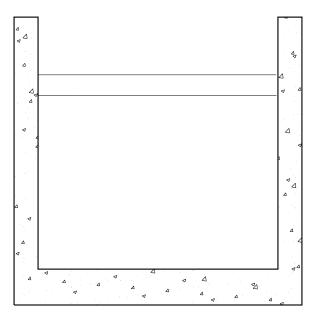






GENERAL STRUCTURAL NOTES

- 1. PILE/SHAFT LENGTHS TO BE DETERMINED.
- 2. EMBEDDED ITEMS REQUIRED FOR UTILITY INSTALLATIONS ARE NOT SHOWN ON THE STRUCTURAL DRAWINGS. CONTRACTOR SHALL REFER TO THE UTILITY DRAWINGS FOR THE LOCATIONS AND DETAILS OF THESE ITEMS.
- 3. MSE WALL REINFORCEMENT AND PANEL TO BE DETERMINED.



STRUCTURAL CONCRETE, BRACE f'c = 6 ksi at 28 days

STRUCTURAL CONCRETE, GRADE SEPARATION f'c = 5 ksi at 28 days

CONCRETE STRENGTH AND TYPE LIMITS

NO SCALE

DESIGNED BY A. ARMSTRONG F. PALERMO CHECKED BY N CHARGE R. COFFIN 2/17/12 DATE BY CHK APP DESCRIPTION

PROPOSED PRELIMINARY DESIGN NOT FOR

CONSTRUCTION



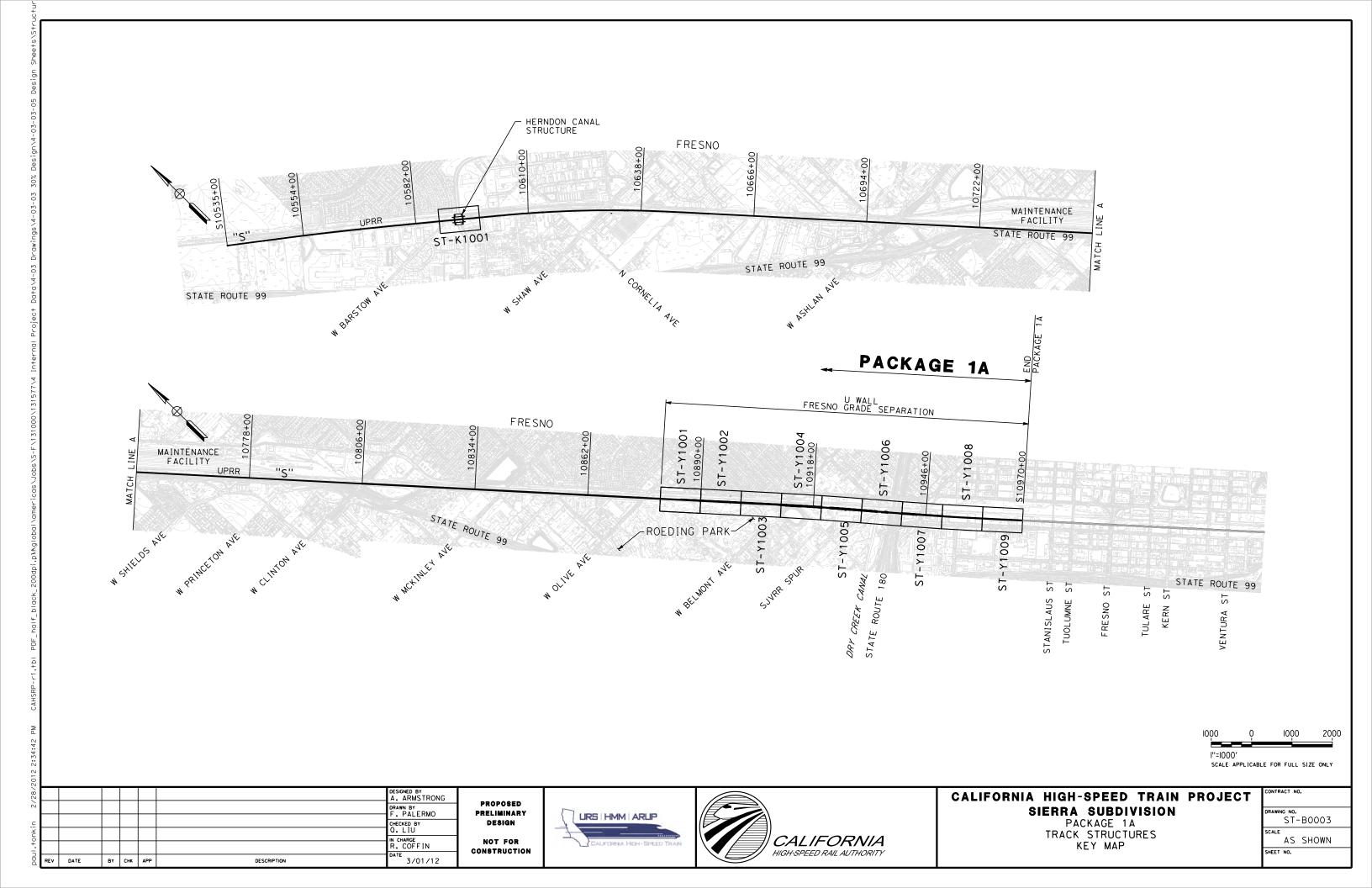


CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A TRACK STRUCTURES GENERAL NOTES SHEET 2 OF 2

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AS SHOWN SHEET NO.



CALIFORNIA

HIGH-SPEED RAIL AUTHORITY

SIERRA SUBDIVISION

PACKAGE 1A

TRACK STRUCTURES

HST BRIDGE AT HERNDON CANAL

GENERAL PLAN

ST-K1001-A5

AS SHOWN

SHEET NO.

PROPOSED

PRELIMINARY

DESIGN

NOT FOR

CONSTRUCTION

2020 L Street, Suite 300 Sacramento, CA 958II

CH2MHILL

E. CISNEROS

T. DUDLEY

A 2012-08 EC HL DM REVISED FOR ADDENDUM 5

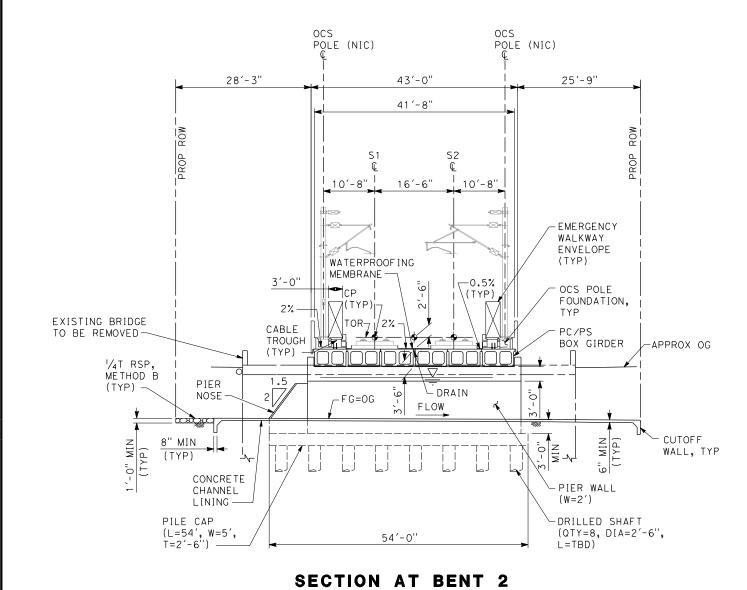
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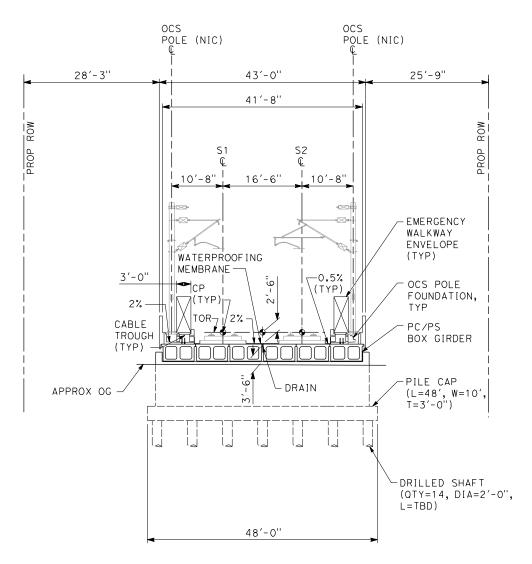
BY CHK APP

D. MINISTER

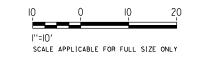
02/01/2012

- 1. ROW SHOWN REPRESENTS THE MINIMUM ANTICIPATED ROW REQUIREMENTS. ACCURATE ROW AND ACCESS DATA WILL BE MADE AVAILABLE PRIOR TO NOTE TO PROCEED.
- 2. OCS POSTS SHOWN ARE INDICATIVE ONLY AND MAY LOCATE OUTSIDE BRIDGE STRUCTURE.
- 3. CUTTOFF WALLS SHALL EXTEND ALONG CHANNEL BOTTOM & SIDES TO TOP OF SLOPE.





SECTION AT ABUTMENT 3



					<u>_</u>	DESIGNED BY H. LEE DRAWN BY	
					!	E. CISNEROS CHECKED BY T. DUDLEY	
REV	DATE	ВҮ	СНК	APP	<u> </u>	N CHARGE D. MINISTER DATE 02/01/2012	С

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CONSTRUCTION

AECOM Technical Services, Inc. 2020 L Street, Suite 300 Sacromento, CA 95811 CH2MHILL



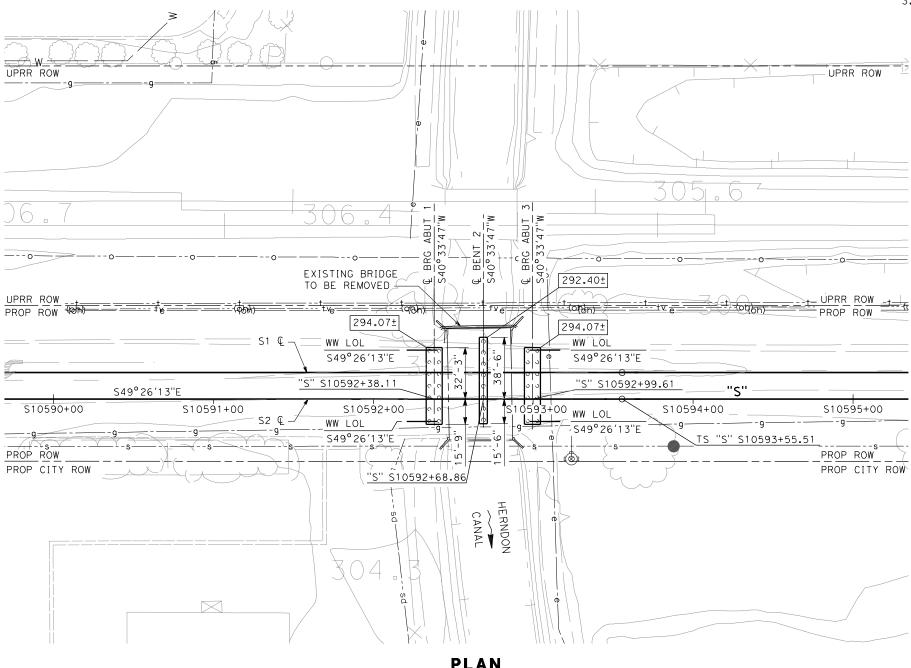
CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A TRACK STRUCTURES HST BRIDGE AT HERNDON CANAL TYPICAL SECTIONS

CONTR	ACT N	0.			
DRAWIN	DRAWING NO.				
	ST-	-K1002			
SCALE					
	AS	SHOWN			
SHEET	NO.				

NOTES:

- 1. ROW SHOWN REPRESENTS THE MINIMUM ANTICIPATED ROW REQUIREMENTS. ACCURATE ROW AND ACCESS DATA WILL BE MADE AVAILABLE PRIOR TO NOTICE TO PROCEED.
- 2. EXISTING BRIDGE AND UTILITY ARE APPROXIMATE.
- 3. EXISTING UNDERGROUND AND OVERHEAD UTILITIES TO BE DETERMINED.



PLANSCALE: 1" = 30'

						DESIGNED BY H. LEE DRAWN BY E. CISNEROS CHECKED BY T. DUDLEY IN CHARGE D. MINISTER DATE
REV	DATE	ВҮ	СНК	APP	DESCRIPTION	12/08/11

PROPOSED
PRELIMINARY
DESIGN
NOT FOR

CONSTRUCTION

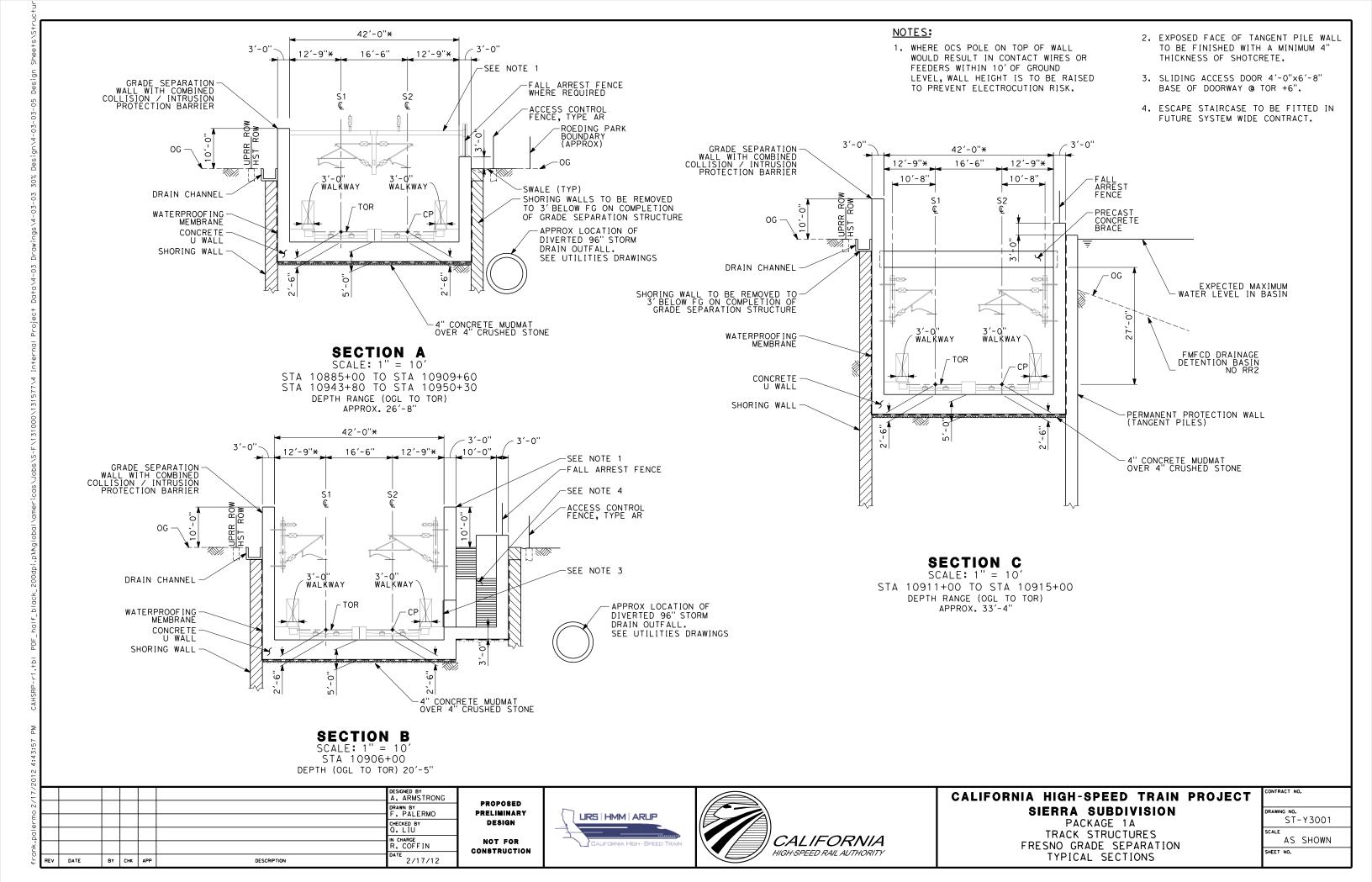
AECOM
Technical Services, Inc.
2020 L Street, Suite 300
Sacromento, CA 9581
CH2MHILL



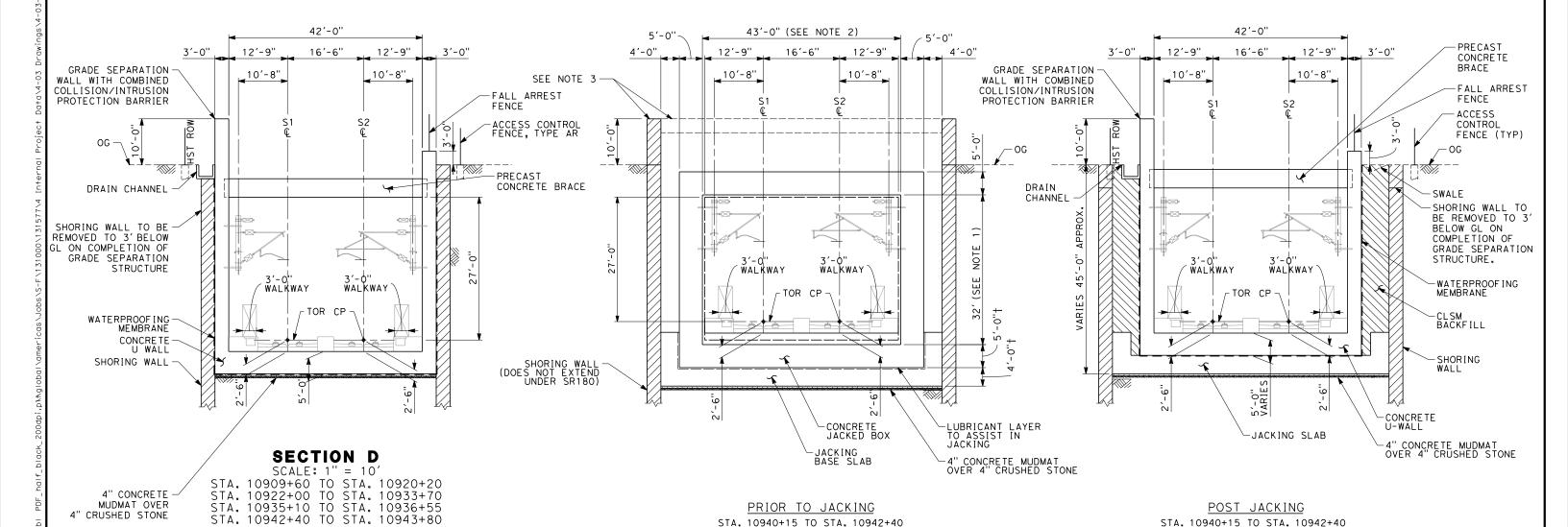
CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A TRACK STRUCTURES HST BRIDGE AT HERNDON CANAL FOUNDATION PLAN

CONTRA	ACT NO).	
DRAWIN	G NO.		
	ST-	-K1003	
SCALE			
	AS	SHOWN	
SHEET	NO.		



- 1. THE DIMENSION OF 32'-0" HAS BEEN DETERMINED AS INDICATED BELOW: 27'-0" - MINIMUM ENVELOPE
 - 2'-6" TRACK MAKEUP
 - 0'-6" -ALLOWANCE FOR VERTICAL CURVATURE OF TOR 2'-0" - VERTICAL TOLERANCE
- 2. THE DIMENSION OF 43'-0" HAS BEEN DETERMINED AS INDICATED BELOW: 42'-0" - MINIMUM ENVELOPE 1'-0" - HORIZONTAL TOLERANCE THE COSS-SECTIONAL AREA SHALL ACCOMODATE THE "FREE AREA" CRITERIA FOR MEDICAL HEALTH AND COMFORT.
- 3. TEMPORARY SHORING WALL MAY BE EXTENDED ABOVE OG TO PERMIT INSTALLATION OF HIGH LEVEL BRACING. SHORING WALL TO BE CAPABLE OF SUPPORTING EXCAVATION WITHOUT BRACING IN HEIGHT OF BOX.



STA. 10940+15 TO STA. 10942+40

SECTION E SCALE: 1" = 10

						DESIGNED BY A. ARMSTRONG	
						DRAWN BY	
						CHECKED BY	
						Q. LIU	
						IN CHARGE R. COFFIN	
REV	DATE	ВΥ	СНК	APP	DESCRIPTION	DATE 2/17/12	

DEPTH RANGE (OGL TO TOR) APPROX. 26'-8" TO 43'-2"

> PROPOSED PRELIMINARY DESIGN NOT FOR

CONSTRUCTION

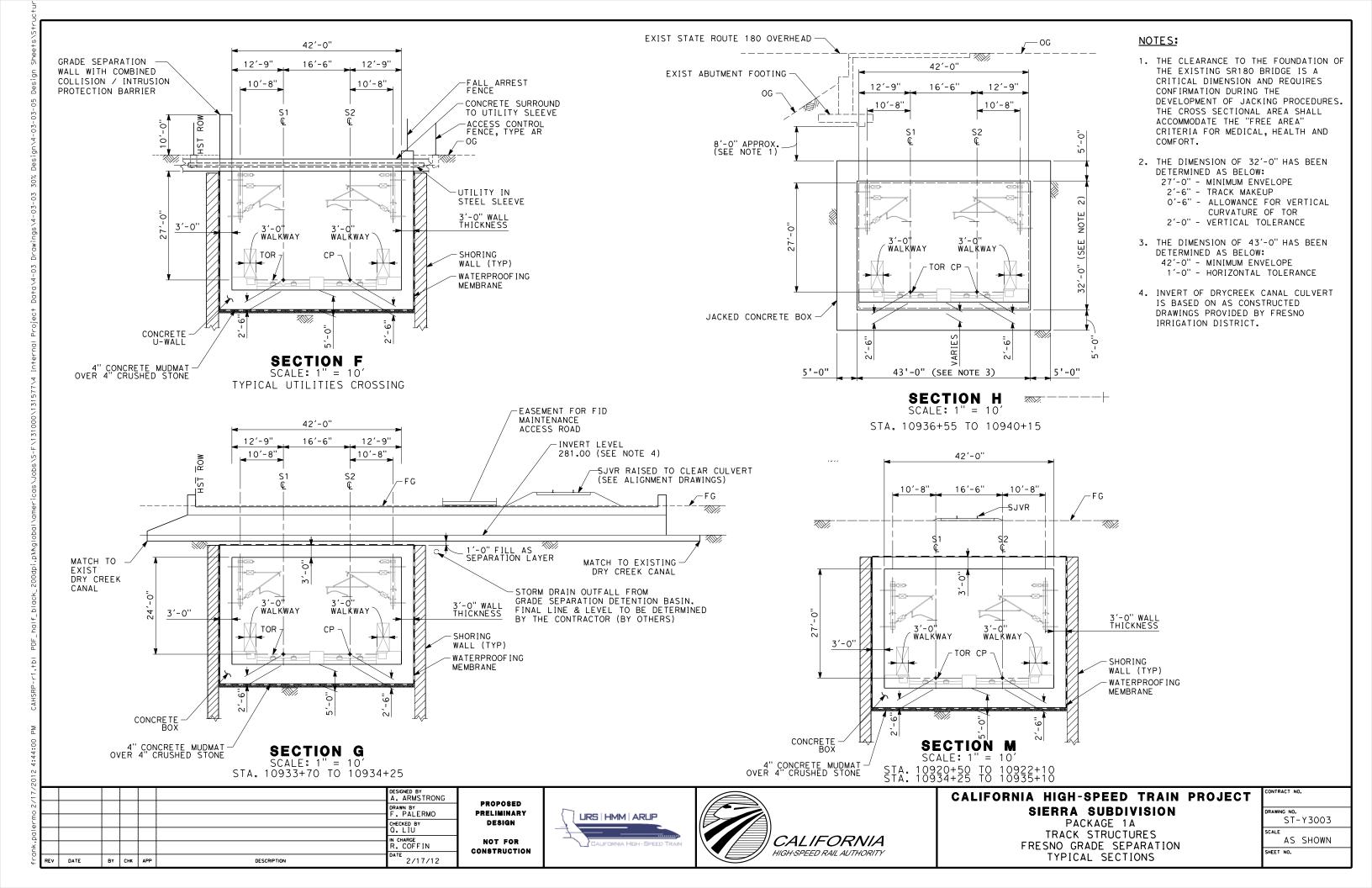


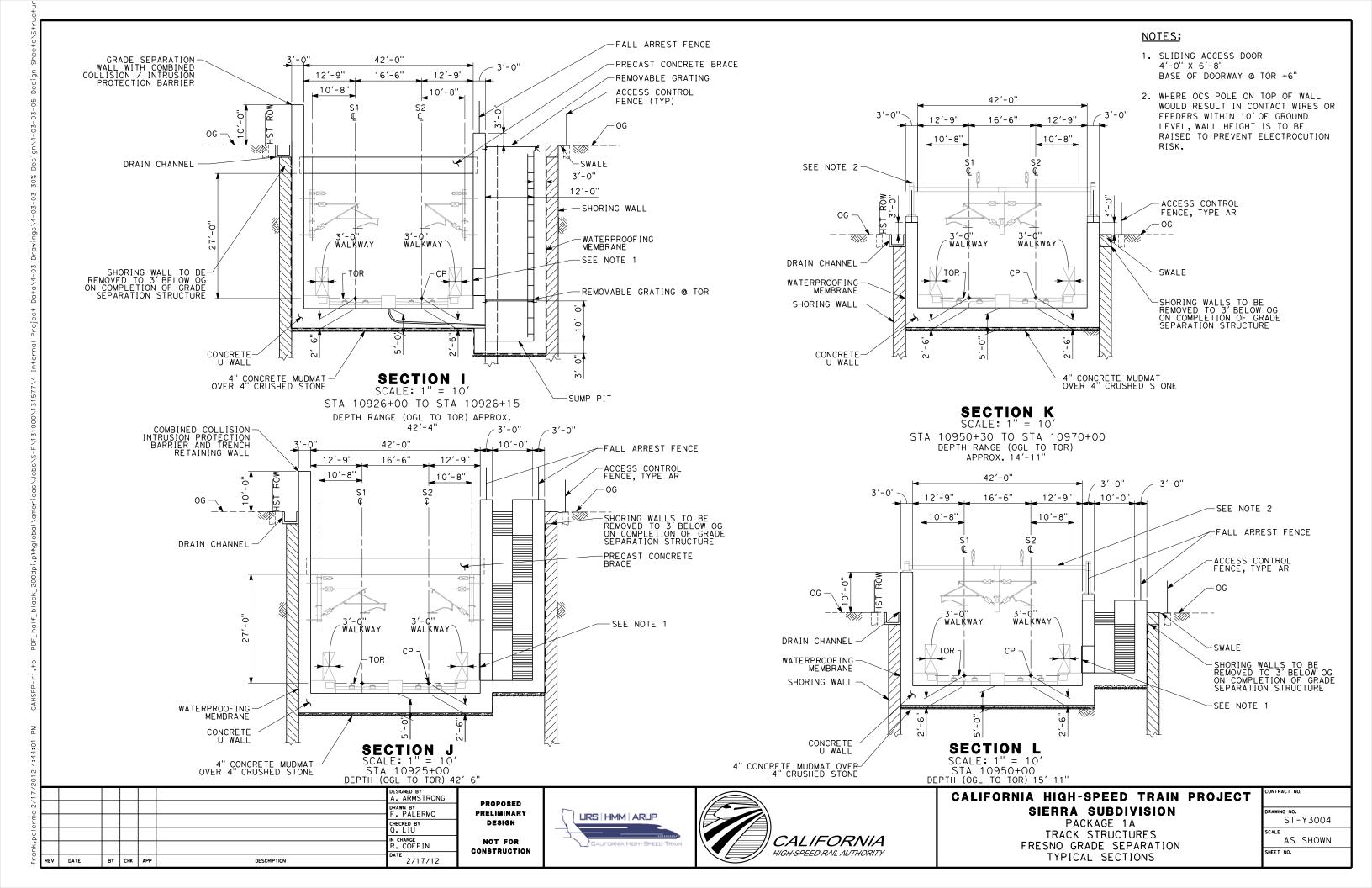


CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A TRACK STRUCTURES FRESNO GRADE SEPARATION TYPICAL SECTIONS

		-	
DRAWIN		-Y3002	
SCALE			
	AS	SHOWN	





BVC 10878+82.08 ELEV 299.83 BVC 10898+82.08 ELEV 281.39 NOTES: 1. GRADE ELEVATIONS SHOWN ARE EQUAL TO TOP OF RAIL. 0.11% 2000' VC R/C= -0.101%/STA -1.90% 2. ALL PIERS ARE NORMAL TO THE STATION LINE UNLESS OTHERWISE SHOWN. TOP OF RAIL "S" LINE 3. REFER TO TRACK ALIGNMENT DATA TABLE FOR CURVE AND TANGENT INFORMATION. NO SCALE 4. SEE COMPOSITE UTILITY
DRAWINGS FOR FULL DETAILS
OF EXISTING UNDERGROUND,
OVERHEAD AND RELOCATED TOTAL LENGTH OF GRADE SEPARATION = 8500'-00" (MEASURED ALONG "S" LINE) BEGIN GRADE SEPAR STA 10885+00 A **LEGEND:** GRADE SEPARATION
WALL WITH COMBINED
COLLISION / INTRUSION
PROTECTION BARRIER GRADE SEPARATION WALL WITH COMBINED COLLISION / INTRUSION -EXPANSION PROTECTION BARRIER TOP OF RAIL JOINT (TYP) - APPROX OG -BOTTOM OF GRADE SEPARATION SLAB DATUM ELEV (NGVD 88) = 200.00 S10880+00 S10881+00 S10882+00 S10883+00 S10884+00 S10885+00 S10886+00 S10887+00 S10888+00 S10889+00 S10890+00 **ELEVATION** SCALE 1'' = 40'EXPANSION JOINT BOTH SIDES NOT IN SLAB (TYP) UPRR UPRR UPRR +ROW UPRR ROW PROP. ROW. PROP ROW "S" S10881+00 <u>S</u>10882+00 S10883+00 S10884±00 S108<u>85+00</u> S10886+00 \$10887±00 v S10888+00 S10889+00 S10880+00 PROP ROW PROP ROW PROP TCE BEGIN GRADE SEPARATION STA 10885+00 TOR ELEV 298.59 PROP TCE GRADE SEPARATION WALL **PLAN** SCALE 1'' = 40'DESIGNED BY A. ARMSTRONG CALIFORNIA HIGH-SPEED TRAIN PROJECT **PROPOSED** F. PALERMO SIERRA SUBDIVISION PRELIMINARY URS HMM ARUP ST-Y1001 DESIGN PACKAGE 1A CHECKED BY TRACK STRUCTURES N CHARGE R. COFFIN **CALIFORNIA** AS SHOWN NOT FOR FRESNO GRADE SEPARATION CONSTRUCTION HIGH-SPEED RAIL AUTHORITY SHEET NO. PLAN AND ELEVATION 2/17/12 DATE BY CHK APP DESCRIPTION

DATE

BY CHK A

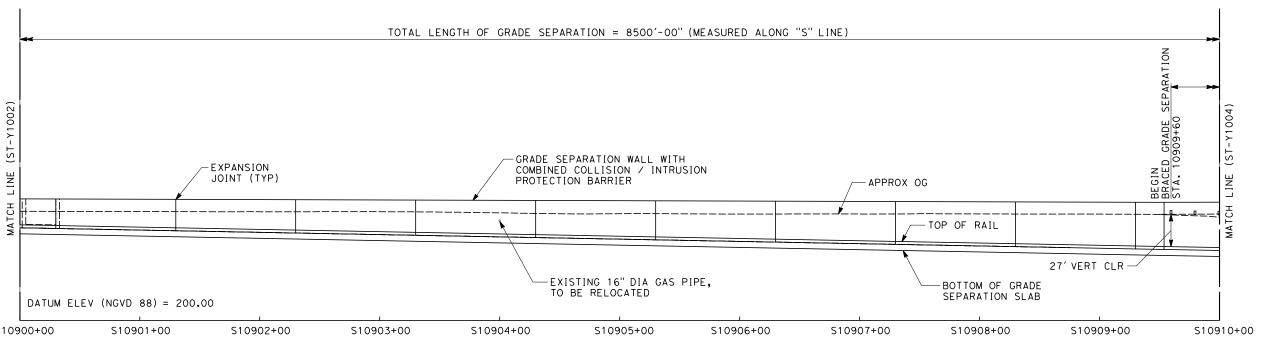


3300' VC R/C = +0.105%/STA

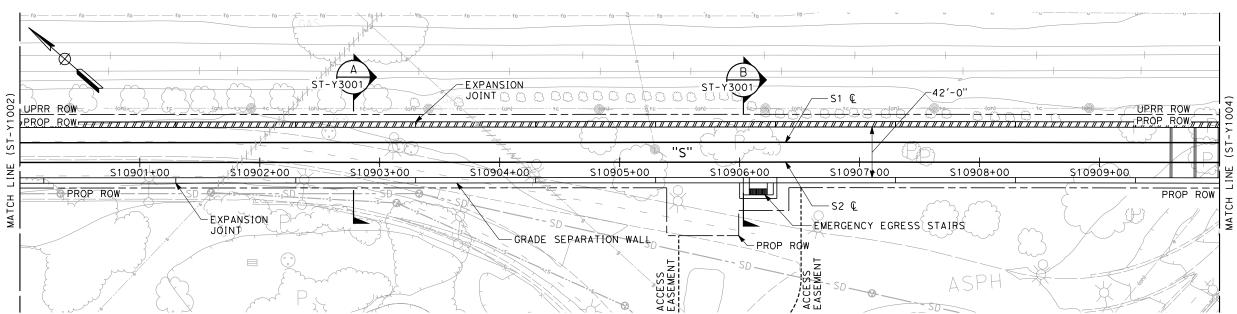
NOTES:

1. FOR NOTES AND LEGEND SEE DRAWING NO. ST-Y1001.

TOP OF RAIL "S" LINE







APP	DESCRIPTION	DATE 2/17/12			THEIT OF ELD WAIL ACTIVITIES
		IN CHARGE R. COFFIN	NOT FOR CONSTRUCTION	CALIFORNIA HIGH - SPEED TRAIN	CALIFORNIA HIGH-SPEED RAIL AUTHORITY
		CHECKED BY Q. LIU	DESIGN		
		DRAWN BY F. PALERMO	PROPOSED Preliminary	URS HMM ARUP	
		A. ARMSTRONG	2222222		

CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A TRACK STRUCTURES FRESNO GRADE SEPARATION PLAN AND ELEVATION

CONTR	CONTRACT NO.					
DRAWIN	IG NO.					
	ST-	-Y1003				
SCALE						
	AS	SHOWN				
SHEET	NO.					

BVC 10908+75.48 ELEV 263.06 EVC 10941+75.48 ELEV 257.28 NOTES: 1. FOR NOTES AND LEGEND SEE -1.90% DRAWING NO. ST-Y1001. 3300' VC R/C= +0.105%/STA 1.55% 2. BETWEEN THE LIMITS INDICATED ON THE PLAN, THE PERMANENT PROTECTION TOP OF RAIL "S" LINE WALL SHALL BE PART OF THE PERMANENT NO SCALE CONSTRUCTION AND SHALL BE CONSTRUCTED USING TANGENT PILES. DEPTH TO BE DESIGNED BY CONTRACTOR, FINISHED TOTAL LENGTH OF GRADE SEPARATION = 8500'-00" (MEASURED ALONG "S" LINE) GRADE TO MATCH LEVEL BRACED GRADE SEPARATION = 1050'-0" OF EXISTING ROADWAY AT BASIN PERIMETER. EXISTING 8" DIA GAS PIPE, TO BE RELOCATED -EXISTING 12" DIA GAS PIPE, -PROPOSED E BELMONT AVE TO BE RELOCATED GRADE SEPARATION WALL WITH COMBINED COLLISION/ INTRUSION PROTECTION BARRIER -EXPANSION MULTIPLE UTILITY CROSSING TITITITI 20'-0" EQUAL SPACING ←APPROX OG → 10'-0" EQUAL SPACING JOINT (TYP) 12" WATER, 12" GAS, 8" GAS, 16" GAS (SEE COMPOSITE UTILITY DRAWINGS) Hardania a a a a da a a a a a a d 0 0 0 0 0 0 0 0 TOP OF RAIL 400'-0" = PERMANENT PROTECTION WALL (TANGENT PILES) -BOTTOM OF GRADE SEPARATION SLAB DATUM ELEV (NGVD 88) = 200.00 S10910+00 S10911+00 S10912+00 \$10913+00 S10914+00 S10915+00 S10916+00 S10917+00 S10918+00 S10919+00 S10920+00 **ELEVATION** SCALE 1'' = 40'ABANDONED BELMONT AVE UNDERCROSSING **UPRR** TO BE BACKFILLED (SEE ROADWAY PLANS) PROP UPRR ---<u>JC</u>E 42'-0" Y3001 — S1/ € ST-Y3003 UPRR ROW UPRR ROW PROP ROW PROP ROW PROP TCE MATCH GRADE SEPARATION WALL MULTIPLE UTILITY CROSSING (SEE COMPOSITE UTILITY DRAWINGS) BEGIN TANGENT STA 10911+00 (SEE NOTE 2) PROP ROW 96" STORM DRAIN TO BE DIVERTEDPRIOR TO GRADE SEPARATION CONSTRUCTION (SEE COMPOSITE UTILITY DRAWINGS) PLAN SCALE 1'' = 40'CALIFORNIA HIGH-SPEED TRAIN PROJECT A. ARMSTRONG PROPOSED F. PALERMO SIERRA SUBDIVISION **PRELIMINARY** URS HMM ARUP DESIGN PACKAGE 1A CHECKED BY TRACK STRUCTURES N CHARGE R. COFFIN **CALIFORNIA** NOT FOR FRESNO GRADE SEPARATION CONSTRUCTION HIGH-SPEED RAIL AUTHORITY SHEET NO. PLAN AND ELEVATION 2/17/12 BY CHK APP DESCRIPTION

DATE

ST-Y1004

AS SHOWN

BVC 10908+75.48 ELEV 263.06 EVC 10941+75.48 ELEV 257.28 NOTES: 1. FOR NOTES AND LEGEND SEE DRAWING NO. ST-Y1001. -1.90% 3300' VC R/C= +0.105%/STA 1.55% 2. DRY CREEK CANAL MAINTENANCE ACCESS PATHWAY PROVIDED FOR FRESNO IRRIGATION DISTRICT. PERMANENT EASEMENT REQUIRED. TOP OF RAIL "S" LINE NO SCALE TOTAL LENGTH OF GRADE SEPARATION = 8500'-00" (MEASURED ALONG "S" LINE) BRACED GRADE SEPARATION = 1150'-0" END 1050'-0" BRACED GRADE SEPARATION EGIN BRACED RADE SEPARATION TA 10922+20 (ST-Y1004) 10'-0" EQUAL SPACING GRADE SEPARATION WALL WITH -EXPANSION JOINT BOTH SIDES NOT COMBINED COLLISION / INTRUSION PROTECTION BARRIER -EXPANSION JOINT (TYP) -DRAINAGE SUMP ACCESS LADDER LINE LINE IN SLAB (TYP) APPROX OG MATCH EXISTING -27'-0" MIN VERT CLR TOP OF RAIL GRADE -BOTTOM OF GRADE SEPARATION SLAB DATUM ELEV (NGVD 88) = 200.00 10'-0" DRAINAGE SUMP PIT S10921+00 S10922+00 S10923+00 S10924+00 S10925+00 S10926+00 S10928+00 S10929+00 S10920+00 S10927+00 S10930+00 **ELEVATION** SCALE 1'' = 40'UPRR PROP TCE UPRR ROW Y[.]3003 ST-Y3004 (ST S10921+00 PROP ROW -S2 Q GRADE SEPARATION WALL EMERGENCY EGRESS STAIRS POINT OF MIN VERT CLR
"S" 10921+30.05 -DRAINAGE SUMP WITH ACCESS LADDER -SEE NOTE 2 **PLAN** SCALE 1'' = 40'DESIGNED BY A. ARMSTRONG CALIFORNIA HIGH-SPEED TRAIN PROJECT **PROPOSED** F. PALERMO SIERRA SUBDIVISION PRELIMINARY URS HMM ARUP DESIGN PACKAGE 1A CHECKED BY TRACK STRUCTURES N CHARGE R. COFFIN **CALIFORNIA** NOT FOR FRESNO GRADE SEPARATION CONSTRUCTION HIGH-SPEED RAIL AUTHORITY PLAN AND ELEVATION 2/17/12 DATE BY CHK APP DESCRIPTION

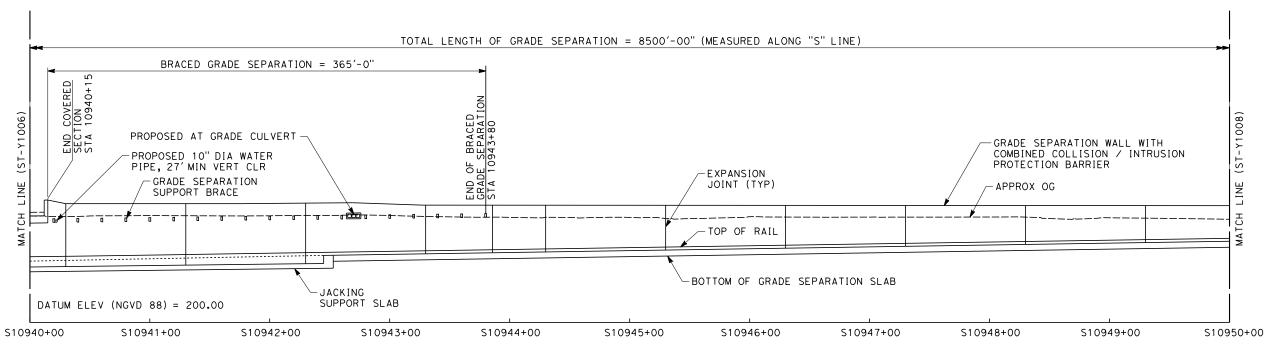
ST-Y1005

AS SHOWN

EVC 10941+75.48 /ELEV 257.28 BVC 10908+75.48 ELEV 263.06 NOTES: 1. FOR NOTES AND LEGEND SEE DRAWING NO. ST-Y1001. 1.55% 3300' VC R/C= +0.105%/STA -1.90% 2. DRY CREEK CANAL MAINTENANCE ACCESS PATHWAY PROVIDED FOR FRESNO IRRIGATION DISTRICT. PERMANENT EASEMENT REQUIRED. TOP OF RAIL "S" LINE 3. DRY CREEK INVERT BASED ON ELEVATION OF 281'-O" AS INDICATED ON "AS CONSTRUCTED" NO SCALE PLANS FROM FRESNO IRRIGATION DISTRICT TOTAL LENGTH OF GRADE SEPARATION = 8500'-00" (MEASURED ALONG "S" LINE) BRACED GRADE SEPARATION = 1150'-0" BRACED GRADE SEPARATION = 130'-0" BEGIN COVERED SECTION STA 10936+15 BEGIN BOX STA 10937+05 END BRACED GRADE SEPARATION STA 10933+70 EQUAL SPACING STATE ROUTE 180 SOUTH_BOUND STATE ROUTE 180 NORTH BOUND RELOCATED 12" GAS LINE 100 EXPANSION JOINT (TYP) PROPOSED 30" DIA SEWER LINE, 24' MIN VERT CLR (ST-Y100 ALLOWANCE FOR FUTURE WIDENING OF SR 180 — -GRADE SEPARATION WALL WITH COMBINED COLLISION / INTRUSION PROTECTION BARRIER APPROX CLR SHORING WALL - APPROX OG DRY CREEK LINE SJVR CANAL MATCH 8'-0" MIN VERT CLR TOP OF RAIL 24'-0" MIN VERT CLR BOTTOM OF GRADE SEPARATION SLAB SR 180 UNDERCROSSING EXISTING BRIDGE FOUNDATION DATUM ELEV (NGVD 88) = 200.00 SHORING WALL S10932+00 S10933+00 S10934+00 S10936+00 S10937+00 S10939+00 S10930+00 S10931+00 S10935+00 S10938+00 S10940+00 **ELEVATION** SCALE 1'' = 40'RECEIVING PIT SR 180 UNDERCROSSING UPRR ROW Y3003 UPRR ROW - MATCH EXIST (SEE NOTE 3) 3002 005) S10938+00 S10939+00 PROP ROW GRADE SEPARATION WALL X-| S2 | **C**[ROUTE BOUND POINT OF MIN VERT CLR STATE SOUTH STATE NOR TH ALLOWANCE FOR FUTURE WIDENING OF STATE ROUTE 180 MATCH EXIST (SEE NOTE 3) PLAN SCALE 1'' = 40'DESIGNED BY A. ARMSTRONG CALIFORNIA HIGH-SPEED TRAIN PROJECT PROPOSED F. PALERMO SIERRA SUBDIVISION **PRELIMINARY** URS HMM ARUP ST-Y1006 DESIGN PACKAGE 1A CHECKED BY TRACK STRUCTURES N CHARGE R. COFFIN **CALIFORNIA** AS SHOWN NOT FOR FRESNO GRADE SEPARATION CONSTRUCTION HIGH-SPEED RAIL AUTHORITY SHEET NO. PLAN AND ELEVATION 2/17/12 DATE BY CHK APP DESCRIPTION

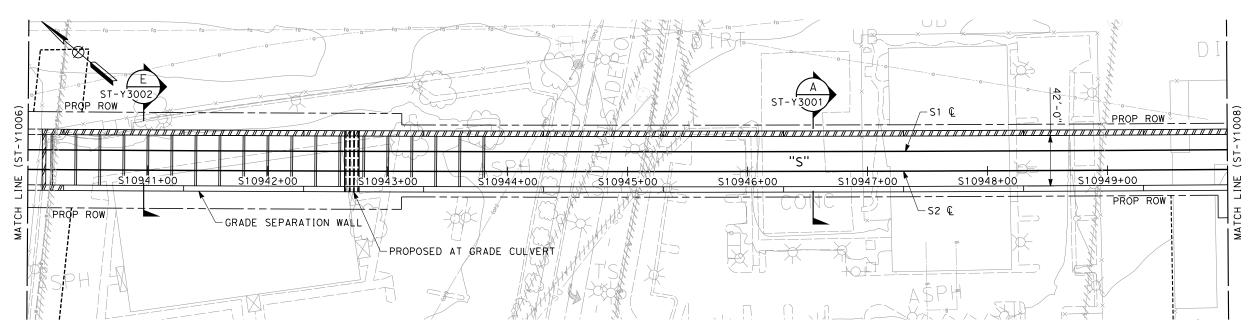
NOTES:

1. FOR NOTES AND LEGEND SEE DRAWING NO. ST-Y1001.



ELEVATION

SCALE 1'' = 40'



PLAN SCALE 1'' = 40'

REV	DATE	ВΥ	СНК	APP	DESCRIPTION	DATE 2/17/12	
						R. COFFIN	l c
						Q. LIU IN CHARGE	
<u> </u>						CHECKED BY	
i						DRAWN BY F. PALERMO	1
<u> </u>						DESIGNED BY A. ARMSTRONG	

PROPOSED PRELIMINARY DESIGN NOT FOR CONSTRUCTION

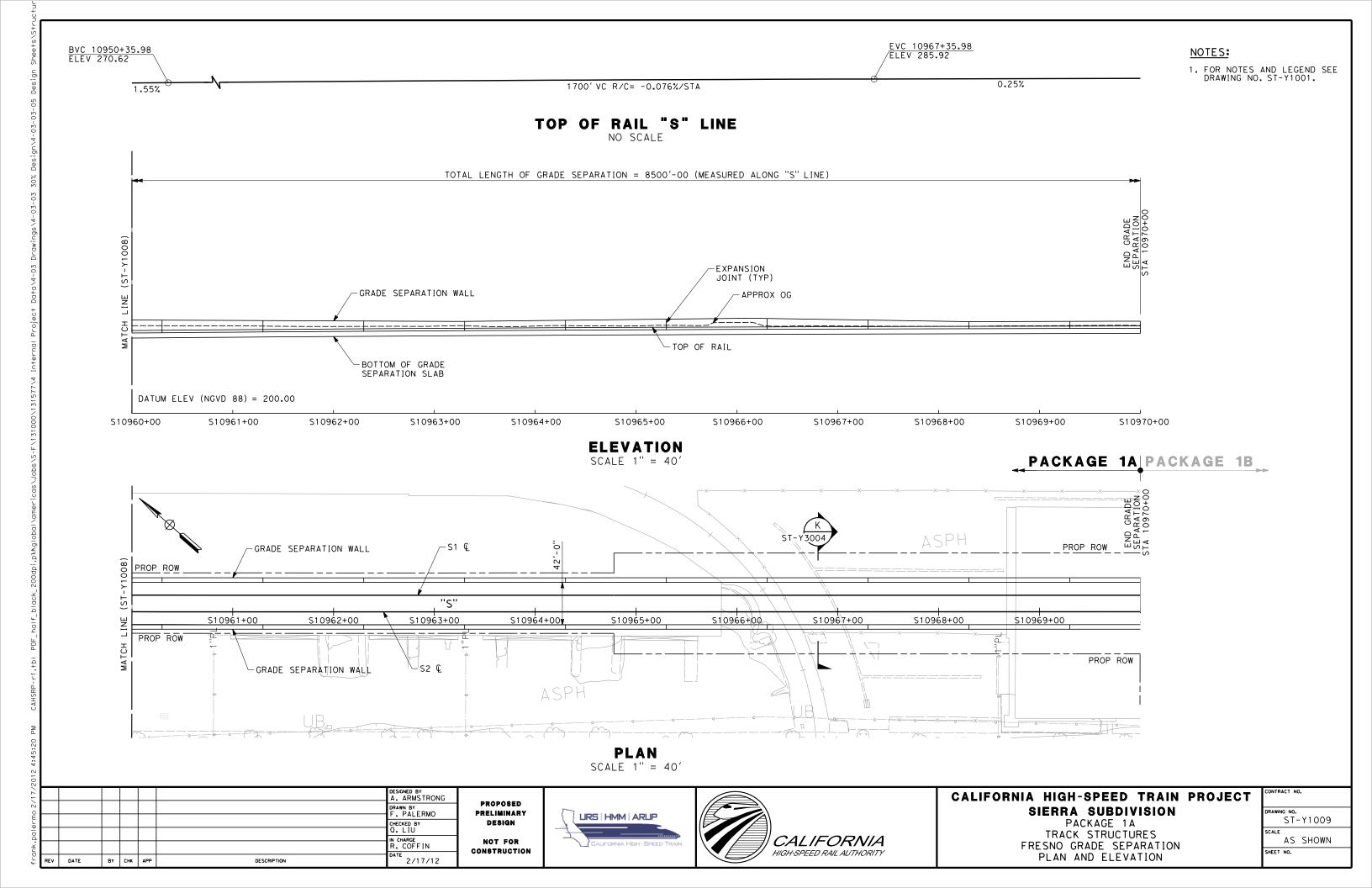




CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A TRACK STRUCTURES FRESNO GRADE SEPARATION PLAN AND ELEVATION

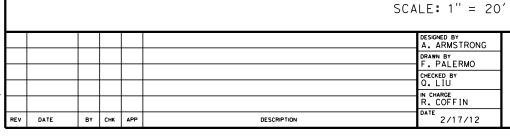
CONTRACT NO.	
DRAWING NO. ST-Y1007	
scale AS SHOWN	











10'-0"

29'-0"

..9-,9



CONSTRUCTION

194'-8"

SIDE ELEVATION SCALE: 1" = 20'

114'-8"

118'-0"

PLAN



—"STOP PLANK" GROOVES (TYP)

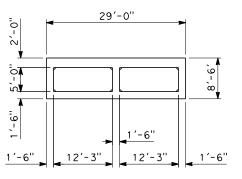
44'-0"

10'-0"



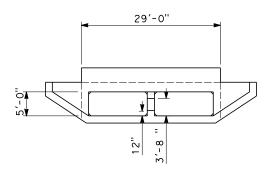
NOTES:

- 1. ALL INTERNAL CORNERS OF BOX TO HAVE A 6" CHAMFER.
- 2. STRUCTURE TO BE DESIGNED TO ACI350.
- INLET AND OUTLET SECTIONS SHALL BE CONSTRUCTED TO PROVIDE A SLOT FOR "STOP PLANKS" IN ACCORDANCE WITH FID (FRESNO IRRIGATION DISTRICT) STANDARD DETAILS.



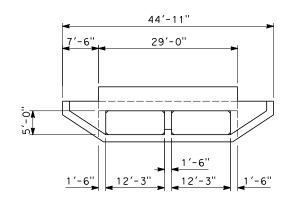
SECTION A

SCALE: 1" = 10'



SOUTH ELEVATION

SCALE: 1" = 10'



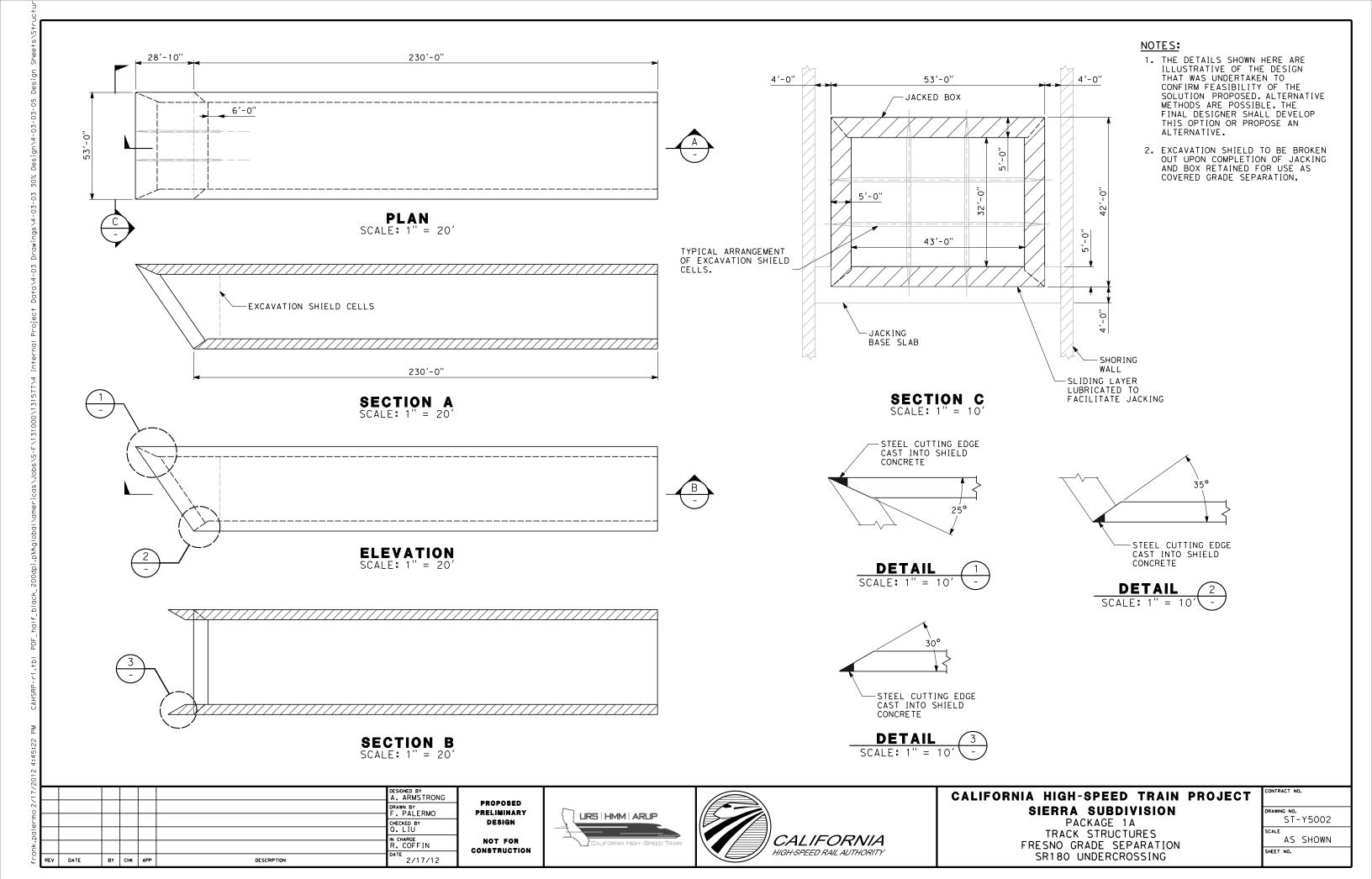
NORTH ELEVATION

SCALE: 1" = 10'

CALIFORNIA	HIGH-SPEED	TRAIN	PROJEC.
S	IERRA SUBDI	VISION	
	PACKAGE 1	Δ	

TRACK STRUCTURES
FRESNO GRADE SEPARATION DRY CREEK CANAL CULVERT

CONTR	ACT NO).
DRAWIN		-Y5001
SCALE		
	ΑS	SHOWN



VOLUME 3 - ROADWAY

DRAWING No.	REV No.	DRAWING DESCRIPTION
		GENERAL
GE-A0003		PACKAGE 1A - ROADWAY- INDEX OF DRAWINGS - SHEET 1 OF 4
GE-A0004		PACKAGE 1A - ROADWAY- INDEX OF DRAWINGS - SHEET 2 OF 4
GE-A0005		PACKAGE 1A - ROADWAY- INDEX OF DRAWINGS - SHEET 3 OF 4
GE-A0006		PACKAGE 1A - ROADWAY- INDEX OF DRAWINGS - SHEET 4 OF 4
CV-B0002		PACKAGE 1A - ROADWAY- GENERAL STRUCTURAL NOTES
CV-B0003		PACKAGE 1A - ROADWAY - INDEX MAP
		ROADWAY
CV-T1001		PACKAGE 1A - ROADWAY - GRADE SEPARATION LAYOUT - W MCKINLEY AVE
CV-T1002		PACKAGE 1A - ROADWAY - GRADE SEPARATION PROFILE - W MCKINLEY
CV-T1003		PACKAGE 1A - ROADWAY - GRADE SEPARATION PROFILE - MCKINLEY AVE CONNECTOR/N WEBER AVE
CV-T1004		PACKAGE 1A - ROADWAY - GRADE SEPARATION LAYOUT - N GOLDEN STATE BLVD
CV-T1005		PACKAGE 1A - ROADWAY - GRADE SEPARATION PROFILE - N GOLDEN STATE BLVD
CV-T1006		PACKAGE 1A - ROADWAY - GRADE SEPARATION PROFILE - N GOLDEN STATE BLVD
CV-T1007		PACKAGE 1A - ROADWAY - GRADE SEPARATION LAYOUT - W OLIVE AVE
CV-T1008		PACKAGE 1A - ROADWAY - GRADE SEPARATION PROFILE - W OLIVE AVE
CV-T1009		PACKAGE 1A - ROADWAY - GRADE SEPARATION LAYOUT - BELMONT AVE
CV-T1010		PACKAGE 1A - ROADWAY - GRADE SEPARATION PROFILE - BELMONT AVE
CV-T1011		PACKAGE 1A - ROADWAY - GRADE SEPARATION PROFILE - N H ST/N WESLEY AVE
CV-T1012		PACKAGE 1A - ROADWAY - GRADE SEPARATION PROFILE - N CARUTH AVE/N FARRIS AVE
CV-R1001-THN		PACKAGE 1A - ROADWAY - ROADWAY LAYOUT - N THORNE AVE
CV-R1002-THN		PACKAGE 1A - ROADWAY - GRADE SEPERATION PROFILE- N THORNE AVE
CV-T3001		PACKAGE 1A - ROADWAY - ROADWAY / GRADE SEPARATION - TYPICAL SECTIONS
CV-T3002		PACKAGE 1A - ROADWAY - ROADWAY / GRADE SEPARATION - TYPICAL SECTIONS
CV-T3003		PACKAGE 1A - ROADWAY - ROADWAY / GRADE SEPARATION - TYPICAL SECTIONS
CV-T3004		PACKAGE 1A - ROADWAY - ROADWAY / GRADE SEPARATION - TYPICAL SECTIONS
CV-T3005		PACKAGE 1A - ROADWAY - ROADWAY / GRADE SEPARATION - TYPICAL SECTIONS
CV-T3006		PACKAGE 1A - ROADWAY - ROADWAY / GRADE SEPARATION - TYPICAL SECTIONS
CV-T5000		PACKAGE 1A - ROADWAY - CONSTRUCTION DETAILS
CV-T5001		PACKAGE 1A - ROADWAY - ROADWAY CLOSURE DETAILS
ST-I1001		PACKAGE 1A - ROADWAY - GOLDEN STATE BOULEVARD BRIDGE AT HERNDON CANAL - GENERAL PLAN
ST-I1002		PACKAGE 1A - ROADWAY - GOLDEN STATE BOULEVARD BRIDGE AT HERNDON CANAL - TYPICAL SECTIONS
ST-I1003		PACKAGE 1A - ROADWAY - GOLDEN STATE BOULEVARD BRIDGE AT HERNDON CANAL - FOUNDATION PLAN
ST-I1004		PACKAGE 1A - ROADWAY - SHAW AVE OVERHEAD - GENERAL PLAN NO. 1
ST-I1005		PACKAGE 1A - ROADWAY - SHAW AVE OVERHEAD - GENERAL PLAN NO. 2
ST-I1006		PACKAGE 1A - ROADWAY - SHAW AVE OVERHEAD - TYPICAL SECTIONS
ST-I1007		PACKAGE 1A - ROADWAY - SHAW AVE OVERHEAD - FOUNDATION PLAN
ST-I1008		PACKAGE 1A - ROADWAY - ASHLAN OVERHEAD - GENERAL PLAN
ST-I1009		PACKAGE 1A - ROADWAY - ASHLAN OVERHEAD - TYPICAL SECTION

VOLUME 3 - ROADWAY

ST-I1010	DACKACE 1A - DOADWAY - ACHI AN OVERHEAD - FOUNDATION DI ANI
	PACKAGE 1A - ROADWAY - ASHLAN OVERHEAD - FOUNDATION PLAN
ST-I1011	PACKAGE 1A - ROADWAY - ASHLAN OVERHEAD - BENT ELEVATION
ST-I1012	PACKAGE 1A - ROADWAY - CLINTON AVE OVERCROSSING - GENERAL PLAN
ST-I1013	PACKAGE 1A - ROADWAY - CLINTON AVE OVERCROSSING - TYPICAL SECTION
ST-I1014	PACKAGE 1A - ROADWAY - CLINTON AVE OVERCROSSING - FOUNDATION PLAN
ST-I1015	PACKAGE 1A - ROADWAY - CLINTON AVE OVERCROSSING - CONSTRUCTION DETAILS NO. 1
ST-I1016	PACKAGE 1A - ROADWAY - CLINTON AVE OVERCROSSING - CONSTRUCTION DETAILS NO. 2
ST-I1017	PACKAGE 1A - ROADWAY - FRESNO YARD OVERHEAD (CLINTON/UPRR) - GENERAL PLAN
ST-I1018	PACKAGE 1A - ROADWAY - FRESNO YARD OVERHEAD (CLINTON/UPRR) - TYPICAL SECTION
ST-I1019	PACKAGE 1A - ROADWAY - FRESNO YARD OVERHEAD (CLINTON/UPRR) - FOUNDATION PLAN
ST-I1020	PACKAGE 1A - ROADWAY - FRESNO YARD OVERHEAD (CLINTON/UPRR)- BENT ELEVATION
ST-I1021	PACKAGE 1A - ROADWAY - MCKINLEY BLVD UNDERCROSSING (WIDEN) - GENERAL PLAN
ST-I1022	PACKAGE 1A - ROADWAY - MCKINLEY BLVD UNDERCROSSING (WIDEN) - TYPICAL SECTION
ST-I1023	PACKAGE 1A - ROADWAY - MCKINLEY BLVD UNDERCROSSING (WIDEN) - FOUNDATION PLAN
ST-I1024	PACKAGE 1A - ROADWAY - VETERANS BOULEVARD OVERHEAD - DRAFT GENERAL PLAN
ST-I1025	PACKAGE 1A - ROADWAY - VETERANS BOULEVARD OVERHEAD - DRAFT GENERAL PLAN
ST-K1026	PACKAGE 1A - ROADWAY - DRAFT GENERAL PLAN - W MCKINLEY AVE
ST-K1027	PACKAGE 1A - ROADWAY - DRAFT GENERAL PLAN - W OLIVE AVE
ST-K1028	PACKAGE 1A - ROADWAY - DRAFT GENERAL PLAN - BELMONT AVE
ST-K1029	PACKAGE 1A - ROADWAY - DRAFT GENERAL PLAN - BELMONT AVE
CV-GEOO1	PACKAGE 1A - ROADWAY - GENERAL NOTES
CV-R0001	PACKAGE 1A - ROADWAY - KEY MAP - GOLDEN STATE BOULEVARD
CV-R0002	PACKAGE 1A - ROADWAY - KEY MAP - SR 99 RE-ALIGNMENT
CV-R1001-GSB	PACKAGE 1A - ROADWAY - PLAN AND PROFILE - GOLDEN STATE BOULEVARD
CV-R1002-GSB	PACKAGE 1A - ROADWAY - PLAN AND PROFILE - GOLDEN STATE BOULEVARD
CV-R1003-GSB	PACKAGE 1A - ROADWAY - PLAN AND PROFILE - GOLDEN STATE BOULEVARD
CV-R1004-GSB	PACKAGE 1A - ROADWAY - PLAN AND PROFILE - GOLDEN STATE BOULEVARD
CV-R1005-GSB	PACKAGE 1A - ROADWAY - PLAN AND PROFILE - GOLDEN STATE BOULEVARD
CV-R1006-GSB	PACKAGE 1A - ROADWAY - PLAN AND PROFILE - GOLDEN STATE BOULEVARD
CV-R1007-GSB	PACKAGE 1A - ROADWAY - PLAN AND PROFILE - GOLDEN STATE BOULEVARD
CV-R1008-GSB	PACKAGE 1A - ROADWAY - PLAN AND PROFILE - GOLDEN STATE BOULEVARD
CV-R1009-GSB	PACKAGE 1A - ROADWAY - PLAN AND PROFILE - GOLDEN STATE BOULEVARD
CV-R1010-GSB	PACKAGE 1A - ROADWAY - PLAN AND PROFILE - GOLDEN STATE BOULEVARD
CV-R1011-GSB	PACKAGE 1A - ROADWAY - PLAN AND PROFILE - GOLDEN STATE BOULEVARD
CV-R1012-GSB	PACKAGE 1A - ROADWAY - PLAN AND PROFILE - VETERANS BOULEVARD
CV-R1013-GSB	PACKAGE 1A - ROADWAY - PLAN AND PROFILE - BULLARD AVENUE
CV-R1014-GSB	PACKAGE 1A - ROADWAY - PLAN AND PROFILE - SHAW AVENUE
	The state of the s
CV-R1015-GSB	PACKAGE 1A - ROADWAY - PLAN AND PROFILE - SHAW AVENUE
CV-R1015-GSB CV-R1016-GSB	PACKAGE 1A - ROADWAY - PLAN AND PROFILE - SHAW AVENUE PACKAGE 1A - ROADWAY - PLAN AND PROFILE - CORNELIA AVENUE/SANTA ANA AVENUE

						DESIGNED BY A. SHIELDS
						DRAWN BY A. SHIELDS
						CHECKED BY A. BOONE IN CHARGE
						A. BOONE
REV	DATE	BY	СНК	APP	DESCRIPTION	02/01/2012

PROPOSED PRELIMINARY DESIGN NOT FOR

CONSTRUCTION

AECOM Technical Services, Inc. 2020 L Street, Suite 300 Sacramento, CA 958II **CH2MHILL**



CALIFORNIA HIGH-SPEED TRAIN PROJECT CONTRACT NO. SIERRA SUBDIVISION

PACKAGE 1A ROADWAY
INDEX OF DRAWINGS
SHEET 1 OF 4

DRAWING NO.
GE-A0003
SCALE

dsa 2/20/2012 9:46:49 AM cahsrp

VOLUME 3 - ROADWAY

CV-R3001-GSB	PACKAGE 1A - ROADWAY - TYPICAL SECTION - GOLDEN STATE BOULEVARD
CV-R3002-GSB	PACKAGE 1A - ROADWAY - TYPICAL SECTION - GOLDEN STATE BOULEVARD
CV-R3003-GSB	PACKAGE 1A - ROADWAY - TYPICAL SECTION - GOLDEN STATE BOULEVARD
CV-R3004-GSB	PACKAGE 1A - ROADWAY - TYPICAL SECTION - GOLDEN STATE BOULEVARD
CV-R3005-GSB	PACKAGE 1A - ROADWAY - TYPICAL SECTION - GOLDEN STATE BOULEVARD
CV-R3006-GSB	PACKAGE 1A - ROADWAY - TYPICAL SECTION - SHAW AVENUE
CV-R3007-GSB	PACKAGE 1A - ROADWAY - TYPICAL SECTION - SHAW AVENUE
CV-R3008-GSB	PACKAGE 1A - ROADWAY - TYPICAL SECTION - SHAW AVENUE & VETERANS BOULEVARD
CV-R3009-GSB	PACKAGE 1A - ROADWAY - TYPICAL SECTION - VETERANS BOULEVARD & BULLARD AVENUE
CV-R3010-GSB	PACKAGE 1A - ROADWAY - TYPICAL SECTION - CORNELIA AVENUE AND MOTEL DRIVE
CV-R3011-GSB	PACKAGE 1A - ROADWAY - TYPICAL SECTION - MOTEL, JENNIFER, KATHRYN & BARSTOW
CV-R3012-GSB	PACKAGE 1A - ROADWAY - TYPICAL SECTION - WEBER, MARKET & STATE
CV-I1001-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1 - GOLDEN STATE BOULEVARD
CV-I1002-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1 - GOLDEN STATE BOULEVARD
CV-I1003-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1 - GOLDEN STATE BOULEVARD
CV-I1004-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1 - GOLDEN STATE BOULEVARD
CV-I1005-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1 - GOLDEN STATE BOULEVARD
CV-I1006-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1 - GOLDEN STATE BOULEVARD
CV-I1007-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1 - GOLDEN STATE BOULEVARD
CV-I1008-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1 - GOLDEN STATE BOULEVARD
CV-I1009-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1 - GOLDEN STATE BOULEVARD
CV-I1010-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1 - GOLDEN STATE BOULEVARD
CV-I1011-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1 - GOLDEN STATE BOULEVARD
CV-I1012-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2 - GOLDEN STATE BOULEVARD
CV-I1013-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2 - GOLDEN STATE BOULEVARD
CV-I1014-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2 - GOLDEN STATE BOULEVARD
CV-I1015-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2 - GOLDEN STATE BOULEVARD
CV-I1016-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2 - GOLDEN STATE BOULEVARD
CV-I1017-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2 - GOLDEN STATE BOULEVARD
CV-I1018-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2 - GOLDEN STATE BOULEVARD
CV-I1019-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2 - GOLDEN STATE BOULEVARD
CV-I1020-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2 - GOLDEN STATE BOULEVARD
CV-I1021-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2 - GOLDEN STATE BOULEVARD
CV-I1022-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 3 - GOLDEN STATE BOULEVARD
CV-I1023-GSB	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 3 - GOLDEN STATE BOULEVARD
CV-G1001-GSB	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - GOLDEN STATE BOULEVARD
CV-G1002-GSB	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - GOLDEN STATE BOULEVARD
CV-G1003-GSB	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - GOLDEN STATE BOULEVARD
CV-G1004-GSB	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - GOLDEN STATE BOULEVARD

VOLUME 3 - ROADWAY

CV-G1005-GSB	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - GOLDEN STATE BOULEVARD
CV-G1006-GSB	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - GOLDEN STATE BOULEVARD
CV-G1007-GSB	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - GOLDEN STATE BOULEVARD
CV-G1008-GSB	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - GOLDEN STATE BOULEVARD
CV-G1009-GSB	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - GOLDEN STATE BOULEVARD
CV-G1010-GSB	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - GOLDEN STATE BOULEVARD
CV-G1011-GSB	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - GOLDEN STATE BOULEVARD
CV-R1001-R99	PACKAGE 1A - ROADWAY - LAYOUTS - SR 99 - HIGHWAY
CV-R1002-R99	PACKAGE 1A - ROADWAY - LAYOUTS - SR 99 - HIGHWAY
CV-R1003-R99	PACKAGE 1A - ROADWAY - LAYOUTS - SR 99 - HIGHWAY, RAMPS
CV-R1004-R99	
	PACKAGE 1A - ROADWAY - LAYOUTS - SR 99 - HIGHWAY, RAMPS
CV-R1005-R99	PACKAGE 1A - ROADWAY - LAYOUTS - SR 99 - HIGHWAY, RAMPS
CV-R1006-R99	PACKAGE 1A - ROADWAY - LAYOUTS - SR 99 - HIGHWAY, LOCAL ROADS
CV-R1007-R99	PACKAGE 1A - ROADWAY - LAYOUTS - SR 99 - HIGHWAY, LOCAL ROADS
CV-R1008-R99	PACKAGE 1A - ROADWAY - LAYOUTS - SR 99 - HIGHWAY
CV-R1009-R99	PACKAGE 1A - ROADWAY - LAYOUTS - SR 99 - HIGHWAY, LOCAL ROADS
CV-R1010-R99	PACKAGE 1A - ROADWAY - LAYOUTS - SR 99 - HIGHWAY, RAMPS, LOCAL ROADS
CV-R1011-R99	PACKAGE 1A - ROADWAY - LAYOUTS - SR 99 - HIGHWAY, RAMPS, LOCAL ROADS
CV-R1012-R99	PACKAGE 1A - ROADWAY - LAYOUTS - SR 99 - W CLINTON AVE
CV-R1013-R99	PACKAGE 1A - ROADWAY - LAYOUTS - SR 99 - W CLINTON AVE
CV-R1014-R99	PACKAGE 1A - ROADWAY - LAYOUTS - SR 99 - N GOLDEN STATE BLVD
CV-R1015-R99	PACKAGE 1A - ROADWAY - LAYOUTS - SR 99 - RAMPS
CV-R1016-R99	PACKAGE 1A - ROADWAY - LAYOUTS - SR 99 - LOCAL ROADS
CV-R1017-R99	PACKAGE 1A - ROADWAY - LAYOUTS - SR 99 - LOCAL ROADS
CV-R1018-R99	PACKAGE 1A - ROADWAY - LAYOUTS - SR 99 - ASHLAN AVE
CV-R1019-R99	PACKAGE 1A - ROADWAY - LAYOUTS - SR 99 - ASHLAN AVE
CV-R1020-R99	PACKAGE 1A - ROADWAY - LAYOUTS - SR 99 - RAMPS
CV-R1021-R99	PACKAGE 1A - ROADWAY - LAYOUTS - SR 99 - RAMPS
CV-R1101-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - HIGHWAY "A"
CV-R1102-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - HIGHWAY "A"
CV-R1103-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - HIGHWAY "A"
CV-R1104-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - HIGHWAY "A"
CV-R1105-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - HIGHWAY "A"
CV-R1106-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - HIGHWAY "A"
CV-R1107-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - HIGHWAY "A"
CV-R1108-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - HIGHWAY "A"
CV-R1109-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - HIGHWAY "A"
CV-R1110-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - W CLINTON AVE "C"
CV-R1111-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - W CLINTON AVE "C"

						DESIGNED BY A. SHIELDS
						DRAWN BY A. SHIELDS
						CHECKED BY A. BOONE
						IN CHARGE A. BOONE
REV	DATE	ВΥ	СНК	APP	DESCRIPTION	02/01/2012

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CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A ROADWAY INDEX OF DRAWINGS SHEET 2 OF 4



VOLUME 3 - ROADWAY

CV-R1112-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - RAMP "C1"
CV-R1113-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - RAMP "C2"
CV-R1114-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - RAMP "C3"
CV-R1115-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - RAMP "C4"
CV-R1116-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - RAMP "MS"
CV-R1117-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - RAMP "MS"
CV-R1118-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - RAMP "MS"
CV-R1119-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - RAMP "MN4"
CV-R1120-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - RAMP "MN"
CV-R1121-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - RAMP "MN"
CV-R1122-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - ASHLAN AVE "AS"
CV-R1123-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - ASHLAN AVE "AS"
CV-R1124-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - RAMP "AS1", "AS2"
CV-R1125-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - RAMP "AS3"
CV-R1126-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - RAMP "AS4"
CV-R1127-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - RAMP "AS4"
CV-R1128-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - RAMP "AS4"
CV-R1129-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - RAMP "AS5"
CV-R1130-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - RAMP "H1"
CV-R1131-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - RAMP "H1"
CV-R1132-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - RAMP "H2"
CV-R1133-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - PLEASANT AVE "P"
CV-R1134-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - SHIELDS AVE "S", "SC"
CV-R1135-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - VALENTINE AVE "VAL"
CV-R1136-R99	PACKAGE 1A - ROADWAY - PROFILE AND SUPERELEVATION - SR 99 - N PARKWAY DR "V1"
CV-R0001-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - HIGHWAY
CV-R0002-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - HIGHWAY
CV-R0003-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - HIGHWAY
CV-R0004-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - HIGHWAY
CV-R0005-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - HIGHWAY
CV-R0006-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - HIGHWAY
CV-R0007-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - HIGHWAY
CV-R0008-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - HIGHWAY
CV-R0009-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - HIGHWAY
CV-R0010-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - RAMP "C1"
CV-R0011-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - RAMP "C2"
CV-R0012-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - RAMP "C3"
CV-R0013-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - RAMP "C4"
CV-R0014-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - W CLINTON AVE
CV-R0015-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - W CLINTON AVE

VOLUME 3 - ROADWAY

CV-R0016-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - RAMP "MS", "MN"
CV-R0017-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - RAMP "MN4"
CV-R0018-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - RAMP "AS1", "AS2", "AS3"
CV-R0019-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - RAMP "AS4"
CV-R0020-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - RAMP "AS4", "AS5"
CV-R0021-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - ASHLAN AVE
CV-R0022-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - RAMP "H1", "H2"
CV-R0023-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - LOCAL ROAD "W"
CV-R0024-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - LOCAL ROAD "S", "SC", "P", "VAL", 'V1"
CV-R0025-R99	PACKAGE 1A - ROADWAY - TYPICAL SECTIONS - SR 99 - LOCAL ROAD "WP", "NP", "VA"
CV-R7001-R99	PACKAGE 1A - ROADWAY - PAVEMENT DELINEATION AND SIGNING - SR 99 RE - ALIGNMENT
CV-R7002-R99	PACKAGE 1A - ROADWAY - PAVEMENT DELINEATION AND SIGNING - SR 99 RE - ALIGNMENT
CV-R7003-R99	PACKAGE 1A - ROADWAY - PAVEMENT DELINEATION AND SIGNING - SR 99 RE - ALIGNMENT
CV-R7004-R99	PACKAGE 1A - ROADWAY - PAVEMENT DELINEATION AND SIGNING - SR 99 RE - ALIGNMENT
CV-R7005-R99	PACKAGE 1A - ROADWAY - PAVEMENT DELINEATION AND SIGNING - SR 99 RE - ALIGNMENT
CV-R7006-R99	PACKAGE 1A - ROADWAY - PAVEMENT DELINEATION AND SIGNING - SR 99 RE - ALIGNMENT
CV-R7007-R99	PACKAGE 1A - ROADWAY - PAVEMENT DELINEATION AND SIGNING - SR 99 RE - ALIGNMENT
CV-R7008-R99	PACKAGE 1A - ROADWAY - PAVEMENT DELINEATION AND SIGNING - SR 99 RE - ALIGNMENT
CV-R7009-R99	PACKAGE 1A - ROADWAY - PAVEMENT DELINEATION AND SIGNING - SR 99 RE - ALIGNMENT
CV-R7010-R99	PACKAGE 1A - ROADWAY - PAVEMENT DELINEATION AND SIGNING - SR 99 RE - ALIGNMENT
CV-R7011-R99	PACKAGE 1A - ROADWAY - PAVEMENT DELINEATION AND SIGNING - SR 99 RE - ALIGNMENT
CV-R7012-R99	PACKAGE 1A - ROADWAY - PAVEMENT DELINEATION AND SIGNING - SR 99 RE - ALIGNMENT
CV-R7013-R99	PACKAGE 1A - ROADWAY - PAVEMENT DELINEATION AND SIGNING - SR 99 RE - ALIGNMENT
CV-R7014-R99	PACKAGE 1A - ROADWAY - PAVEMENT DELINEATION AND SIGNING - SR 99 RE - ALIGNMENT
CV-R7015-R99	PACKAGE 1A - ROADWAY - PAVEMENT DELINEATION AND SIGNING - SR 99 RE - ALIGNMENT
CV-R7016-R99	PACKAGE 1A - ROADWAY - PAVEMENT DELINEATION AND SIGNING - SR 99 RE - ALIGNMENT
CV-R7017-R99	PACKAGE 1A - ROADWAY - PAVEMENT DELINEATION AND SIGNING - SR 99 RE - ALIGNMENT
CV-R7018-R99	PACKAGE 1A - ROADWAY - PAVEMENT DELINEATION AND SIGNING - SR 99 RE - ALIGNMENT
CV-R7019-R99	PACKAGE 1A - ROADWAY - PAVEMENT DELINEATION AND SIGNING - SR 99 RE - ALIGNMENT
CV-R7020-R99	PACKAGE 1A - ROADWAY - PAVEMENT DELINEATION AND SIGNING - SR 99 RE - ALIGNMENT
CV-R7021-R99	PACKAGE 1A - ROADWAY - PAVEMENT DELINEATION AND SIGNING - SR 99 RE - ALIGNMENT
CV-I1001-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1A - SR 99 RE - ALIGNMENT
CV-I1002-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1A - SR 99 RE - ALIGNMENT
CV-I1003-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1A - SR 99 RE - ALIGNMENT
CV-I1004-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1A - SR 99 RE - ALIGNMENT
CV-I1005-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1A - SR 99 RE - ALIGNMENT
CV-I1006-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1A - SR 99 RE - ALIGNMENT
CV-I1007-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1A - SR 99 RE - ALIGNMENT
CV-I1008-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1B - SR 99 RE - ALIGNMENT

						DESIGNED BY A. SHIELDS
						DRAWN BY A. SHIELDS
						CHECKED BY A. BOONE
						IN CHARGE A. BOONE
REV	DATE	ВҮ	СНК	APP	DESCRIPTION	02/01/2012

PROPOSED PRELIMINARY DESIGN NOT FOR

CONSTRUCTION

AECOM Technical Services, Inc. 2020 L Street, Suite 300 Sacramento, CA 958II **CH2MHILL**



CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A ROADWAY INDEX OF DRAWINGS SHEET 3 OF 4 GE-A0005

NONE SHEET NO.

VOLUME 3 - ROADWAY

CY-11070-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 18 - SR 99 RE - ALIGNMENT CY-11071-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 18 - SR 99 RE - ALIGNMENT CY-11071-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 18 - SR 99 RE - ALIGNMENT CY-11071-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 16 - SR 99 RE - ALIGNMENT CY-11071-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 16 - SR 99 RE - ALIGNMENT CY-11071-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT CY-11071-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT CY-11071-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT CY-11071-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT CY-11071-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT CY-11071-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT CY-11071-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT CY-11071-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT CY-11071-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT CY-11071-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT CY-11071-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT CY-11072-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT CY-11072-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT CY-11072-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT CY-11072-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT CY-11072-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT CY-11072-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CY-11072-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CY-11073-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CY-110		
CV-11011-R99	CV-I1009-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1B - SR 99 RE - ALIGNMENT
CV-11012-R99	CV-I1010-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1B - SR 99 RE - ALIGNMENT
CV-11013-R99	CV-I1011-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1B - SR 99 RE - ALIGNMENT
CV-11014-R99	CV-I1012-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1B - SR 99 RE - ALIGNMENT
CV-11015-R999	CV-I1013-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT
CV-11016-R999	CV-I1014-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT
CV-II017-R99	CV-I1015-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT
CV-11018-R99	CV-I1016-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT
PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT	CV-I1017-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT
CV-11020-R99	CV-I1018-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT
CV-11021-R99	CV-I1019-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT
CV-11022-R99	CV-I1020-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT
CV-11023-R99	CV-I1021-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT
CV-11024-R99	CV-I1022-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT
CV-11025-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT CV-11026-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11027-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11028-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11030-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11030-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11031-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11032-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11033-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11033-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11035-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11035-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11037-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-11038-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-11040-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-11040-R99<	CV-I1023-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT
CV-11026-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11027-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11028-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11029-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11030-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11031-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11032-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11033-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11033-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11034-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11035-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11036-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11037-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-11038-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-11040-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-11040-R99<	CV-I1024-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT
CV-11027-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11028-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11029-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11030-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11031-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11032-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11033-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11034-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11035-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11036-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11037-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-11038-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-11038-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-11038-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-1104-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-1104-R99 <td>CV-I1025-R99</td> <td>PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT</td>	CV-I1025-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 1C - SR 99 RE - ALIGNMENT
CV-I1028-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1029-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1030-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1031-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1032-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1033-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1034-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1035-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1036-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1037-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1038-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1038-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1040-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1040-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1040-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1041-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1043-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1043-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1043-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1043-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1046-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1046-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT	CV-I1026-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT
CV-11029-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11030-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11031-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11032-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11033-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11034-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11035-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11036-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-11037-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-11038-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-11038-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-11040-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-11041-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-11042-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-11043-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-11044-R99<	CV-I1027-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT
CV-I1030-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1031-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1032-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1033-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1034-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1035-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1036-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1037-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1038-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1039-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1040-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1041-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1042-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT	CV-I1028-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT
CV-I1031-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1032-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1033-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1034-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1035-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1036-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1037-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1038-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1039-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1040-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1041-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1042-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1043-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1044-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1047-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1046-R99<	CV-I1029-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT
CV-I1032-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1033-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1034-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1035-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1036-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1037-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1038-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1039-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1040-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1041-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1042-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1044-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT	CV-I1030-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT
CV-I1033-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1034-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1035-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1036-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1037-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1038-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1039-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1040-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1041-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1042-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1043-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1043-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1046-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1046-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1047-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT	CV-I1031-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT
CV-I1034-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1035-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1036-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1037-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1038-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1039-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1040-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1041-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1042-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1043-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1044-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1046-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1046-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1047-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1047-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT	CV-I1032-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT
CV-I1035-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1036-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1037-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1038-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1039-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1040-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1041-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1042-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1043-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1044-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1044-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1046-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1047-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1047-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1047-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT	CV-I1033-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT
CV-I1036-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT CV-I1037-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1038-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1039-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1040-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1041-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1042-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1043-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1044-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1045-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1046-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1047-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1047-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT	CV-I1034-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT
CV-I1037-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1038-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1039-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1040-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1041-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1042-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1043-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1044-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1045-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1046-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1047-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1047-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT	CV-I1035-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT
CV-I1038-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1039-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1040-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1041-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1042-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1043-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1044-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1045-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1046-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1047-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT	CV-I1036-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2A - SR 99 RE - ALIGNMENT
CV-I1039-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1040-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1041-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1042-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1043-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1044-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1045-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1046-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1047-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT	CV-I1037-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT
CV-I1040-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1041-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1042-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1043-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1044-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1045-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1046-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1047-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT	CV-I1038-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT
CV-I1041-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1042-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1043-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1044-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1045-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1046-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1047-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT	CV-I1039-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT
CV-I1042-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1043-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1044-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1045-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1046-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1047-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT	CV-I1040-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT
CV-I1043-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1044-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1045-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1046-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1047-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT	CV-I1041-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT
CV-I1044-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1045-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1046-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1047-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT	CV-I1042-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT
CV-I1045-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1046-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1047-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT	CV-I1043-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT
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CV-I1048-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT	CV-I1046-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT
	CV-I1047-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT
CV-I1049-R99 PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT	CV-I1048-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT
	CV-I1049-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT

VOLUME 3 - ROADWAY

VOLUME 3	- ROADWAT
CV-I1050-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT
CV-I1051-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT
CV-I1052-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT
CV-I1053-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT
CV-I1054-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT
CV-I1055-R99	PACKAGE 1A - CONSTRUCTION STAGING - STAGE 2B - SR 99 RE - ALIGNMENT
CV-G1001-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT
CV-G1002-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT
CV-G1003-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT
CV-G1003A-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT
CV-G1004-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT
CV-G1004A-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT
CV-G1005-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT
CV-G1006-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT
CV-G1007-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT
CV-G1008-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT
CV-G1009-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT
CV-G1010-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT
CV-G1011-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT
CV-G1012-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT
CV-G1013-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT
CV-G1014-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT
CV-G1015-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT
CV-G1016-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT
CV-G1017-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT
CV-G1018-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT
CV-G1019-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT
CV-G1020-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT
CV-G1021-R99	PACKAGE 1A - ROADWAY - GRADING AND DRAINAGE PLAN - SR 99 RE-ALIGNMENT

						DESIGNED BY A. SHIELDS	
						DRAWN BY A. SHIELDS	
						CHECKED BY A. BOONE	
						A. BOONE	
REV	DATE	BY	СНК	APP	DESCRIPTION	02/01/2012	L

PROPOSED PRELIMINARY DESIGN NOT FOR

CONSTRUCTION

AECOM Technical Services, Inc. 2020 L Street, Suite 300 Sacramento, CA 958II **CH2MHILL**



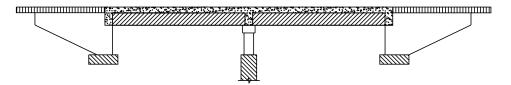
CALIFORNIA HIGH-SPEED TRAIN PROJECT CONTRACT NO. SIERRA SUBDIVISION

PACKAGE 1A ROADWAY INDEX OF DRAWINGS SHEET 4 OF 4

DRAWING NO.
GE-A0006
SCALE

GENERAL STRUCTURAL NOTES

- 1. PILE/SHAFT LENGTHS TO BE DETERMINED
- 2. EMBEDDED ITEMS REQUIRED FOR UTILITY INSTALLATIONS ARE NOT SHOWN ON THE STRUCTURAL DRAWINGS. CONTRACTOR SHALL REFER TO THE UTILITY DRAWINGS FOR THE LOCATIONS AND DETAILS OF THESE ITEMS.
- 3. MSE WALL REINFORCEMENT AND PANEL TO BE DETERMINED
- 4. ALL STRUCTURAL DRAWINGS SHALL BE READ IN CONJUNCTION WITH THE SPECIFICATIONS AND OTHER DRAWINGS RELATED TO THE WORK.
- 5. ALL SPANS ARE DESIGNED CONTINUOUS FOR LIVE LOAD.



- STRUCTURAL CONCRETE, BRIDGE f'c = 5 ksi AT 28 DAYS
- STRUCTURAL CONCRETE, APPROACH SLAB
- STRUCTURAL CONCRETE, SUPERSTRUCTURE f'c = 5 ksi AT 28 DAYS
- STRUCTURAL CONCRETE, BRIDGE FOOTING f'C = 4 ksi AT 28 DAYS
- PRECAST PRESTRESSED GIRDER
 f'c = SEE PRESTRESSED CONCRETE

CONCRETE STRENGTH AND TYPE LIMITS

NO SCALE

ADDENDUM 3 - RFP HSR 11-16

DESIGNED BY
M. BURGARD
DRAWN BY
D. BARNHART
CHECKED BY
L. SMITH
IN CHARGE
Y. BERI

DESCRIPTION

2.0

Period (sec)

GENERAL NOTES
LOAD AND RESISTANCE FACTOR DESIGN

HIGH SPEED RAIL SPECIFICATIONS,

DATED MAY 2006

CALTRANS STANDARD SPECIFICATIONS

HIGH SPEED RAIL DESIGN CRITERIA,

VERSION 1.6, NOVEMBER 2010

HL93 AND PERMIT DESIGN LOAD

100 MPH WIND VELOCITY

LOW RELAXATION STRANDS

f'c = 7 ksi AT 28 DAYS

f'ci = 5 ksi AT TIME OF RELEASE

fy = 60 ksi

fpy = 243 ksifpu = 270 ksi

CLASS 200 PILES

CLASS 140 PILES

CLASS 90 PILES

CALTRANS SEISMIC DESIGN CRITERIA (SDC)

INCLUDES 35 PSF FOR FUTURE WEARING SURFACE

f'c = SEE CONCRETE STRENGTH AND TYPE LIMITS

B2-8

/B2-5\

0.25

0.20

Φ 0.15

0.10

S 0.05

0.00

06/12/12

0.0

THE ÁRS CURVES SHOWN BELOW ARE FOR PRELIMINARY DESIGN. REVISED ARS CURVES WILL BE PROVIDED FOR DETAILED DESIGN.

ARS: MCE (5%)

ARS: OBE (5%)

5.0

DESIGN:

STANDARD

SEISMIC

DESIGN:

DEAD

LIVE LOADING:

WIND LOAD:

REINFORCED CONCRETE:

PRESTRESSED CONCRETE:

PILES:

0.60

(b) 0.50

0.40

0.30

0.20

0.10

0.00

DATE

BY CHK APP

1.0

LOAD:

SPECIFICATIONS:

HIGH SPEED RAIL DESIGN CRITERIA, AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS

4th EDITION AND CALTRANS AMENDMENTS

PROPOSED
PRELIMINARY
DESIGN
NOT FOR

CONSTRUCTION

1.0

2.0

Period (sec)



3.0

4.0

5.0



CALIFORNIA HIGH-SPEED TRAIN PROJECT
SIERRA SUBDIVISION
PACKAGE 1A

ROADWAY
GENERAL STRUCTURAL NOTES

DRAWING NO.
CV-B0002-A3

SCALE
AS SHOWN

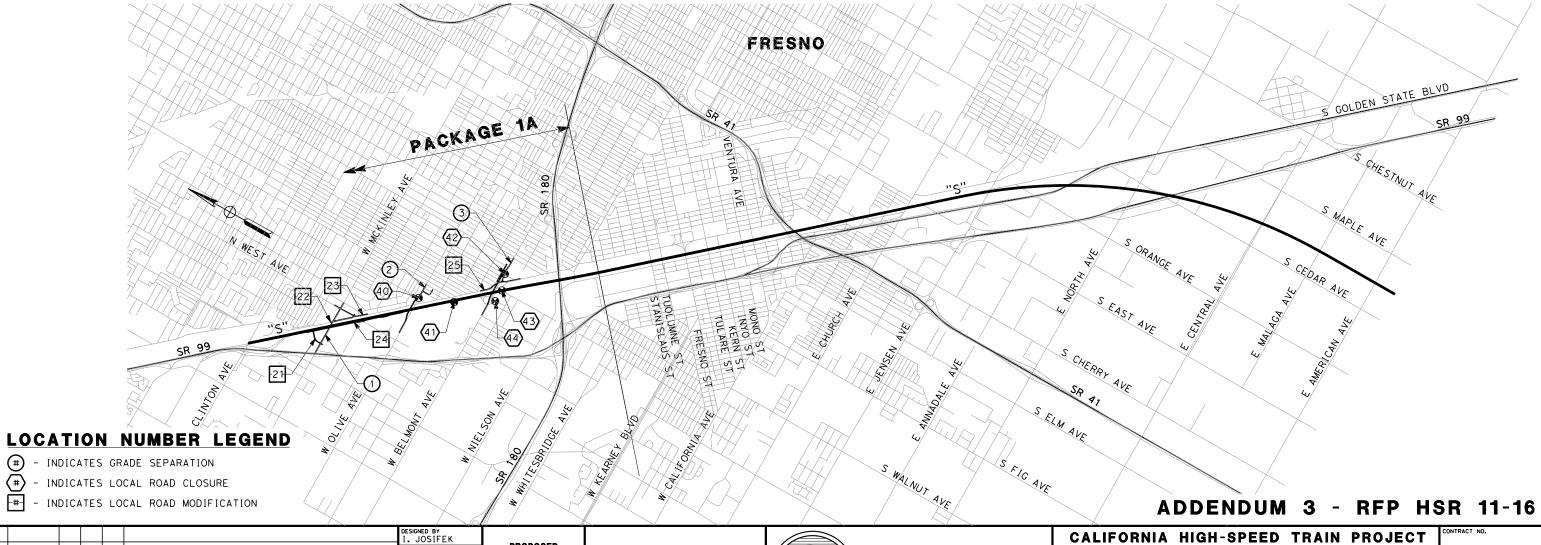
CV-B0003-A3

NO SCALE

LOCATION NO.	SHEET TITLE	ROADWAY DWG NO.	STRUCTURE DWG NO.
1	GRADE SEPARATION - MCKINLEY AVE	CV-T1001 - CV-T1003	ST-K1026
2	GRADE SEPARATION - OLIVE AVE	CV-T1007 - CV-T1008	ST-K1027
3	GRADE SEPARATION - BELMONT AVE	CV-T1009 - CV-T1010	ST-K1028 - ST-K1029
-21	LOCAL ROAD - MCKINLEY AVE CONNECTOR	CV-T1001 & CV-T1003	-
22	LOCAL ROAD - N WEBER AVE	CV-T1001 & CV-T1003	-
23	LOCAL ROAD - N WEST AVE	CV-T1004	-
-24	LOCAL ROAD - N GOLDEN STATE BLVD	CV-T1001 - CV-T1006	-
25	LOCAL ROAD - N H ST / N WEBER AVE	CV-T1009 & CV-T1011	-

LOCATION NO.	SHEET TITLE	ROADWAY DWG NO.	STRUCTURE DWG NO.
40	ROAD CLOSURE - N DELNO AVE	CV-T1007	-
41)	ROAD CLOSURE - N GOLDEN STATE BLVD	CV-T1004 - CV-T1007	-
42	ROAD CLOSURE - N HARRISON AVE	CV-T1009	-
43	ROAD CLOSURE - N THORNE AVE	CV-T1009	-
44	ROAD CLOSURE - N WESLEY AVE	CV-T1009 & CV-T1011	-

SIERRA SUBDIVISION
PACKAGE 1A
ROADWAY
INDEX MAP



CALIFORNIA HIGH-SPEED RAIL AUTHORITY

PROPOSED PRELIMINARY

DESIGN

NOT FOR

CONSTRUCTION

URS HMM ARUP

DRAWN BY V. ORTEGA

CHECKED BY B. RAWSON

06/12/12

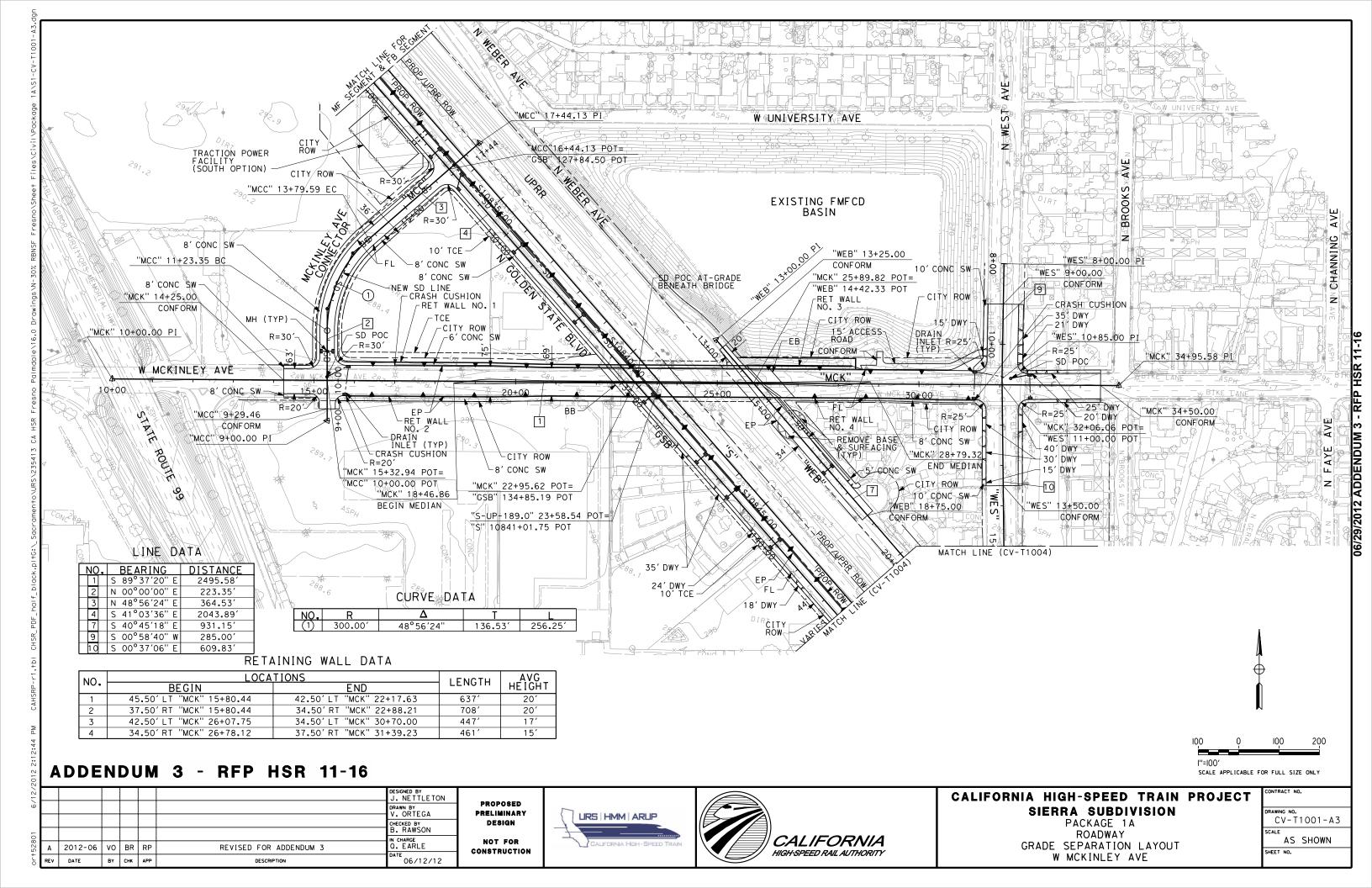
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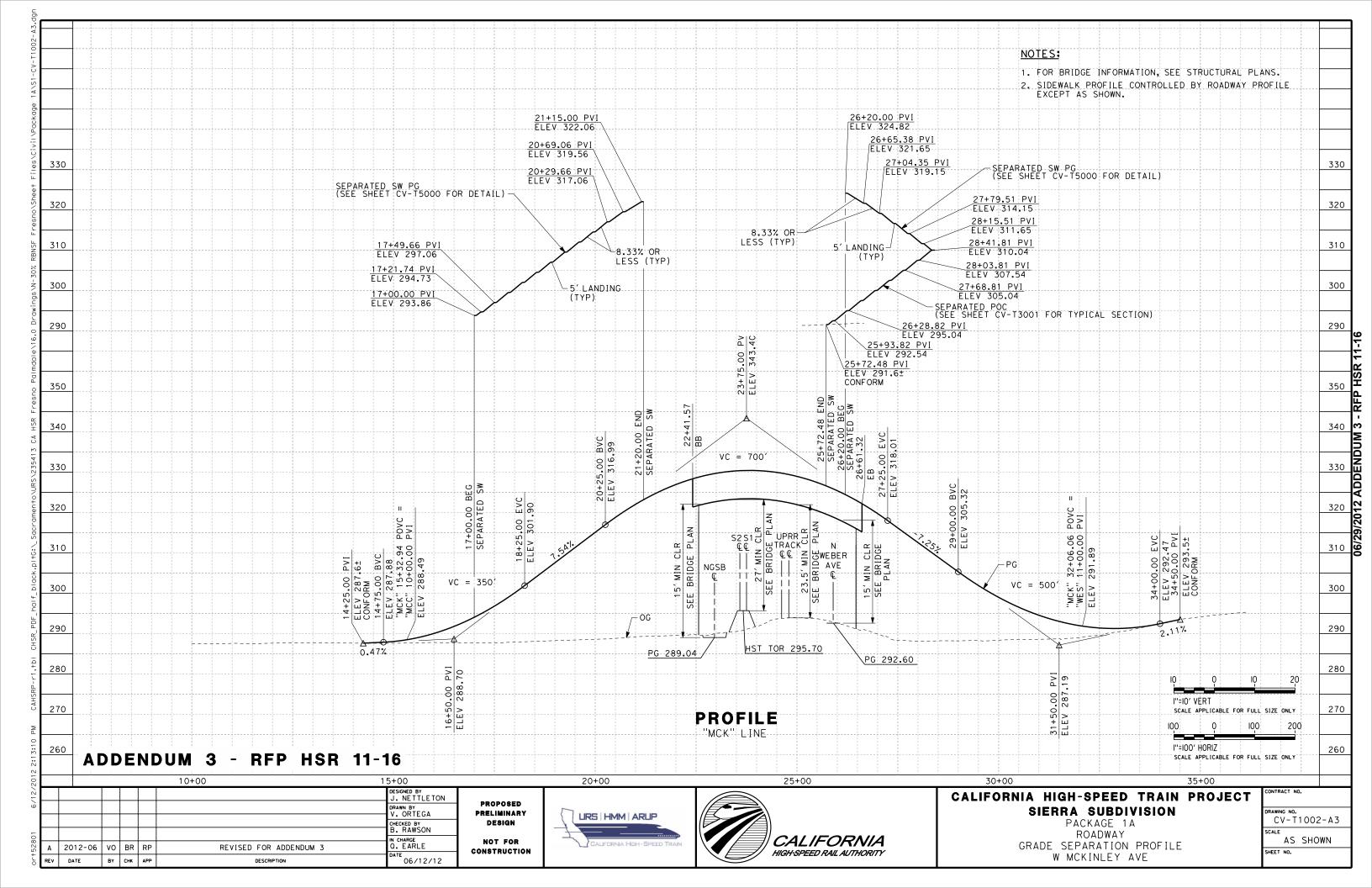
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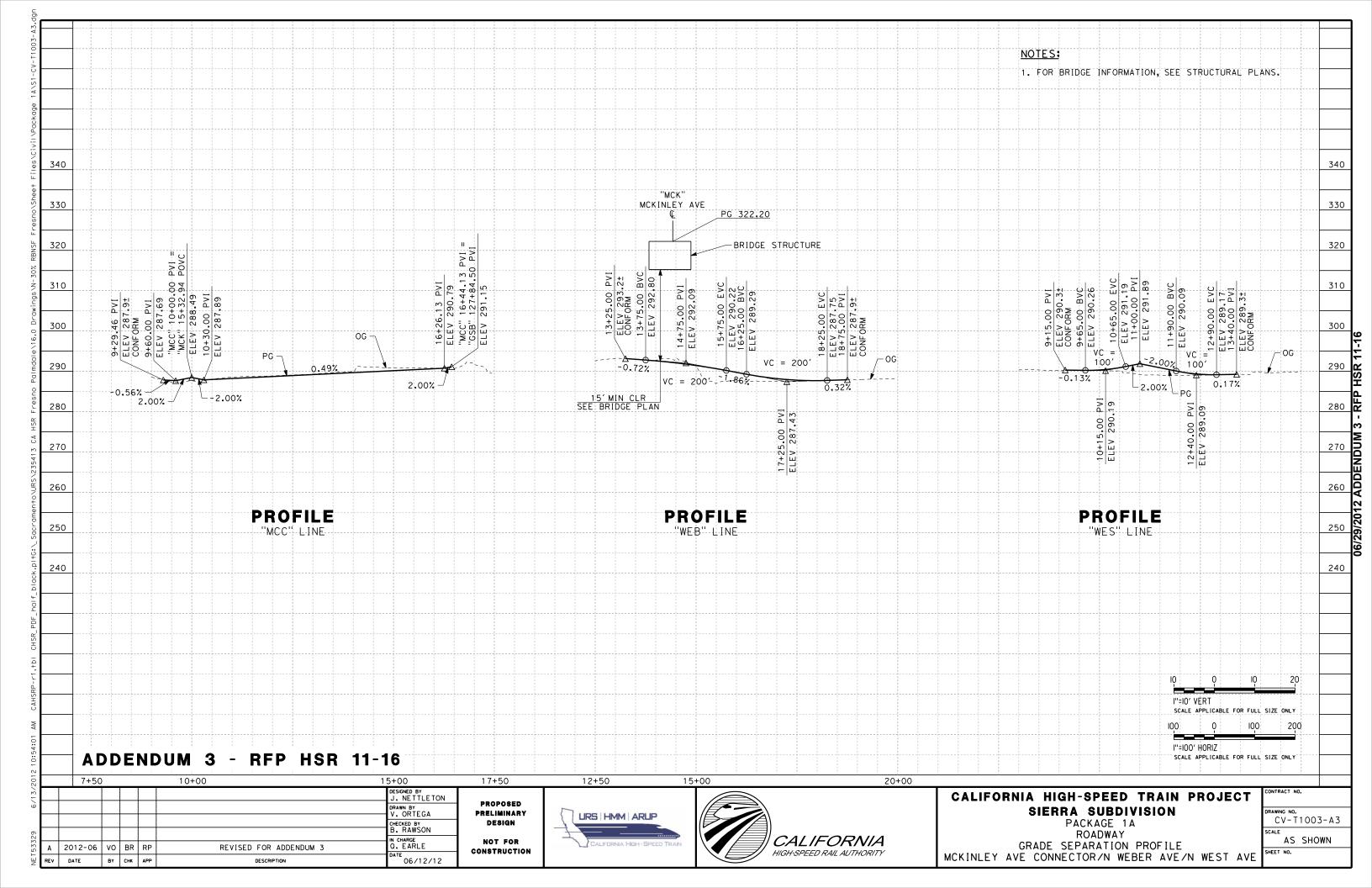
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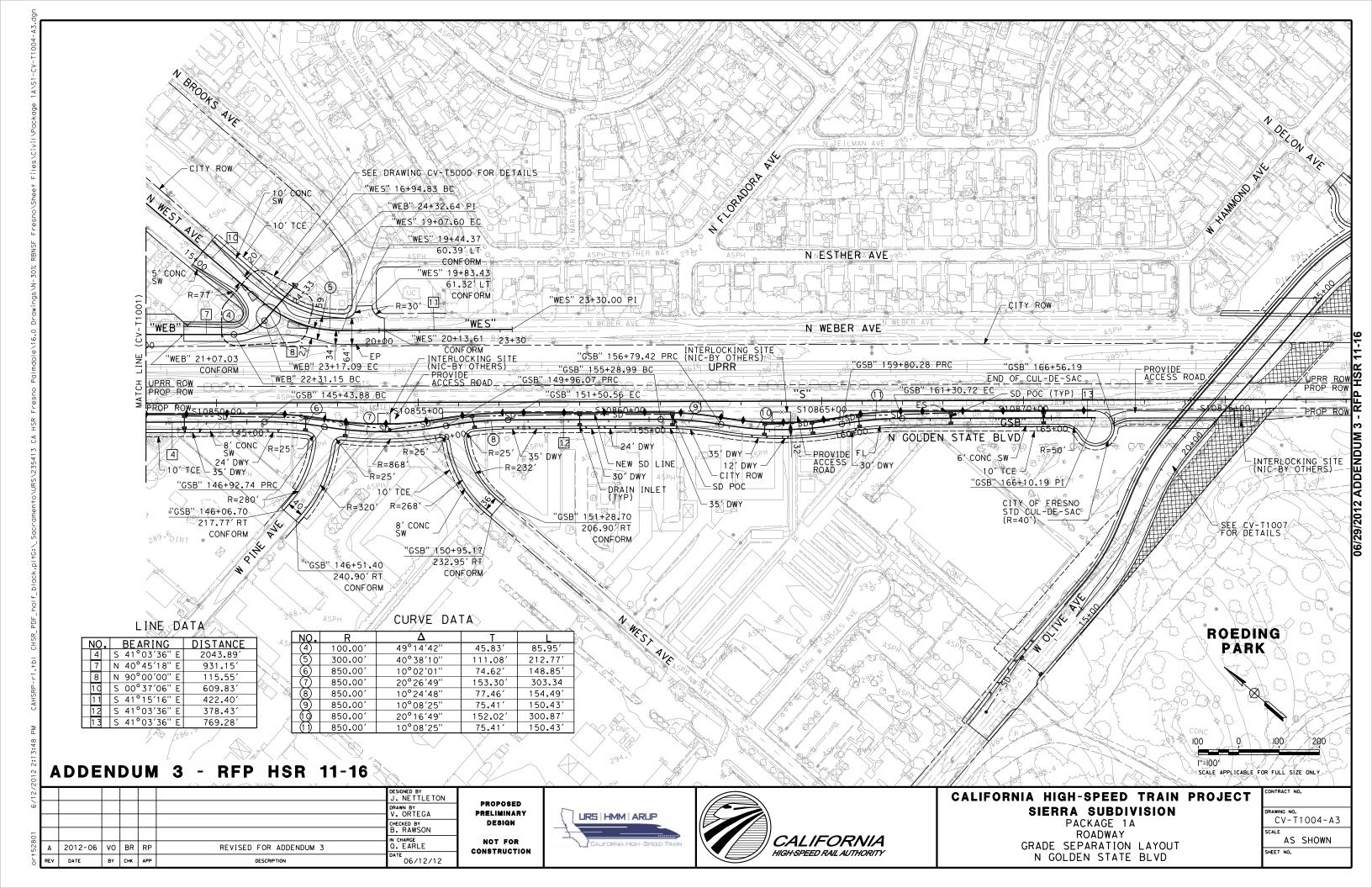
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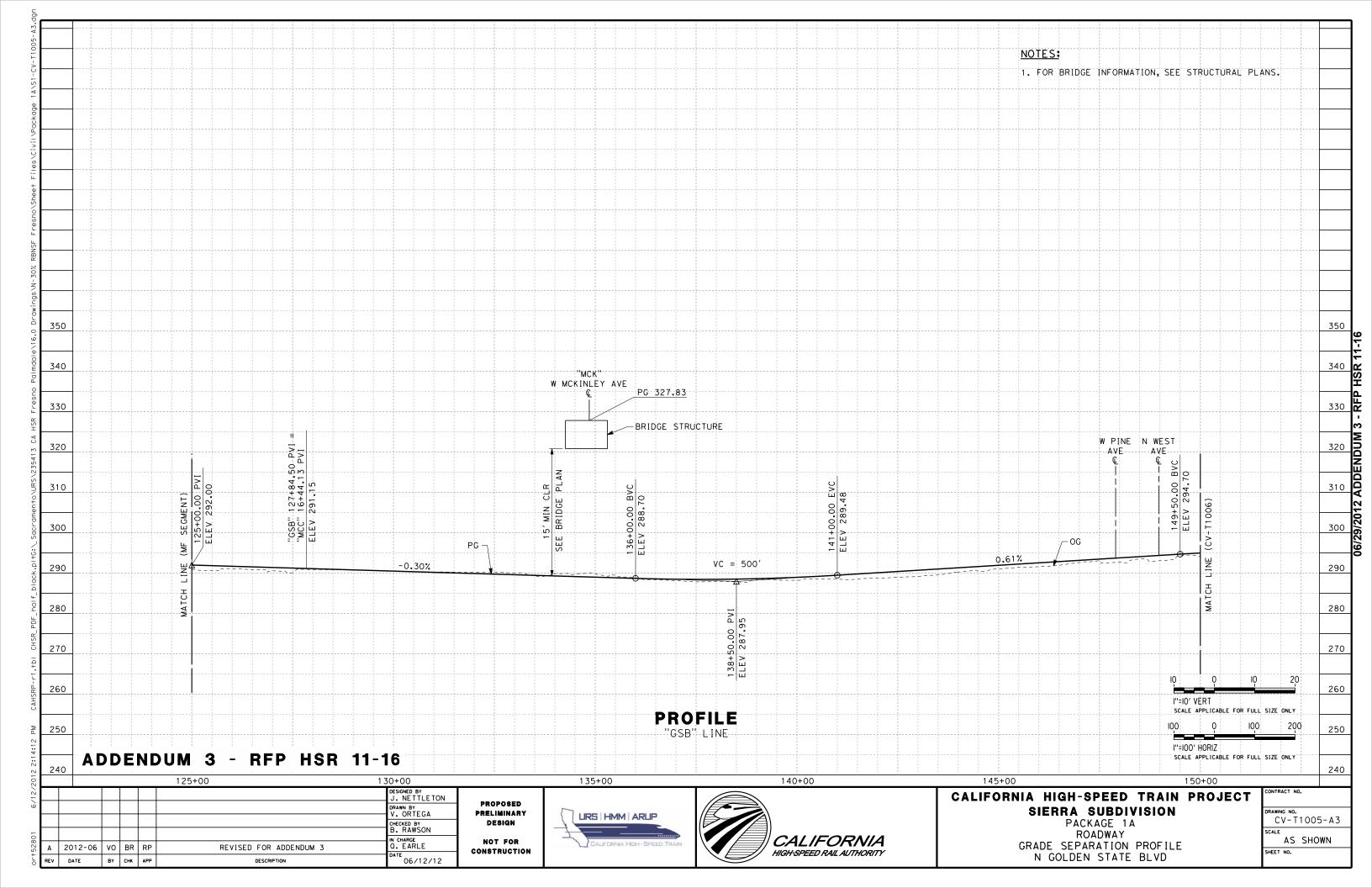
BY CHK APP

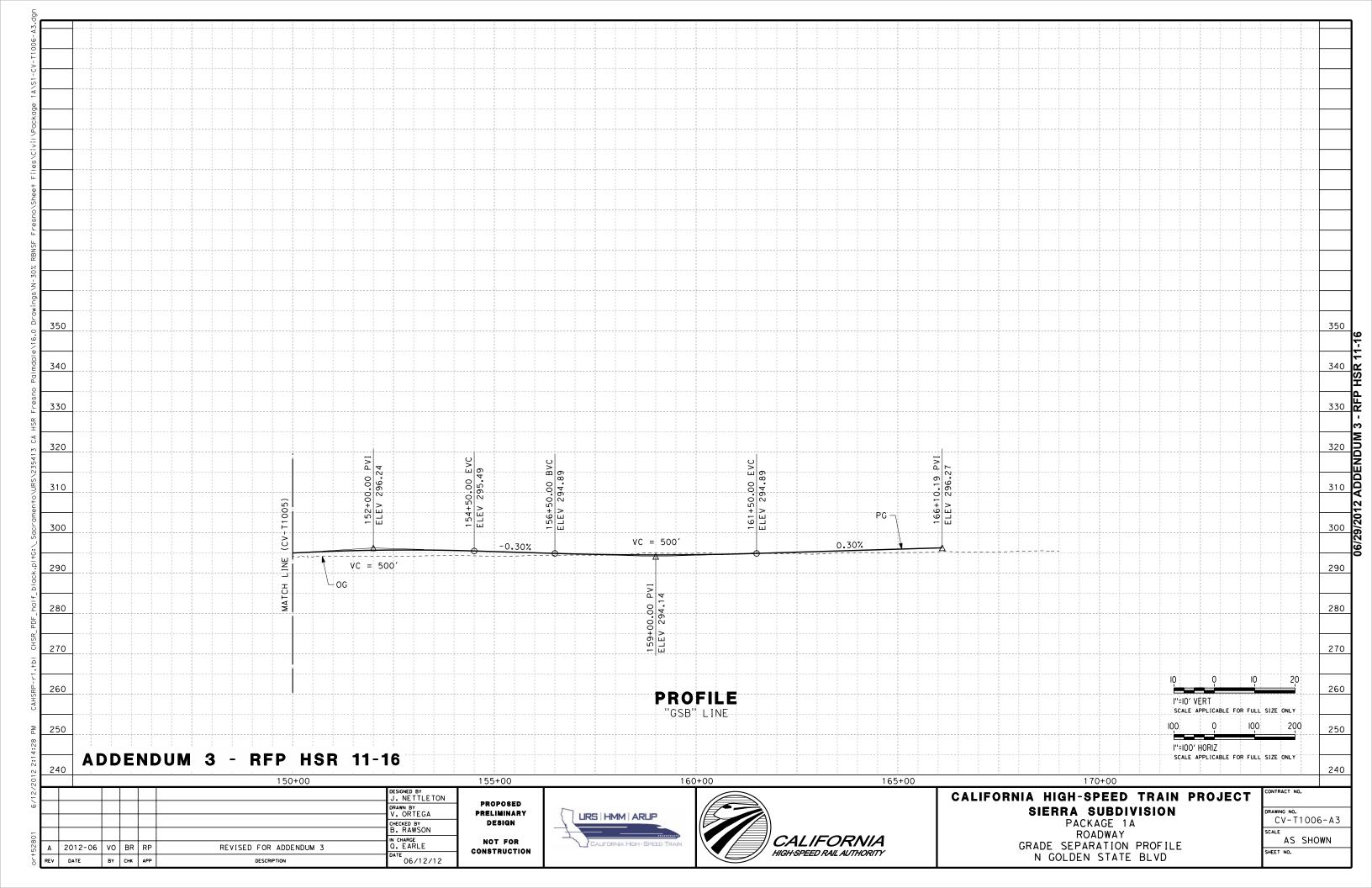


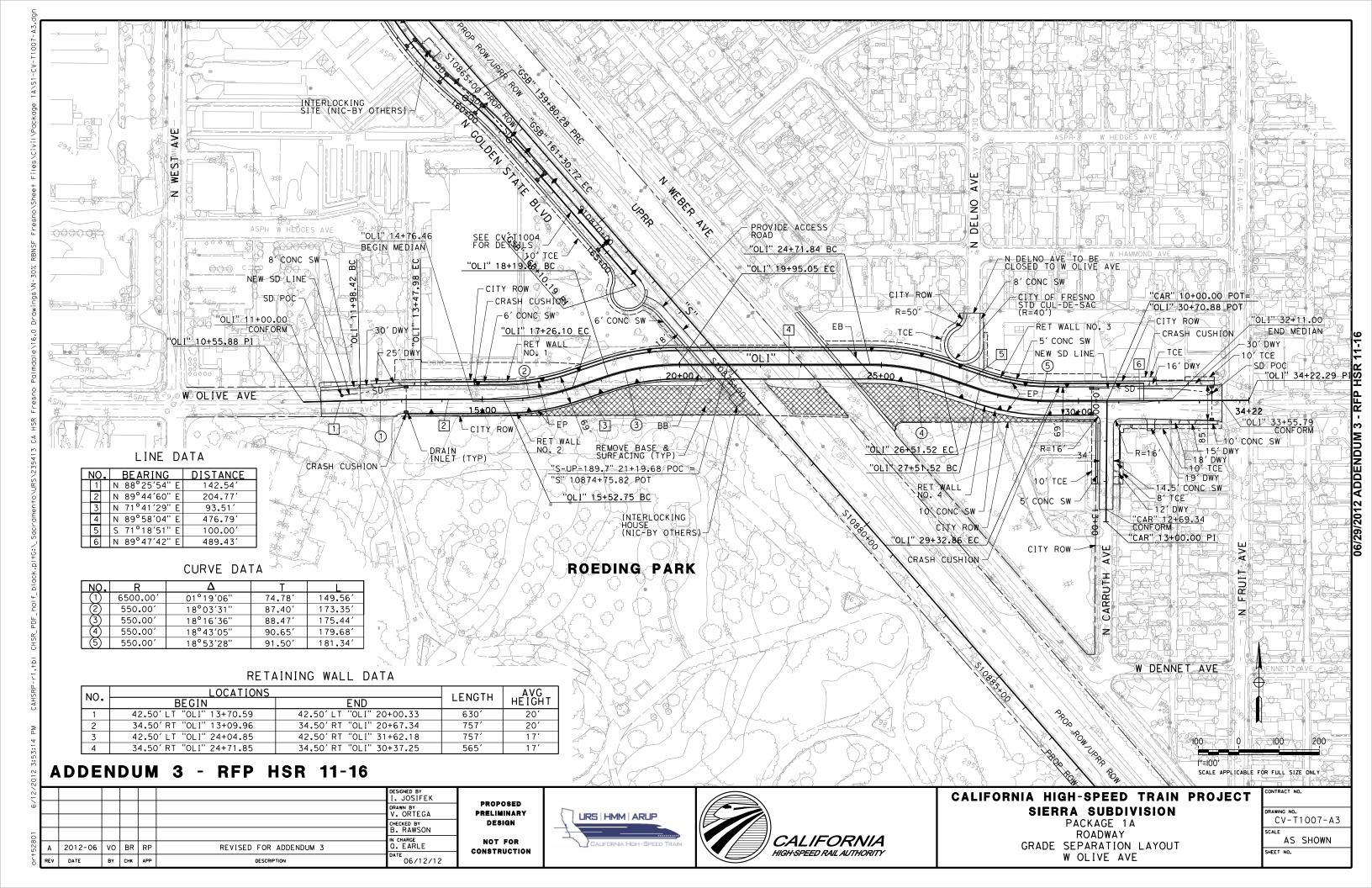


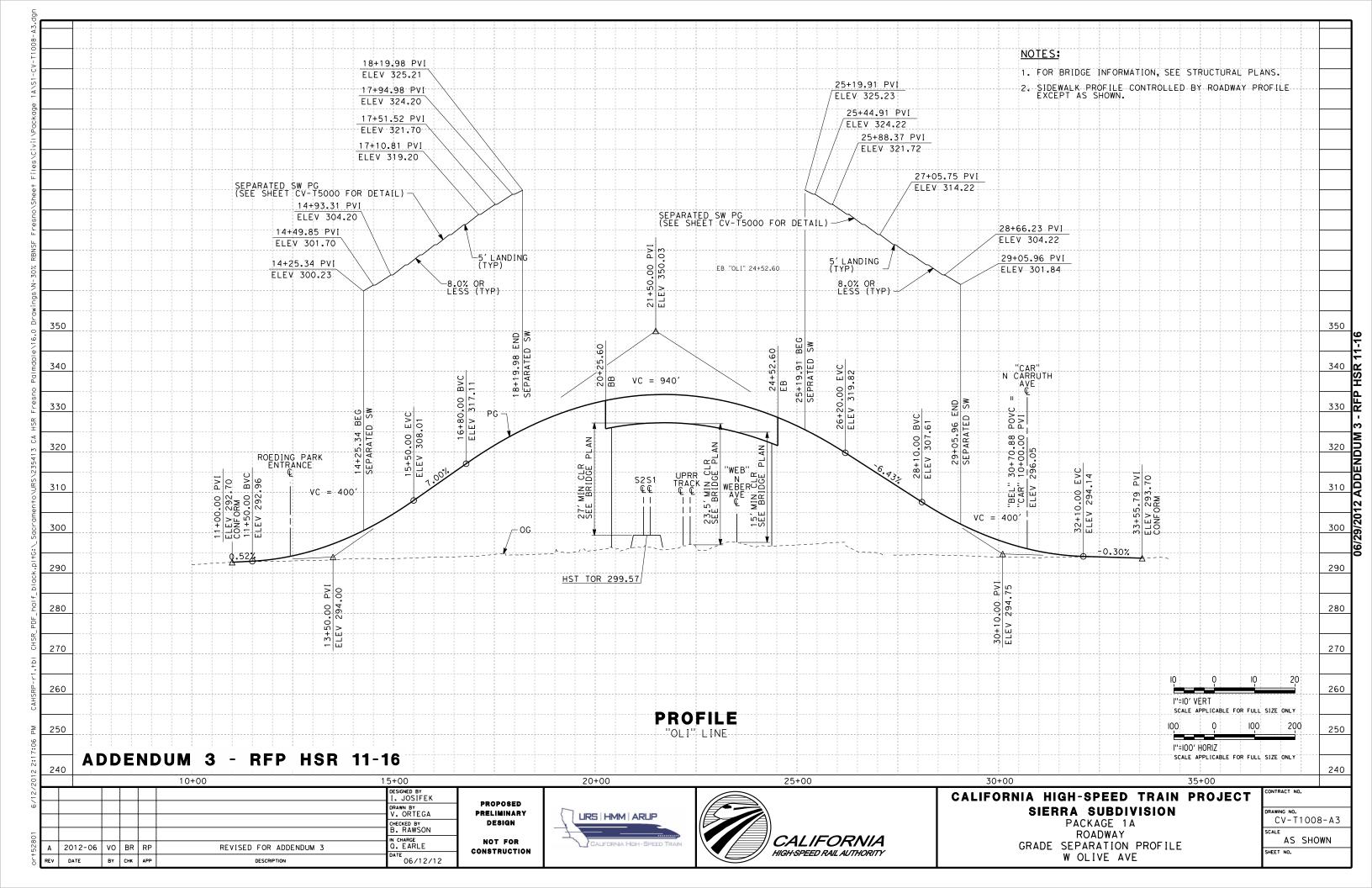


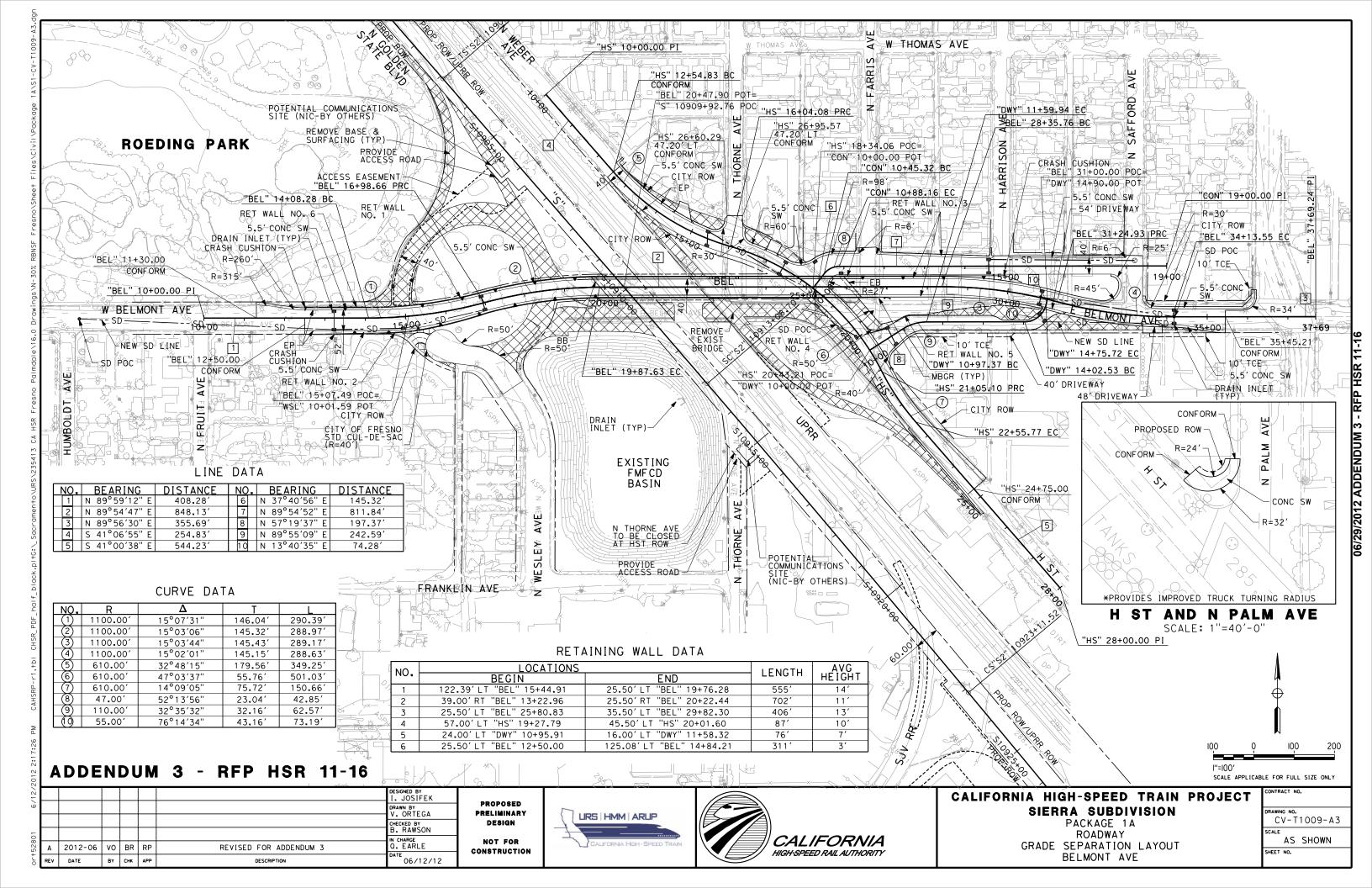


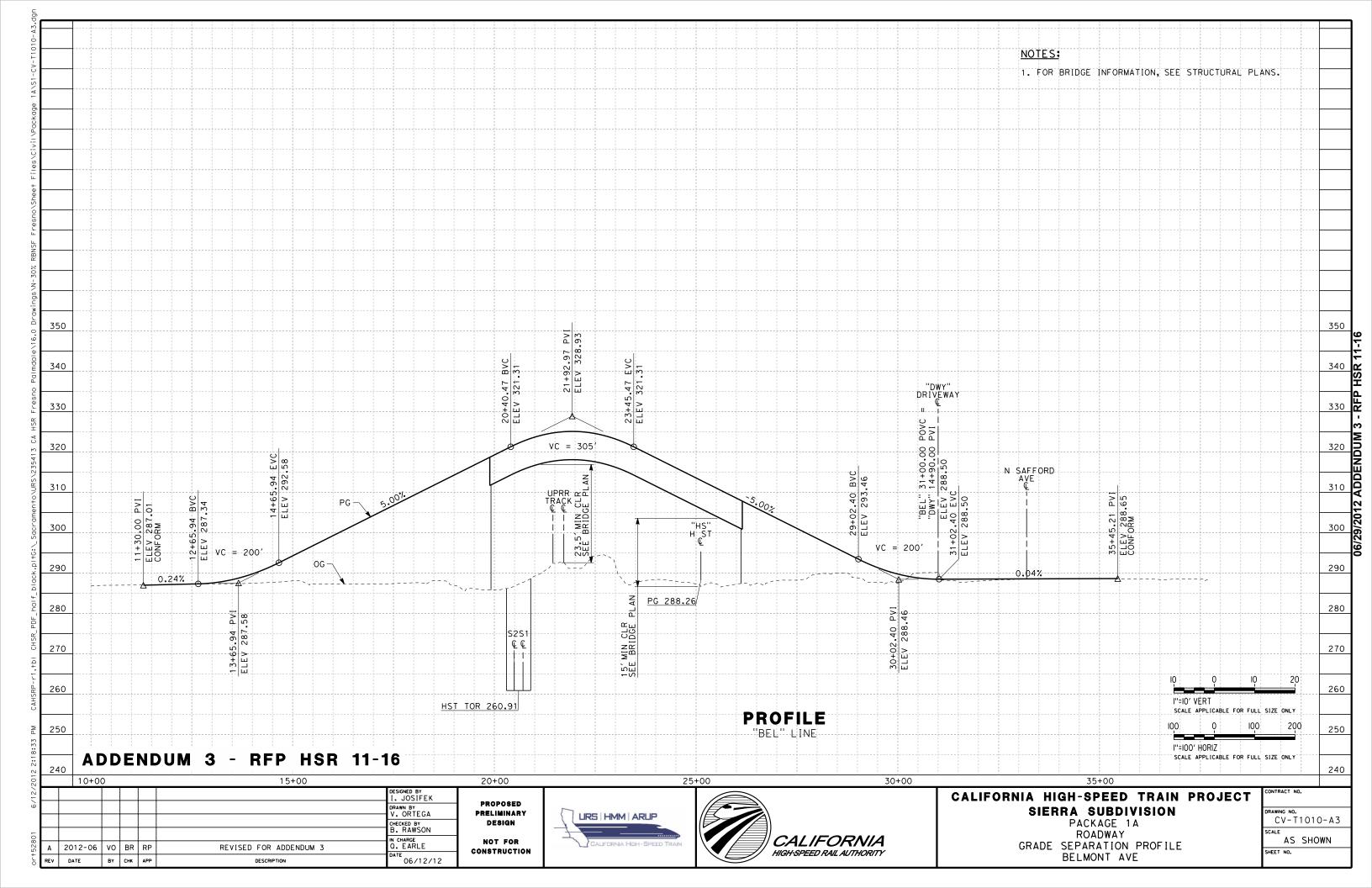


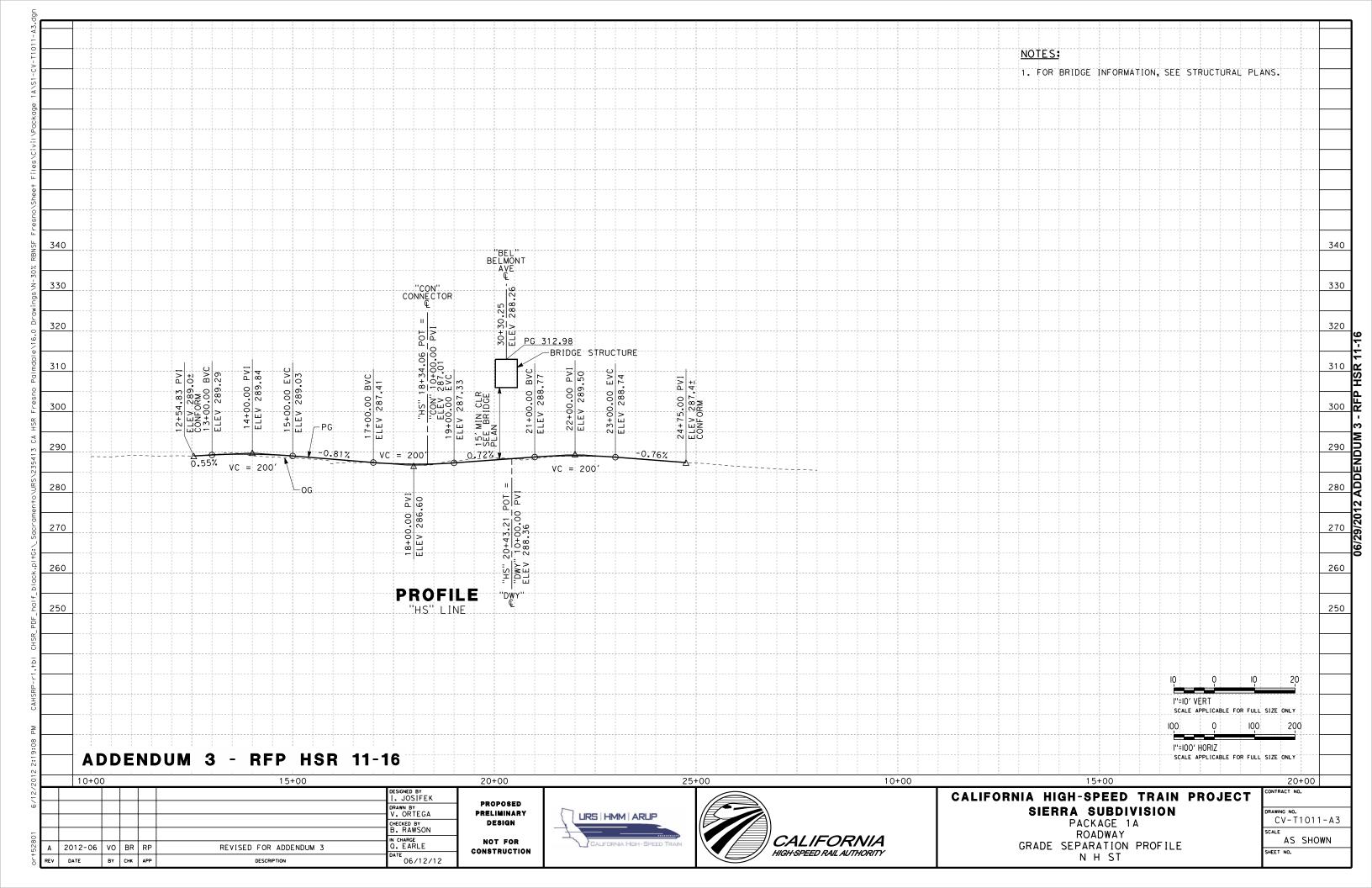


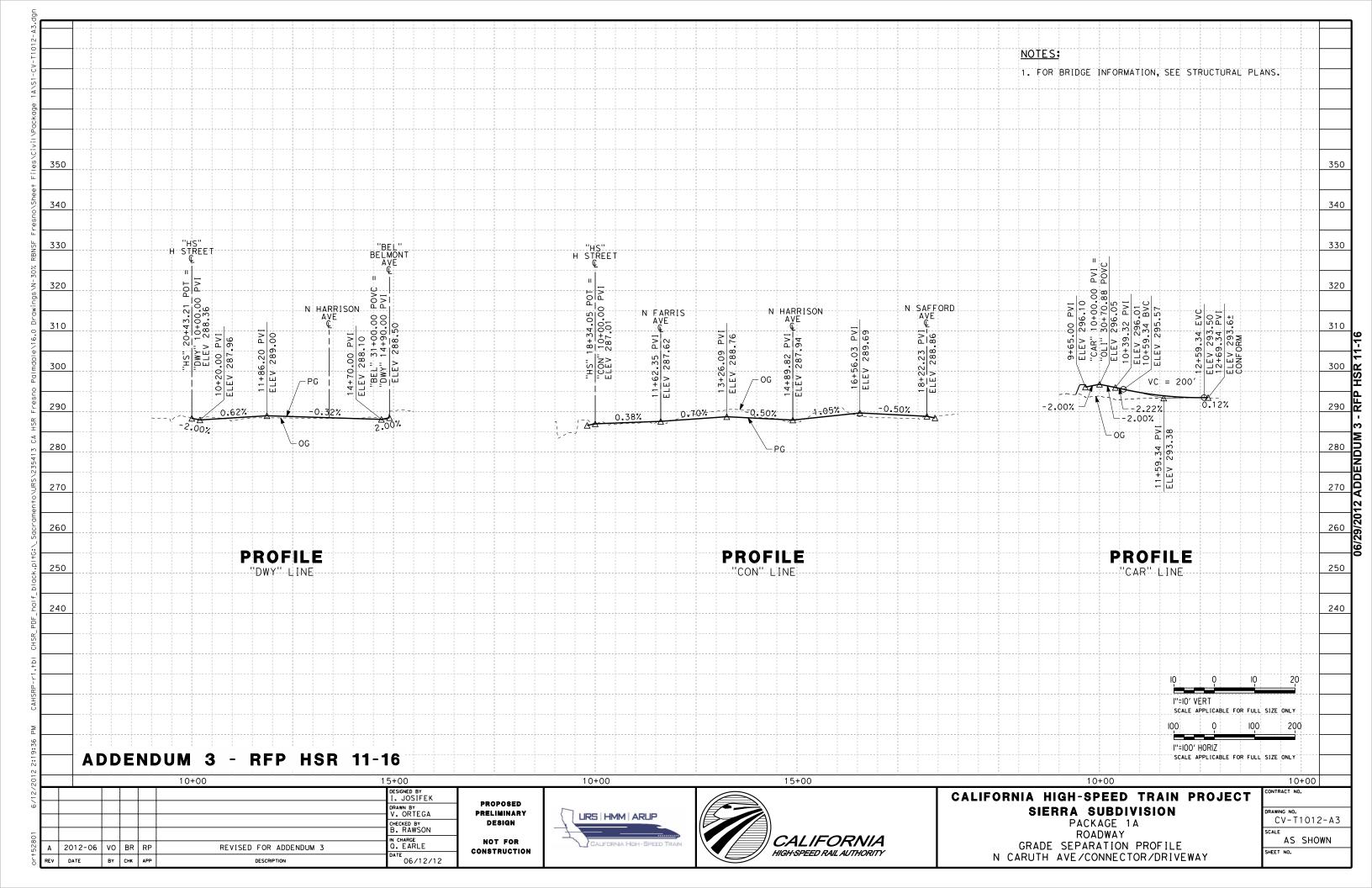


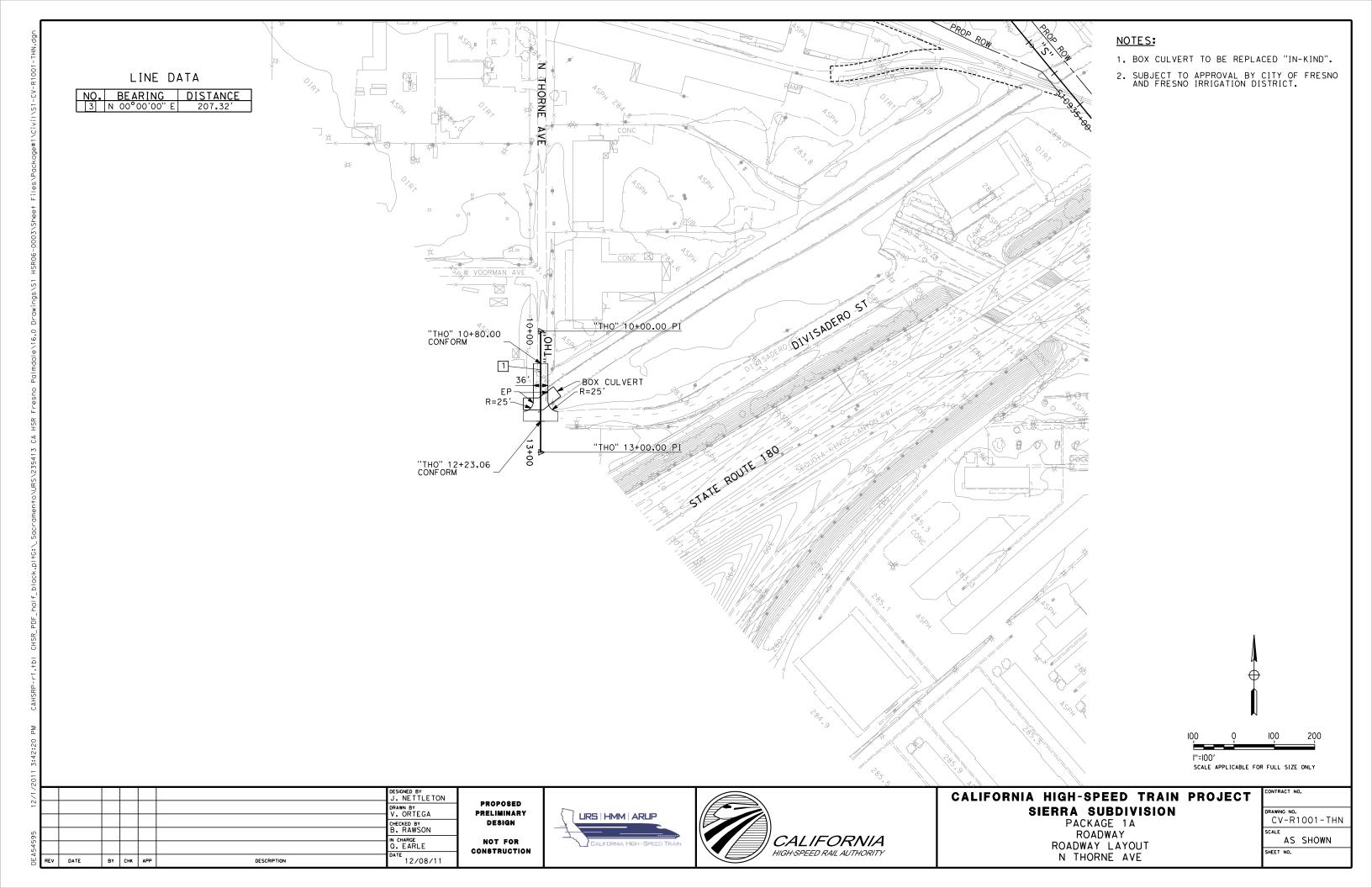


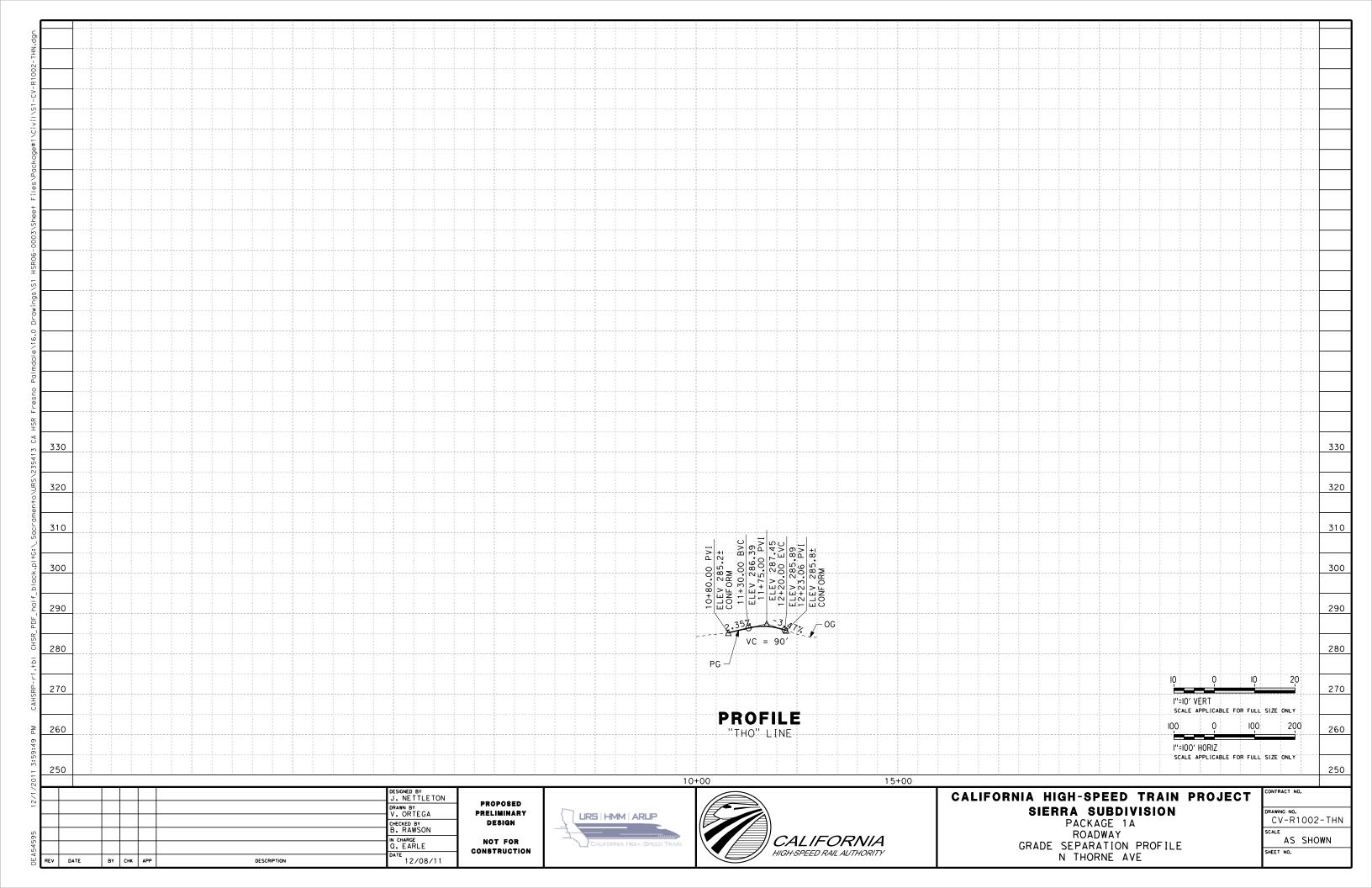












"MCK" 18+23.08 TO 22+41.57 "MCK" 26+61.32 TO 28+13.80

VAR 10.5′-15′ RET WALL (TYP) ACCESS ROAD "MCK" 28+13.80 TO 30+70.00 6′ SW CONC BARRIER (TYPE 60C) (TYP) "MCK" 17+00.00 TO 18+23.08 RET WALL (TYP) 8' 7' SW SHLD SEE DRAWING CV-T5000— FOR DETAIL CONC BARRIER— (TYPE 736A) (TYP) CONC BARRIER (TYPE 736A) "MCK" 15+73.32 TO 17+00.00 RET WALL (TYP) "MCK" 15+73.32 TO 18+23.08 "MCK" 28+13.80 TO 34+25.00 RET WALL - FL VAR VAR VAR VAR VAR VAR 11'-13' LANE 0-7' SHLD TURN LANE O-7' SHLD LANE LANE LANE ______ -C&G (TYP) W MCKINLEY AVE "MCK" 14+25.00 TO 18+23.08 "MCK" 28+13.80 TO 34+25.00*

NOTES:

- 1. SUPERELEVATION AS SHOWN OR AS DIRECTED BY THE ENGINEER.
- 2. PAVEMENT SECTIONS ARE SHOWN FOR INFORMATIONAL PURPOSES ONLY.

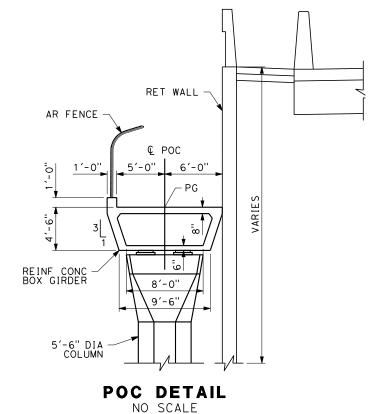
DESIGN SPEED TABLE

STREET NAME	ALIGNMENT DESIGNATION	DESIGN SPEED (MPH)
W MCKINLEY AVE	MCK	35
W OLIVE AVE	OLI	35
BELMONT AVE	BEL	30
MCKINLEY AVE CONNECTOR	MCC	30
N GOLDEN STATE BLVD	GSB	50
N WEBER AVE	WEB	40
N H ST	HS	35
DRIVEWAY	DWY	<20
N WEST AVE	WES	30
N CARUTH AVE	CAR	30
CONNECTOR	CON	30

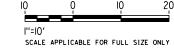
STRUCTURAL SECTIONS TYPICAL ROADWAY

1-0.50' HMA (TYPE A) 1.50' CL2 AB

2-[0.35' PCC 0.35' CL2 AB



*PROVIDE E.B. RIGHT TURN LANE FROM STA 30+50.00 TO 31+39.23



ADDENDUM 3 - RFP HSR 11-16

						DESIGNED BY I. JOSIFEK	PROPOSED
						DRAWN BY V. ORTEGA	PROPOSED PRELIMINARY
						CHECKED BY B. RAWSON	DESIGN
Α	2012-06	VO	BR	RP	REVISED FOR ADDENDUM 3	IN CHARGE Q. EARLE	NOT FOR CONSTRUCTION
REV	DATE	ВΥ	СНК	APP	DESCRIPTION	DATE 06/12/12	CONSTRUCTION

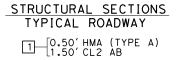




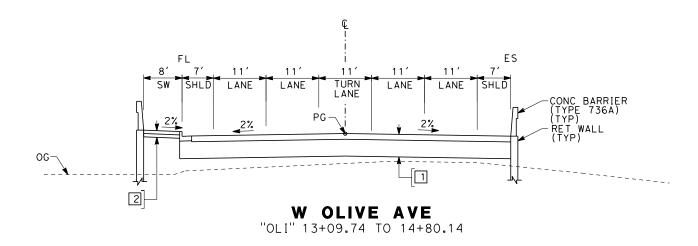
CALIFORNIA HIGH-SPEED TRAIN PROJEC SIERRA SUBDIVISION

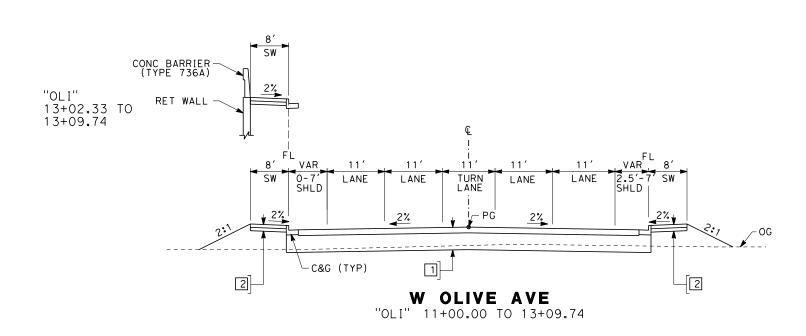
PACKAGE 1A ROADWAY ROADWAY / GRADE SEPARATION TYPICAL SECTIONS

Т	CONTRACT NO.
	DRAWING NO. CV-T3001-A3
	SCALE AS SHOWN
	SHEET NO.



2-0.35' PCC 0.35' CL2 AB





ADDENDUM 3 - RFP HSR 11-16

						DESIGNED BY	
						DRAWN BY	
						CHECKED BY	1
						B. RAWSON	
			-			IN CHARGE	
Α	2012-06	vo	BR	RP	REVISED FOR ADDENDUM 3	Q. EARLE	۱ ،
REV	DATE	BY	СНК	APP	DESCRIPTION	DATE 06/12/12	L

PROPOSED
PRELIMINARY
DESIGN
NOT FOR
CONSTRUCTION

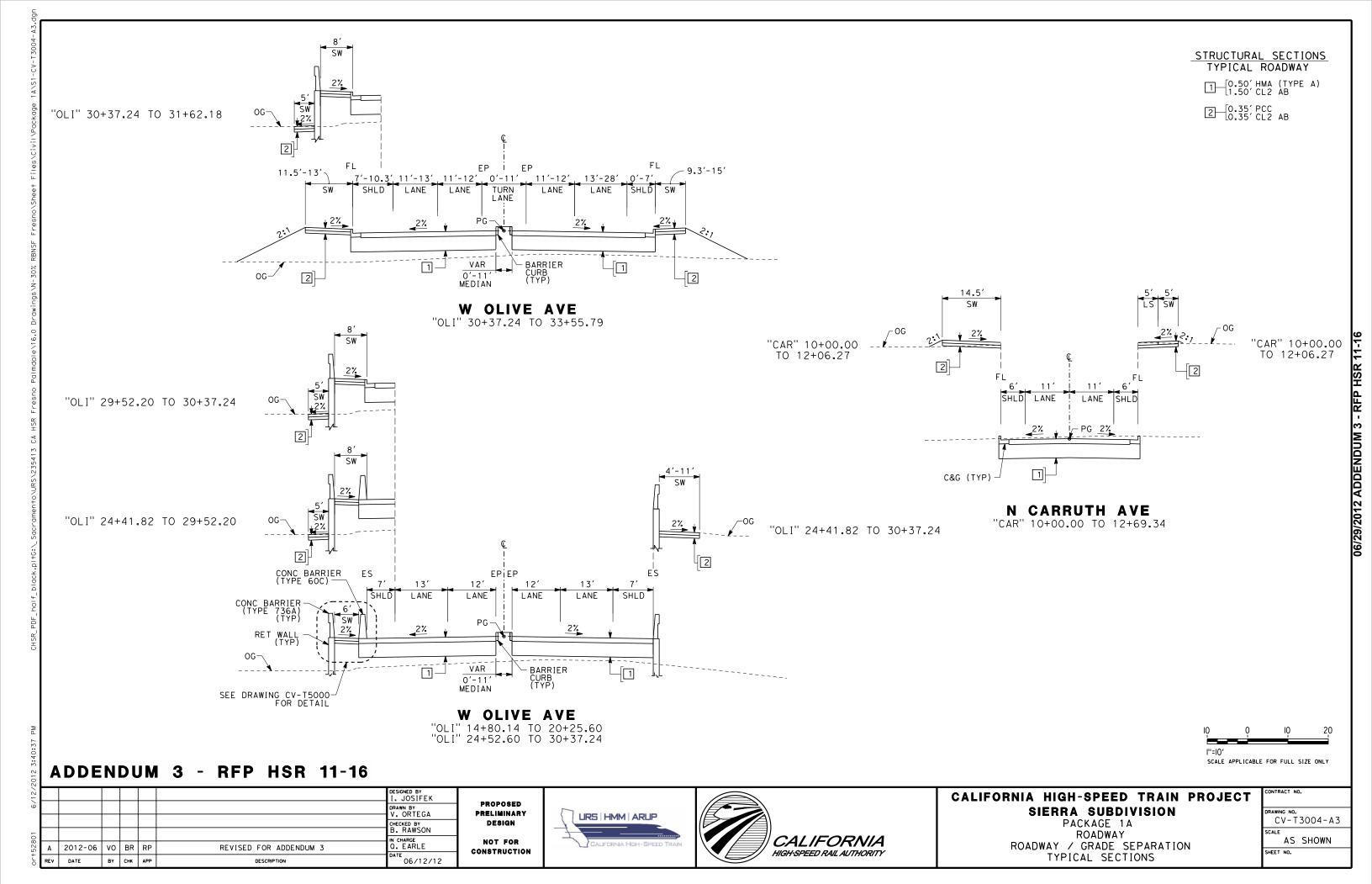




CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

SIERRA SUBDIVISION
PACKAGE 1A
ROADWAY
ROADWAY / GRADE SEPARATION
TYPICAL SECTIONS

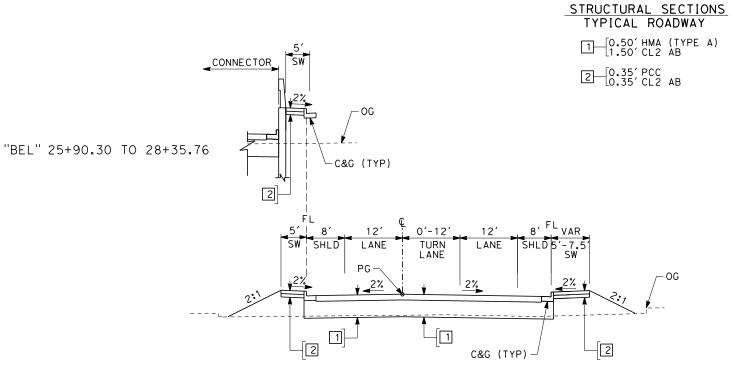
T	CONTRACT NO.
	DRAWING NO. CV-T3003-A3
	SCALE AS SHOWN
	SHEET NO.



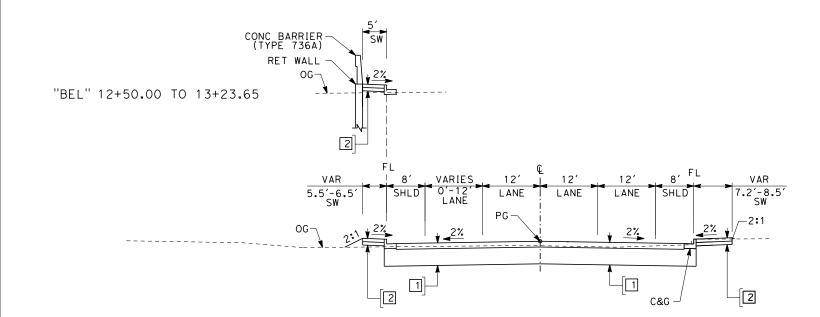
CONSTRUCTION

URS HMM ARUP





BELMONT AVE "BEL" 26+14.64 TO 35+50.00



12'

LANE

-[2

12′

LANE

BELMONT AVE

"BEL" 13+23.65 TO 19+88.63

0'-12'

LANE

BELMONT AVE

CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A ROADWAY ROADWAY / GRADE SEPARATION TYPICAL SECTIONS

T	CONTRACT NO.
	DRAWING NO. CV-T3005-A3
	SCALE AS SHOWN
	SHEET NO.

SCALE APPLICABLE FOR FULL SIZE ONLY

ADDENDUM 3 - RFP HSR 11-16 "BEL" 11+30.00 TO 13+23.65

REVISED FOR ADDENDUM 3

DESCRIPTION

CONC BARRIER (TYPE 736A)

RET WALL

DESIGNED BY
I. JOSIFEK **PROPOSED** DRAWN BY V. ORTEGA PRELIMINARY CHECKED BY
B. RAWSON IN CHARGE Q. EARLE NOT FOR

06/12/12

-CONC BARRIER (TYPE 736A)

RET WALL

2

-C&G (TYP)

DESCRIPTION

PROPOSED
PRELIMINARY
DESIGN
NOT FOR
CONSTRUCTION

06/12/12

8'-10.5' 12'-19' I

LANE

1

12′

LANE

H ST"HS" 12+54.83 TO 24+75.00

LANE

[2]

-C&G (TYP)

SHLD

SHLD

SW

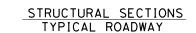
2

'HS" 22+83.75 TO 24+75.00

LIRS | HMM | ARLIP

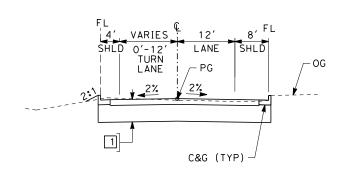
"HS" 22+55.77 TO 24+75.00



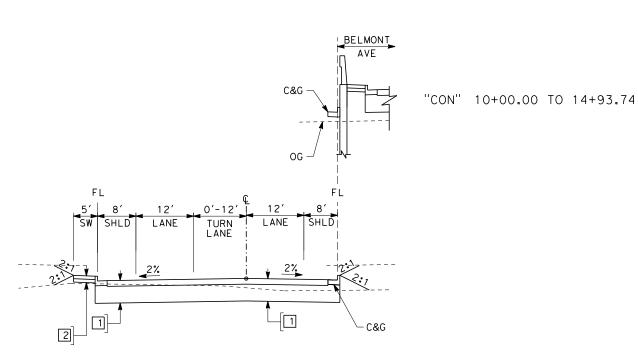


1-0.50' HMA (TYPE A) 1.50' CL2 AB

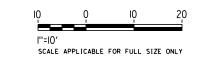
2-0.35' PCC 0.35' CL2 AB



DRIVEWAY"DWY" 10+00.00 TO 14+90.00



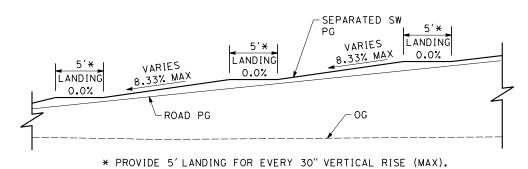
CONNECTOR"CON" 10+00.00 TO 18+20.68



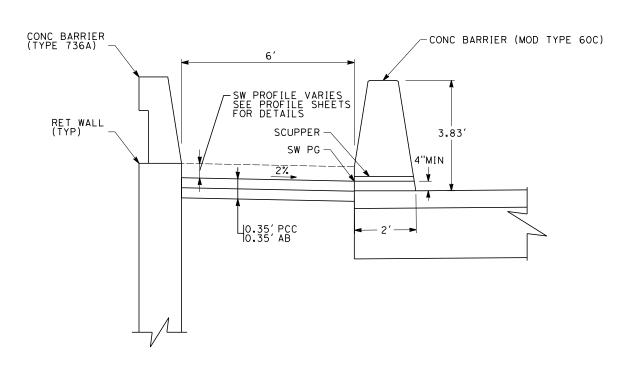
CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A ROADWAY ROADWAY / GRADE SEPARATION TYPICAL SECTIONS

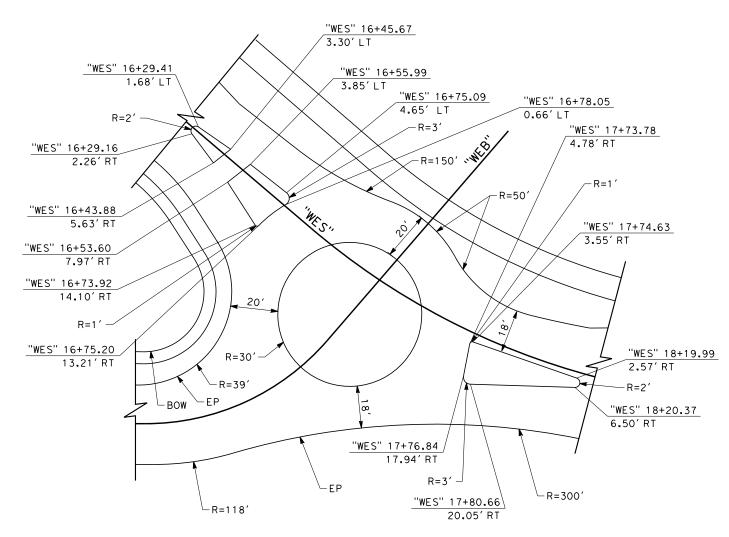
CONTRACT NO.
CV-T3006-A
AS SHOWN
SHEET NO.



SEPARATED SIDEWALK PROFILE DETAIL SCALE: NO SCALE



SEPARATED SIDEWALK SECTION DETAIL SCALE: NO SCALE



N WEST AVE TRAFFIC CIRCLE SCALE: 1"=20'-0"

2								
, ,							DESIGNED BY J. NETTLETON	
7							DRAWN BY V. ORTEGA	PROPOSED Preliminary
,							CHECKED BY B. RAWSON	DESIGN
							IN CHARGE Q. EARLE	NOT FOR CONSTRUCTION
וֹל	REV	DATE	BY	СНК	APP	DESCRIPTION	12/08/11	



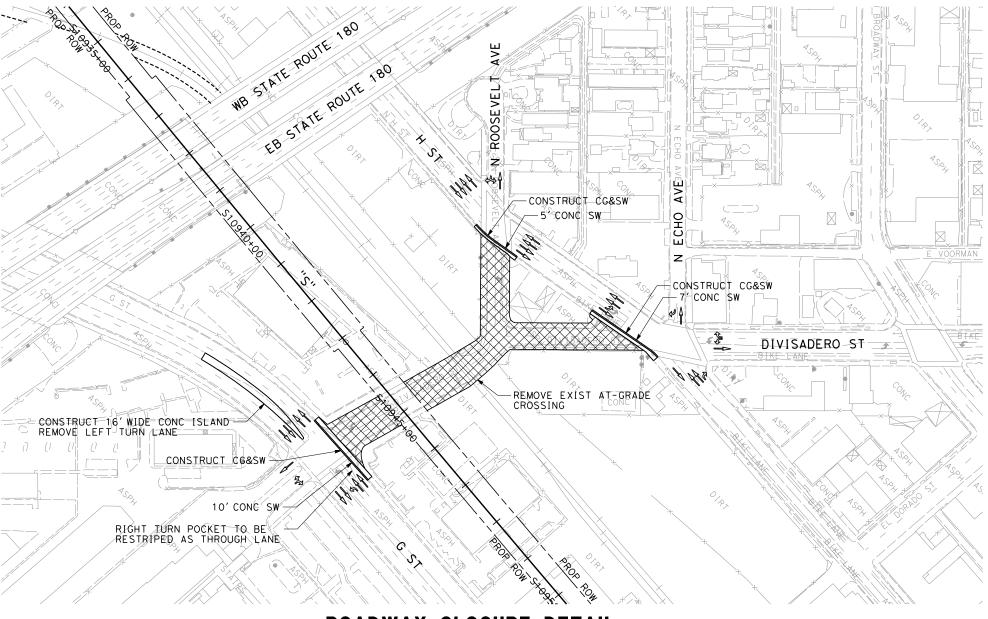


CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A ROADWAY CONSTRUCTION DETAILS

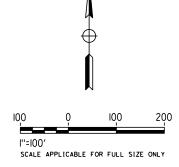
CONTR	ACT NO).	
DRAWIN	IG NO.		
	CV-	-T5000	
SCALE			
	ΑS	SHOWN	
SHEET	NO.		

REMOVE BASE, SURFACING, & MINOR CONCRETE



ROADWAY CLOSURE DETAIL

DIVISADERO ST



REV	DATE	ВΥ	СНК	APP	DESCRIPTION	0. EARLE DATE 12/08/11	co
3						B. RAWSON IN CHARGE	
						CHECKED BY	
L						DRAWN BY J. BINNING	Pi
						DESIGNED BY J. BINNING	
í							

PROPOSED PRELIMINARY DESIGN NOT FOR CONSTRUCTION





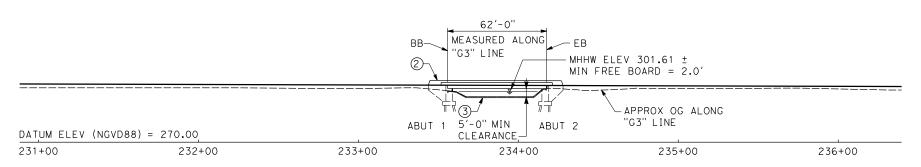
CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A ROADWAY ROADWAY CLOSURE DETAILS

CONTRACT NO.
DRAWING NO.
CV-T5001
CV-13001
SCALE
AS SHOWN

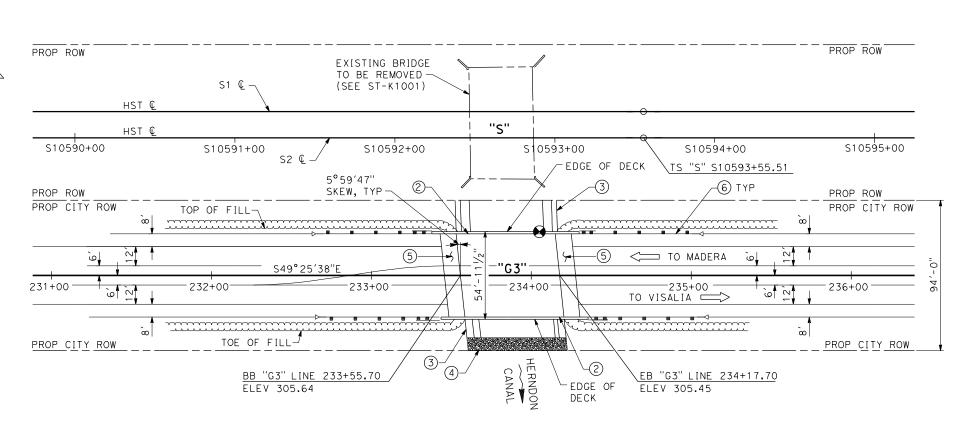
PROFILE GRADE

NO SCALE



ELEVATION

SCALE: 1"=30'-0"



PLAN SCALE: 1"=30'-0"

AECOM 2020 L Street, Suite 300 Sacramento, CA 958II **CH2MHILL**



- 1. ROW SHOWN REPRESENTS THE MINIMUM ANTICIPATED ROW REQUIREMENTS. ACCURATE ROW AND ACCESS DATA WILL BE MADE AVAILABLE PRIOR TO NOTICE TO PROCEED.
- 2 CONCRETE BARRIER (TYPE 736 MODIFIED)
- 3 CONCRETE CHANNEL LINING
- 4 ROCK SLOPE PROTECTION
- (5) STRUCTURE APPROACH SLAB, TYPE EQ(10)
- 6 METAL BEAM GUARD RAILING
- 7 14" DIA WATER LINE
- (8) 4" DIA GAS LINE

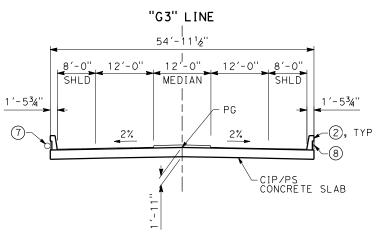
LEGEND:

NOTES:

DIRECTION OF TRAFFIC NEW CONSTRUCTION

EXISTING STRUCTURE

POINT OF MINIMUM VERTICAL CLEARANCE



TYPICAL SECTION SCALE: 1"=10'-0"

CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION PACKAGE 1A

ROADWAY

GOLDEN STATE BLVD BRIDGE AT HERNDON CANAL

GENERAL PLAN

ST-I1001 AS SHOWN SHEET NO.

DATE

BY CHK APP

H. LEE E. CISNEROS T. DUDLEY D. MINISTER 02/01/2012

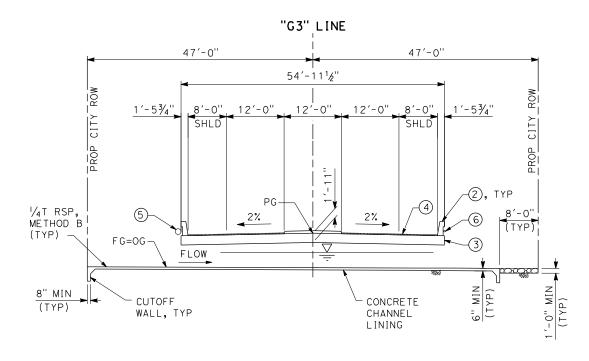
DESCRIPTION

PROPOSED PRELIMINARY DESIGN NOT FOR

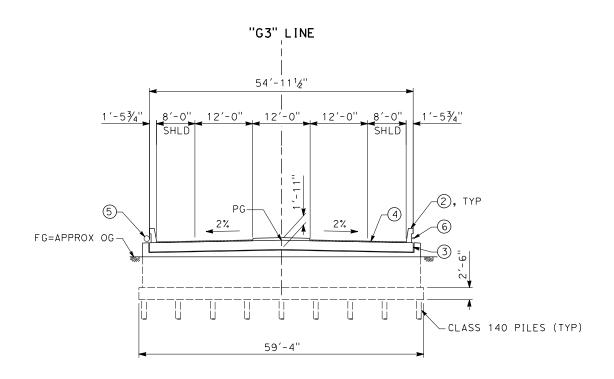
CONSTRUCTION

NOTES:

- 1. ROW SHOWN REPRESENTS THE MINIMUM ANTICIPATED ROW REQUIREMENTS. ACCURATE ROW AND ACCESS DATA WILL BE MADE AVAILABLE PRIOR TO NOTICE TO PROCEED.
- 2 CONCRETE BARRIER (TYPE 736 MODIFIED)
- 3 CIP/PS CONCRETE SLAB
- 4 3/4" POLYESTER CONCRETE OVERLAY
- 5 14" DIA WATER LINE
- 6 4" DIA GAS LINE



SECTION AT MID-SPAN
SCALE: 1"= 10'-0"



SECTION AT ABUTMENT 2

SCALE: 1"= 10'-0"

						DESIGNED BY H. LEE	
						DRAWN BY E. CISNEROS	1
						CHECKED BY	l
						T. DUDLEY	
						D. MINISTER	؍ ا
REV	DATE	ВΥ	СНК	APP	DESCRIPTION	DATE 02/01/2012	١ ١

PROPOSED
PRELIMINARY
DESIGN
NOT FOR
CONSTRUCTION

AECOM Technical Services, Inc. 2020 L Street, Suite 300 Sacromento, CA 95811 CH2MHILL

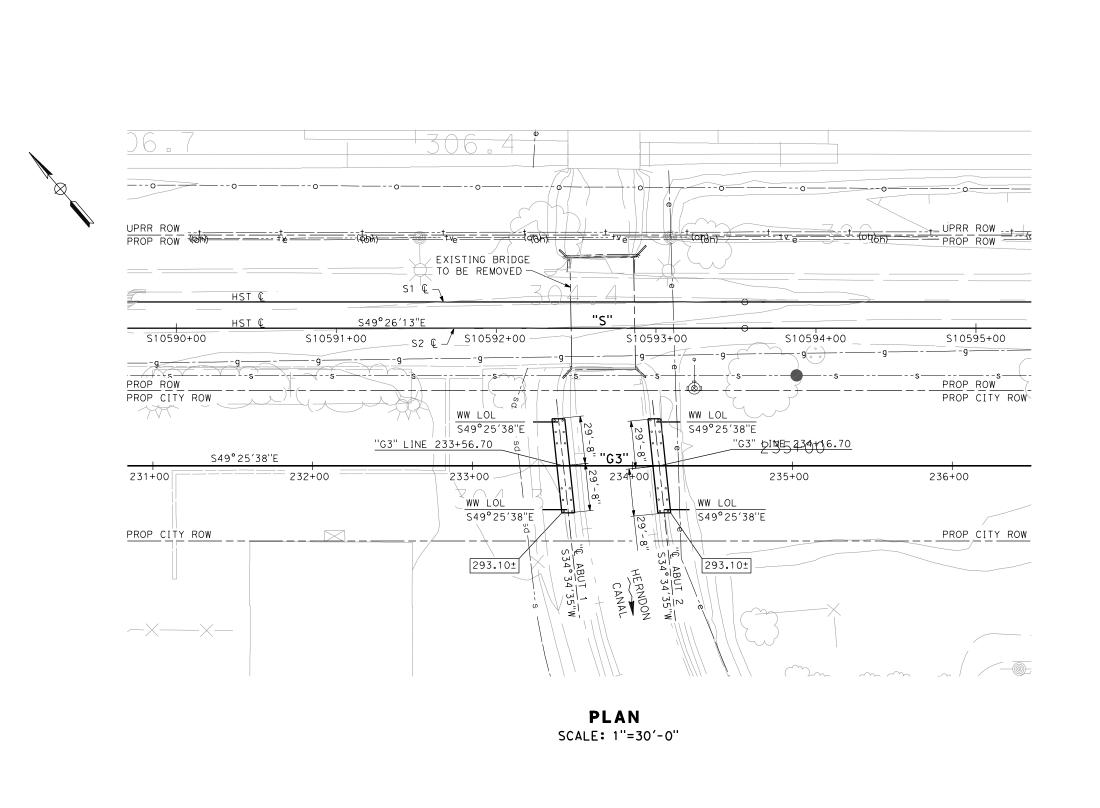


CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A
ROADWAY
GOLDEN STATE BLVD BRIDGE AT HERNDON CANAL
TYPICAL SECTIONS

STATE STATE AS
SHEET NO.

CONTRACT NO.	
DRAWING NO. ST-I1002	
SCALE	
AS SHOWN	



AECOM

2020 L Street, Suite 300 Sacramento, CA 958II

CH2MHILL

CALIFORNIA

HIGH-SPEED RAIL AUTHORITY

H. LEE

REV DATE

BY CHK APP

DESCRIPTION

. CISNEROS

. DUDLEY

N CHARGE D. MINISTER

12/08/11

PROPOSED

PRELIMINARY

DESIGN

NOT FOR

CONSTRUCTION

NOTES:

- 1. ROW SHOWN REPRESENTS THE MINIMUM ANTICIPATED ROW REQUIREMENTS. ACCURATE ROW AND ACCESS DATA WILL BE MADE AVAILABLE PRIOR TO NOTICE TO PROCEED.
- 2. EXISTING BRIDGE AND UTILITY ARE APPROXIMATE.
- 3. EXISTING UNDERGROUND AND OVERHEAD UTILITIES TO BE DETERMINED.

LEGEND:

BOTTOM OF FOOTING ELEVATION

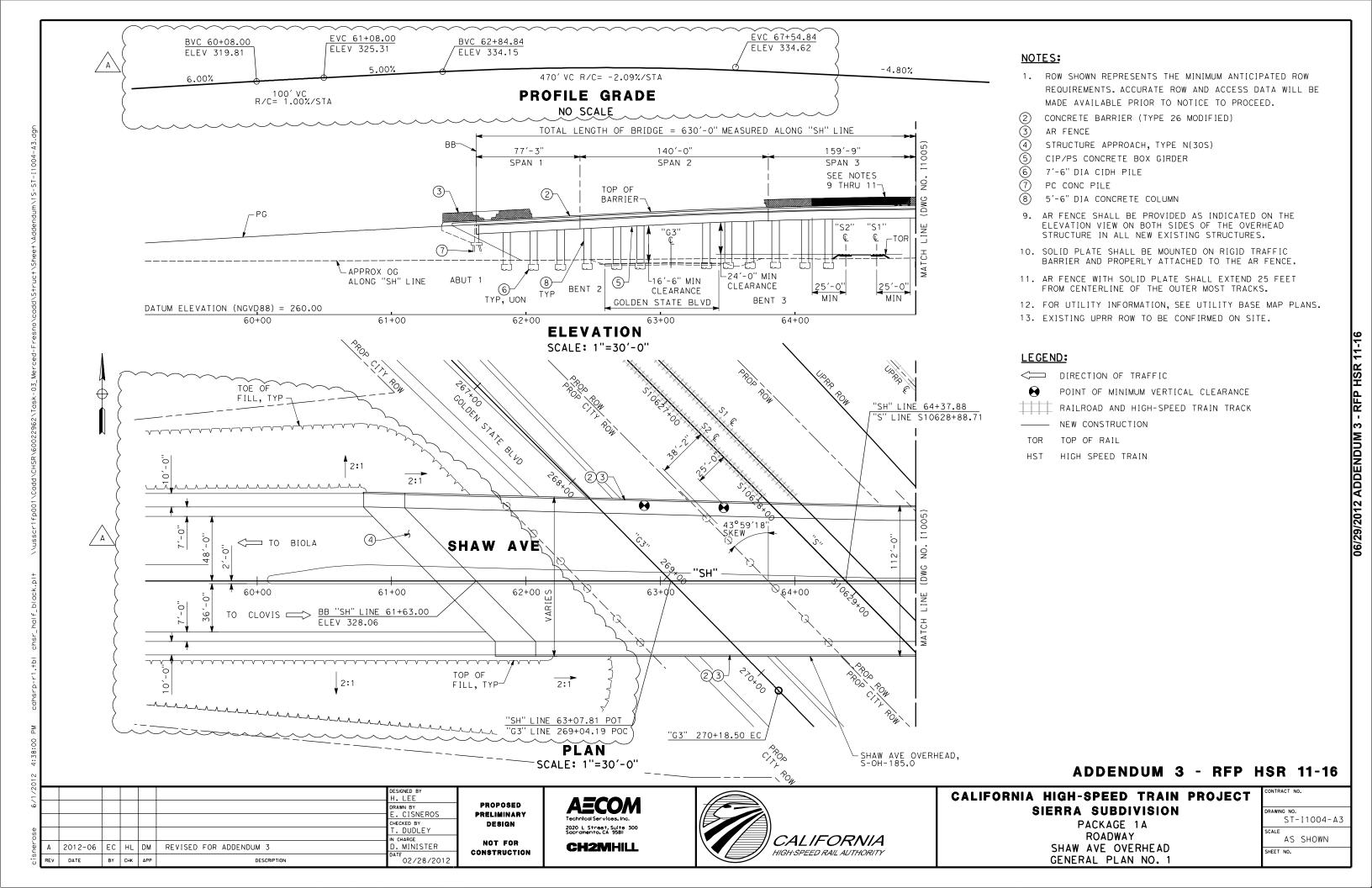
PC/PS PILE

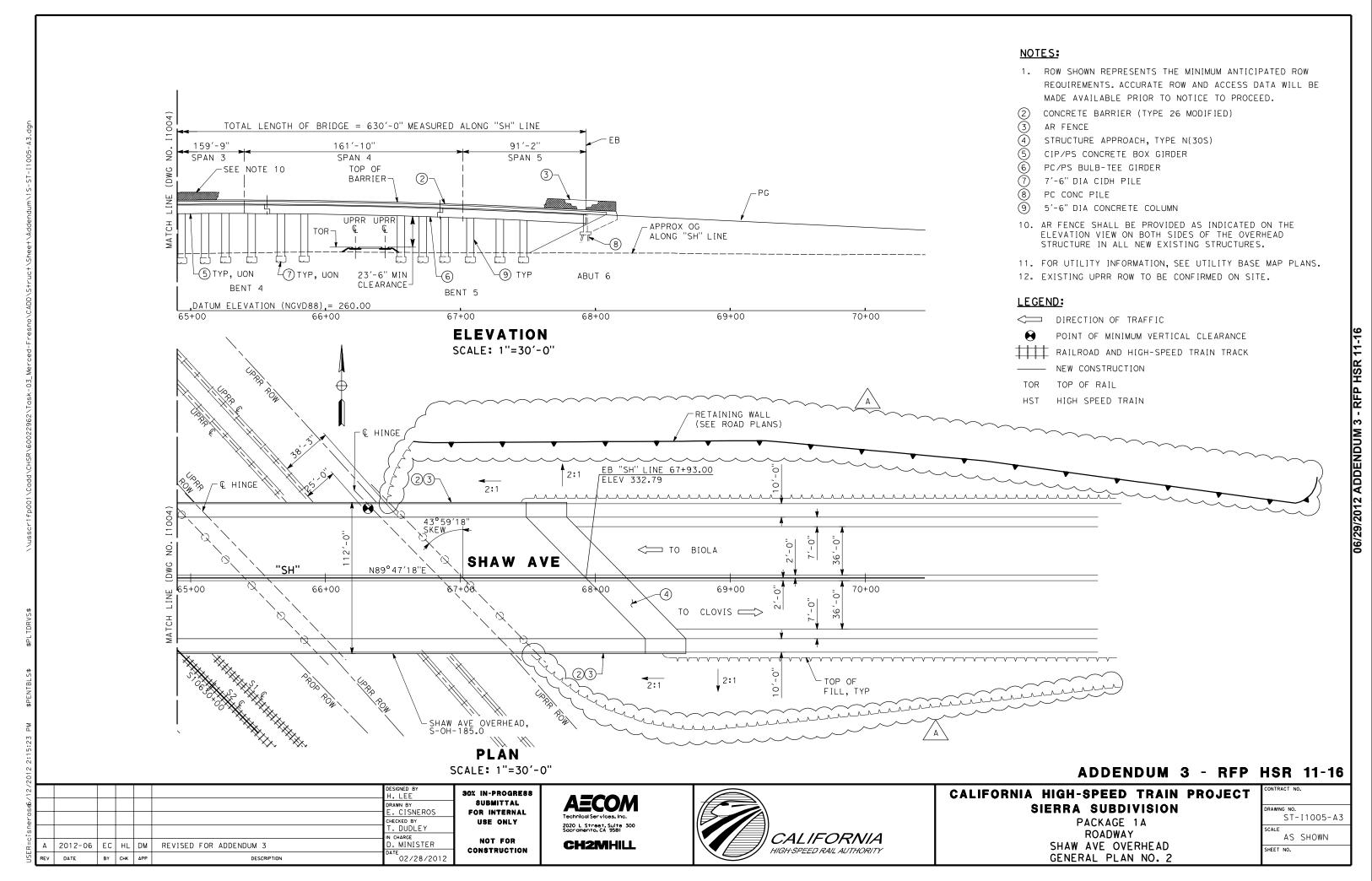
EXISTING STRUCTURE

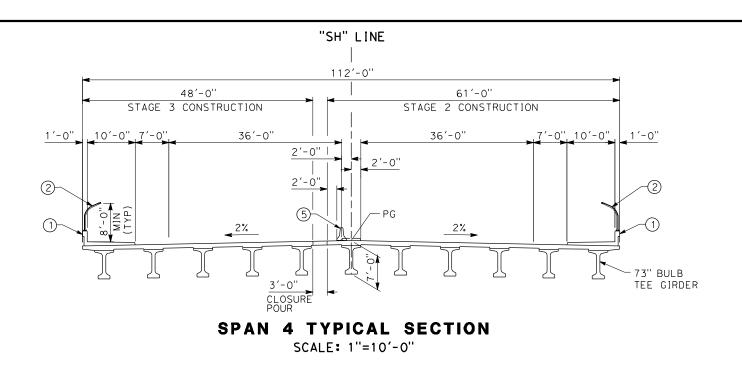
CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A
ROADWAY
GOLDEN STATE BLVD BRIDGE AT HERNDON CANAL
FOUNDATION PLAN

CONTRA	ACT NU	•	
DRAWIN	G NO.		
	ST-	-I1003	
SCALE			
	AS	SHOWN	
SHEET	NO.		







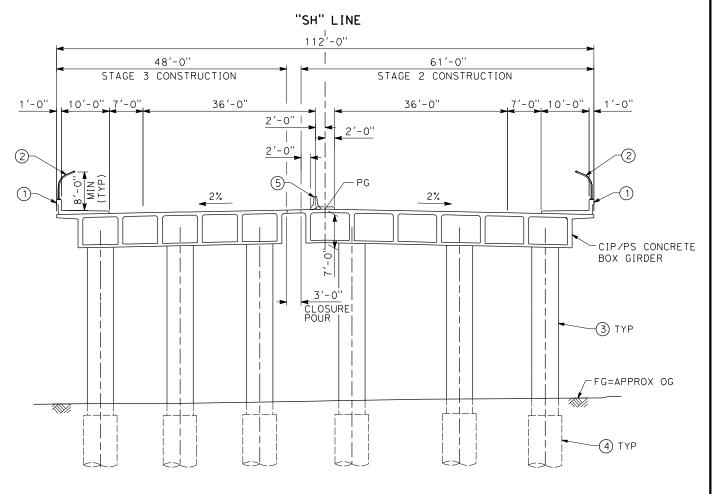
"SH" LINE VARIES VARIES 61'-0" STAGE 3 CONSTRUCTION STAGE 2 CONSTRUCTION 1'-0" 10'-0" 7'-0" 36'-0" VARIES 36'-0" 7'-0"_10'-0" 2% 2% CIP/PS CONCRETE BOX GIRDER 3'-0" CLOSURE POUR | -(3) TYP -FG=APPROX OG

SPANS 1 THRU 3 TYPICAL SECTION

SCALE: 1"=10'-0"

NOTES:

- (1) CONCRETE BARRIER (TYPE 26 MODIFIED)
- ② AR FENCE
- 3 5'-6" DIA CONCRETE COLUMN
- 4 7'-6" DIA CIDH PILE
- 5 TEMPORARY RAILING TYPE K
- 6. AR FENCE SHALL BE PROVIDED AS INDICATED ON THE CROSS SECTIONS VIEWS ON BOTH SIDES OF THE OVERHEAD STRUCTURE IN ALL NEW EXISTING STRUCTURES.



SPAN 5 TYPICAL SECTION

SCALE: 1"=10'-0"

30/20							DESIGNED BY	
1							H. LEE DRAWN BY E. CISNEROS	
ose							CHECKED BY T. DUDLEY	
ner r							IN CHARGE D. MINISTER	C
SIS	REV	DATE	ВΥ	СНК	APP	DESCRIPTION	12/08/11	

PROPOSED
PRELIMINARY
DESIGN
NOT FOR
CONSTRUCTION

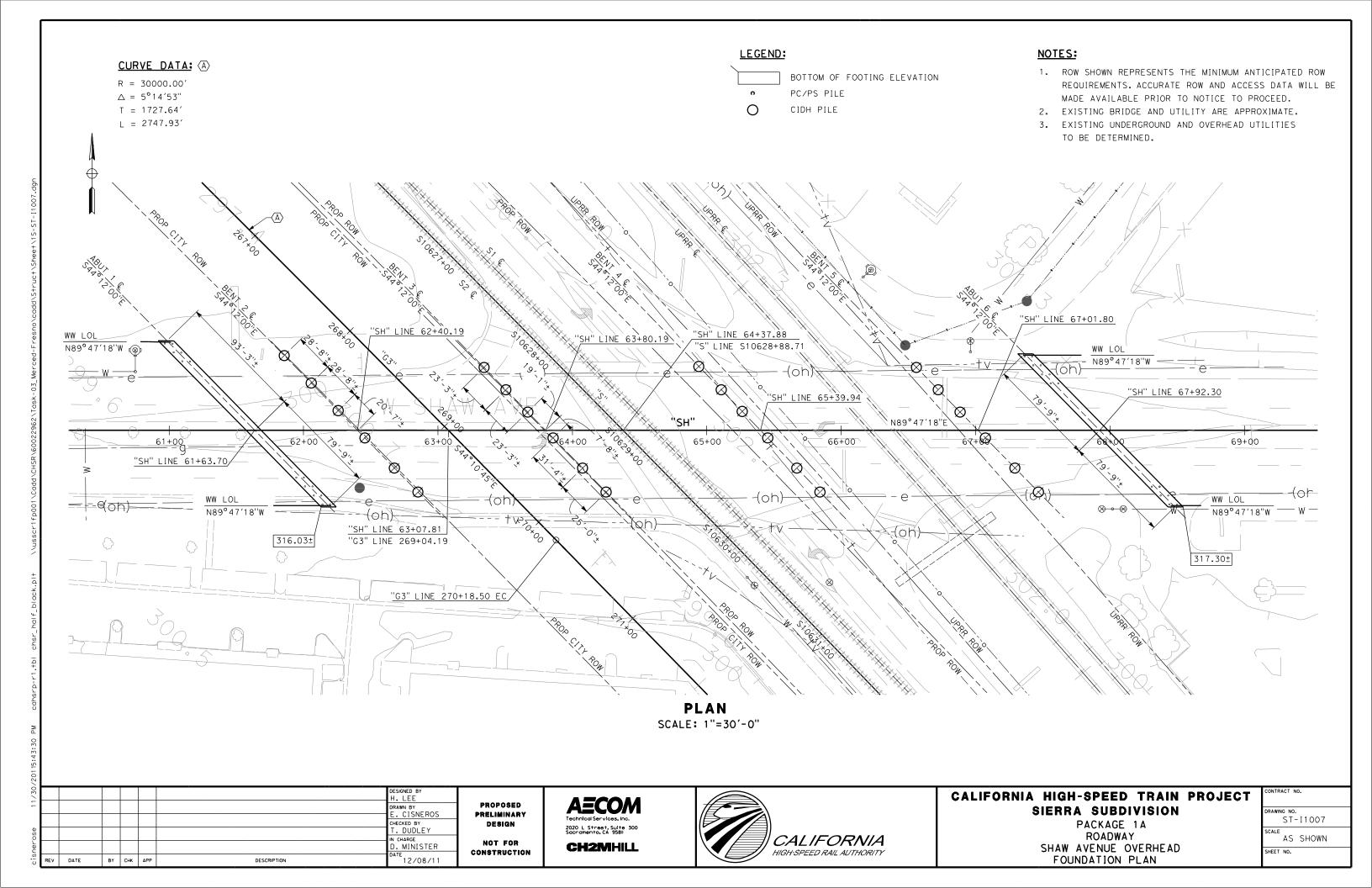
AECOM
Technical Services, Inc.
2020 L Street, Suite 300
Sacromento, CA 358II
CH2MHILL

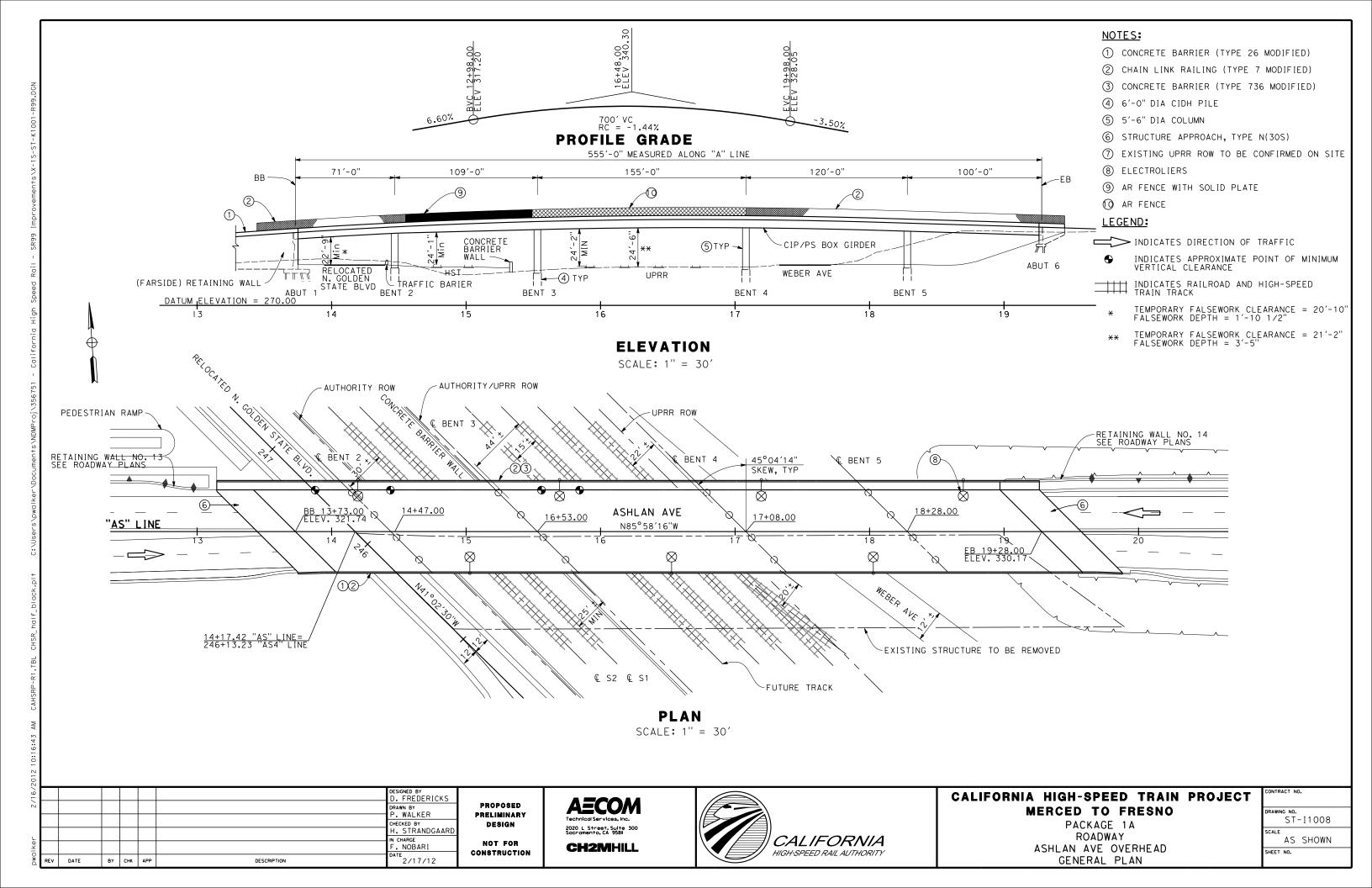


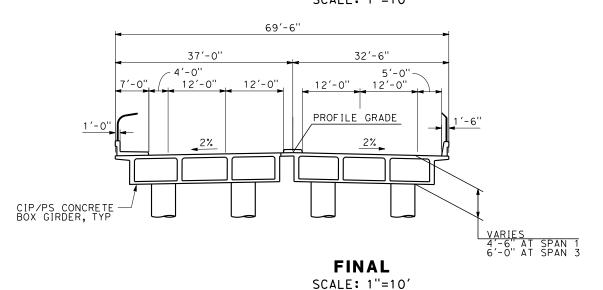
CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A ROADWAY SHAW AVENUE OVERHEAD TYPICAL SECTIONS

DRAWING NO.
ST-I1006
SCALE
AS SHOWN
SHEET NO.







NOTES:

- ① CONCRETE BARRIER (TYPE 26 MODIFIED)
- (2) CONCRETE BARRIER (TYPE 736 MODIFIED)
- (3) CHAIN LINK RAILING (TYPE 7 MODIFIED)
- 4 5'-6" DIA CONCRETE COLUMN
- 5 TEMPORARY RAILING TYPE K
- 6 PEDESTRIAN WALKWAY
- (7) EXISTING BARRIER
- 8 AR FENCE

STAGING NOTES:

STAGE 1:

CONSTRUCT NORTH PORTION OF ASHLAN OVERHEAD.

STAGE 2:

SHIFT ASHLAN AVE WB TRAFFIC TO NEWLY CONSTRUCTED NORTH PORTION OF OVERHEAD.

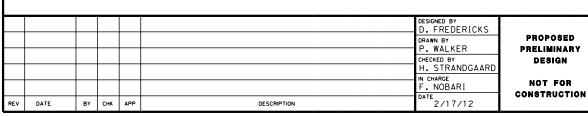
REMOVE NORTH PORTION OF EXISTING

CONSTRUCT SOUTH PORTION OF OVERHEAD.

FINAL STAGE:

SHIFT EB TRAFFIC TO NEW OVERHEAD.

REMOVE REMAINING SOUTH PORTION OF EXISTING OVERHEAD.



PROPOSED PRELIMINARY DESIGN NOT FOR

AECOM 2020 L Street, Suite 300 Sacramento, CA 95811 **CH2MHILL**



CALIFORNIA HIGH-SPEED TRAIN PROJECT MERCED TO FRESNO

PACKAGE 1A ROADWAY ASHLAN AVE OVERHEAD TYPICAL SECTION

DRAWING NO. ST-I1009
51-11009

AS SHOWN SHEET NO.

CLASȘ 140 ALT. "X" ABUTMENT 280 0 BENT 2 6' DIA. CIDH 2200 0 BENT 3 6' DIA. CIDH 26.70 0 6' DIA. CIDH 0 BENT 4 27.70 BENT 5 6' DIA. CIDH 2510 0

LOCATION | PILE TYPE

CLASS 140 ALT. "X"

ABUTMENT

NOTE: DESIGN TIP ELEVATIONS ARE TO BE DETERMINED BY CONTRACTOR

280

PILE DATA TABLE (PRELIMINARY)

TENSION

0

NOMINAL RESISTANCE (kips)

COMPRESSION

APPROXIMATE PILE CUT-OFF

ELEV. (FT)

293:00±

296:00±

293:00±

296:00±

296:00±

313:00±

APPROXIMATE DESIGN TIP ELEV. (FT)

TBD

TBD

TBD

TBD

TBD

TBD

EMBEDDED PILE LENGTH (FT)

TBD

TBD

TBD

TBD

TBD

TBD

LEGEND:

XXX.XX INDICATES BOTTOM OF FOOTING ELEVATION

INDICATES NEW PILE

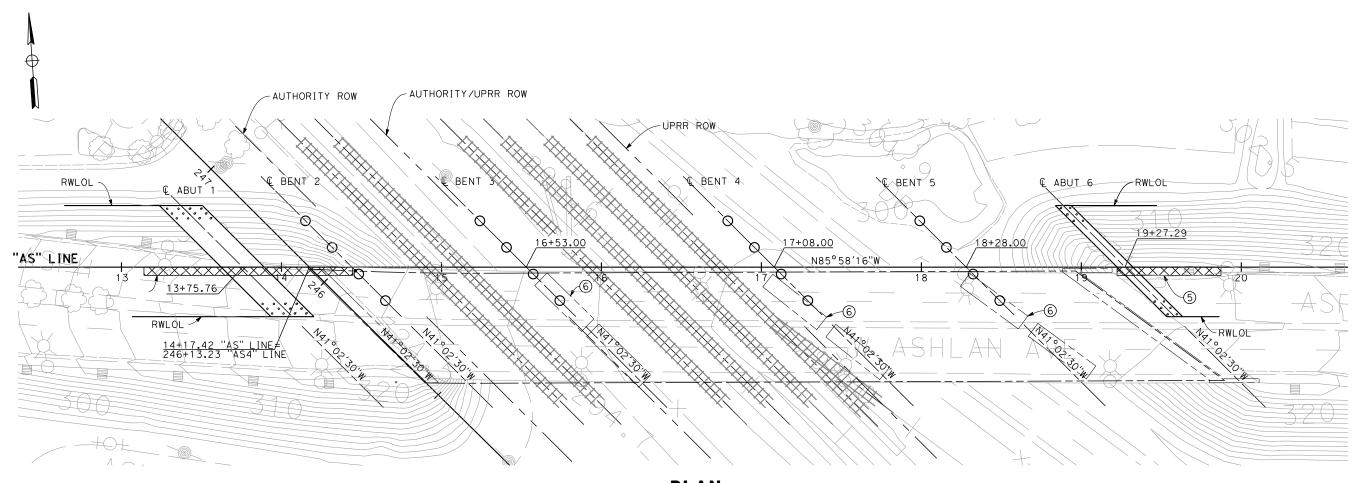
INDICATES NEW BATTER PILE

INDICATES CIDH PILE

INDICATES EXISTING STRUCTURE

NOTES:

- EXISTING BRIDGE, SUBSTRUCTURE, AND UTILITY LOCATIONS ARE APPROXIMATE. CONTRACTOR SHALL FIELD VERIFY CONTROLLING DIMENSIONS PRIOR TO CONSTRUCTION.
- EXISTING UNDERGROUND AND OVERHEAD UTILITIES
 TO BE DETERMINED AND RELOCATED AS NEEDED PRIOR CONSTRUCTION.
- RIGHT OF WAY LIMITS ARE APPROXIMATE AND TO BE CONFIRMED BY THE CONTRACTOR.
- REMOVE EXISTING SUBSTRUCTURE 2' MINIMUM BELOW FINISHED GRADE.
- TEMPORARY SHORING
- EXISTING SPREAD FOOTINGS



SCALE: 1'' = 30'

						D. FREDERICKS	-
						DRAWN BY P. WALKER	PROPOSED Preliminary
						CHECKED BY H. STRANDGAARD	DESIGN
						IN CHARGE F. NOBARI	NOT FOR
REV	DATE	ВΥ	СНК	APP	DESCRIPTION	2/17/12	CONSTRUCTION

2020 L Street, Suite 300 Sacramento, CA 958II **CH2MHILL**



CALIFORNIA HIGH-SPEED TRAIN PROJECT MERCED TO FRESNO

PACKAGE 1A ROADWAY ASHLAN AVE OVERHEAD FOUNDATION PLAN

CONTRACT NO.
DRAWING NO. ST-I1010
SCALE A.C. NOTED

AS NOTED SHEET NO.

PROPOSED PRELIMINARY DESIGN NOT FOR

CONSTRUCTION

AECOM Technical Services, Inc. 2020 L Street, Suite 300 Sacramento, CA 958II CH2MHILL



CALIFORNIA HIGH-SPEED TRAIN PROJECT CONTRACT NO. MERCED TO FRESNO

PACKAGE 1A ROADWAY ASHLAN AVE OVERHEAD BENT ELEVATION

DRAWIN	IG NO.	
	ST	-I1011
SCALE		
l	ΑS	SHOWN
SHEET	NO.	

	"AS" I	LINE	
CONCRETE BARRIER (TYPE 26) WITH CHAIN LINK RAILING		PROFILE GRADE	CONCRETE BARRIER (TYPE 736 MODIFIED) WITH CHAIN LINK RAILING
			CIP/PS CONCRETE BOX GIRDER, TYP
ŀ			

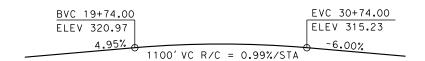
BENT ELEVATION

SCALE: 1"=10'

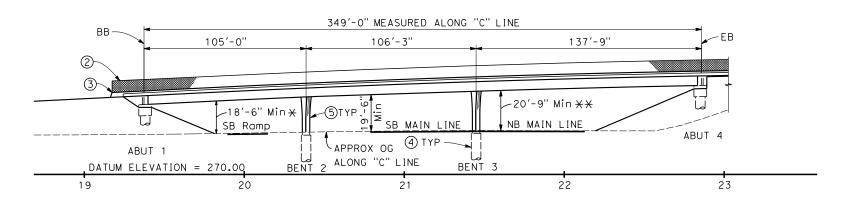
DATE

BY CHK APP

DESCRIPTION



PROFILE GRADE NO SCALE



ELEVATION

SCALE: 1"=30'

DESIGNED BY
D. FREDERICKS

CHECKED BY H. STRANDGAARD

້12/08/11

. WALKER

. NOBARI

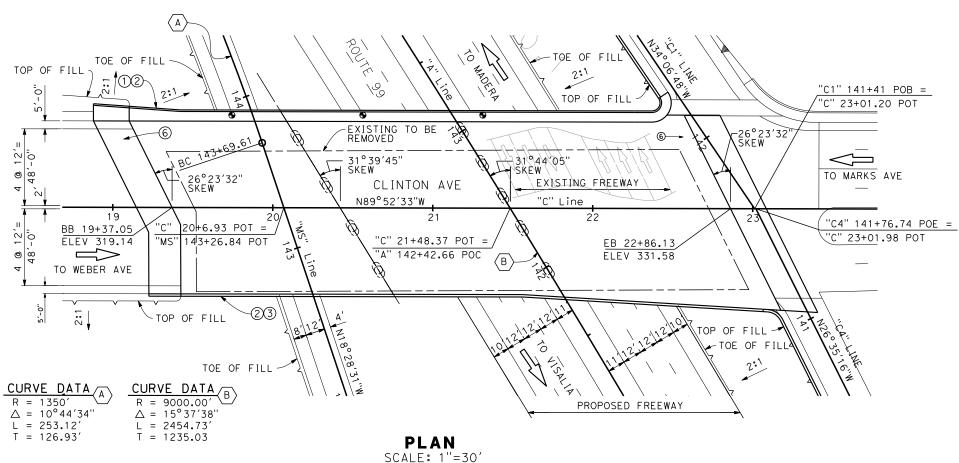
PROPOSED

PRELIMINARY

DESIGN

NOT FOR

CONSTRUCTION



NOTES:

LEGEND:

(1) CONCRETE BARRIER (TYPE 26 MODIFIED) (2) CHAIN LINK RAILING (TYPE 7 MODIFIED)

3 CONCRETE BARRIER (TYPE 736 MODIFIED)

TEMPORARY FALSEWORK CLEARANCE = 16'-6" FALSEWORK DEPTH = 2'-0"

INDICATES APPROXIMATE POINT OF MINIMUM

** TEMPORARY FALSEWORK CLEARANCE = 17'-6" FALSEWORK DEPTH = 3'-3"

6 STRUCTURE APPROACH, TYPE N(30S)

INDICATES DIRECTION OF TRAFFIC

VERTICAL CLEARANCE

4 6'-0" DIA CIDH PILE (5) 4' X 6' OBLONG COLUMN

PACKAGE 1A ROADWAY CLINTON AVE OVERCROSSING GENERAL PLAN

MERCED TO FRESNO

CALIFORNIA HIGH-SPEED TRAIN PROJECT ST-I1012 AS SHOWN

SHEET NO.

AECOM 2020 L Street, Suite 300 Sacramento, CA 95811 **CH2MHILL**

CALIFORNIA HIGH-SPEED RAIL AUTHORITY

STAGING NOTES:

STAGE 1:

SHIFT CLINTON AVE EB & WB TRAFFIC TO SOUTHERLY PORTION OF CLINTON AVE OC.

REMOVE NORTH PORTION OF CLINTON AVE OC.

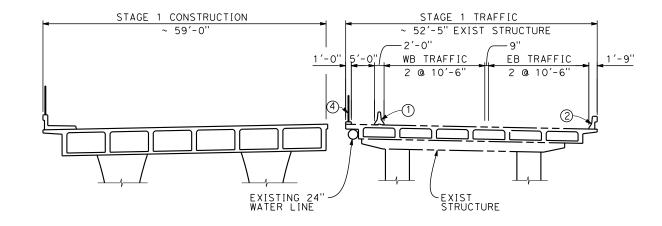
CONSTRUCT NORTH PORTION OF CLINTON AVE OC.

STAGE 2:

SHIFT CLINTON AVE EB & WB TRAFFIC TO NEWLY CONSTRUCTED CLINTON AVE OC. REMOVE REMAINING PORTION OF CLINTON AVE OC.

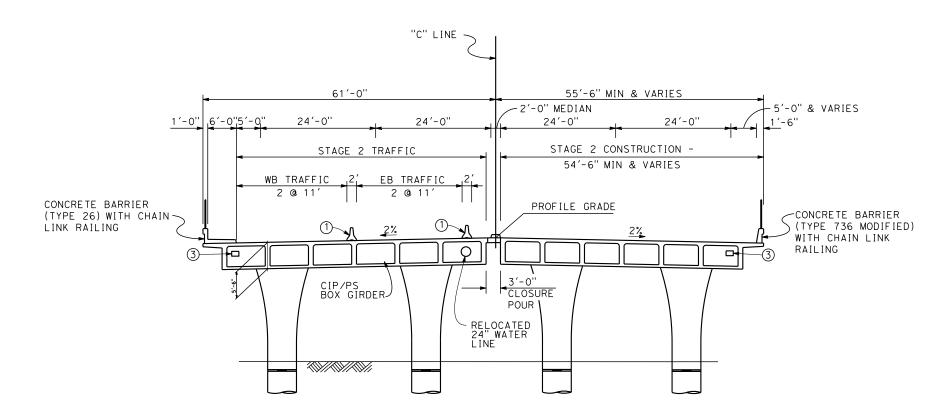
CONSTRUCT SOUTH PORTION OF CLINTON AVE OC.

PLACE CLOSURE POUR.



STAGE CONSTRUCTION

SCALE: 1"=10'



TYPICAL SECTION

SCALE: 1"=10'

20.								
2/2/							DESIGNED BY D. FREDERICKS	'
-							DRAWN BY P. WALKER	PROPOSED Preliminary
							CHECKED BY H. STRANDGAARD	DESIGN
Fer							IN CHARGE F. NOBARI	NOT FOR
Dwd	REV	DATE	BY	СНК	APP	DESCRIPTION	DATE 12/08/11	CONSTRUCTION

AECOM 2020 L Street, Suite 300 Sacramento, CA 95811 **CH2MHILL**



1) TEMPORARY RAILING TYPE K

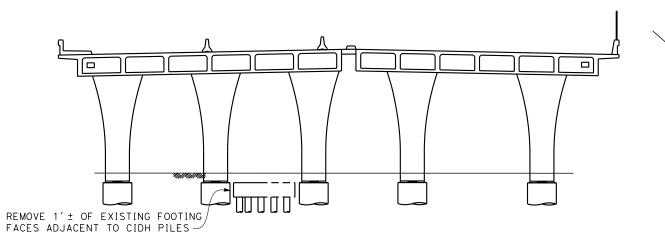
- (2) EXISTING BARRIER
- 3 FUTURE UTILITY OPENING
- (4) TEMPORARY CHAIN-LINK FENCE

CALIFORNIA HIGH-SPEED TRAIN PROJECT MERCED TO FRESNO

PACKAGE 1A ROADWAY CLINTON AVE OVERCROSSING TYPICAL SECTION

CONTRAC	T NO.	
DRAWING		-[101]
SCALE	AS	SHOW

SHEET NO.



LEGEND:

XXX.XX INDICATES BOTTOM OF FOOTING ELEVATION

INDICATES CIDH PILE

INDICATES EXISTING STRUCTURE

INDICATES EXISTING PILE

INDICATES EXISTING BATTER PILE

EXISTING UNDERGROUND AND OVERHEAD UTILITIES TO BE DETERMINED, AND RELOCATED AS NEEDED PRIOR TO CONSTRUCTION.

DIMENSIONS PRIOR TO CONSTRUCTION.

NOTES:

RIGHT OF WAY LIMITS ARE APPROXIMATE AND TO BE CONFIRMED BY THE CONTRACTOR.

REMOVE EXISTING SUBSTRUCTURE 2' MINIMUM BELOW FINISHED GRADE.

EXISTING BRIDGE, SUBSTRUCTURE, AND UTILITY LOCATIONS ARE APPROXIMATE. CONTRACTOR SHALL FIELD VERIFY CONTROLLING

PARTIAL REMOVAL OF EXISTING BENT 3 FOOTING

SECTION A-A NO SCALE



PLAN

CURVE DATA

A 750' CURVE DATA R = 1350'R = 9000.00' $\triangle = 10^{\circ}44'34''$ $\triangle = 15^{\circ}37'38''$ L = 253.12'

T = 126.93'

L = 2454.73'T = 1235.03' SCALE: 1"=30'-0"

PILE DATA TABLE (PRELIMINARY)									
LOCATION	PILE TYPE	NOMINAL RESISTANCE (kips)		APPROXIMATE PILE CUT-OFF	APPROXIMATE DESIGN	EMBEDDED PILE			
LOCATION	' '	COMPRESSION	TENSION			LENGTH (FT)			
ABUTMENT 1	6' DIA. CIDH	2100	0	295 . 00±	TBD	TBD			
BENT 2-3	6' DIA. CIDH	4150	0	295 . 00±	TBD	TBD			
ABUTMENT 4	6' DIA. CIDH	2100	0	295.00±	TBD	TBD			

NOTE: DESIGN TIP ELEVATIONS ARE TO BE DETERMINED BY CONTRACTOR

D. FREDERICKS P. WALKER CHECKED BY H. STRANDGAARD . NOBARI CONSTRUCTION DATE BY CHK APP DESCRIPTION 12/08/11

PROPOSED PRELIMINARY DESIGN NOT FOR

2020 L Street, Suite 300 Sacramento, CA 95811 **CH2MHILL**

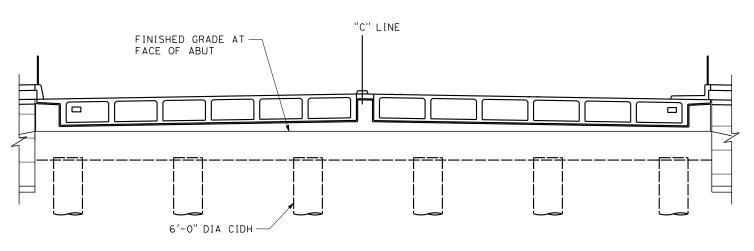


CALIFORNIA HIGH-SPEED TRAIN PROJECT MERCED TO FRESNO

PACKAGE 1A ROADWAY CLINTON AVE OVERCROSSING FOUNDATION PLAN

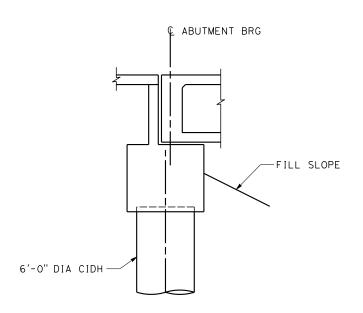
CONTRACT NO.					
DRAWING NO.					
ST-I1014					
SCALE					
AS SHOWN					
SHEET NO.					





ABUTMENT ELEVATION

SCALE: 1" = 10'



ABUTMENT SECTION

SCALE: 1" = 5'-0"

12/2/2(DESIGNED BY D. FREDERICKS DRAWN BY	PROPOSED
L							P. WALKER CHECKED BY H. STRANDGAARD IN CHARGE	PRELIMINARY Design
pwalke	REV	DATE	BY	СНК	APP	DESCRIPTION	F. NOBARI DATE 12/08/11	NOT FOR CONSTRUCTION

AECOM 2020 L Street, Suite 300 Sacramento, CA 95811 CH2MHILL



CALIFORNIA HIGH-SPEED TRAIN PROJECT MERCED TO FRESNO

PACKAGE 1A ROADWAY CLINTON AVE OVERCROSSING CONSTRUCTION DETAIL NO. 1

DRAWING NO. ST-I1015		
SCALE		CHOWN
l	AS	SHOWN

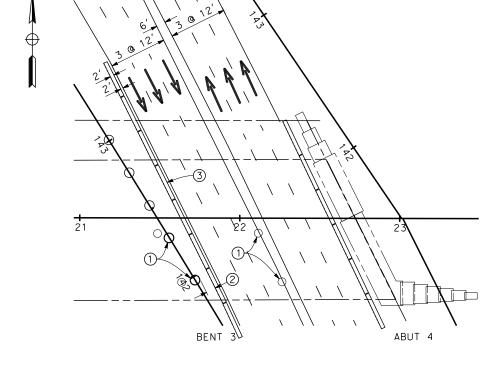
SHEET NO.

PROPOSED PRELIMINARY DESIGN

NOT FOR







STAGE 1 PLAN AT BENT 3

SCALE: 1"=30'

STAGE 2 PLAN AT BENT 3

SCALE: 1"=30'

NOTES:

BENT 3

1 REMOVE SUPERSTRUCTURE, AND SUBSTRUCTURE TO 2' BELOW FG, DURING NIGHT CLOSURES

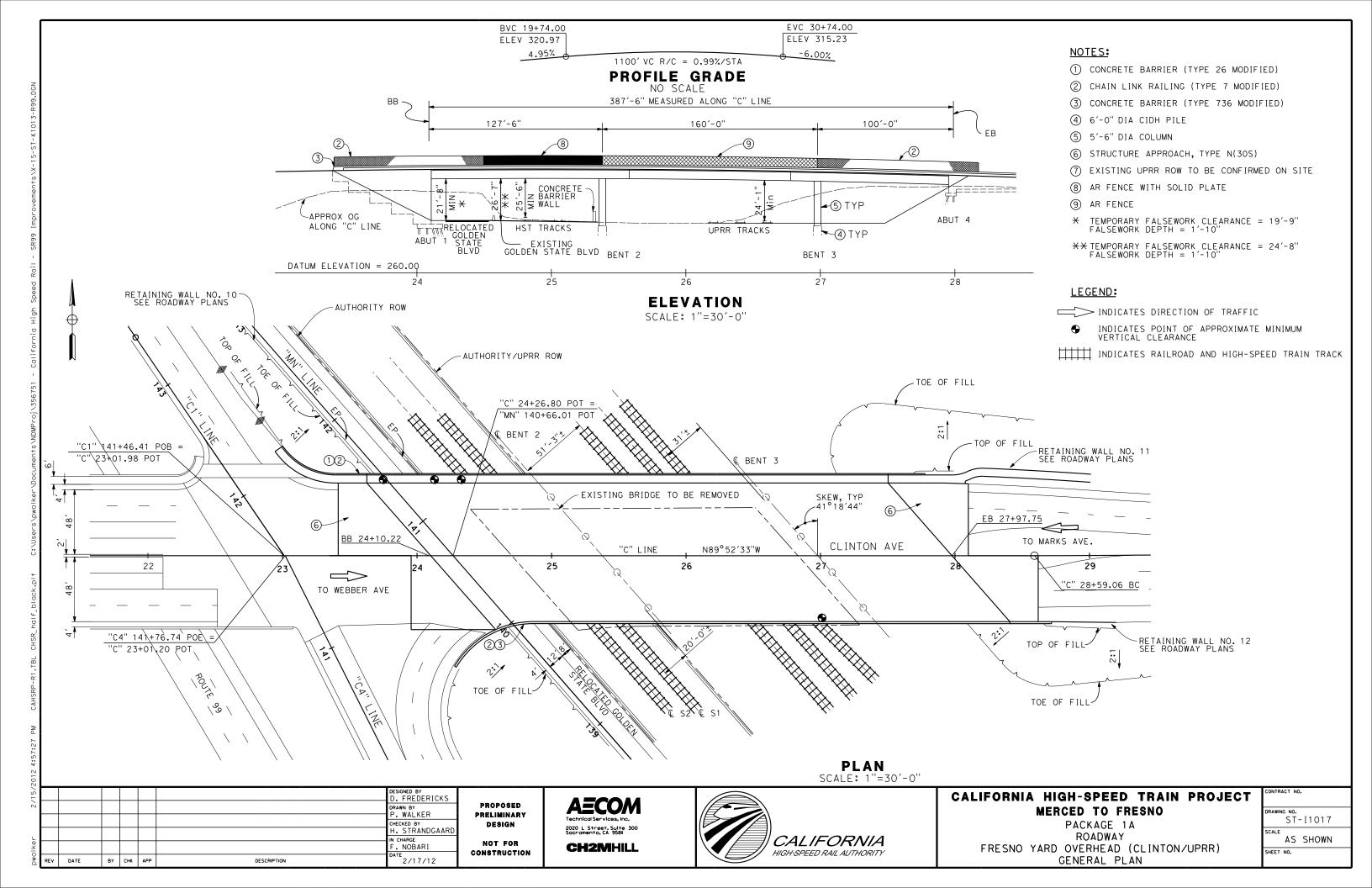
ABUT 4

- 2 6'-6" MINIMUM TO K-RAIL
- 3 TEMPORARY RAILING TYPE K
- 4 REMOVE PORTION OF EXISTING FOOTING

CALIFORNIA HIGH-SPEED TRAIN PROJECT CONTRACT NO. MERCED TO FRESNO

PACKAGE 1A ROADWAY CLINTON AVE OVERCROSSING CONSTRUCTION DETAIL NO. 2

DRAWING NO.				
	ST-	1016		
SCALE	AS	SHOWN		



PROPOSED PRELIMINARY DESIGN NOT FOR

CONSTRUCTION

AECOM 2020 L Street, Suite 300 Sacramento, CA 95811 **CH2MHILL**

STAGE 1 TRAFFIC ~ 52'-5" EXIST STRUCTURE

EXISTING 16" WATER IN 24" CASING

49'-6"

STAGE 2A CONSTRUCTION 48'-6"

-**4** TYP

PROFILE GRADE

EB TRAFFIC →

4'-0" .

MIN

1'-6"

2 @ 10'-6"

WB TRAFFIC

2 @ 10'-6"

STAGE 1 SCALE: 1"=10'-0"

MEDIAN

VAR 16'-32

CLOSURE

POUR

-16" WATER IN 24" CASING

STAGE 2

SCALE: 1"=10'-0"

"C" LINE 🦳

24'-0" LEFT TURN

2'-0"

Ø

EB TRAFFIC

2 @ 11



-PC/PS BULB TEE GIRDER

NOTES:

- (1) CONCRETE BARRIER (TYPE 26 MODIFIED)
- 2) CONCRETE BARRIER (TYPE 736 MODIFIED)
- 3 CHAIN LINK RAILING (TYPE 7 MODIFIED)
- 4 5'-6" DIA COLUMN
- (5) TEMPORARY RAILING TYPE K
- (6) EXISTING BARRIER
- 7 AR FENCE
- (8) TEMPORARY CHAIN-LINK FENCE

STAGING NOTES:

STAGE 1:

SHIFT CLINTON AVE EB & WB TRAFFIC TO SOUTHERLY PORTION OF FRESNO YARD OH.

REMOVE NORTH PORTION OF FRESNO YARD OH.

CONSTRUCT NORTH PORTION OF FRESNO YARD OH.

STAGE 2:

SHIFT CLINTON AVE EB & WB
TRAFFIC TO NEWLY CONSTRUCTED FRESNO YARD OH.

REMOVE REMAINING PORTION OF FRESNO YARD OH.

CONSTRUCT SOUTH PORTION OF FRESNO YARD OH.

CLOSURE POUR.

MERCED TO FRESNO PACKAGE 1A ROADWAY FRESNO YARD OVERHEAD (CLINTON/UPRR)

CALIFORNIA HIGH-SPEED TRAIN PROJECT

TYPICAL SECTION

ST-I1018

AS SHOWN SHEET NO.

49'-6" 60'-0" PROFILE GRADE

"C" LINE

STAGE 2 CONSTRUCTION ~ 58'-0"

60'-0"

STAGE 2A TRAFFIC

-2'-0"

24'-0"

5

CIP/PS Box Girder

WB TRAFFIC

2 @ 11

1'-0" 6'-0",4'-0"

SPAN 2 SCALE: 1"=10'-0"

CH2MHILL

HIGH-SPEED RAIL AUTHORITY

CONSTRUCTION

12/08/11

BY CHK APP

DESCRIPTION

ST-I1019

AS SHOWN

SHEET NO.

FRESNO YARD OVERHEAD (CLINTON/UPRR)

FOUNDATION PLAN

PROPOSED Preliminary Design

NOT FOR

CONSTRUCTION

AECOM Technical Services, Inc. 2020 L Street, Suite 300 Socramento, CA 95811 CH2MHILL



CALIFORNIA HIGH-SPEED TRAIN PROJECT MERCED TO FRESNO DRAWING NO.

PACKAGE 1A ROADWAY FRESNO YARD OVERHEAD (CLINTON/UPRR) BENT ELEVATION

Γ	
	DRAWING NO. ST-I1020
	SCALE AS SHOWN
	AS SHOWIN

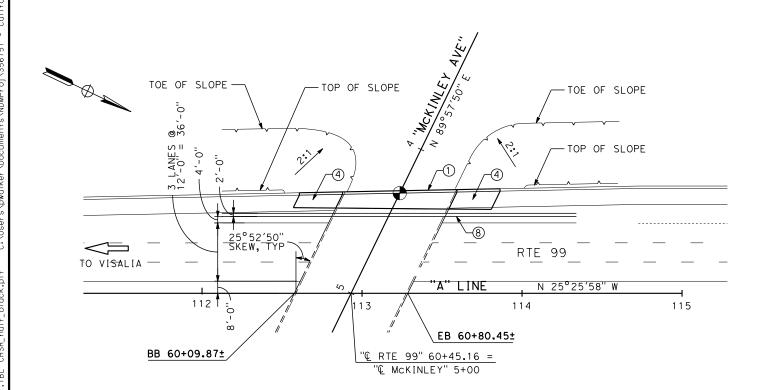
SHEET NO.

	"C" LINE
CONCRETE BARRIER (TYPE 736 MODIFIED) WITH CHAIN LINK RAILING	PROFILE GRADE CONCRETE BARRIER (TYPE 736 MODIFIED) WITH CHAIN LINK RAILING
CIP/PS CONCRETE BOX GIRDER, TYP	

BENT ELEVATION

SCALE: 1"=10'-0"

SCALE: 1"=30'



PLAN SCALE: 1"=30

NOTES:

- ① CONCRETE BARRIER (TYPE 736 MODIFIED)
- 2 FOR UTILITY INFORMATION, SEE UTILITY BASE MAP PLANS
- (3) PAINT "McKINLEY AVE UC" AMD "BR NO 43-181 R/L"
- 4) STRUCTURE APPROACH, TYPE N (305)
- (5) MATCH EXISTING GRADE AND X-SLOPE
- 6 REMOVE EXISTING RAILING
- (7) 2'-0" CLOSURE POUR
- 8 TEMPORARY RALING (TYPE K)
- * STRUCTURE TO BE CAST ON FALSEWORK AND LOWERED INTO PLACE

LEGEND:

INDICATES DIRECTION OF TRAFFIC

---- INDICATES CLEARANCE LINE

INDICATES APPROXIMATE POINT OF MIN VERTICAL CLEARANCE

INDICATES CONCRETE REMOVAL

"RTE 99" LINE 64'-61/2" MAX & VARIES 53'-2¹/₂"± MAX & VARIES 48'-0" 7 -CIP/PS CONCRETE BOX GIRDER -APPROX OG = FG

> TYPICAL SECTION SCALE: 1"=10'

DESIGNED BY
D. FREDERICKS DRAWN BY M. MIYAMOTO CHECKED BY H. STRANDGAARD . NOBARI 12/08/11 BY CHK APP DESCRIPTION

PROPOSED PRELIMINARY DESIGN NOT FOR

CONSTRUCTION

AECOM 2020 L Street, Suite 300 Sacramento, CA 95811 **CH2MHILL**



CALIFORNIA HIGH-SPEED TRAIN PROJECT MERCED TO FRESNO

PACKAGE 1A ROADWAY MCKINLEY BLVD UNDERCROSSING (WIDEN) GENERAL PLAN

ST-I1021 AS SHOWN

SHEET NO.







PROPOSED Preliminary Design

NOT FOR CONSTRUCTION





CALIFORNIA HIGH-SPEED TRAIN PROJECT MERCED TO FRESNO

PACKAGE 1A
ROADWAY
MCKINLEY BLVD UNDERCROSSING (WIDEN)
TYPICAL SECTION

CONTRACT NO.
DRAWING NO. ST-I1022
SCALE AS SHOWN
SHEET NO.

		€ RTE 99
Z'-6" CLOSURE POUR REMOVE EXISTING CONCRETE BARRIER TYPE 25 MODIFIED & REFINISH BRIDGE DECK CONCRETE BARRIER (TYPE 736 MODIFIED)	MAX & VARIES 11'-4" MAX & VARIES 4'-0" SHLD 36'-0" STAGE 1 TRAFFIC TEMPORARY RAILING TYPE K 11/2% CIP/PS CONCRETE BOX GIRDER EXISTING CIP/PS CONCRETE BOX GIRD	CONCRETE BARRIER TYPE 50A 11/2% ER

* MATCH EXISTING GRADE AND X-SLOPE

NOTES:

TYPICAL SECTION
SCALE: 1" = 10'-0"

PILE DATA TABLE (PRELIMINARY)

The British tribed (tribed military)								
LOCATION	PILE TYPE	NOMINAL RESISTANCE (kips)		APPROXIMATE PILE CUT-OFF	APPROXIMATE DESIGN	EMBEDDED PILE		
		COMPRESSION	TENSION	ELEV. (FT)	TIP ELEV. (FT)	LENGTH (FT)		
ABUTMENT 1	CLASS 140 ALT. "X"	280	0	N/A	TBD	TBD		
ABUTMENT 2	CLASS 140 ALT. "X"	280	0	N/A	TBD	TBD		
NOTE: DECI	NATE: DECIDING THE ELECTRICAL PROPERTY OF THE							

NOTE: DESIGN TIP ELEVATIONS ARE TO BE DETERMINED BY CONTRACTOR

LEGEND:

INDICATES BOTTOM OF FOOTING ELEVATION INDICATES PC/PS PILE

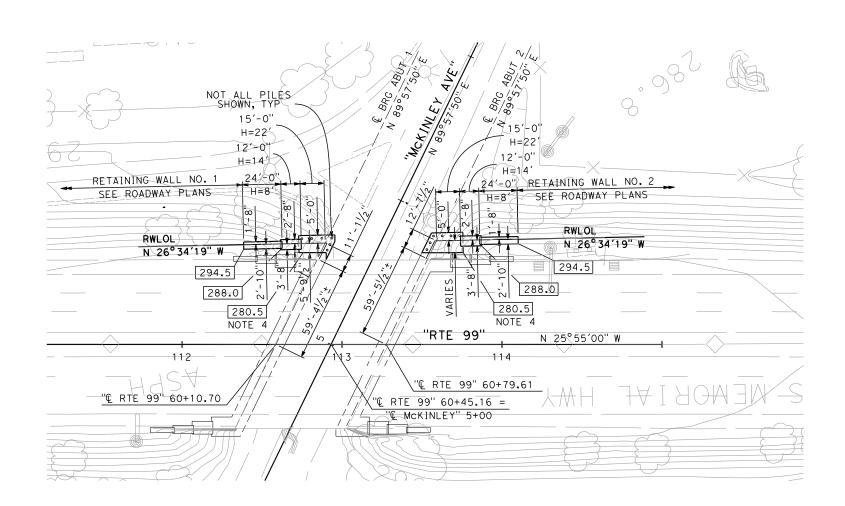
0

— INDICATES EXISTING STRUCTURE

NOTES:

- 1. EXISTING BRIDGE, SUBSTRUCTURE, AND UTILITY LOCATIONS ARE APPROXIMATE. CONTRACTOR SHALL FIELD VERIFY CONTROLLING DIMENSIONS PRIOR TO CONSTRUCTION.
- 2. EXISTING UNDERGROUND AND OVERHEAD UTILITIES TO BE DETERMINED AND RELOCATED AS NEEDED PRIOR TO CONSTRUCTION.
- 3. RIGHT OF WAY LIMITS ARE APPROXIMATE AND TO BE CONFIRMED BY THE CONTRACTOR
- 4. MATCH EXISTING FOOTING ELEVATION





PLAN SCALE: 1"=30'

						DESIGNED BY D. FREDERICKS	
						DRAWN BY M. MIYAMOTO	PROPOSED Preliminary
						CHECKED BY H. STRANDGAARD	DESIGN
						IN CHARGE F. NOBARI	NOT FOR
REV	DATE	BY	СНК	APP	DESCRIPTION	DATE 12/08/11	CONSTRUCTION

2020 L Street, Suite 300 Sacramento, CA 95811 **CH2MHILL**

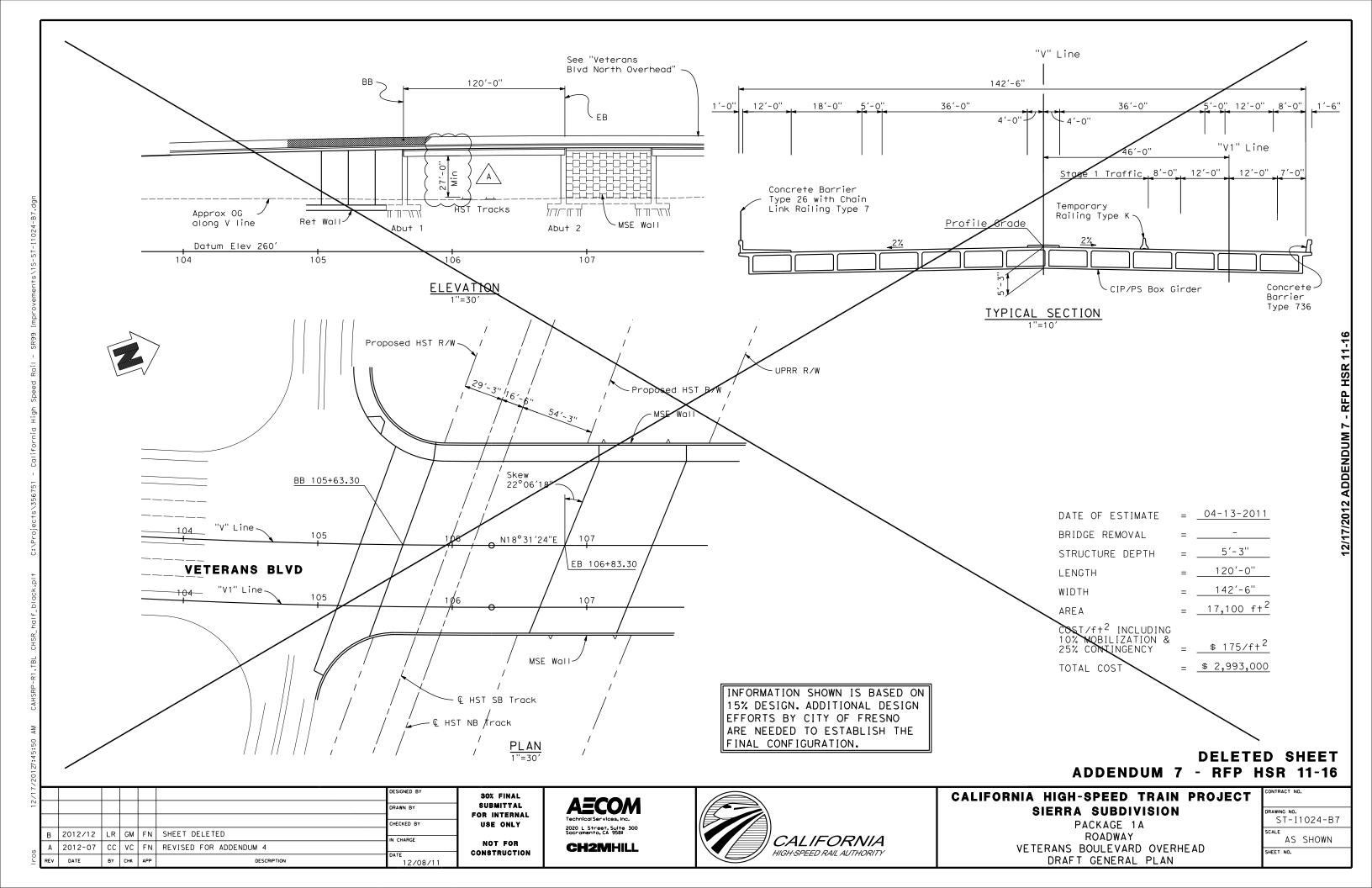


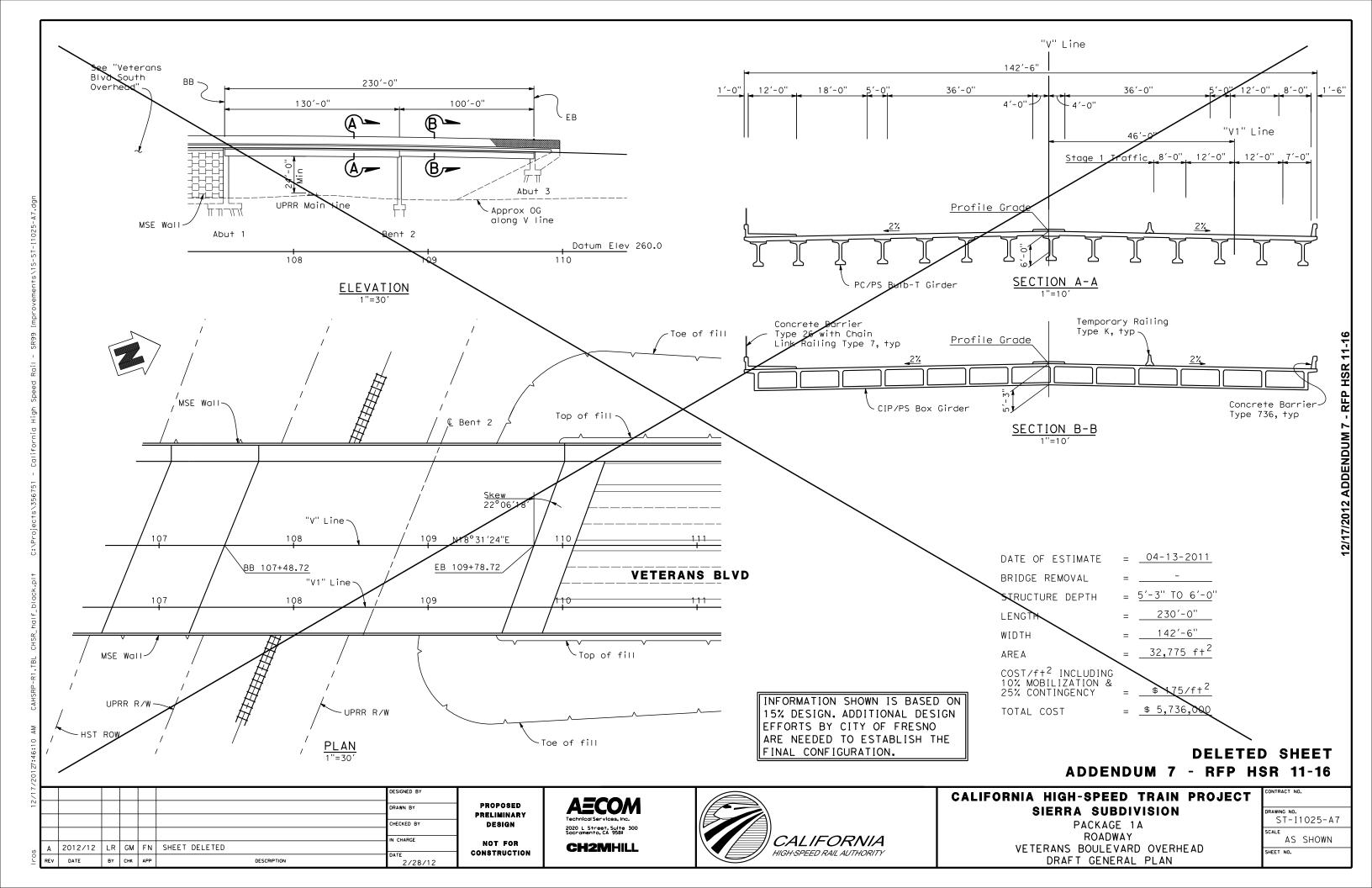
CALIFORNIA HIGH-SPEED TRAIN PROJECT MERCED TO FRESNO

PACKAGE 1A ROADWAY MCKINLEY BLVD UNDERCROSSING (WIDEN) FOUNDATION PLAN

CONTIN	-0	<i>.</i>	
DRAWIN		-I1023	
SCALE			
	ΑS	SHOWN	

SHEET NO.

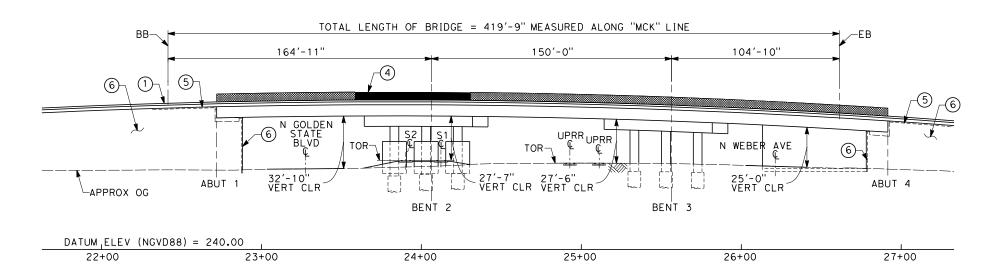


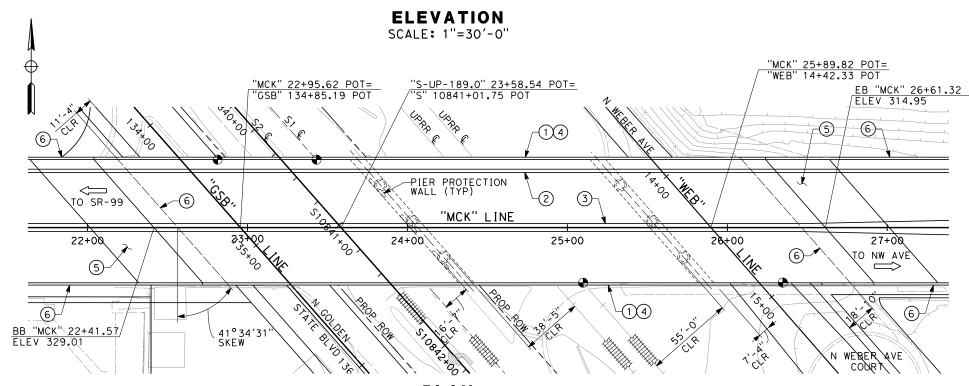


1′-5³⁄4''



PROFILE GRADE NO SCALE





PLAN SCALE: 1"=30'-0"

ESIGNED BY DRAWN BY HECKED BY CHARGE REVISED FOR ADDENDUM 3 2012-06 VO BR RP CONSTRUCTION BY CHK APP 06/12/12 DESCRIPTION

PROPOSED **PRELIMINARY** NOT FOR





ADDENDUM 3 - RFP HSR 11-16

CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION PACKAGE 1A ROADWAY DRAFT GENERAL PLAN

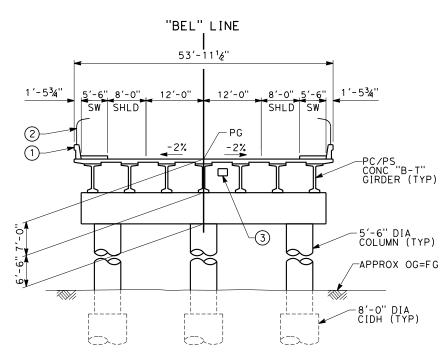
W MCKINLEY AVE

ST-K1026-A3 AS SHOWN SHEET NO.

06/12/12

DESCRIPTION

- 1 CONCRETE BARRIER (TYPE 736 MODIFIED)
- 2 AR FENCE
- 3 FUTURE UTILITY OPENING
- FOR UTILITY INFORMATION, SEE UTILITY BASE MAP PLANS.
- (5) FOR INTRUSION DETECTION/PROTECTION, SEE ALIGNMENT PLANS.



TYPICAL SECTION SCALE: 1"=10'-0"

ADDENDUM 3 - RFP HSR 11-16

V								
/7							DESIGNED BY L. SMITH	
ò							DRAWN BY T. PRETZER	PROPOSED Preliminary
							CHECKED BY M. BURGARD	DESIGN
õ							IN CHARGE	NOT FOR
22	Α	2012-06	vo	BR	RP	REVISED FOR ADDENDUM 3	Y. BERI	CONSTRUCTION
5	REV	DATE	ВΥ	СНК	APP	DESCRIPTION	06/12/12	





CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A ROADWAY DRAFT GENERAL PLAN BELMONT AVE

٠	CONTRACT NO.
	DRAWING NO. ST-K1029-A3
	SCALE AS SHOWN
	SHEET NO.

PRIOR TO BIDDING.

- 1. FIELD VERIFY ALL EXISTING SITE CONDITIONS PRIOR TO THE COMMENCEMENT OF WORK AND REPORT ANY DISCREPANCIES TO THE OWNER'SREPRESENTATIVE. VISIT THE SITE AND BECOME FAMILIAR WITH THE SITE CONDITIONS
- 2. ALL CONSTRUCTION ACTIVITIES SHALL BE COORDINATED WITH LOCAL AND STATE JURISDICTION INSPECTOR(S). NOTIFY LOCAL AND STATE JURISDICTION INSPECTOR(S) _____ DAYS PRIOR TO THE START OF ANY CONSTRUCTION.
- 3. ALL DAMAGE CAUSED BY THE PROJECT SHALL BE RESTORED TO AN "AS GOOD OR BETTER" CONDITION.
- 4. LEAVE THE PROJECT AREA FREE OF DEBRIS AND UNUSED MATERIAL UPON PROJECT COMPLETION.
- 5. FOR STANDARD GENERAL ABBREVIATIONS, SEE DRAWINGS SD-GE-001 THRU SD-GE-004.
- 6. FOR STANDARD GENERAL SYMBOLS, SEE DRAWINGS SD-GE-005 THRU SD-GE-007.
- 7. "ORIGINAL GROUND" SHOWN ON CROSS SECTIONS REFERS TO THE APPROXIMATE EXISTING GROUND LINE AT THE DESIGNATED CENTERLINE, BASELINE, LAYOUT LINE OR SECTION LINE.
- 8. MAINTAIN EXISTING SITE SURVEY CONTROL POINTS AND BENCH MARKS. SURVEY CONTROL POINTS OR BENCH MARKS DAMAGED OR REMOVED AS A RESULT OF THE PROJECT SHALL BE RESTORED BY A REGISTERED LAND SURVEYOR.
- 9. ALL WORK SHALL CONFORM TO ALL LOCAL, STATE AND FEDERAL CODES AND ORDINANCES IN EFFECT. PROVIDE AND MAINTAIN PROPER BARRICADES, RAILINGS, GUARDS, FLAGGING, LIGHTING, OR OTHER DEVICES NECESSARY FOR THE PROTECTION OF LIFE AND PROPERTY.
- 10. VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING MATERIALS.
- 11. ALL HORIZONTAL AND VERTICAL DISTANCES ARE IN FEET AND /OR DECIMALS OF FOOT EXCEPT AS NOTED OTHERWISE.
- 12. ACCESS ROAD TYPE SHALL BE CONFIRMED ON A SITE-BY-SITE BASIS.
- B. DEMOLITION NOTES
 - 1. REVIEW ALL ENVIRONMENTAL STUDIES AND REQUIREMENTS PRIOR TO BIDDING.
 - MAINTAIN TEMPORARY ACCESS DURING CONSTRUCTION. UTILITY OUTAGE AND ACCESS CLOSURES DURING CONSTRUCTION REQUIRE A MINIMUM OF _____ DAYS NOTICE TO OWNER OR SITE TENANTS.
 - 3. FOLLOW LOCAL JURISDICTION REQUIREMENTS FOR CLEARING AND GRUBBING.

C. GRADING

- 1. DO NOT PERFORM ANY GRADING OPERATION SO AS TO CAUSE FALLING ROCKS, SOIL OR DEBRIS IN ANY FORM TO FALL, SLIDE OR FLOW ONTO ADJOINING PROPERTIES, STREETS OR NATURAL WATERCOURSES. SHOULD SUCH VIOLATION OCCUR ALL NECESSARY REMEDIAL ACTIONS MUST BE MADE IMMEDIATELY.
- 2. KEEP THE PROJECT AREA AND SURROUNDING AREA FREE FROM DUST NUISANCE.
- 3. PROVISIONS SHALL BE MADE TO PREVENT SURFACE WATERS FROM DAMAGING THE CUT FACE OF AN EXCAVATION OR THE SLOPED SURFACES OF A FILL. FURTHERMORE, PROVISIONS SHALL BE MADE TO PREVENT SEDIMENT-LADEN RUNOFF FROM LEAVING THE SITE.
- 4. ALL SLOPED AND EXPOSED AREAS SHALL BE SODDED OR PLANTED AS SOON AS FINAL GRADES HAVE BEEN ESTABLISHED. PLANTING SHALL NOT BE DELAYED UNTIL ALL GRADING WORK HAS BEEN COMPLETED. GRADING TO FINAL GRADE SHALL BE CONTINUOUS, AND ANY AREA WITHIN WHICH WORK HAS BEEN INTERRUPTED OR DELAYED SHALL BE PLANTED.
- 5. THE LIMITS OF THE AREA TO BE GRADED SHALL BE FLAGGED BEFORE THE COMMENCEMENT OF THE GRADING WORK.
- 6. ALL GRADING OPERATIONS SHALL BE PERFORMED IN CONFORMANCE WITH THE APPLICABLE PROVISIONS OF THE WATER POLLUTION CONTROL AND WATER QUALITY STANDARDS CONTAINED IN THE LATEST CALTRANS STORM WATER QUALITY HANDBOOKS.
- D. TEMPORARY TRAFFIC CONTROL
 - 1. OBTAIN APPROVAL FOR TRAFFIC CONTROL PLAN FROM AGENCY HAVING JURISDICTION OVER THE ROADWAY PRIOR TO IMPLEMENTATION OF TEMPORARY TRAFFIC CONTROL.
- E. PUBLIC HEATH, SAFETY, AND CONVENIENCE NOTES
 - 1. OBSERVE AND COMPLY WITH ALL FEDERAL, STATE, AND LOCAL LAWS REQUIRED FOR THE PROTECTION OF PUBLIC HEALTH, SAFETY, AND ENVIRONMENTAL QUALITY.
 - 2. APPLY FOR A CONSTRUCTION PERMIT WITH A NOISE POLLUTION CONTROL PLAN.
- F. STOCKPILING NOTES
 - 1. IF THE STOCKPILING WORK INVOLVES CONTAMINATED SOIL, THEN ALL STOCKPILING WORK SHALL BE DONE IN CONFORMANCE WITH APPLICABLE STATE AND FEDERAL REQUIREMENTS.
- G. GRUBBING AND CLEARING NOTES
 - 1. WHERE APPLICABLE AND FEASIBLE THE MEASURES TO CONTROL EROSION AND OTHER POLLUTANTS SHALL BE IN PLACE BEFORE ANY GRUBBING WORK IS INITIATED.

H. ADDITIONAL INFORMATION

1. THE FOLLOWING TABLES PROVIDE INFORMATION ON THE DESIGN SPEED OF CITY OF FRESNO ROADWAYS WITHIN THE LIMITS OF WORK:

DESIGN SPEED TABLE (NORTH OF CLINTON AVE)

STREET NAME	ALIGNMENT DESIGNATION	DESIGN SPEED (MPH)
GOLDEN STATE BLVD	G1	45
GOLDEN STATE BLVD	G2	40*
GOLDEN STATE BLVD	G3	45
GOLDEN STATE BLVD (SOUTH OF SWIFT AVE)	G3	35
SHAW AVE	SH	35
CORNELIA AVENUE/SANTA ANA AVENUE	CS	30
VETERANS BLVD	V	45 *
VETERANS BLVD RAMP	VR	25 *
BULLARD AVENUE	BU	40*
CLINTON AVE	С	45
ASHLAN AVE	AS	40
BARSTOW AVENUE	В	25
KATHRYN WAY	К	25
STATE AVENUE	ST	25
MARKET AVENUE	М	25
MOTEL DRIVE	AS6	25
ASHLAN/MOTEL DRIVE RIGHT TURN	AS7	25
JENNIFER AVENUE	J	25

^{*15%} DESIGN - PENDING UPDATES BY CITY OF FRESNO

DESIGN SPEED TABLE (SOUTH OF CLINTON AVE)

STREET	ALIGNMENT	DESIGN SPEED
NAME	DESIGNATION	(MPH)
W MCKINLEY AVE	MCK	30
W OLIVE AVE	OLI	35
BELMONT AVE	BEL	30
MCKINLEY AVE CONNECTOR	MCC	30
N GOLDEN STATE BLVD	GSB	50
N WEBER AVE	WEB	40
N H ST	HS	35
N WESLEY AVE	WSL	<20
N WEST AVE	WES	30
N CARUTH AVE	CAR	30
N FARRIS AVE	FAR	30

| DESIGNED BY | V. CHIO | DRAWN BY | D. ELKINS | CHECKED BY | J. MATSUI-DRURY | IN CHARGE | F. NOBARI | DATE | BY | CHK | APP | DESCRIPTION | DATE |

PROPOSED Preliminary Design

NOT FOR

AECOM
Technical Services, Inc.
2020 L Street, Suite 300
Sacramento, CA 958II
CH2MHILL

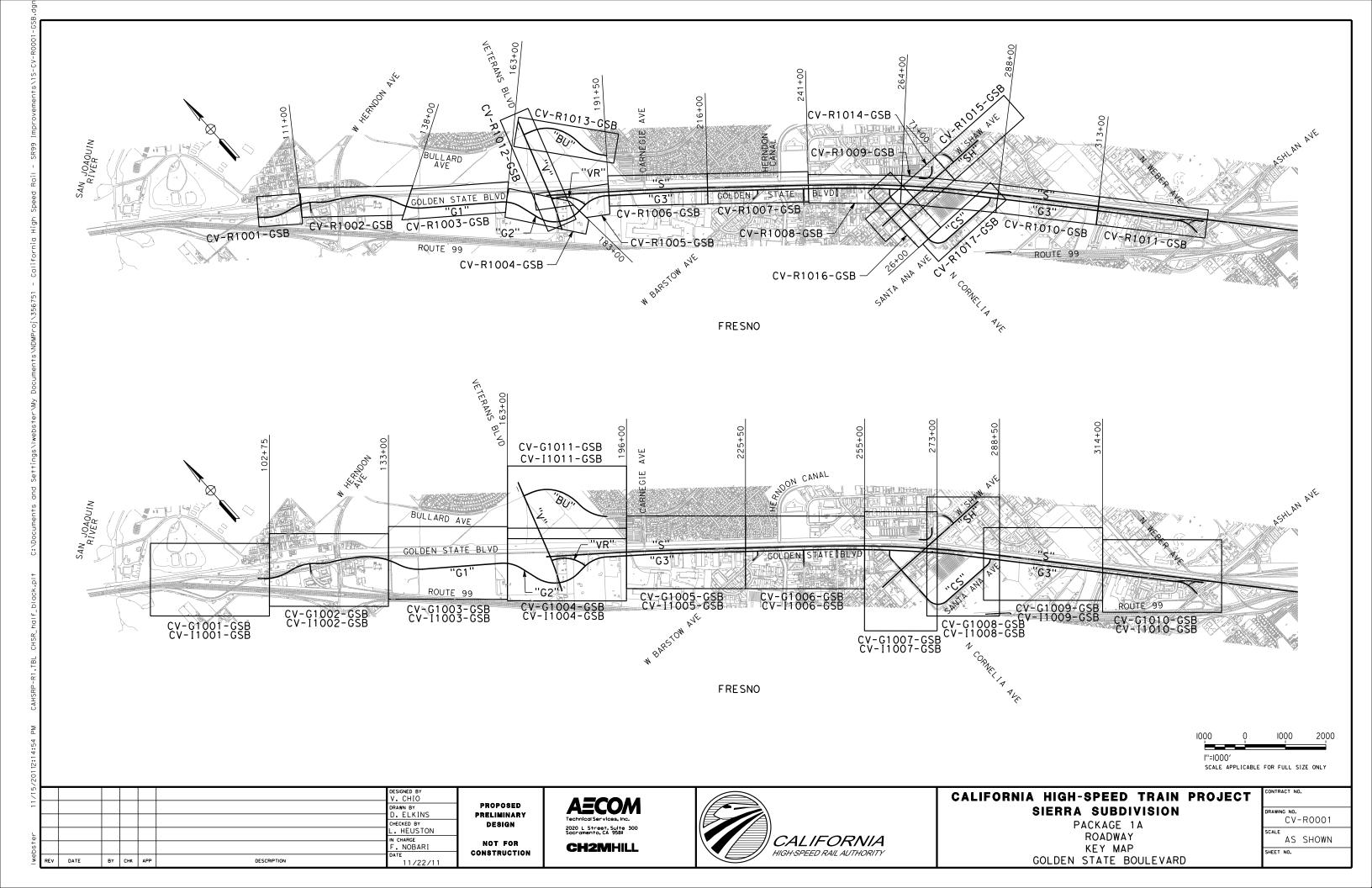


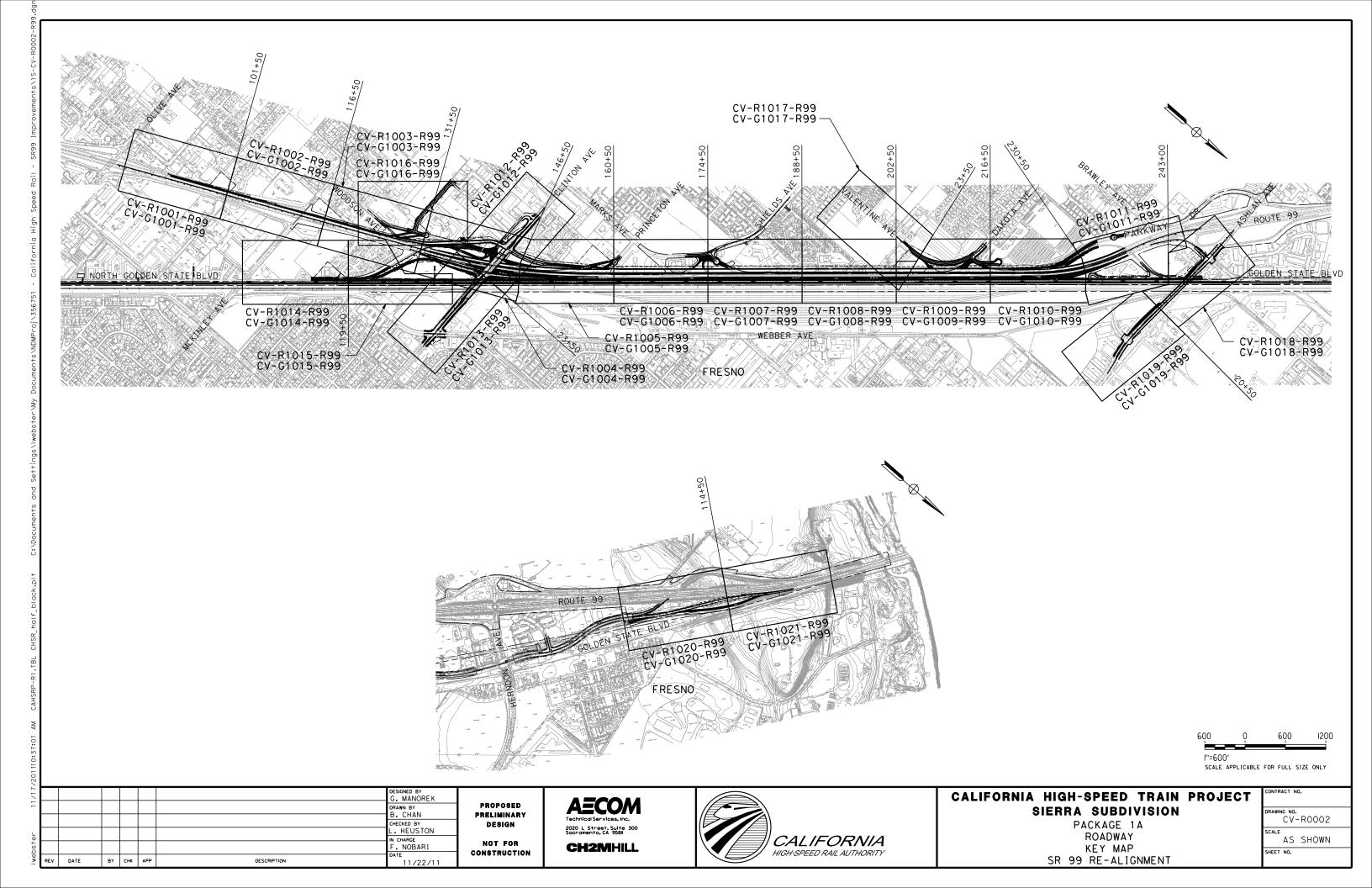
CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

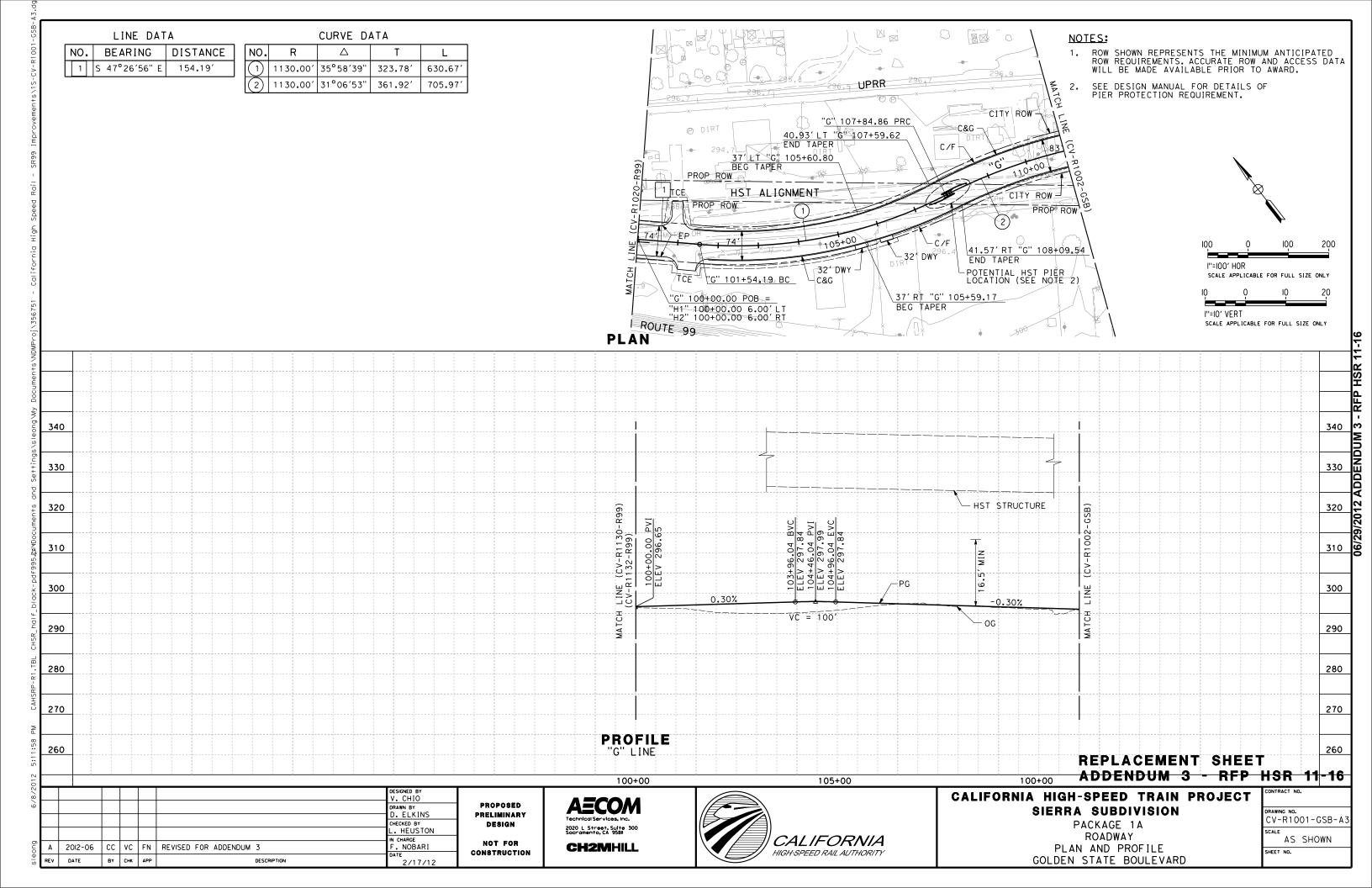
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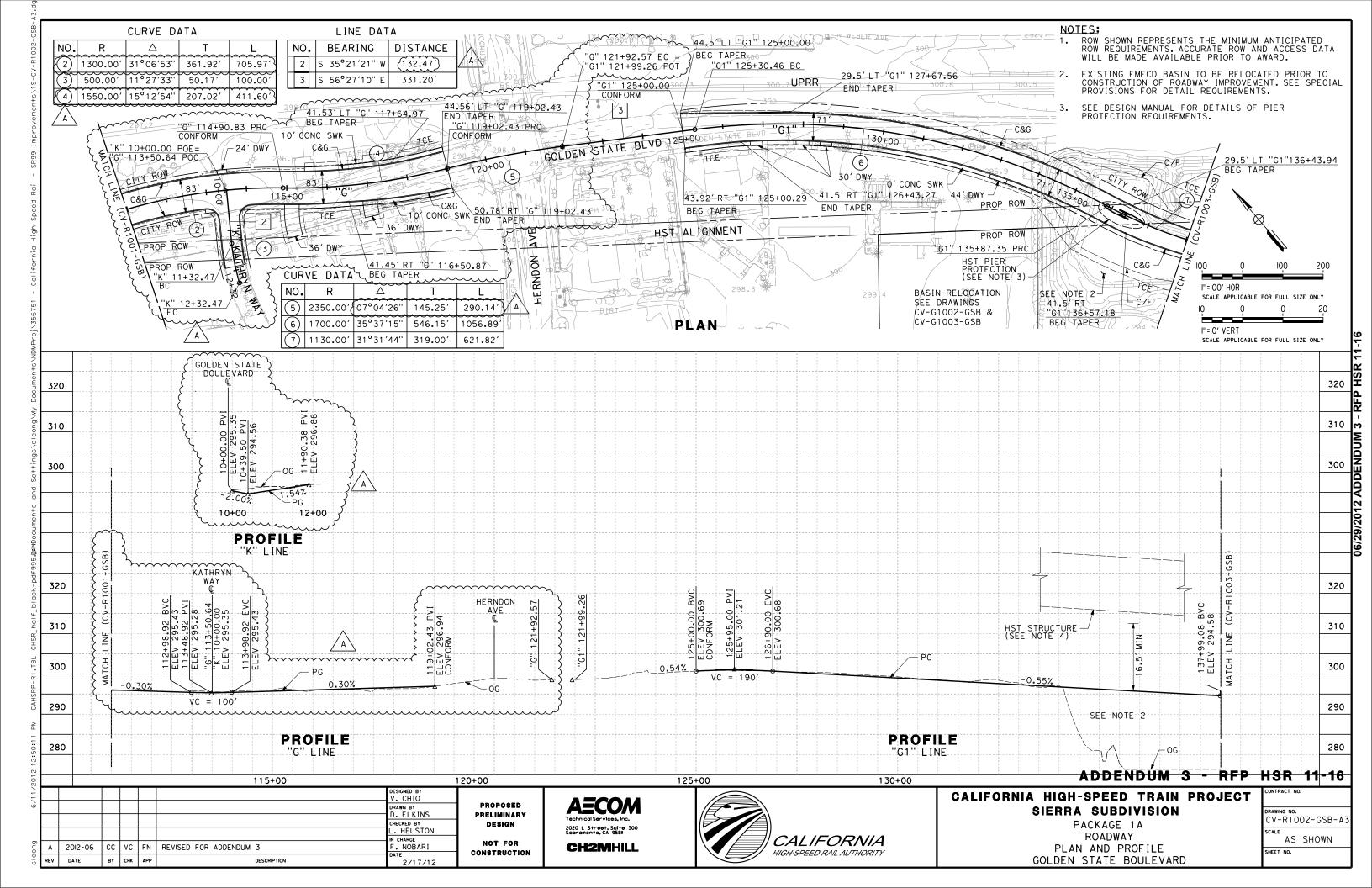
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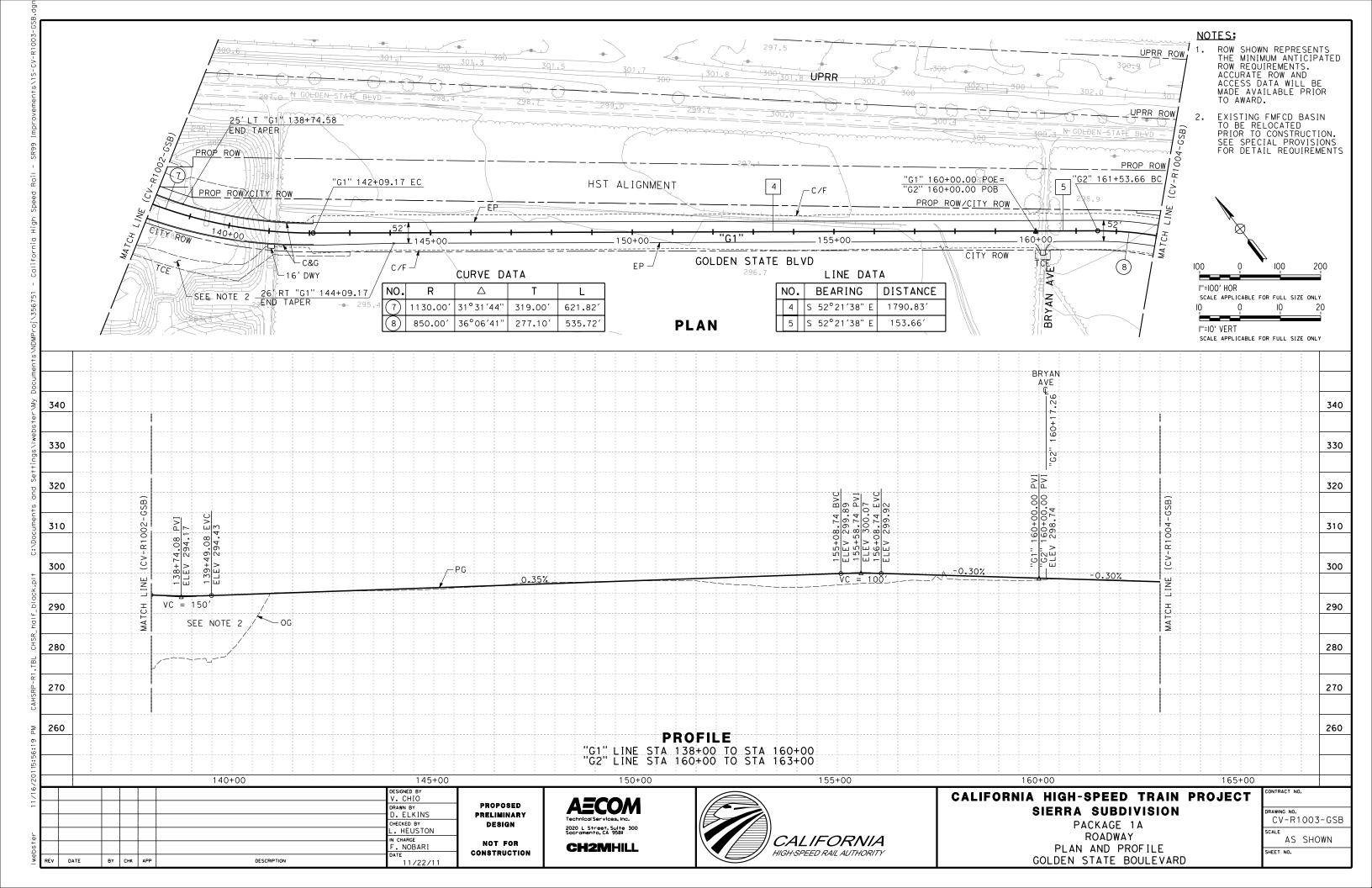
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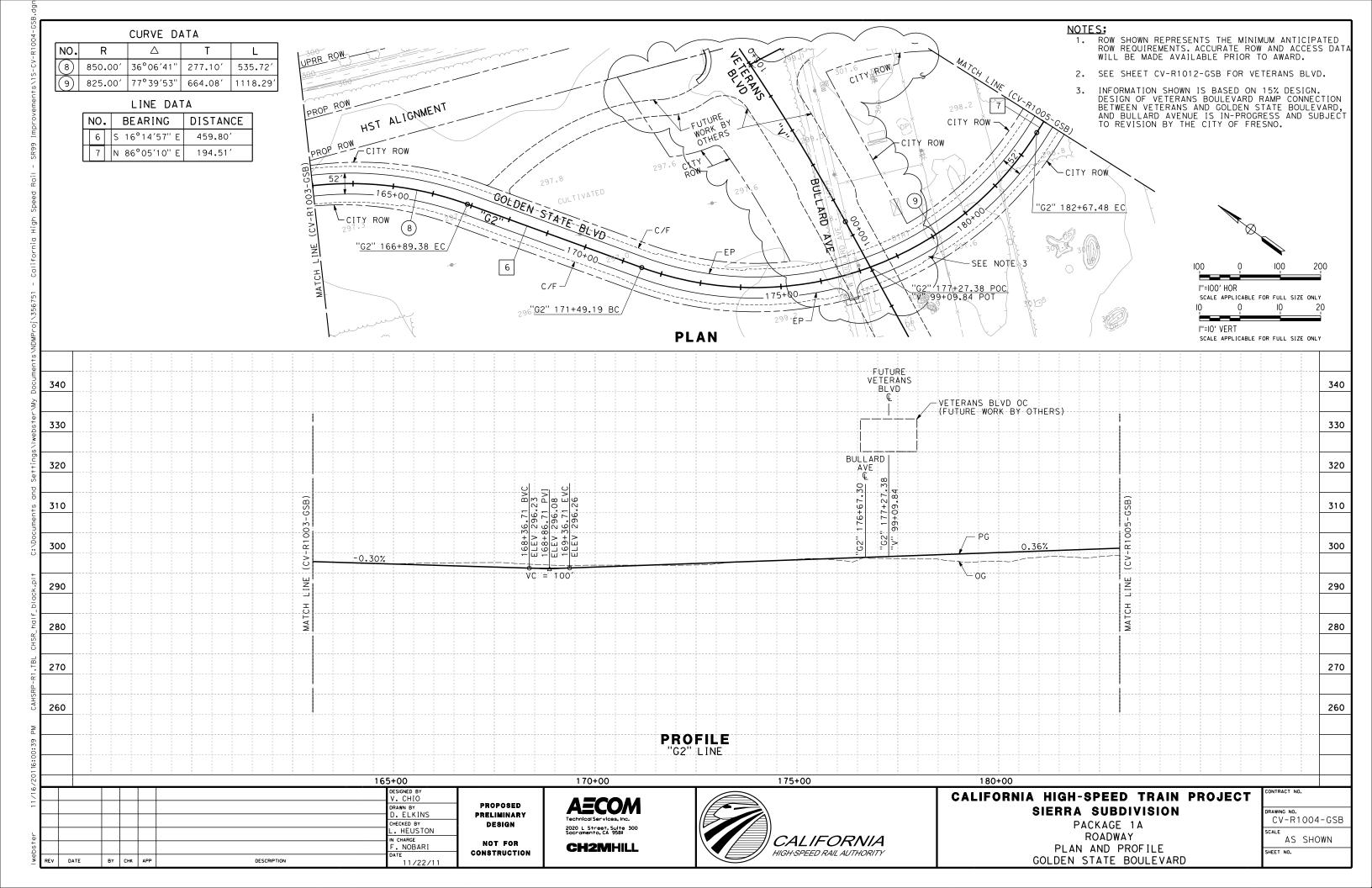


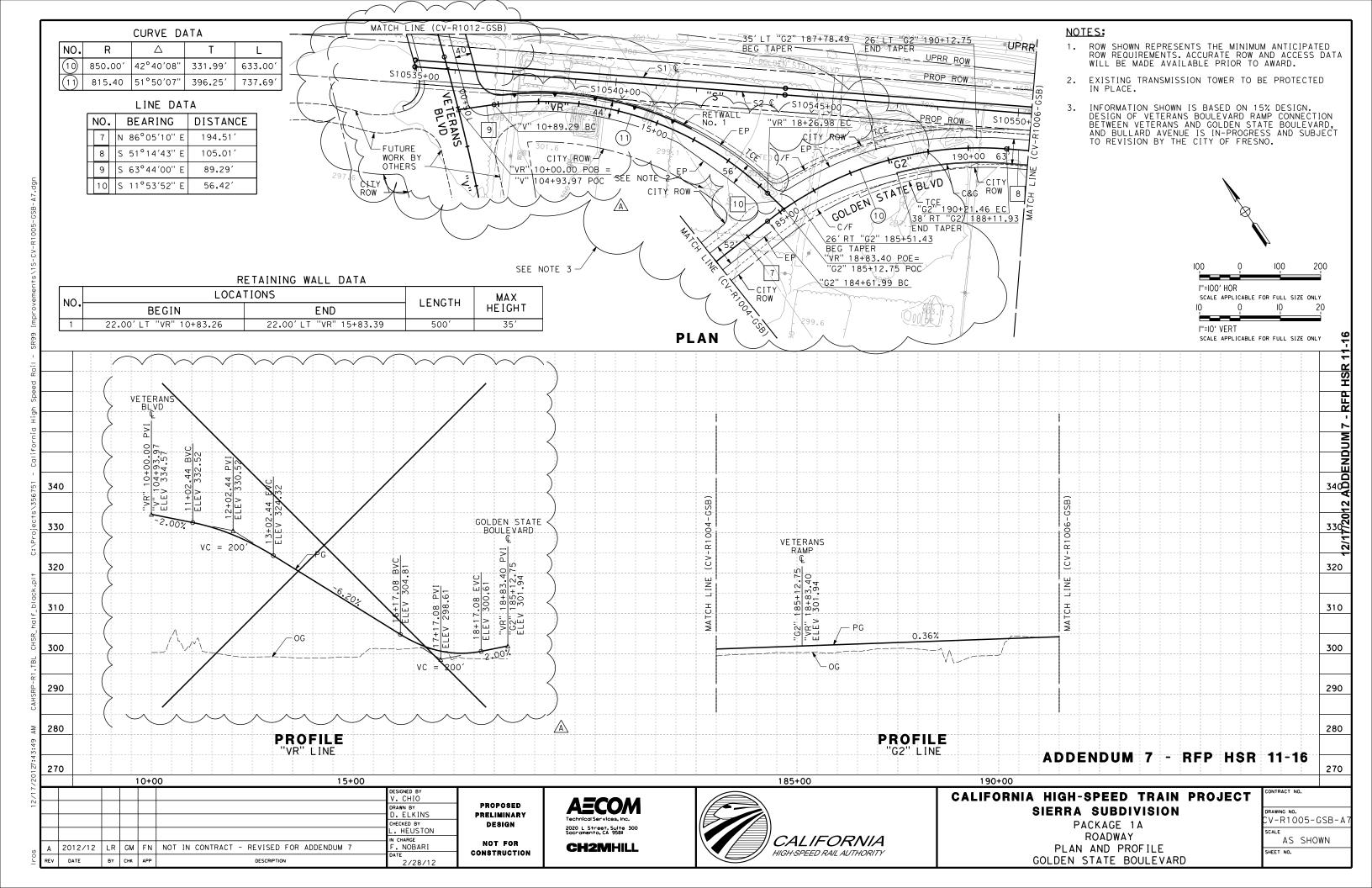


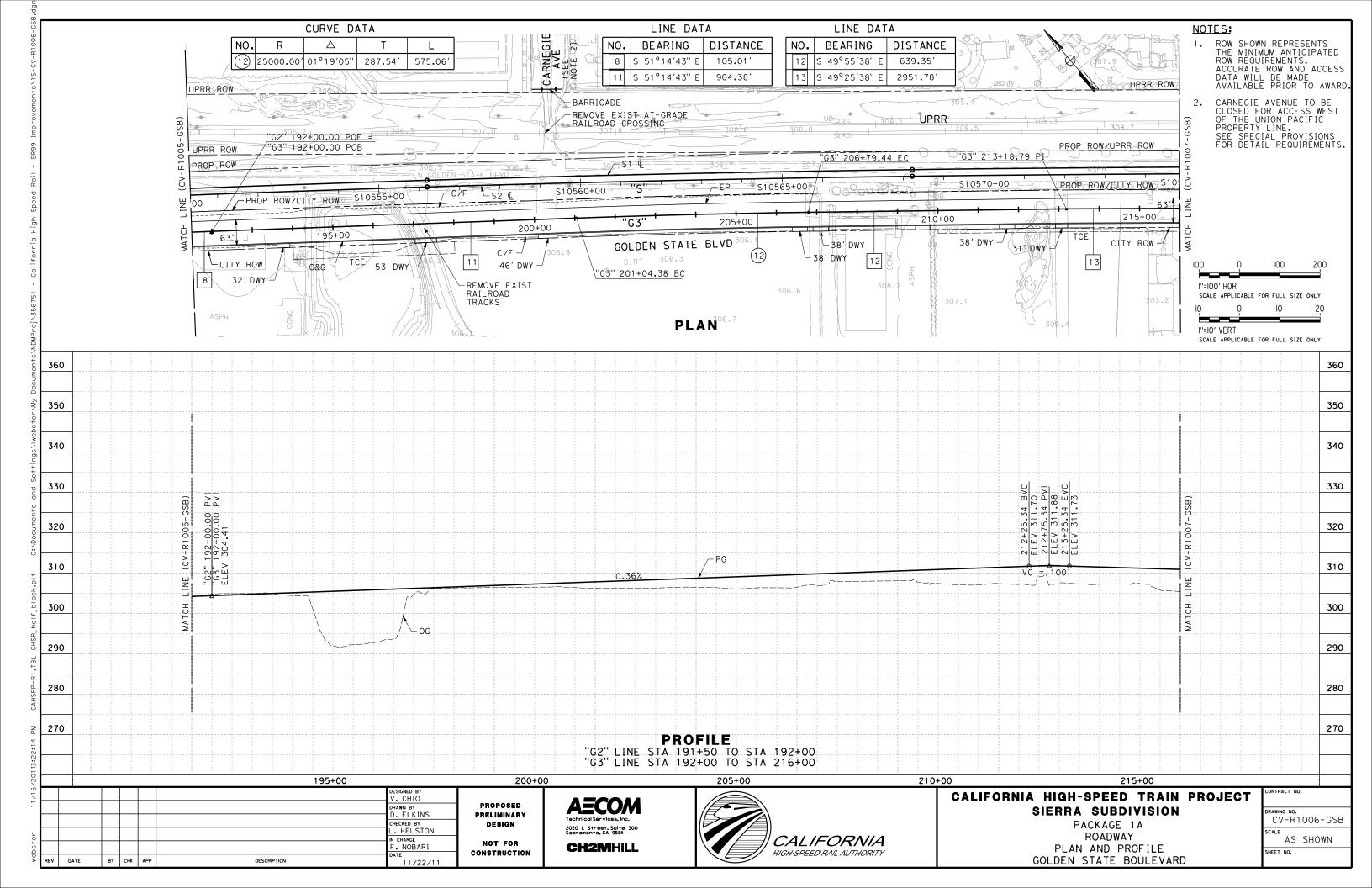


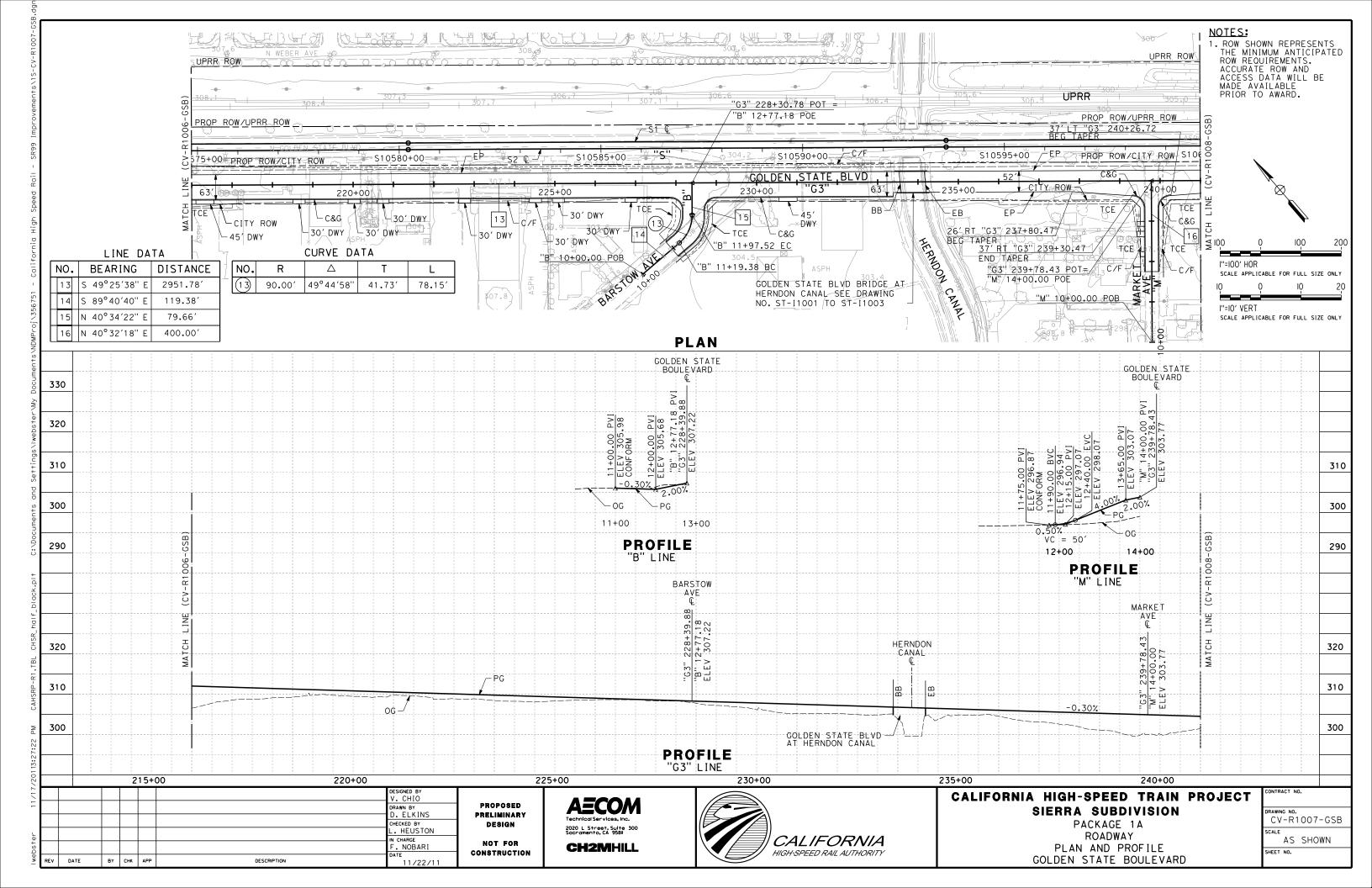


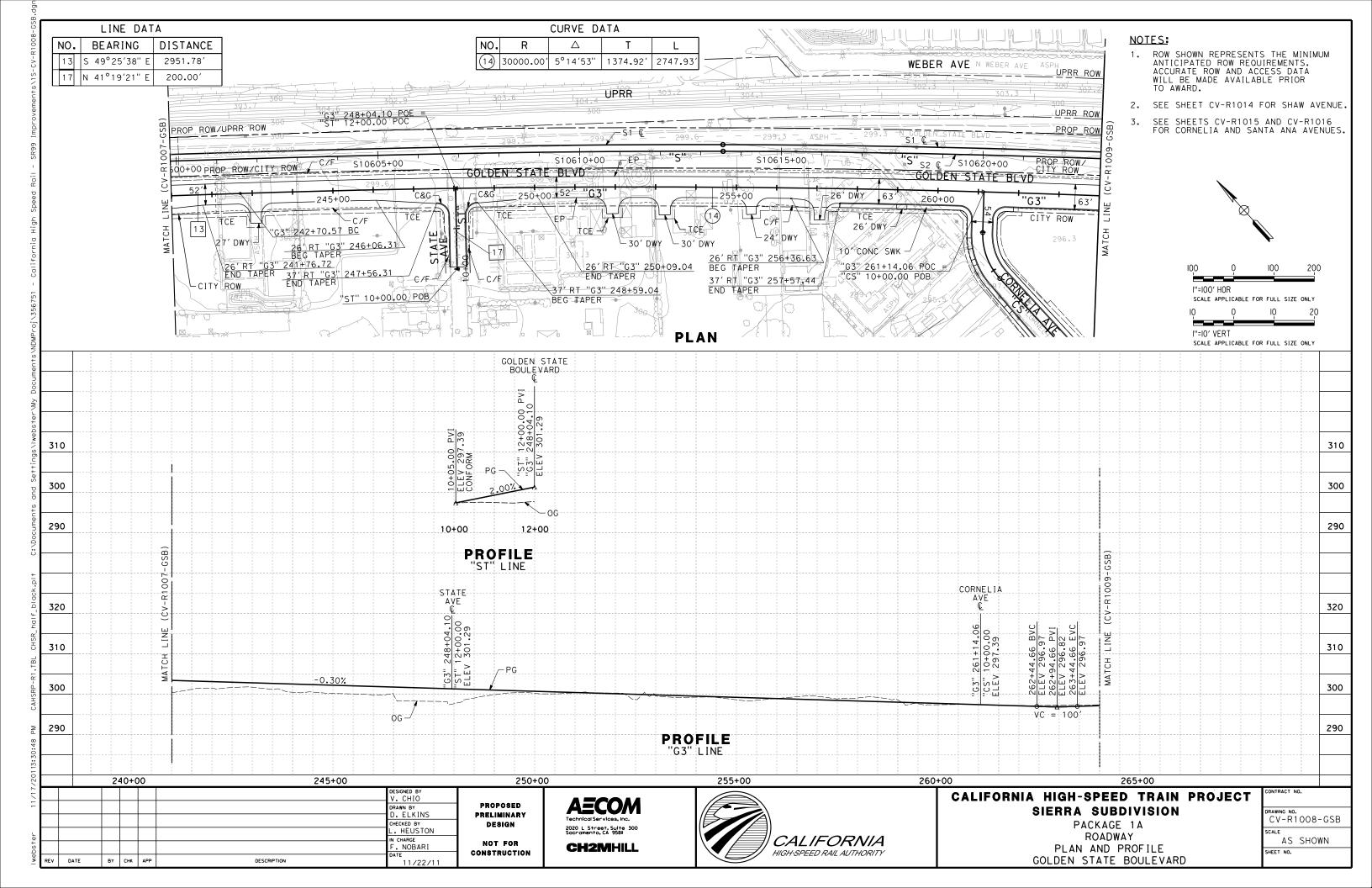


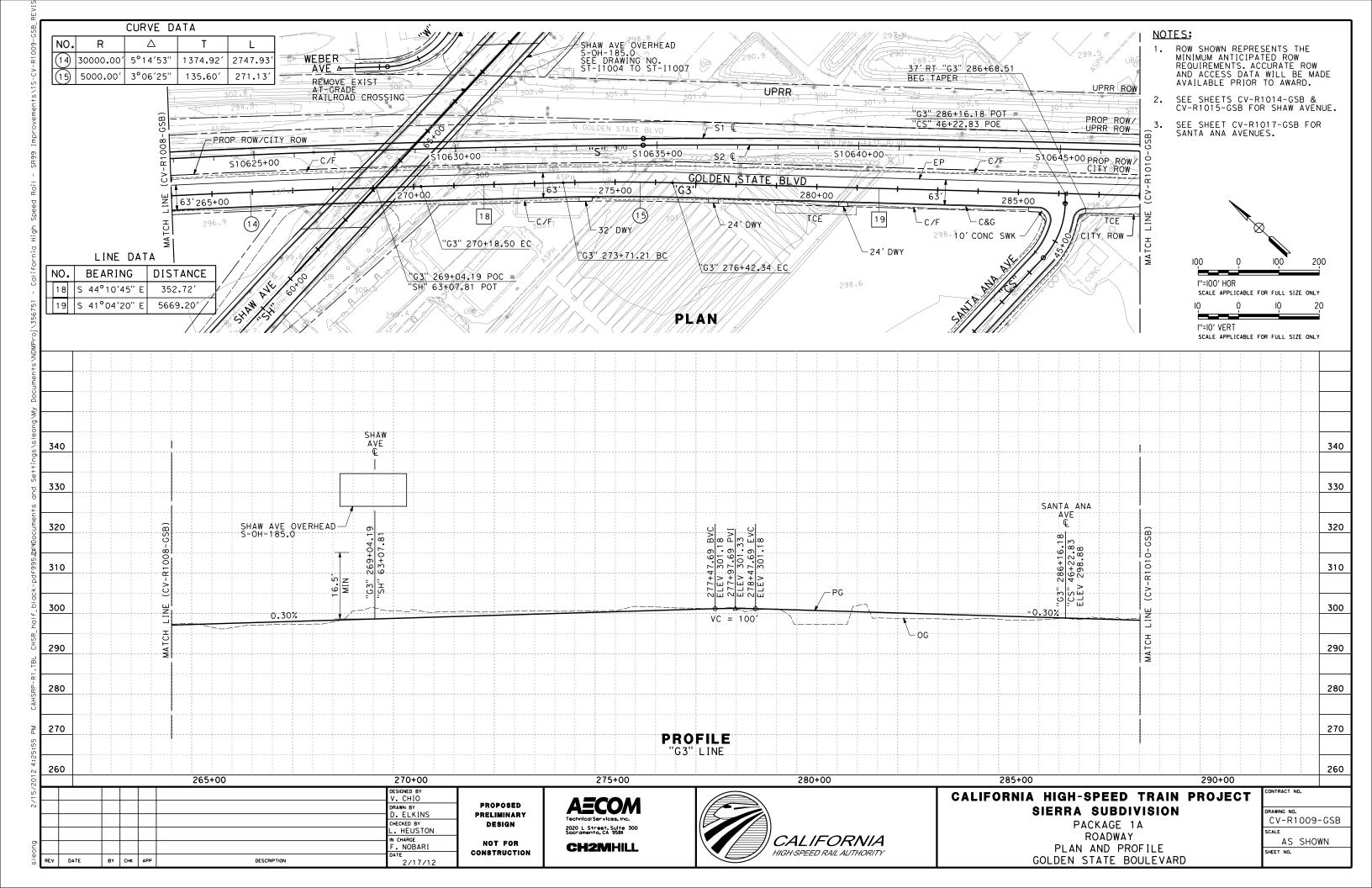


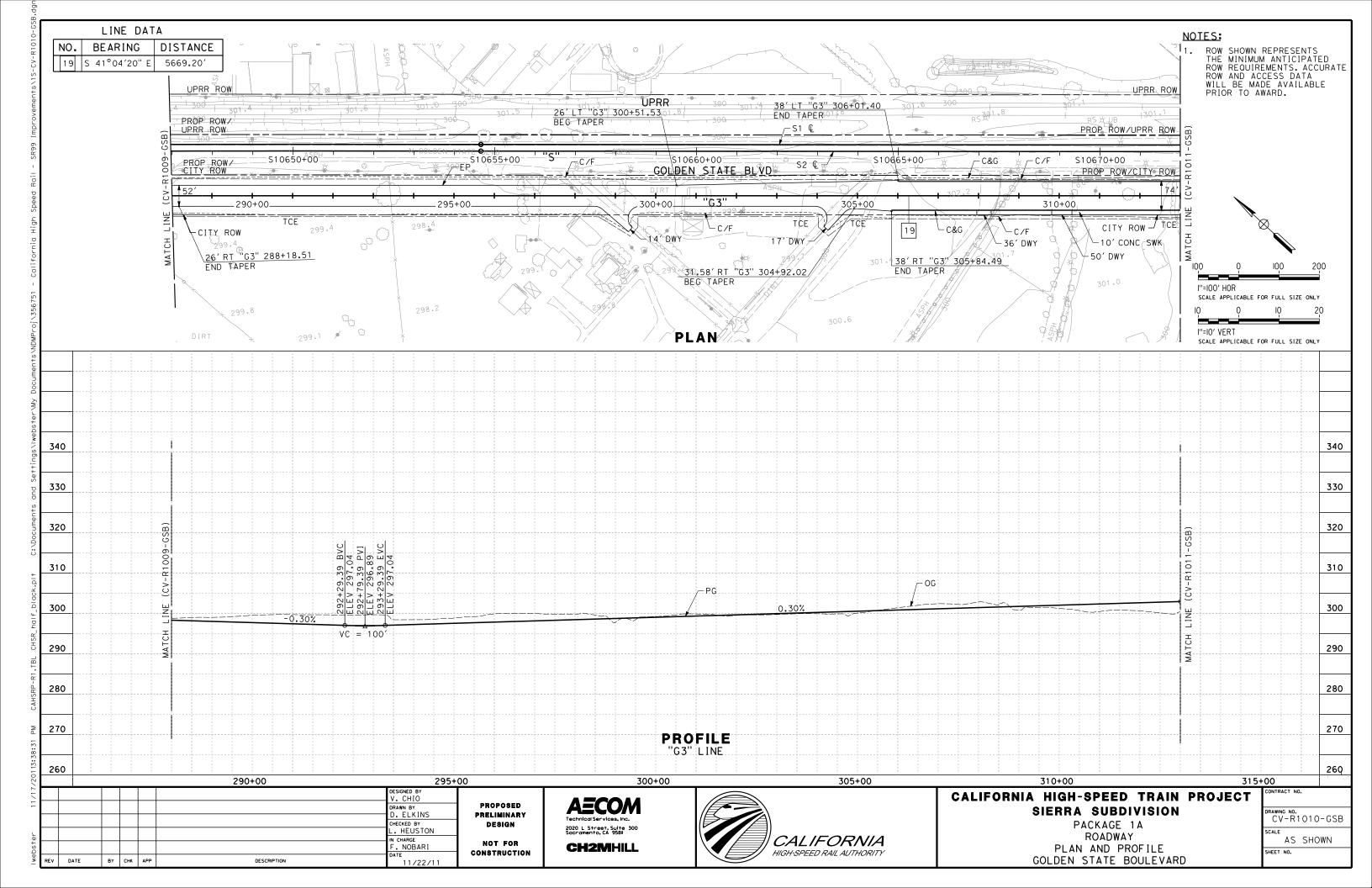


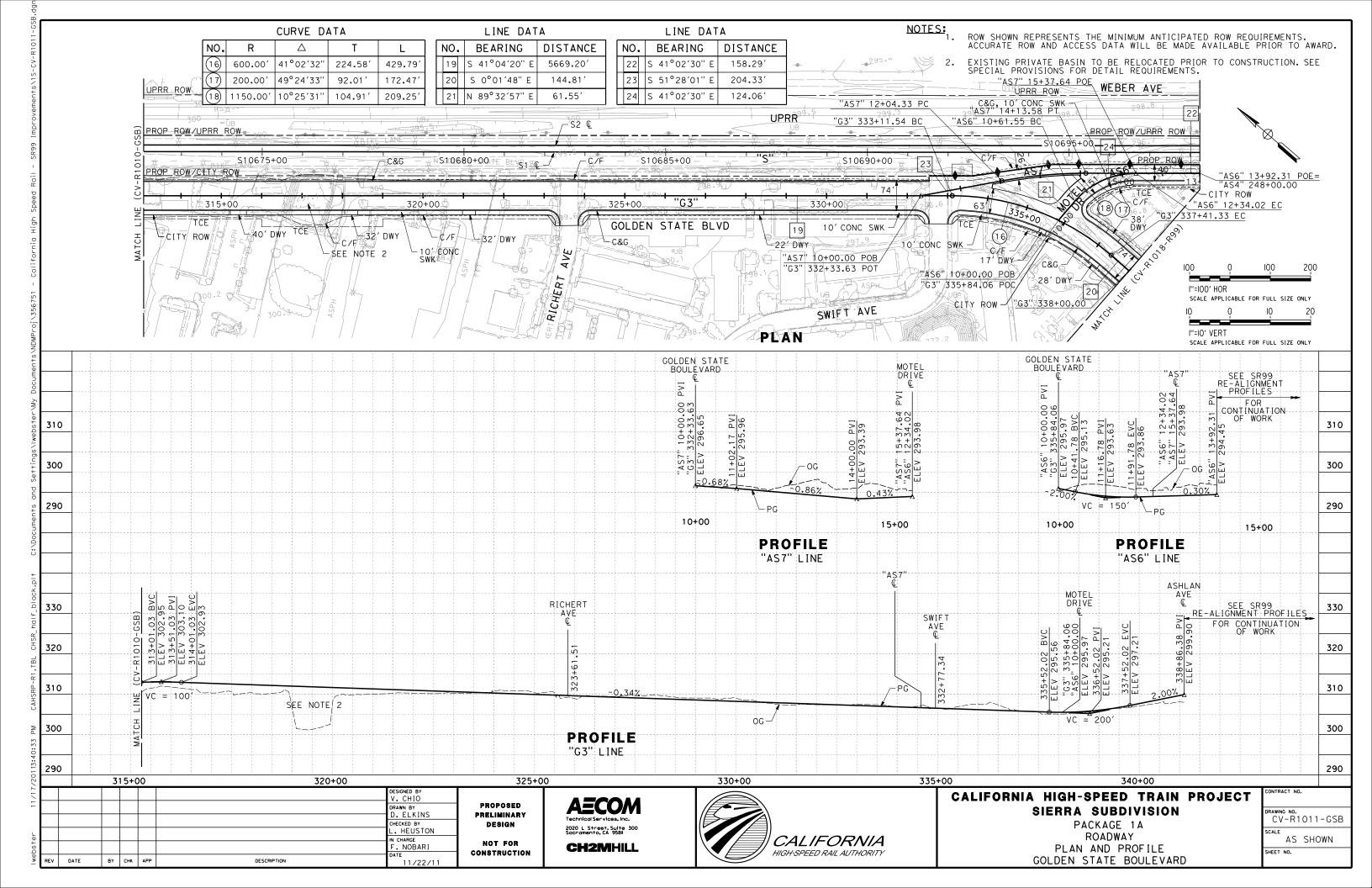


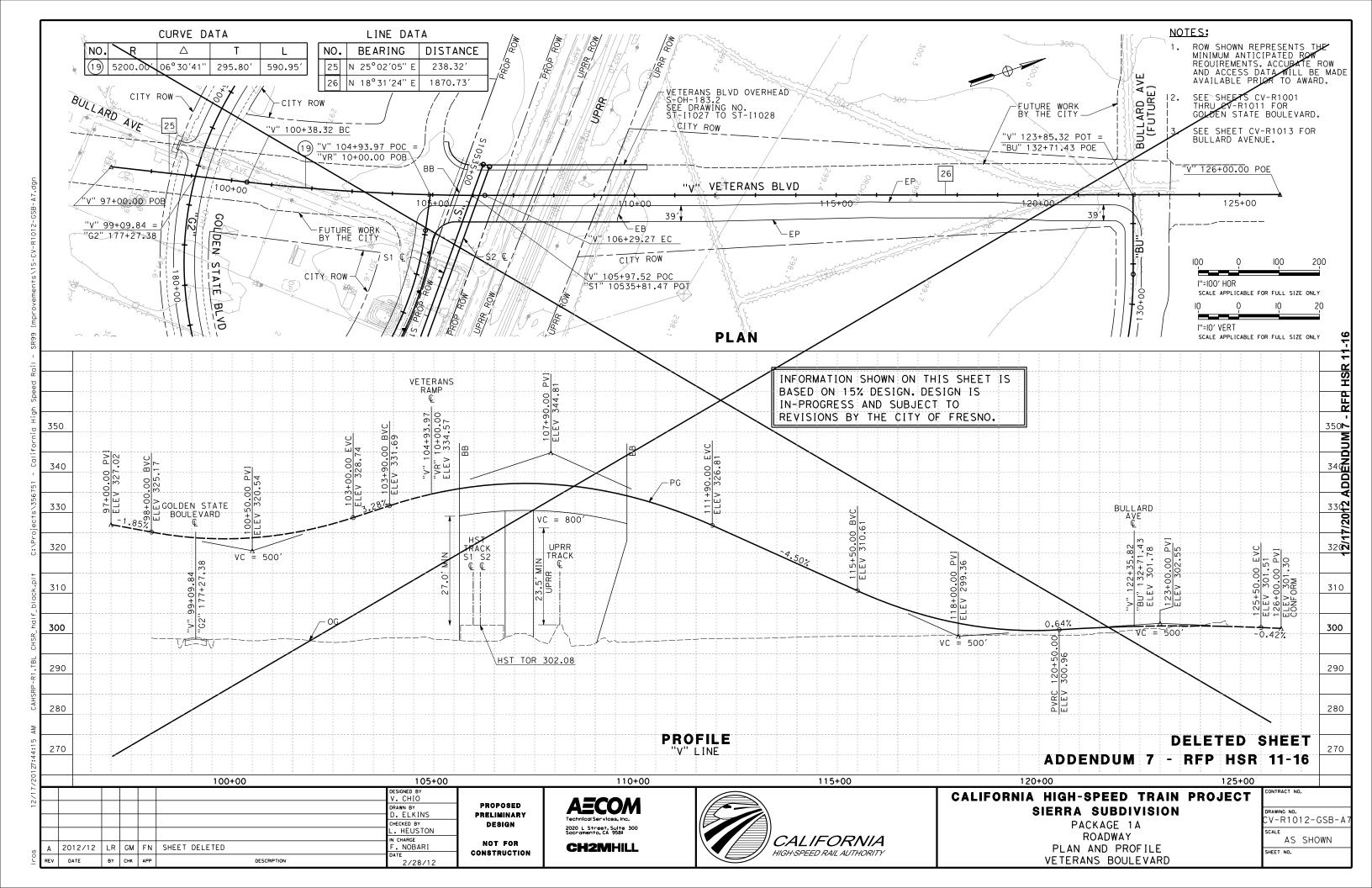


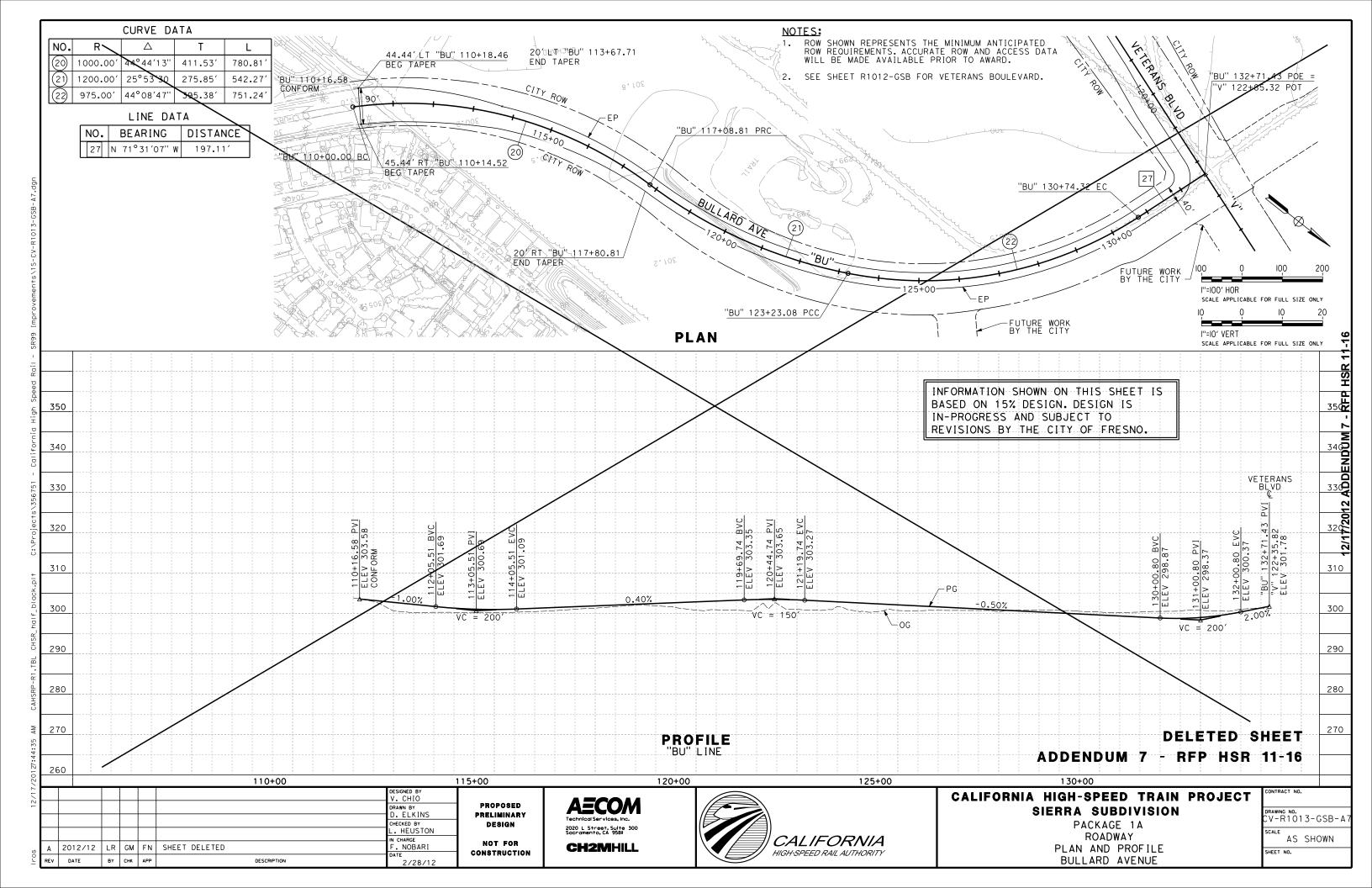


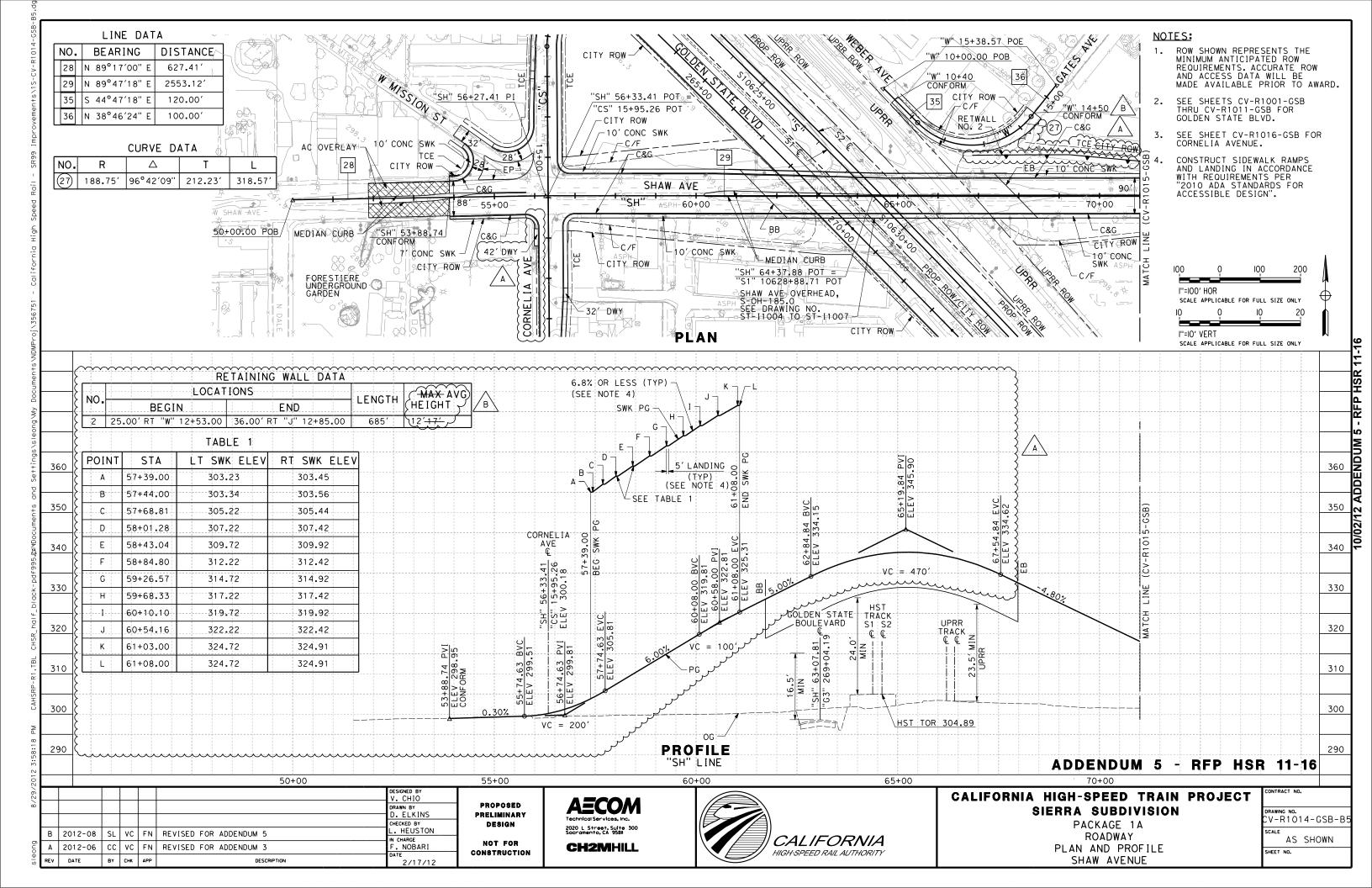


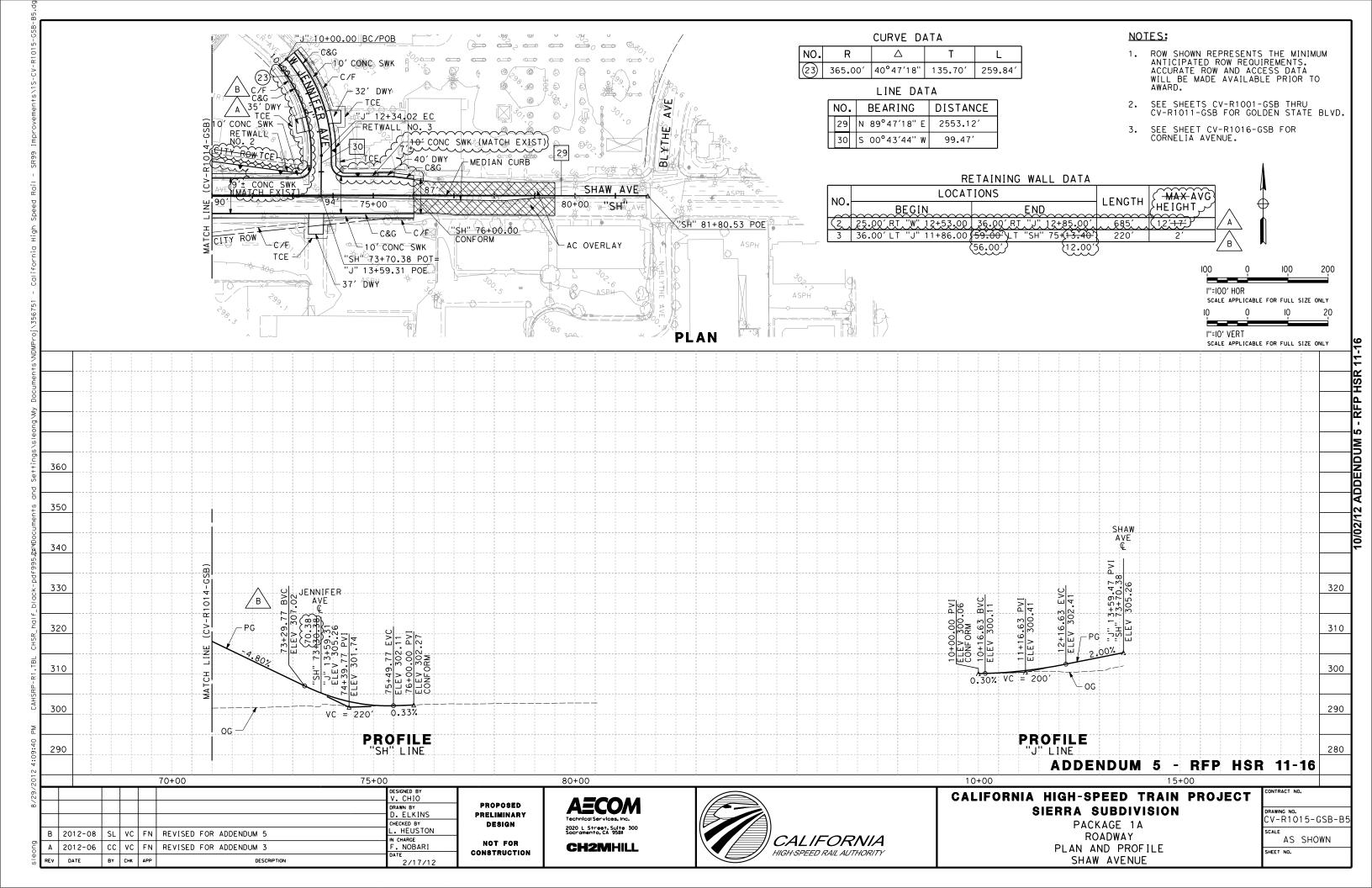


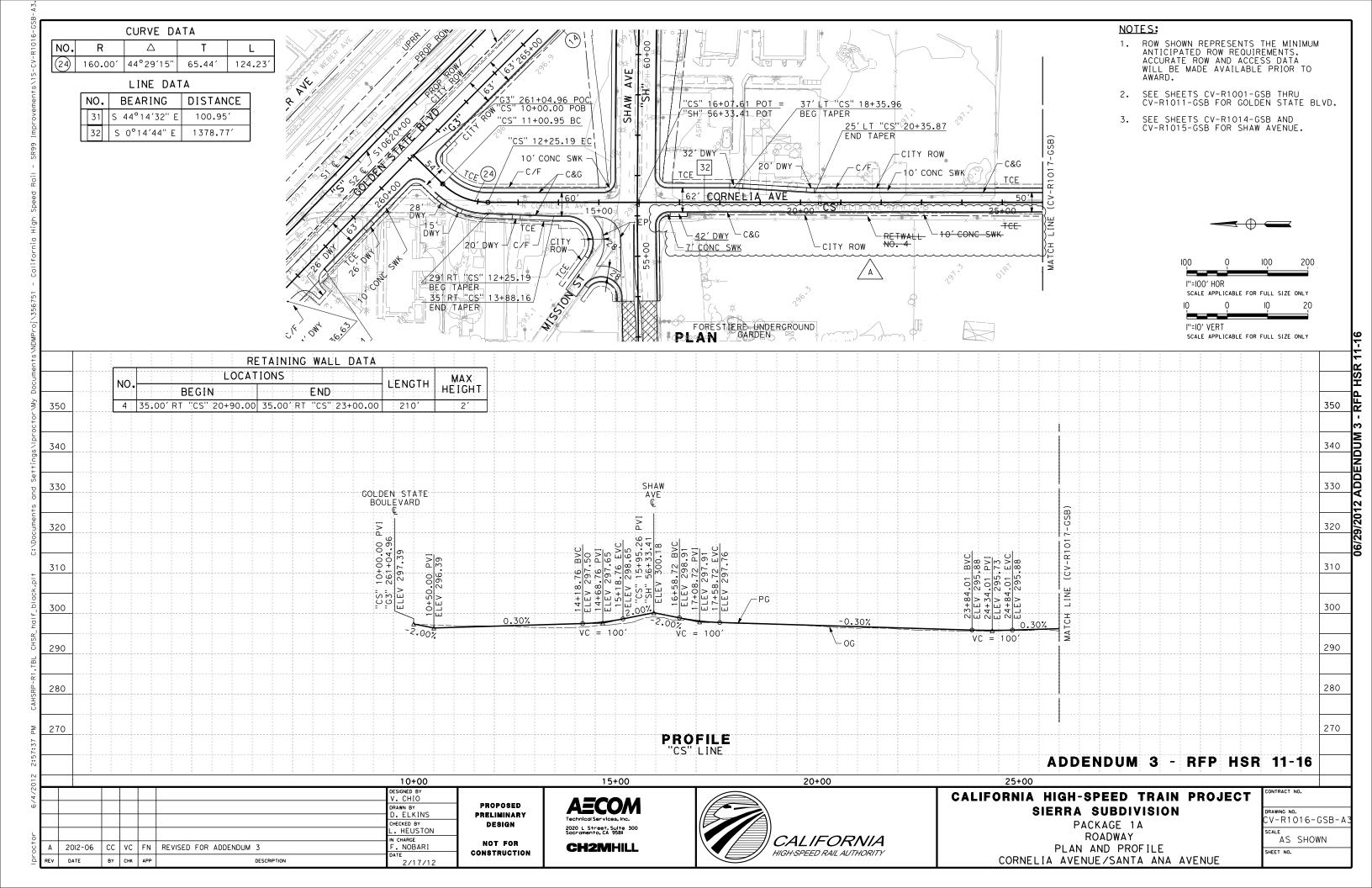


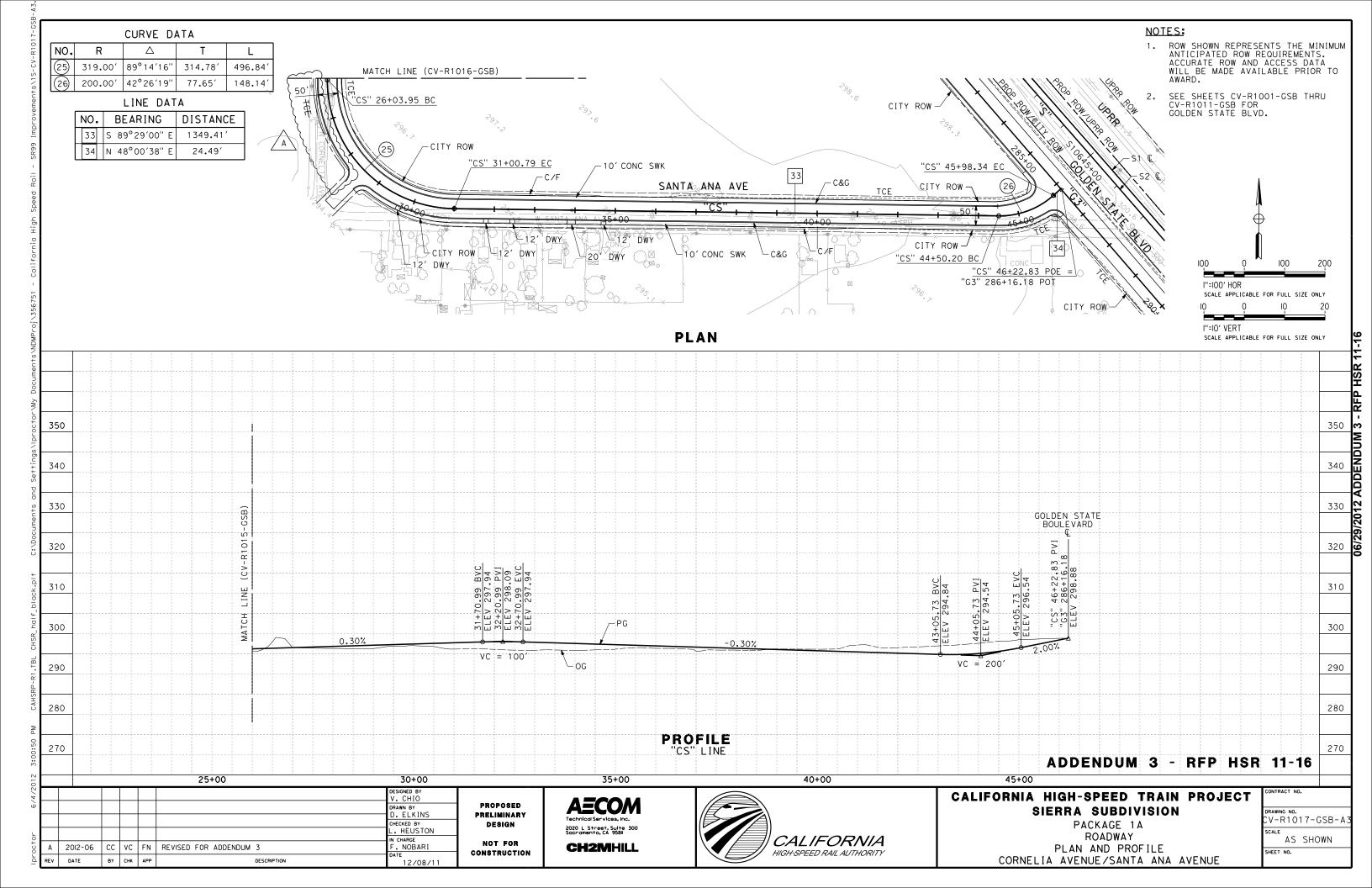


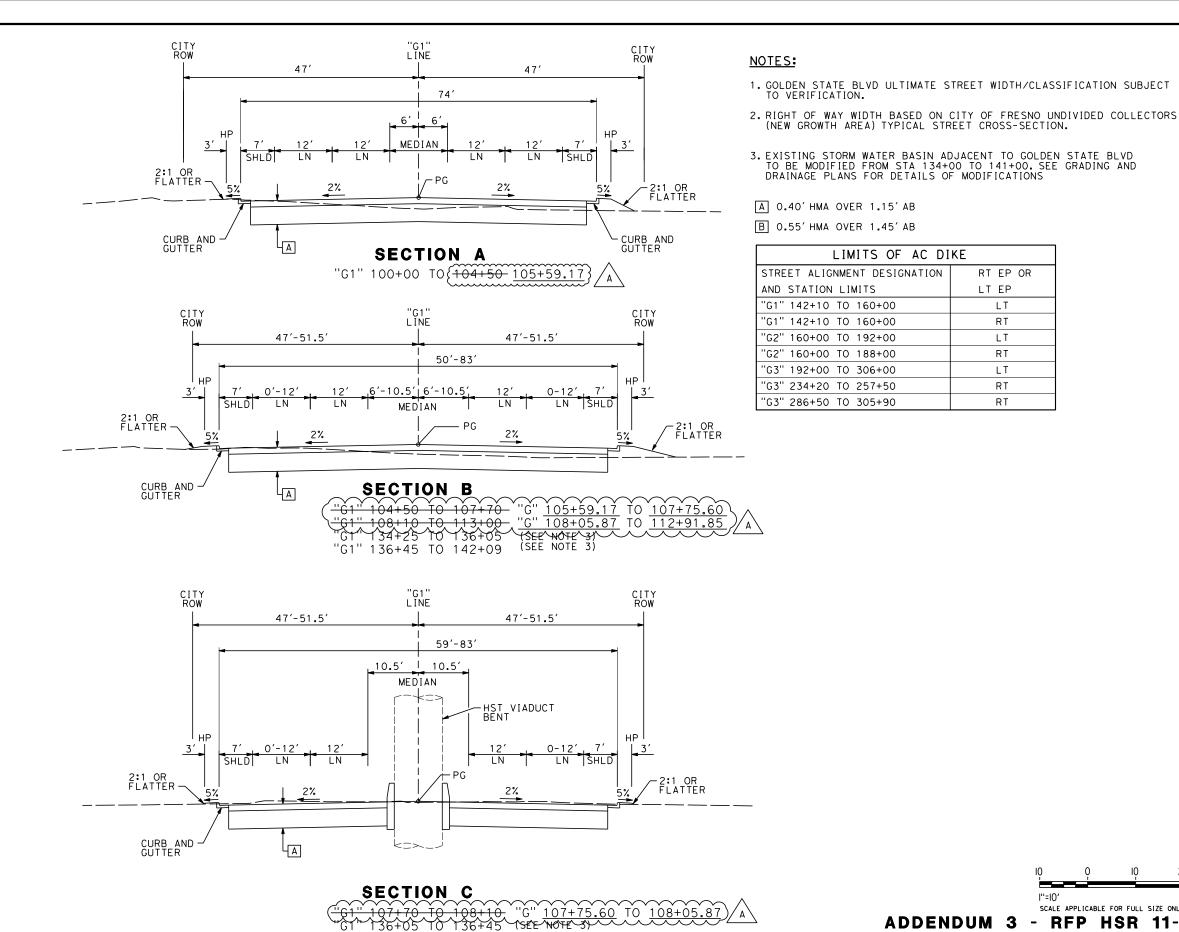












AECOM

2020 L Street, Suite 300 Sacramento, CA 95811

CH2MHILL

CALIFORNIA

HIGH-SPEED RAIL AUTHORITY

DESIGNED BY

D. ELKINS

CHECKED BY

CHARGE NOBARI

2/17/12

2012-06 CC VC FN REVISED FOR ADDENDUM 3

DESCRIPTION

BY CHK APP

PROPOSED

PRELIMINARY

DESIGN

NOT FOR

CONSTRUCTION

CALIFORNIA HIGH-SPEED TRAIN PROJECT

RT EP OR

LΤ

RT

LT

RT

LΤ RT

RT

LT EP

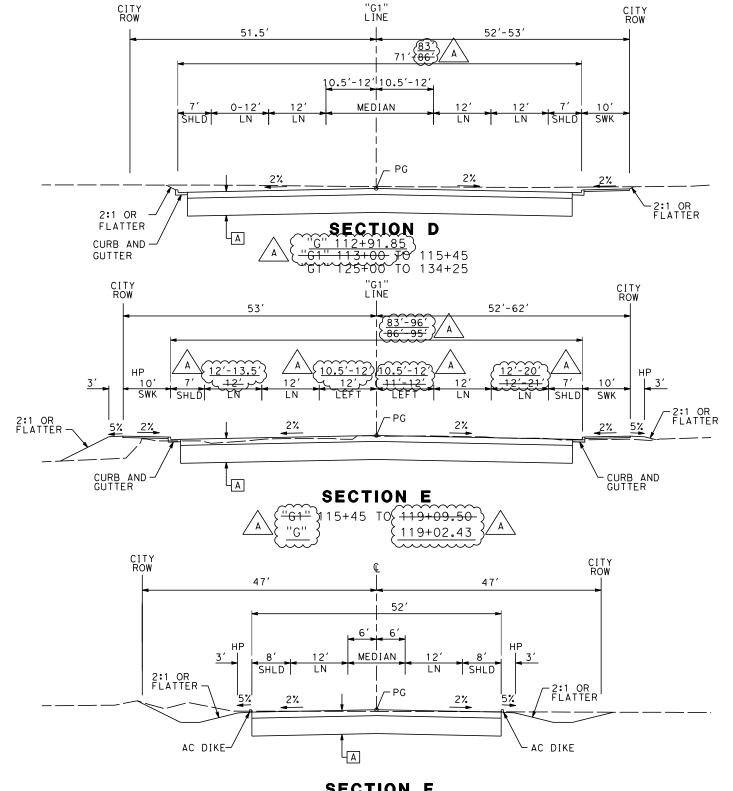
GOLDEN STATE BOULEVARD

CV-R3001-GSB AS SHOWN SHEET NO.

ADDENDUM 3 - RFP HSR 11-16

SIERRA SUBDIVISION

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		ROA[AWC	Υ	
Τ.	YPI	CAL	SE	CTI	NC
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- A 0.40' HMA OVER 1.15' AB
- B 0.55' HMA OVER 1.45' AB

SECTION F

- "G1" 142+09 TO 160+00 "G2" 160+00 TO 185+00 "G3" 286+50 TO 303+75

SCALE APPLICABLE FOR FULL SIZE ONLY ADDENDUM 3 - RFP HSR 11-16 CALIFORNIA HIGH-SPEED TRAIN PROJECT CONTRACT NO.

SIERRA SUBDIVISION PACKAGE 1A ROADWAY TYPICAL SECTION

GOLDEN STATE BOULEVARD

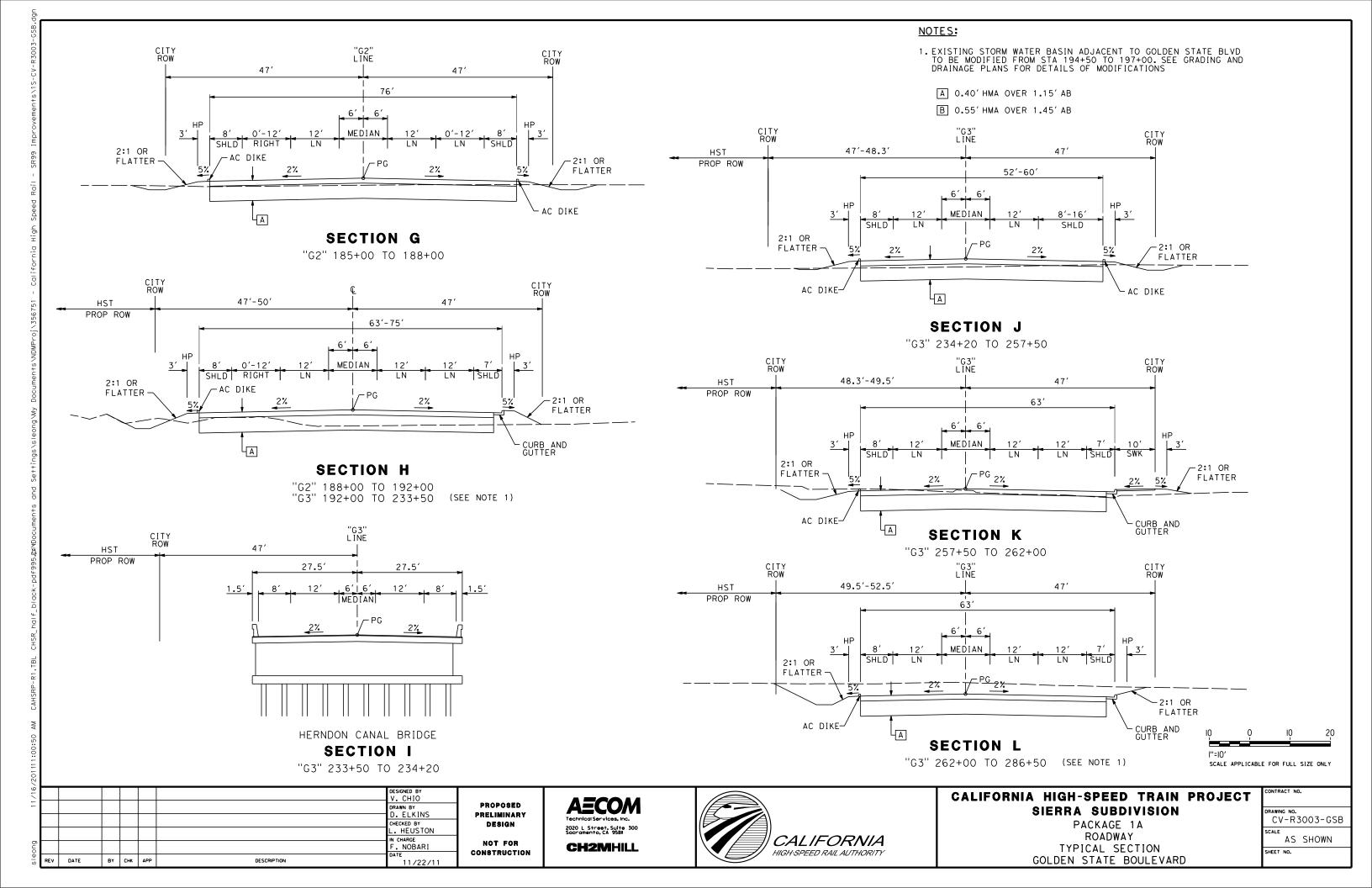
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SHEET	NO.		

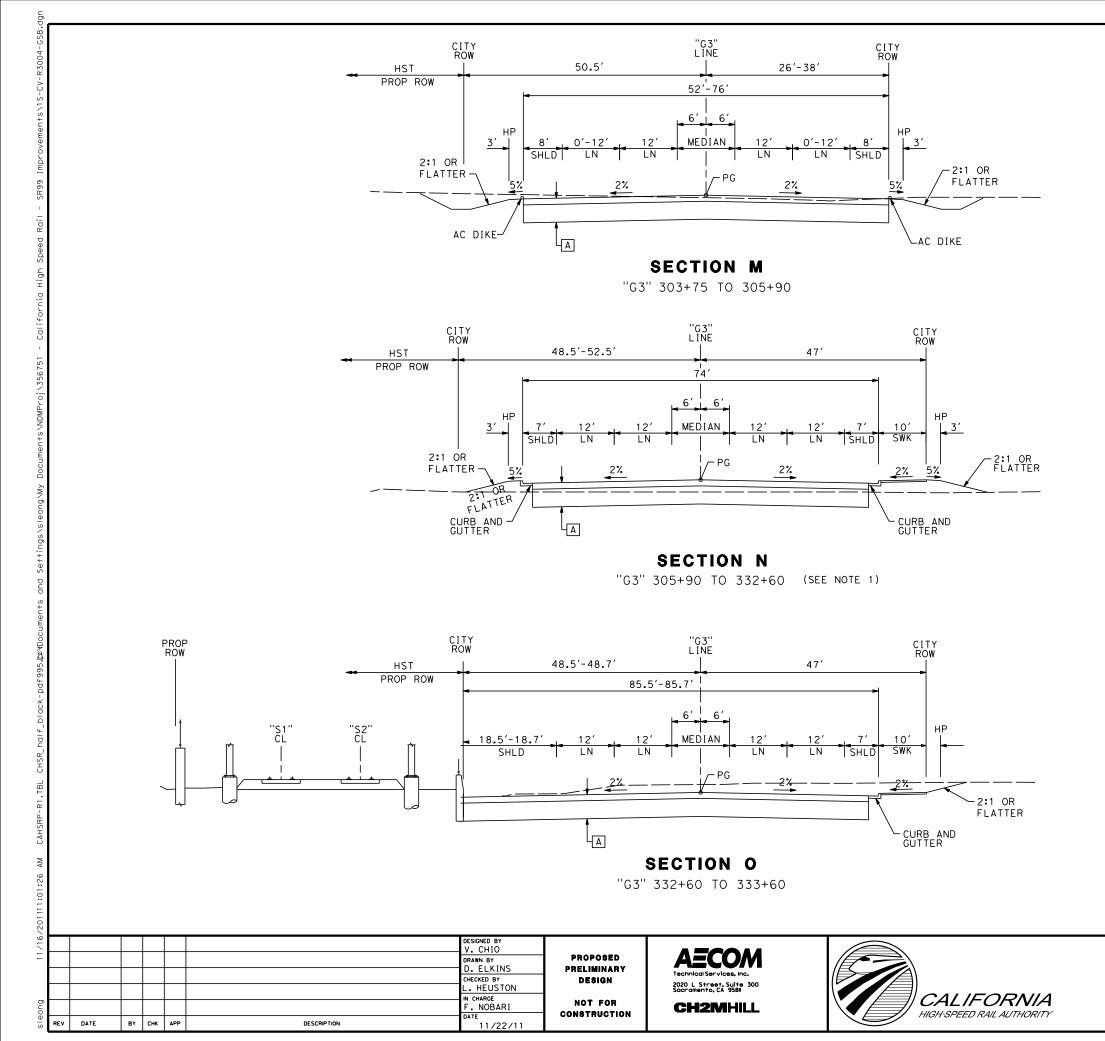
						DESIGNED BY V. CHIO	
						DRAWN BY	P PR
						CHECKED BY	
						L. HEUSTON IN CHARGE	
Α	2012-06	СС	VC	FN	REVISED FOR ADDENDUM 3	F. NOBARI	CON
REV	DATE	ВҮ	СНК	APP	DESCRIPTION	2/17/12	

PROPOSED RELIMINARY DESIGN NOT FOR NSTRUCTION

AECOM 2020 L Street, Suite 300 Sacramento, CA 958II **CH2MHILL**







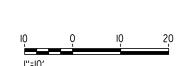
- 1. EXISTING STORM WATER BASIN ADJACENT TO GOLDEN STATE BLVD TO BE MODIFIED FROM STA 279+50 TO 281+00 AND STA 317+00 TO 317+50. SEE GRADING AND DRAINAGE PLANS FOR DETAILS OF MODIFICATIONS
- A 0.40' HMA OVER 1.15' AB
- B 0.55' HMA OVER 1.45' AB



CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A ROADWAY TYPICAL SECTION GOLDEN STATE BOULEVARD DRAWING NO.
CV-R3004-GSB

SCALE
AS SHOWN
SHEET NO.



SCALE APPLICABLE FOR FULL SIZE ONLY

PROPOSED
PRELIMINARY
DESIGN
NOT FOR

CONSTRUCTION

DESIGNED BY

DRAWN BY
D. ELKINS

CHECKED BY

CHARGE NOBARI

11/22/11

DATE

BY CHK APP

DESCRIPTION

Technical Services, Inc.
2020 L Street, Sulte 300
Socramento, CA 958II
CH2MHILL



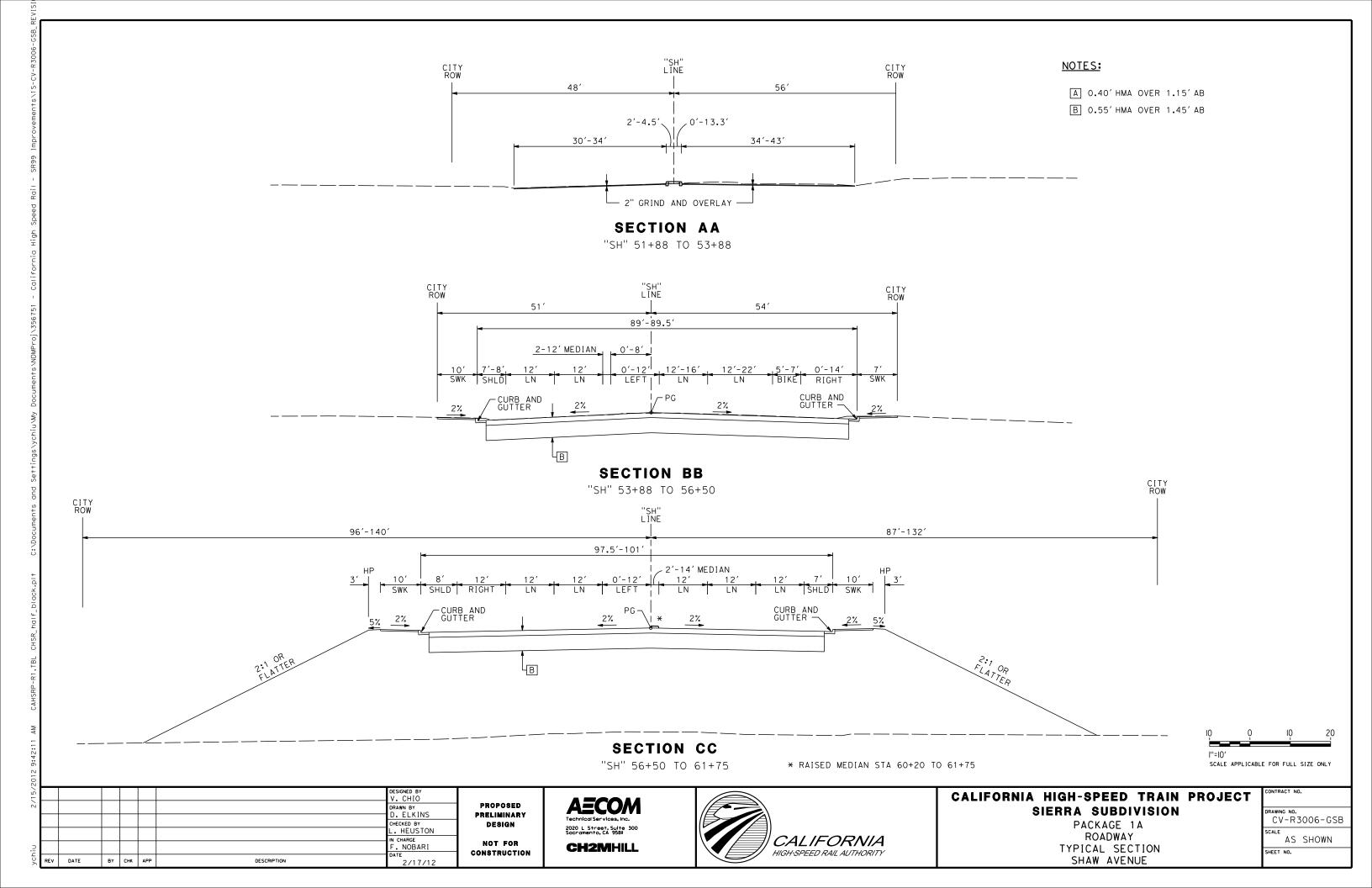
CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A
ROADWAY
TYPICAL SECTION
GOLDEN STATE BOULEVARD

NOTES:

A 0.40' HMA OVER 1.15' AB
B 0.55' HMA OVER 1.45' AB

CONTRACT NO.							
DRAWING NO.							
CV-R3005-GSB							
SCALE							
	ΑS	SHOWN					
SHEET	NO.						



CHECKED BY B 2012-08 SL VC FN REVISED FOR ADDENDUM 5 A 2012-06 CC VC FN REVISED FOR ADDENDUM 3

PROPOSED PRELIMINARY DESIGN NOT FOR CONSTRUCTION

DESIGNED BY

DRAWN BY

CHARGE . NOBARI

AECOM 2020 L Street, Suite 300 Sacramento, CA 95811 **CH2MHILL**





NOTES:

A 0.40' HMA OVER 1.15' AB

B 0.55' HMA OVER 1.45' AB

SHAW AVENUE OVERHEAD

"SH" LINE

90'-97.5'

SHLD

LN

4'-11'

55'-62.5'

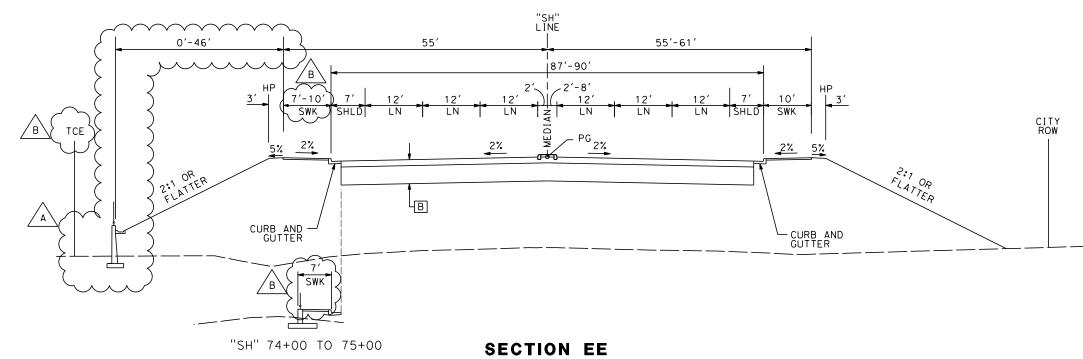
LN

SWK

SHLD

SECTION DD

"SH" 61+75 TO 68+00



"SH" 68+00 TO 76+00

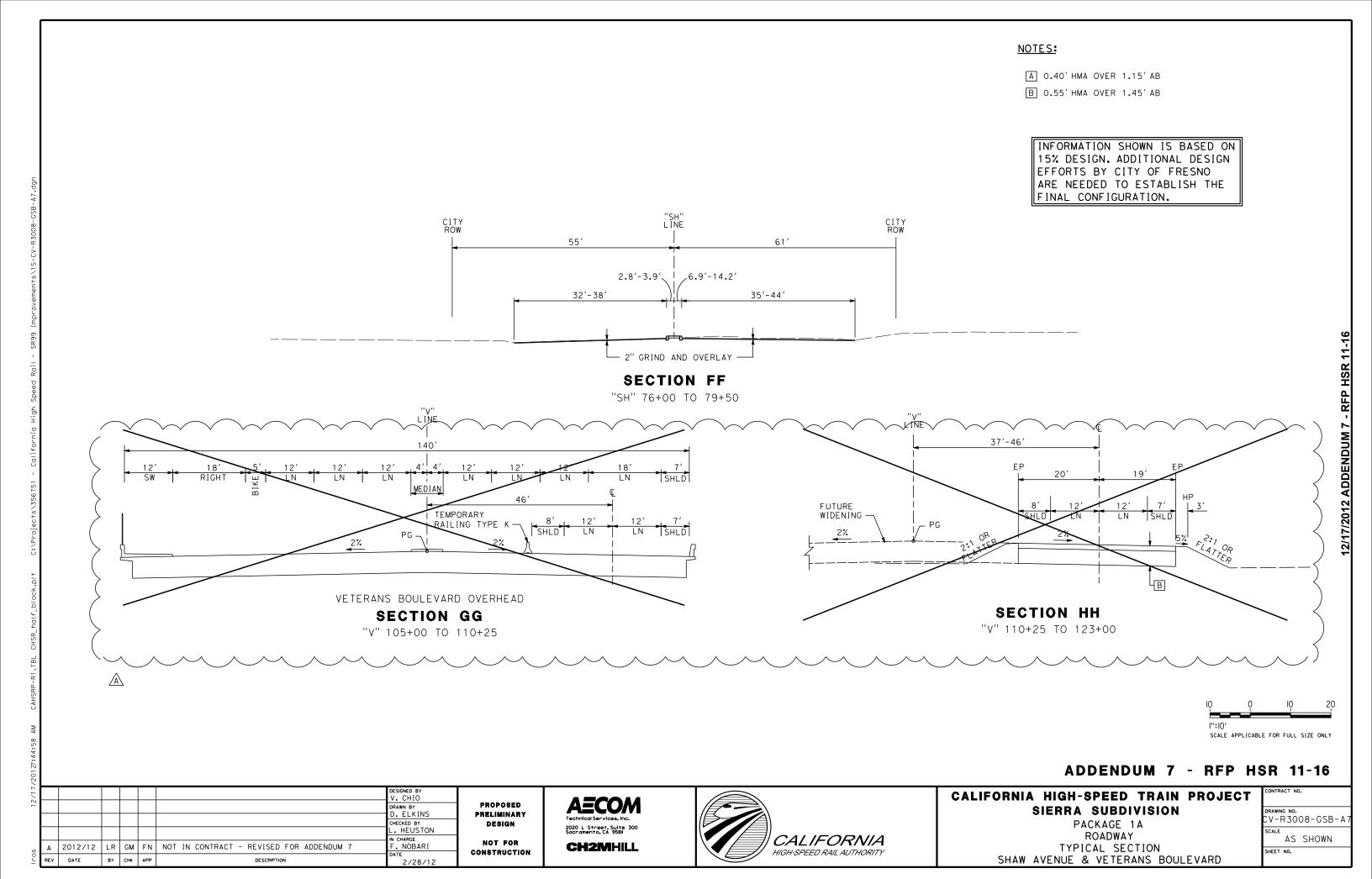
ADDENDUM 5 - RFP HSR 11-16

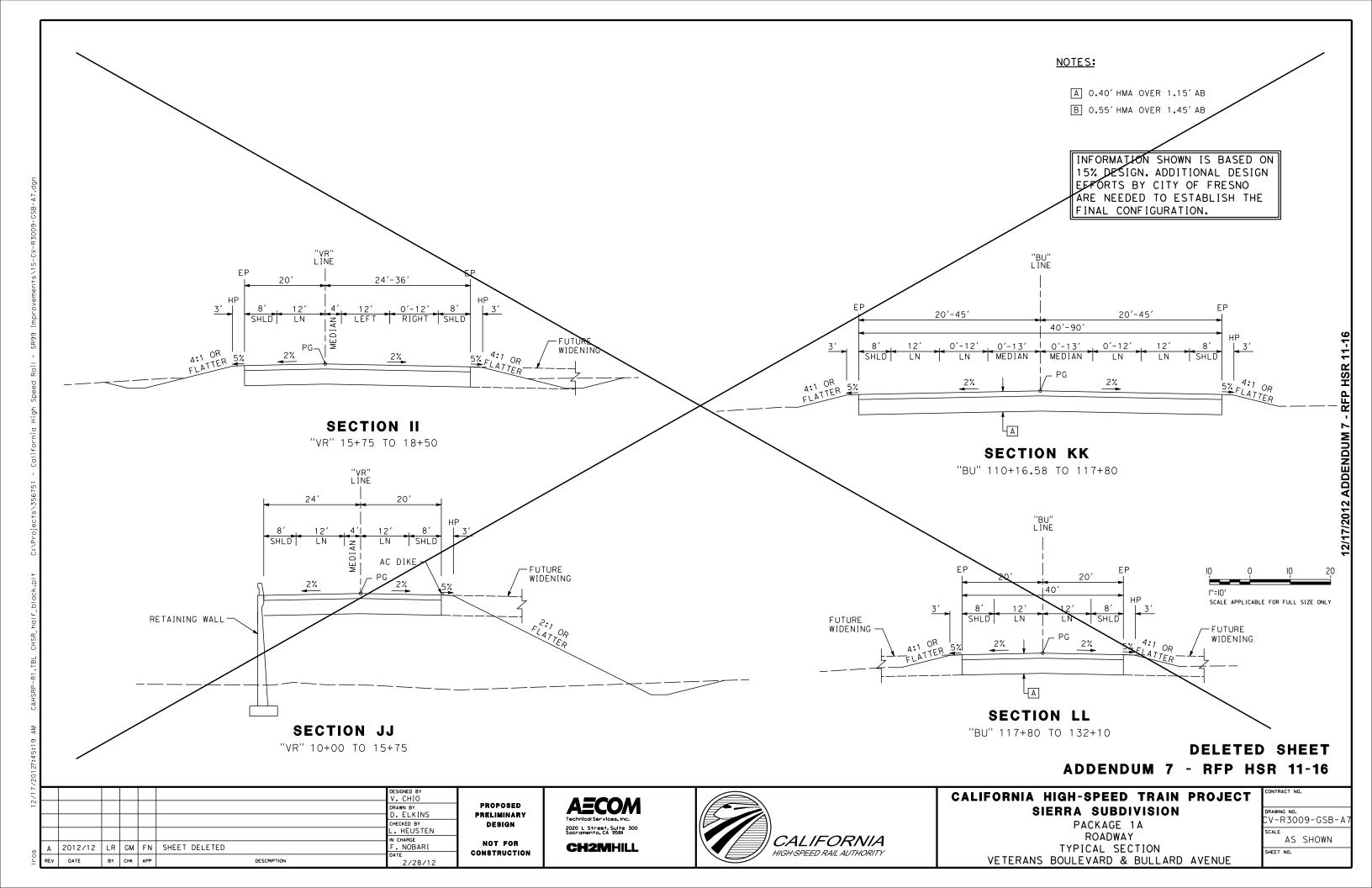
CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

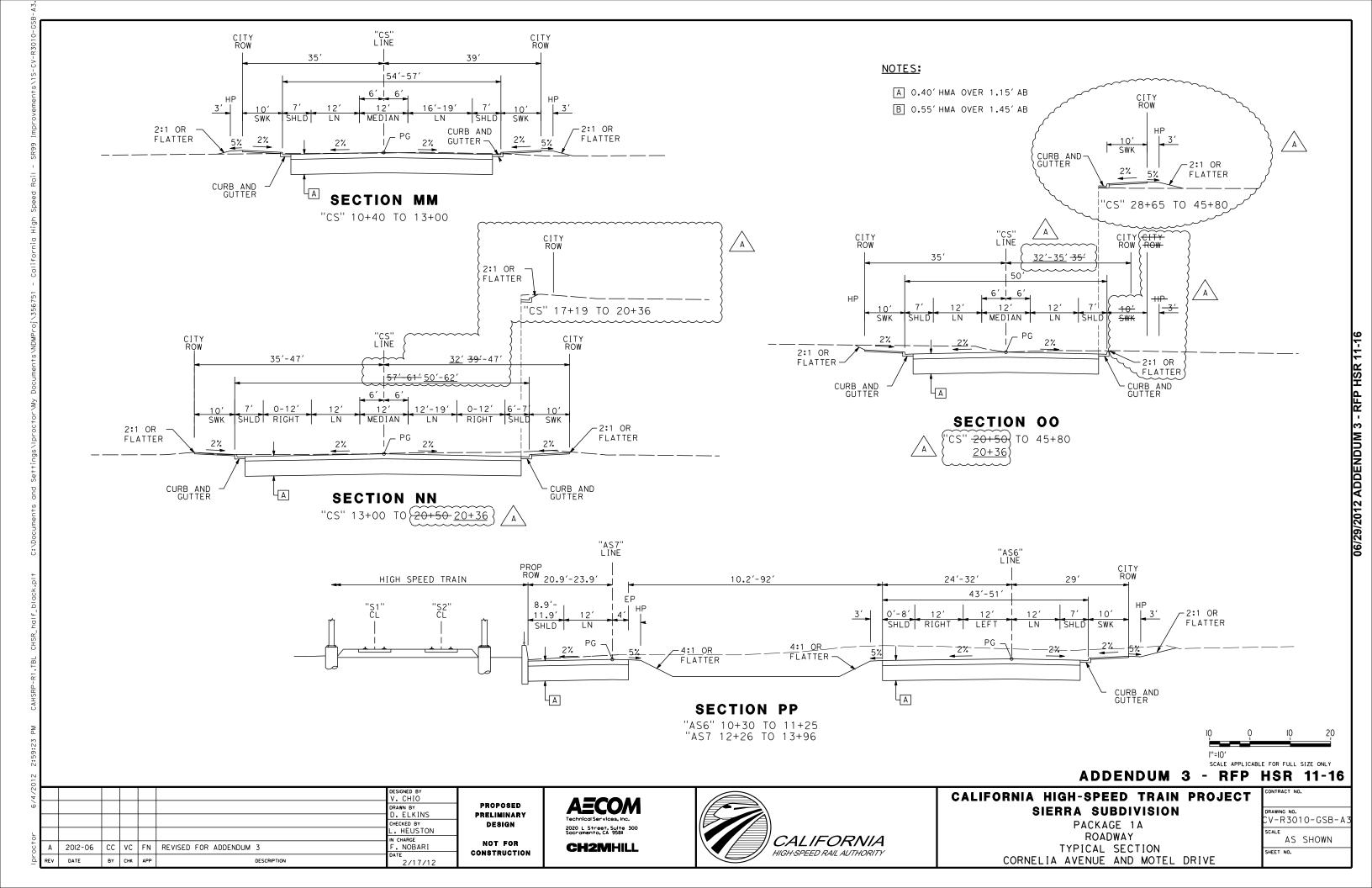
PACKAGE 1A ROADWAY TYPICAL SECTION SHAW AVENUE

T	CONTRACT NO.						
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	AS SHOWN						
	SHEET NO.						

SCALE APPLICABLE FOR FULL SIZE ONLY

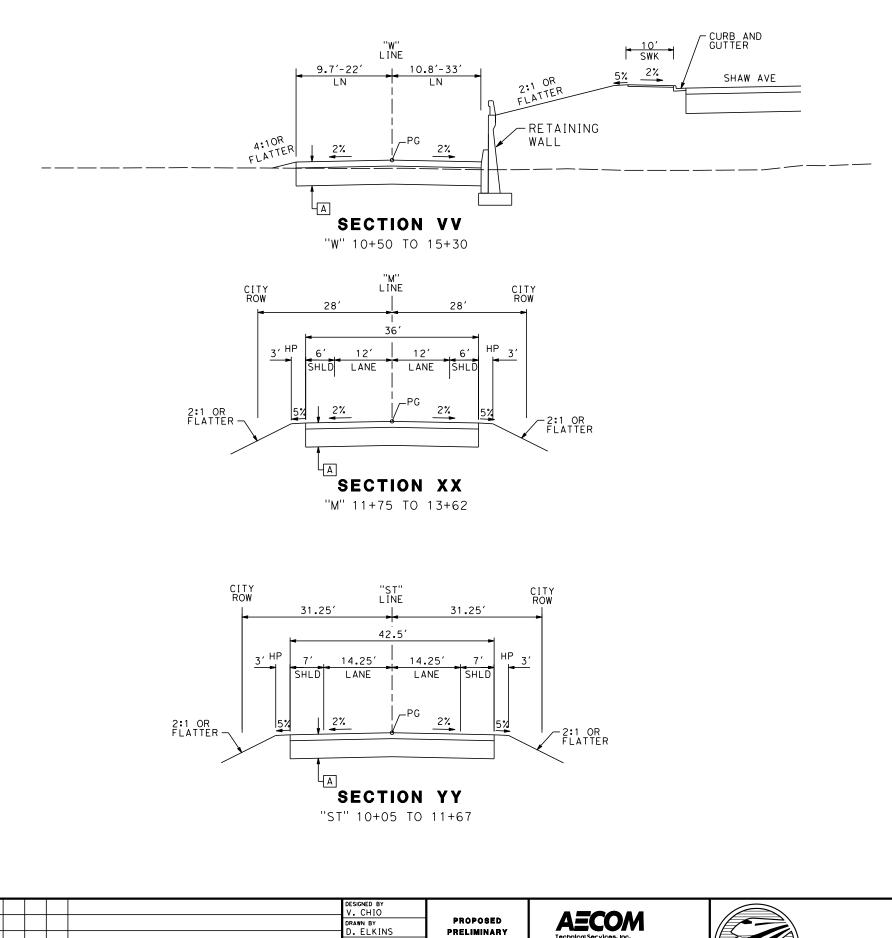






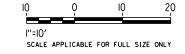
DESCRIPTION

MOTEL, JENNIFER, KATHRYN & BARSTOW



NOTES:

- A 0.40' HMA OVER 1.15' AB
- B 0.55' HMA OVER 1.45' AB



CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION DRAWING NO.

PACKAGE 1A ROADWAY TYPICAL SECTION WEBER, MARKET & STATE

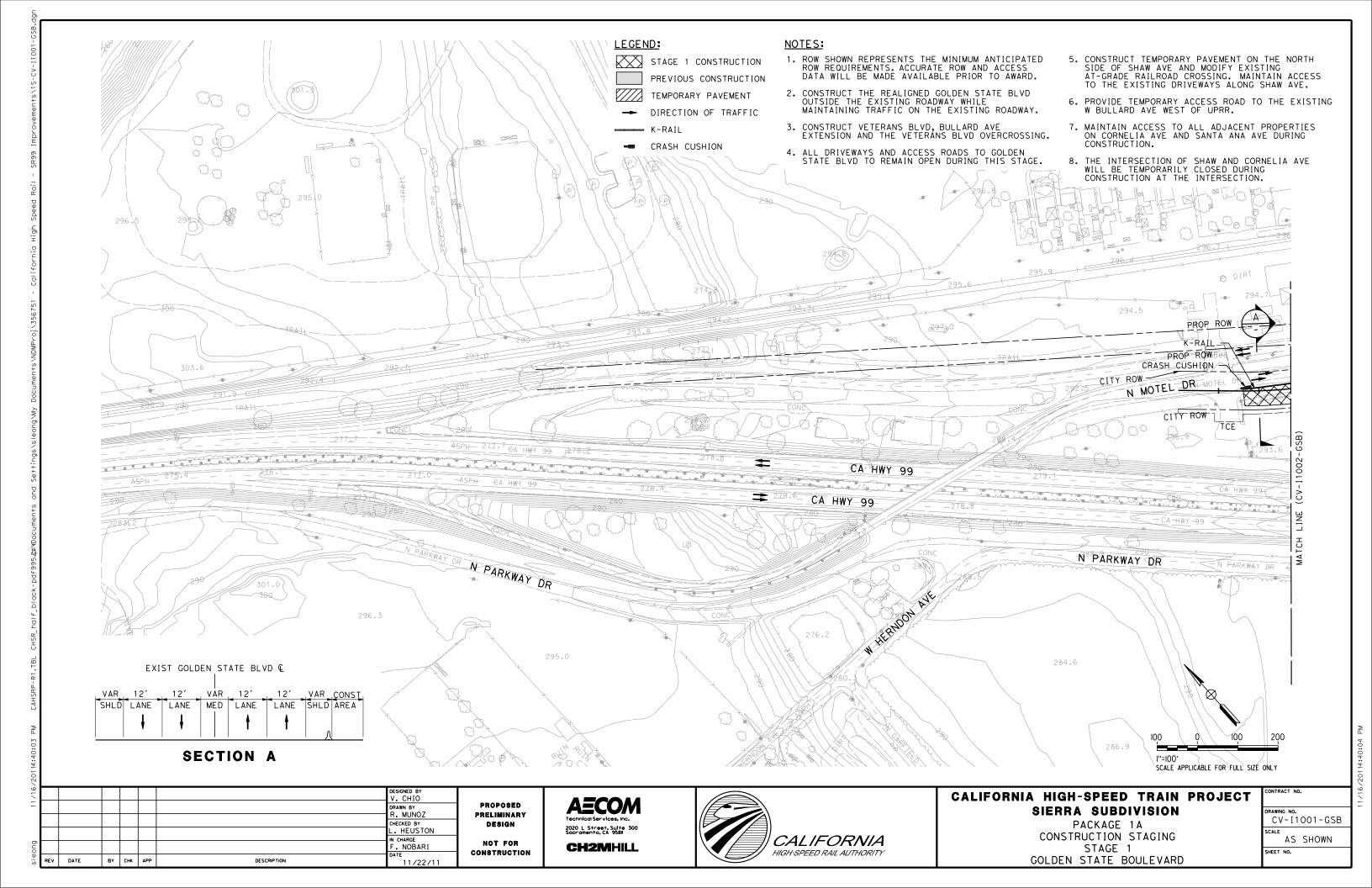
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C۷	'-R3	3012-GSB
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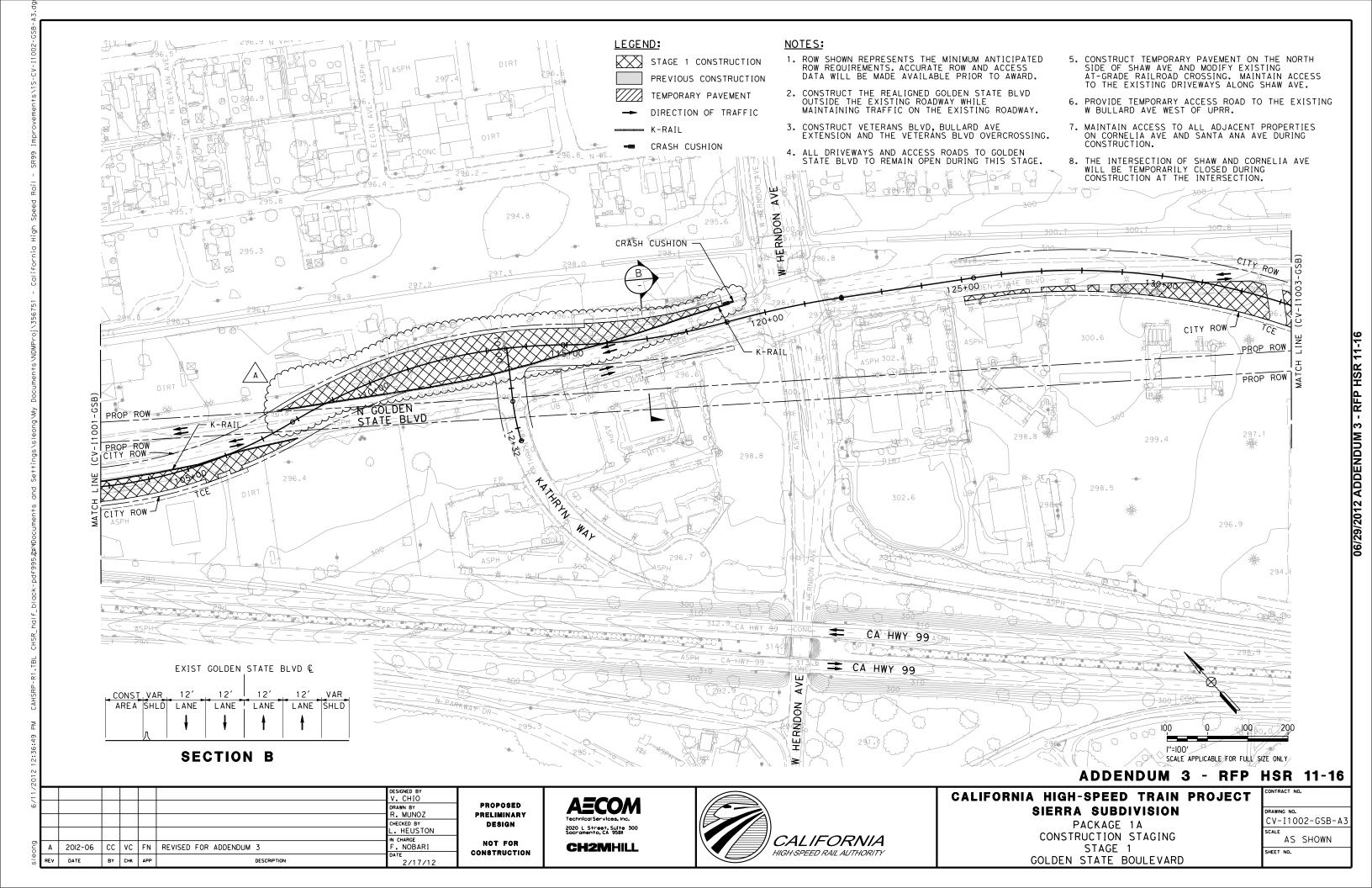
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						CHECKED BY	
						L. HEUSTON	
						IN CHARGE	_
						F. NOBARI	001
						DATE	CON
REV	DATE	BY	CHK	APP	DESCRIPTION	11/22/11	

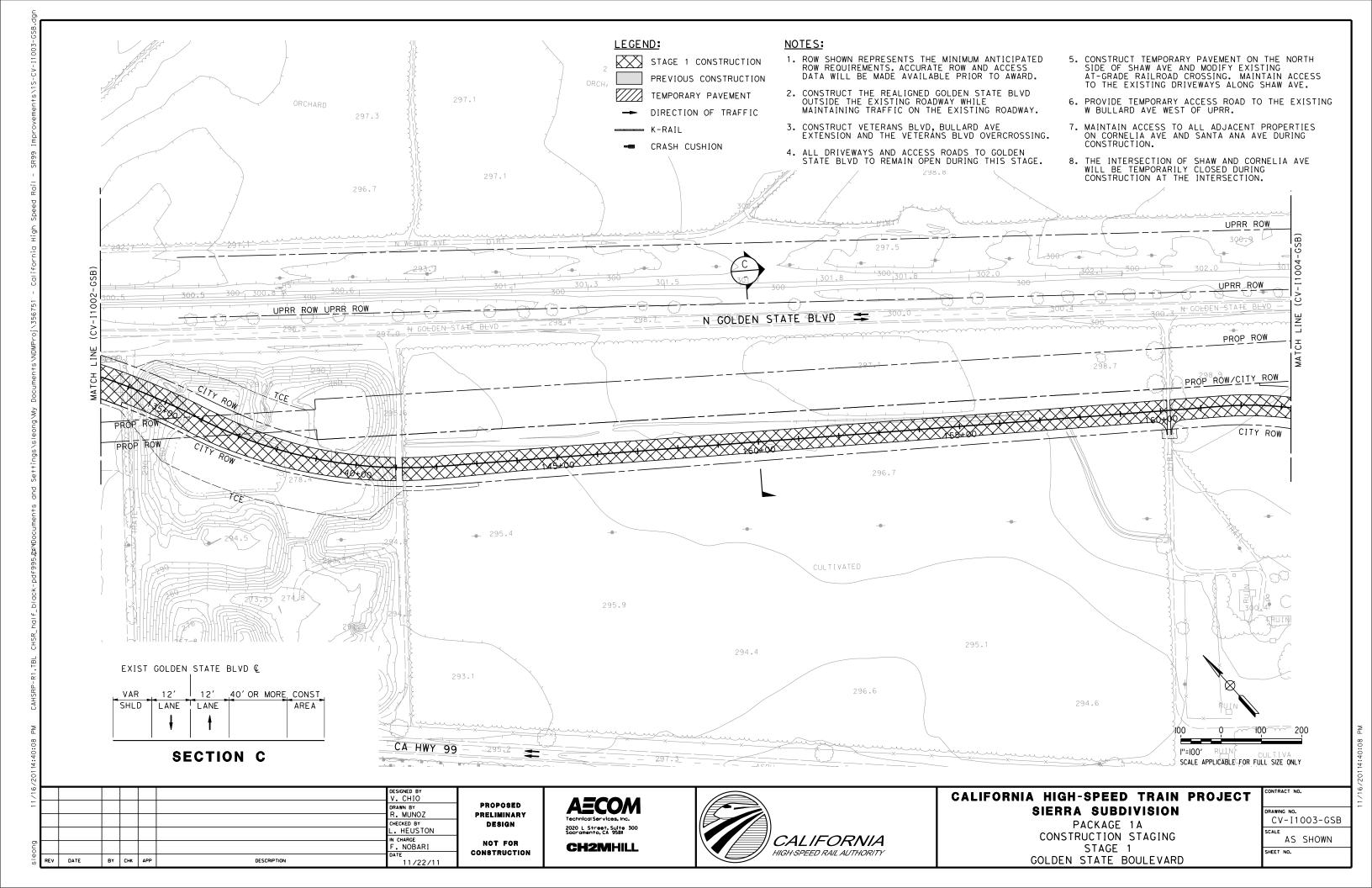
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NOT FOR
CONSTRUCTION

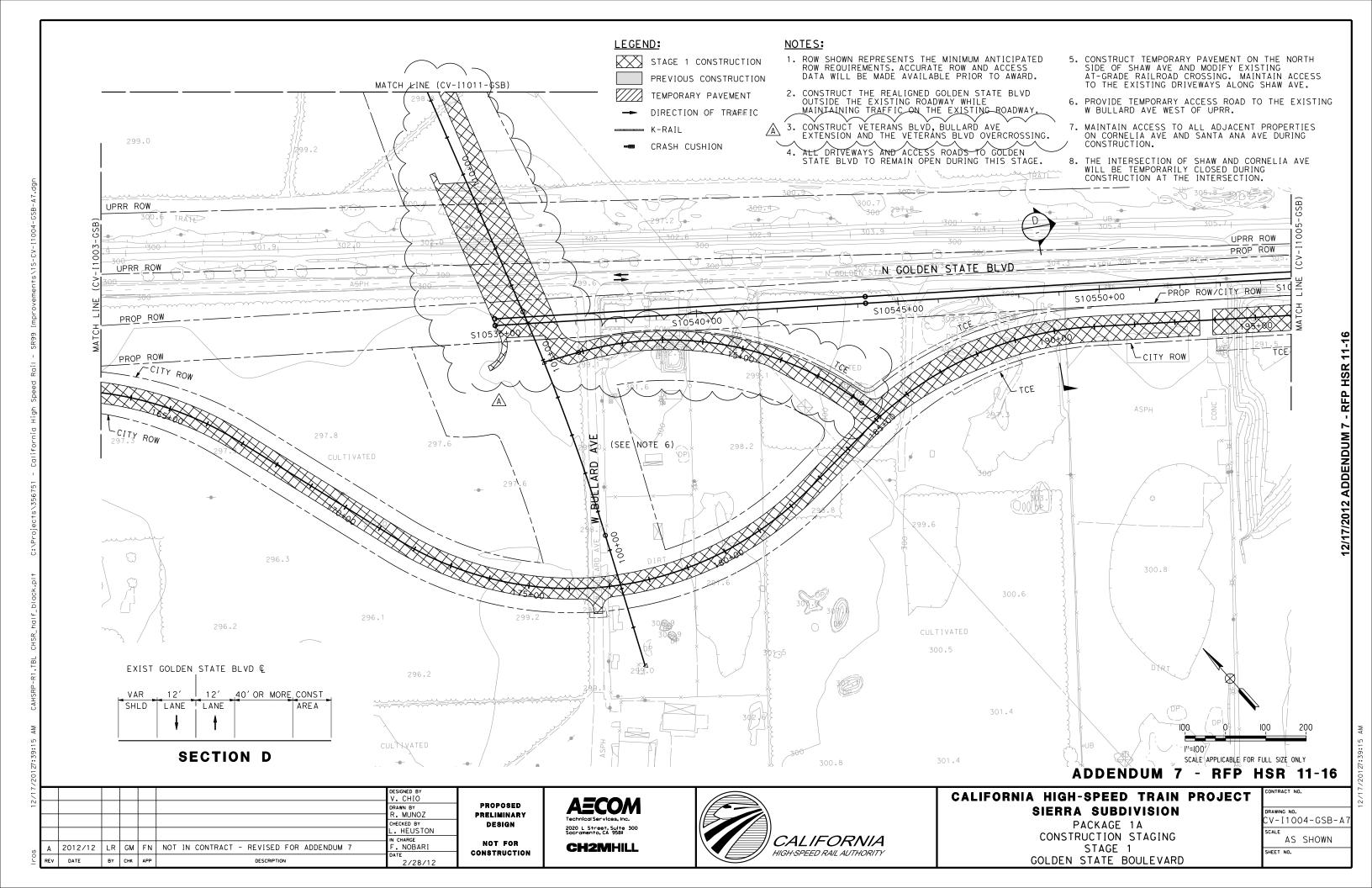
AECOM
Technical Services, Inc.
2020 L. Street, Sulte.
300 Socramento, CA 9581
CH2MHILL

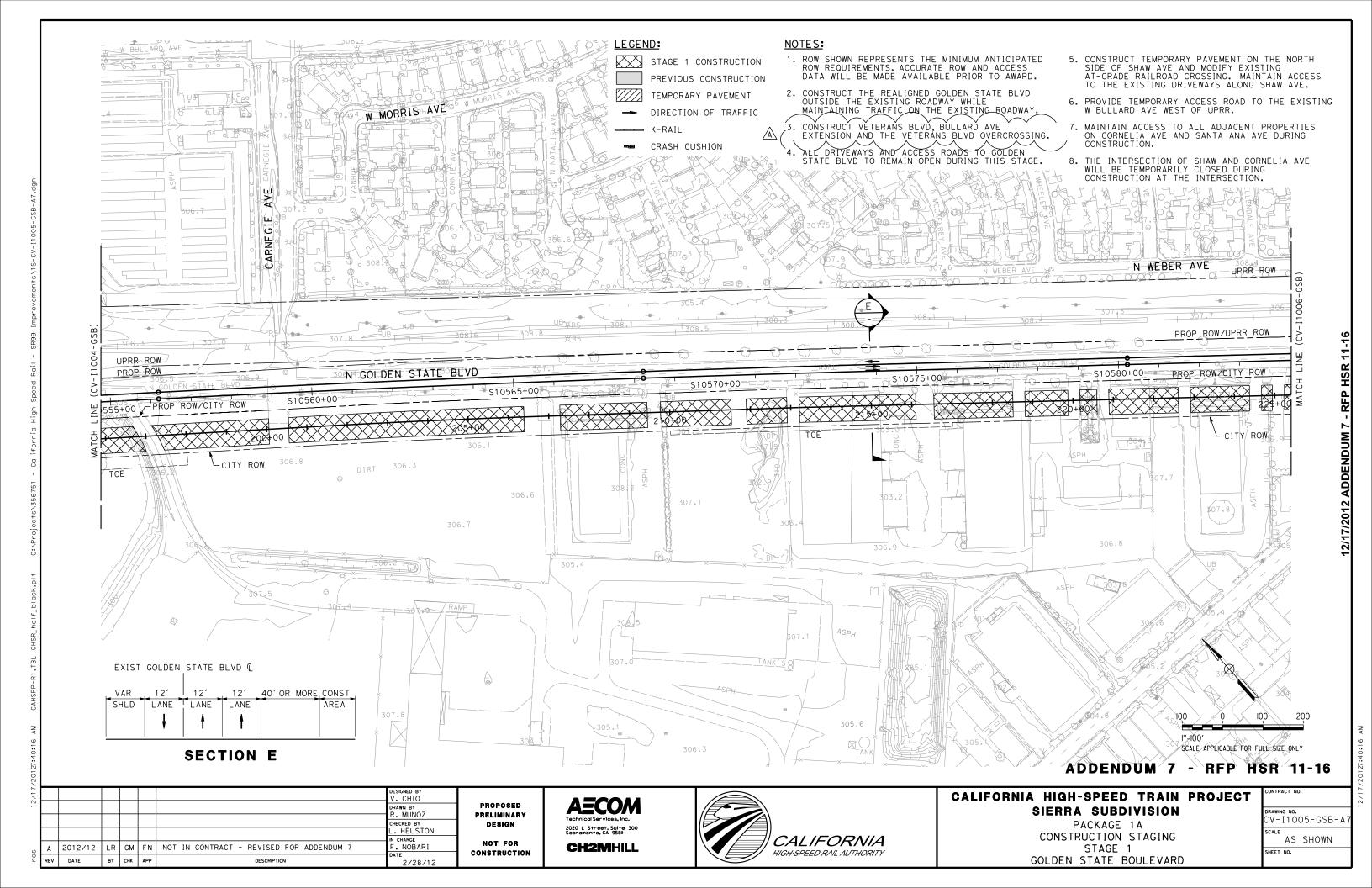


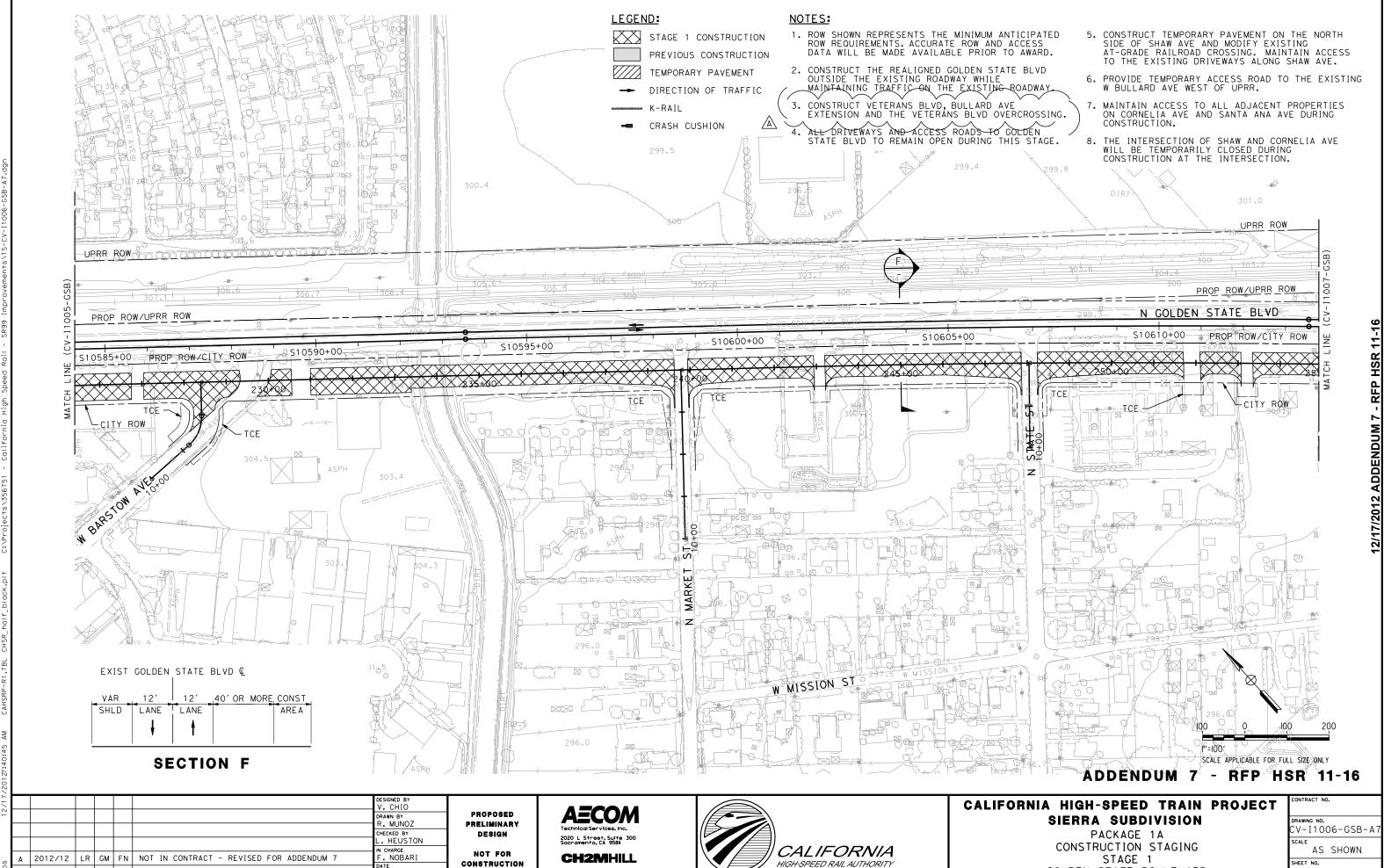




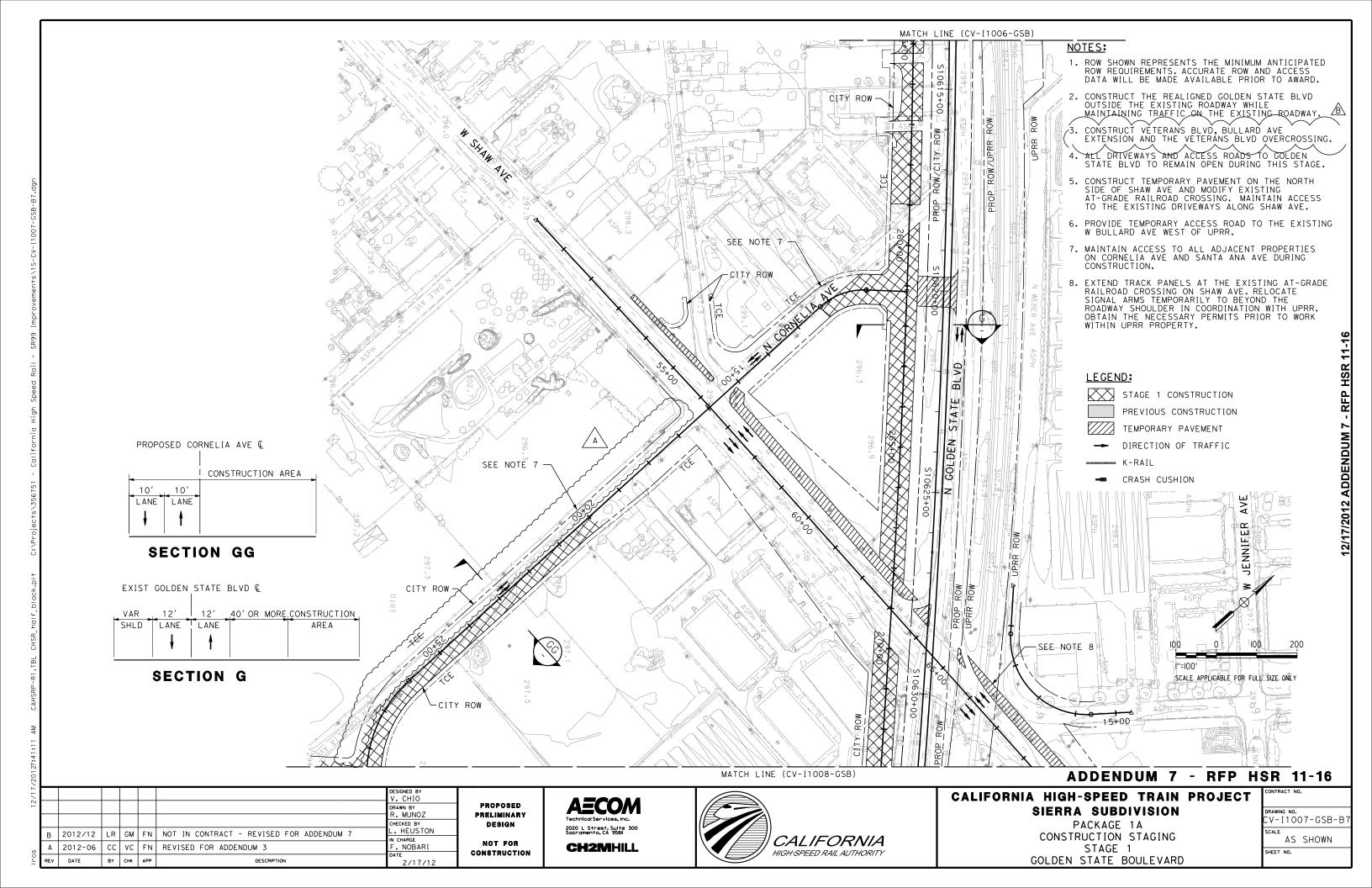


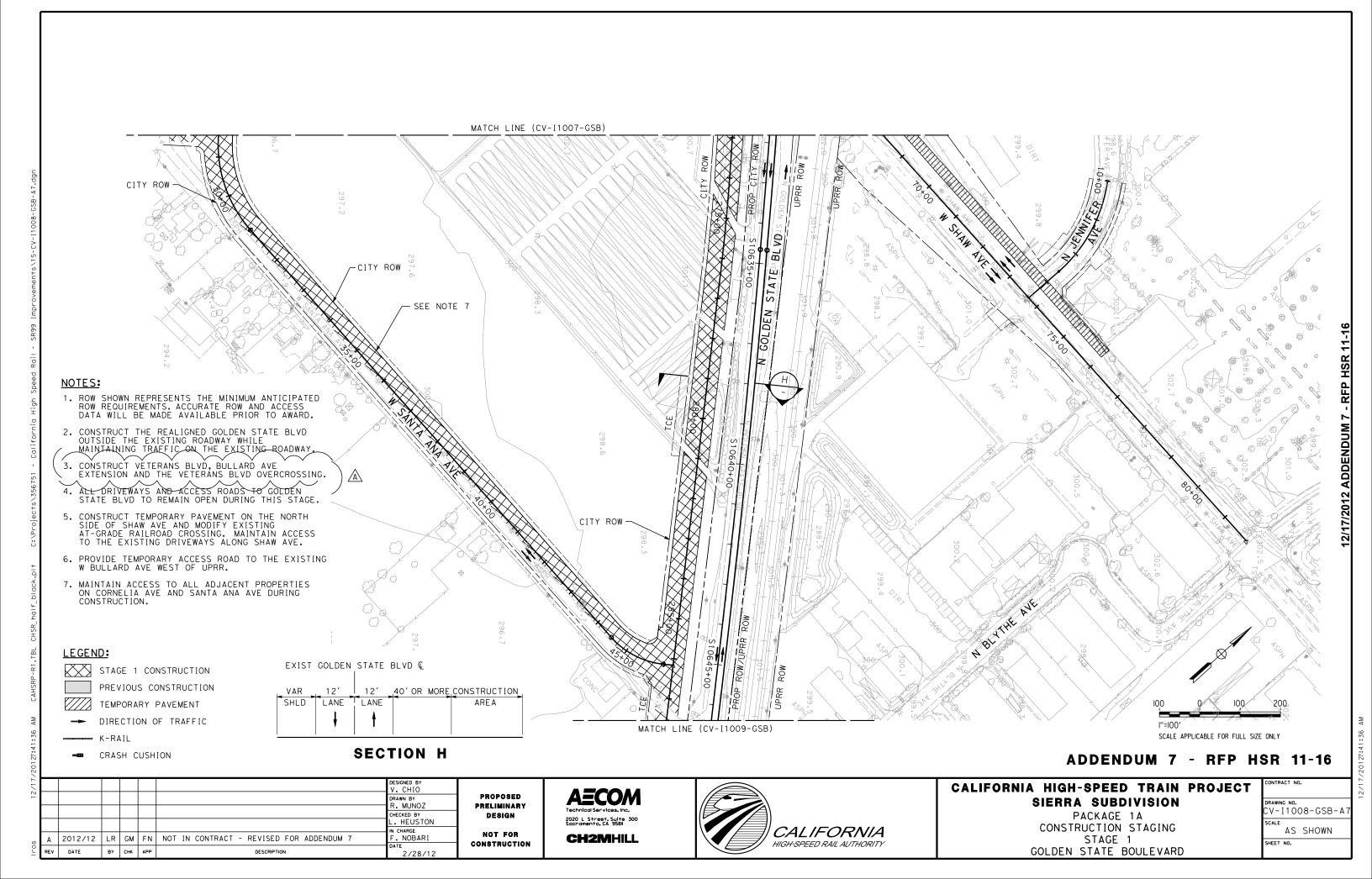


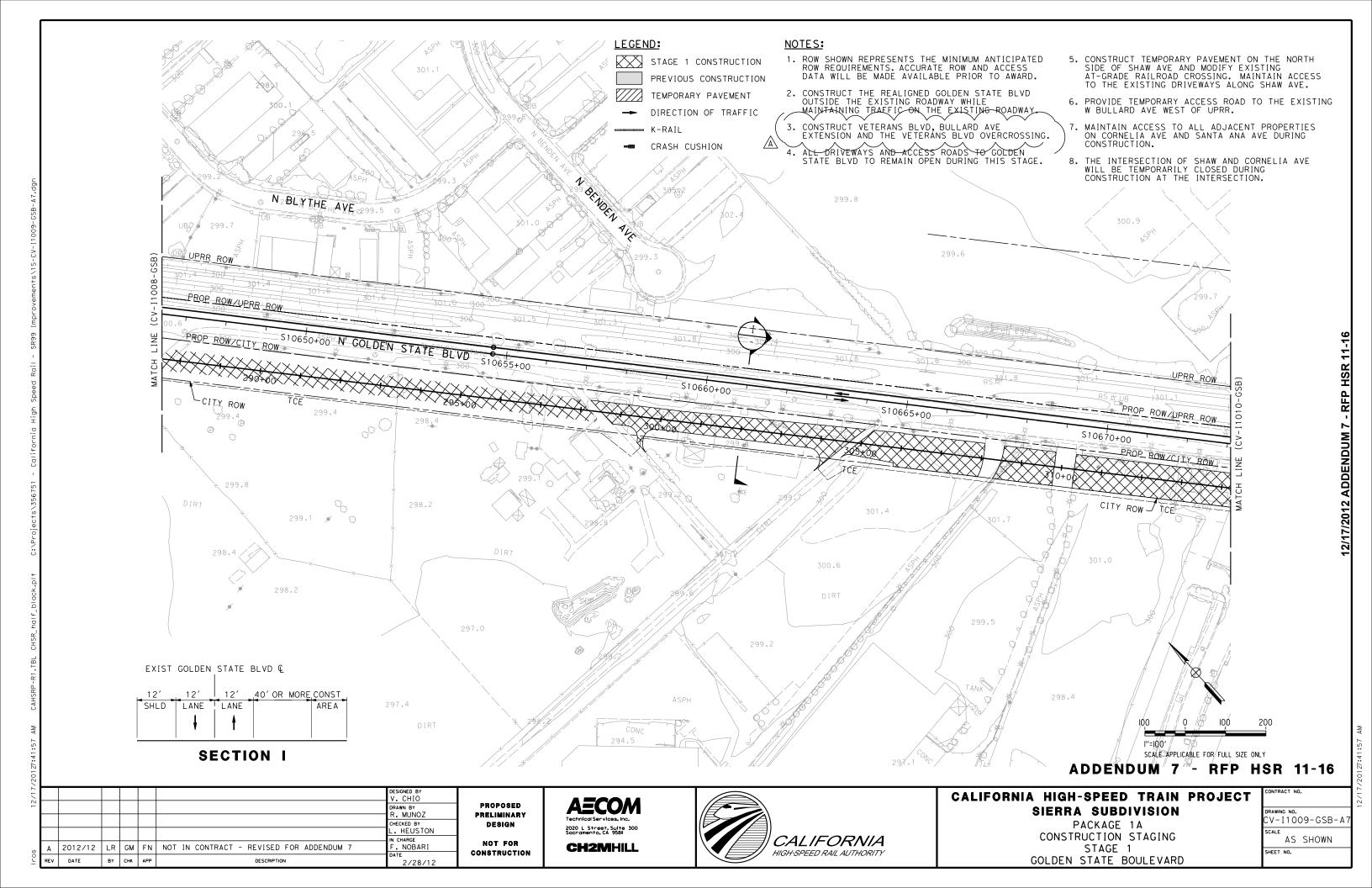


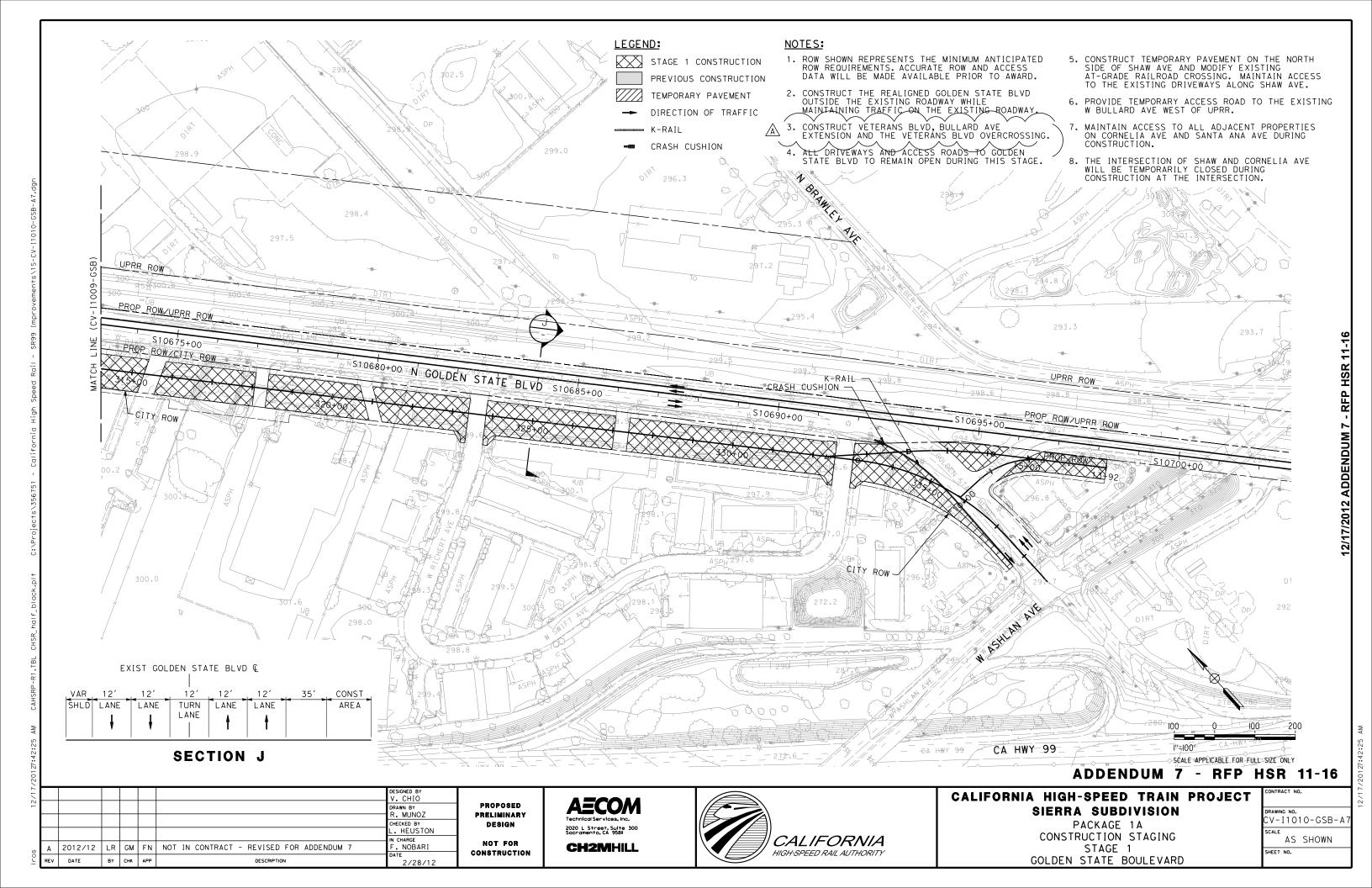


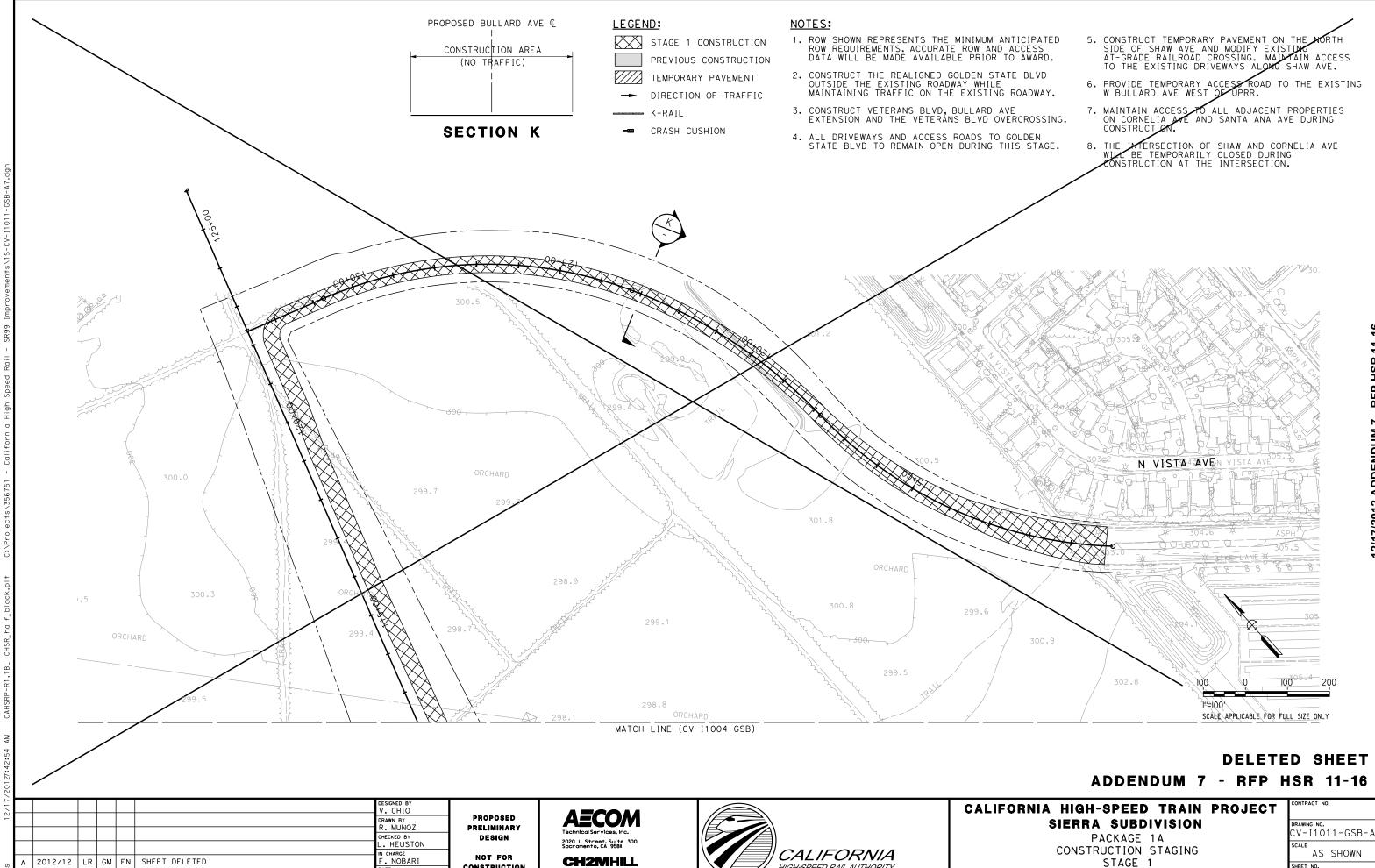
GOLDEN STATE BOULEVARD











HIGH-SPEED RAIL AUTHORITY

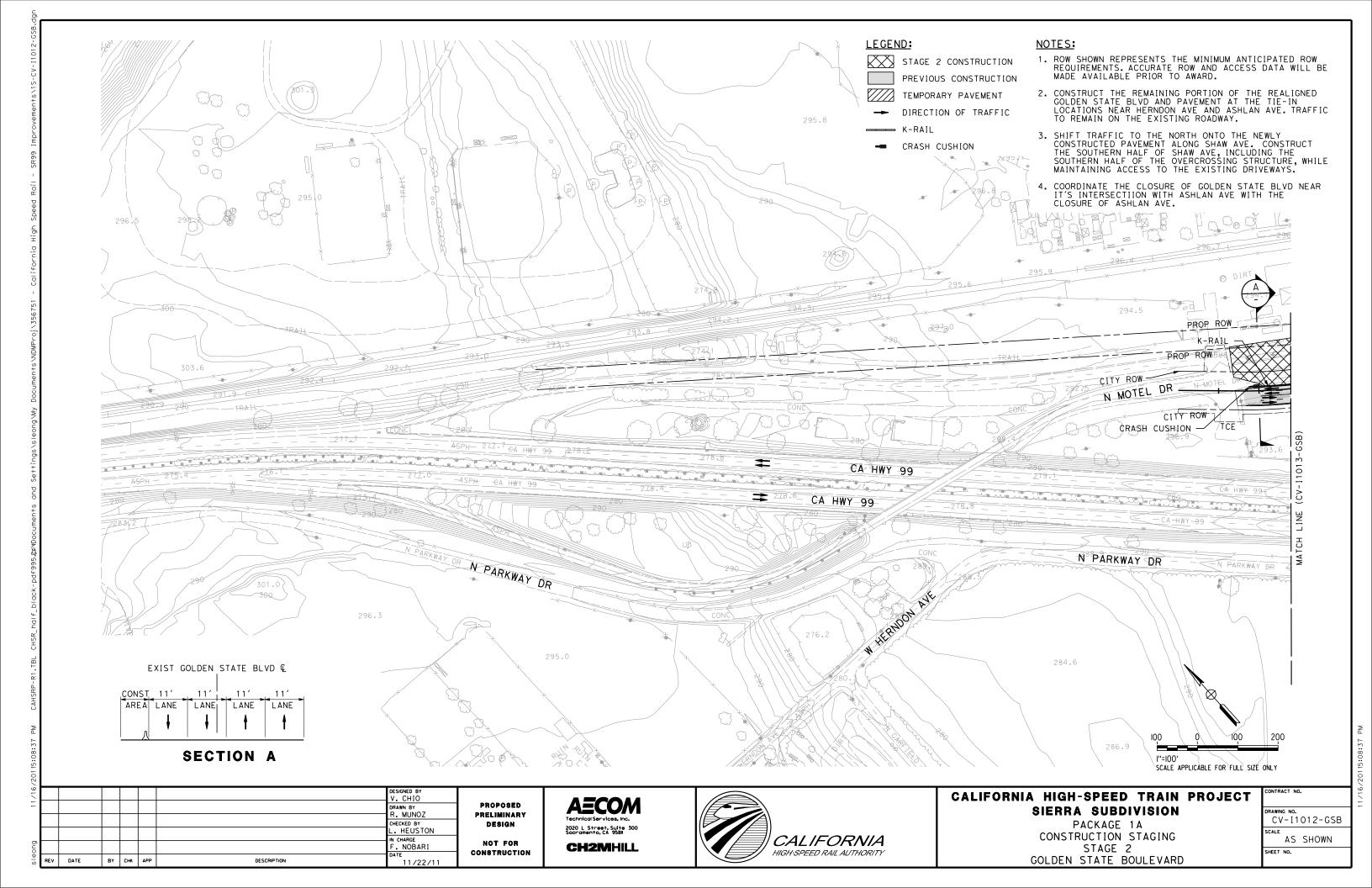
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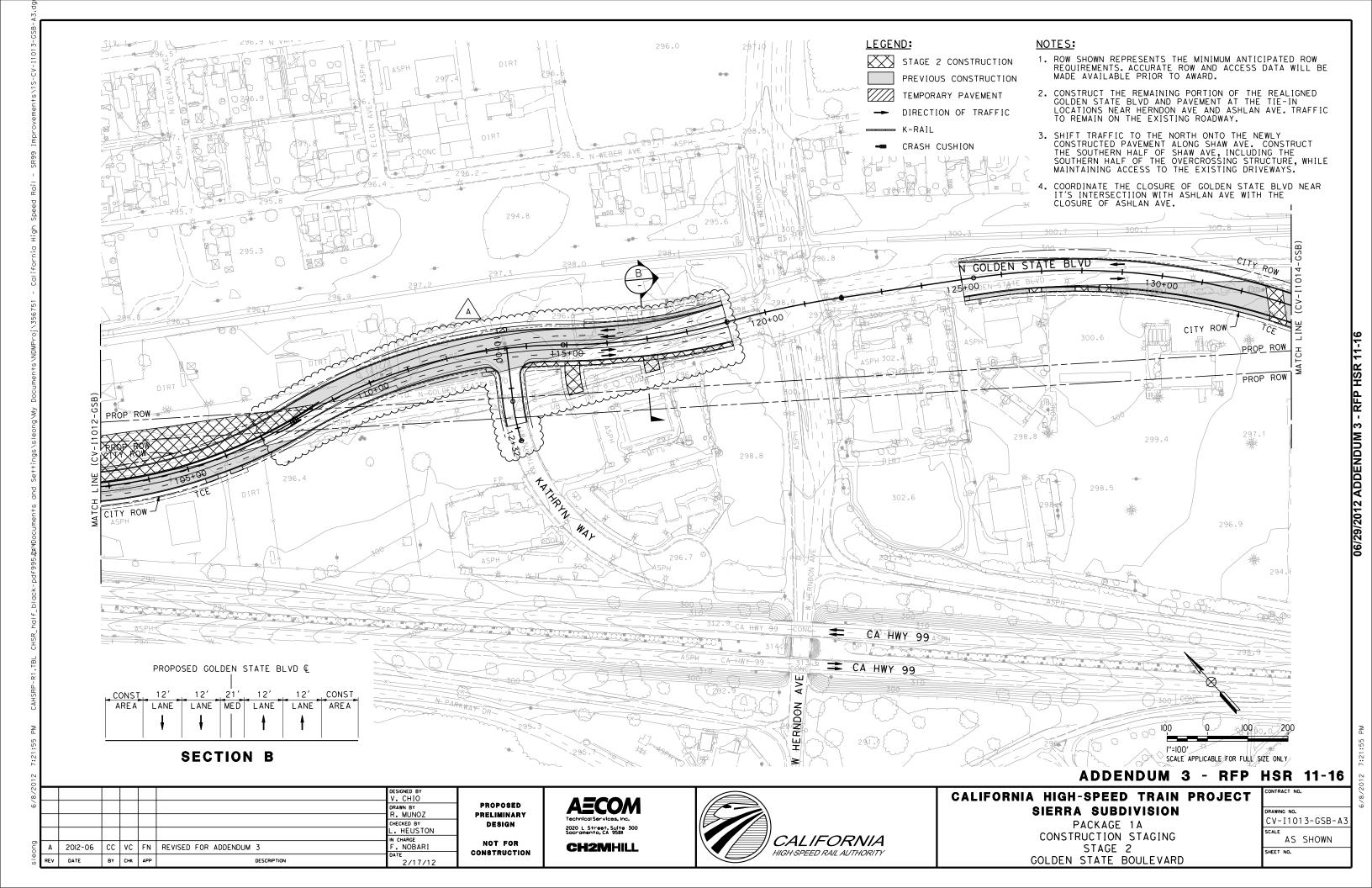
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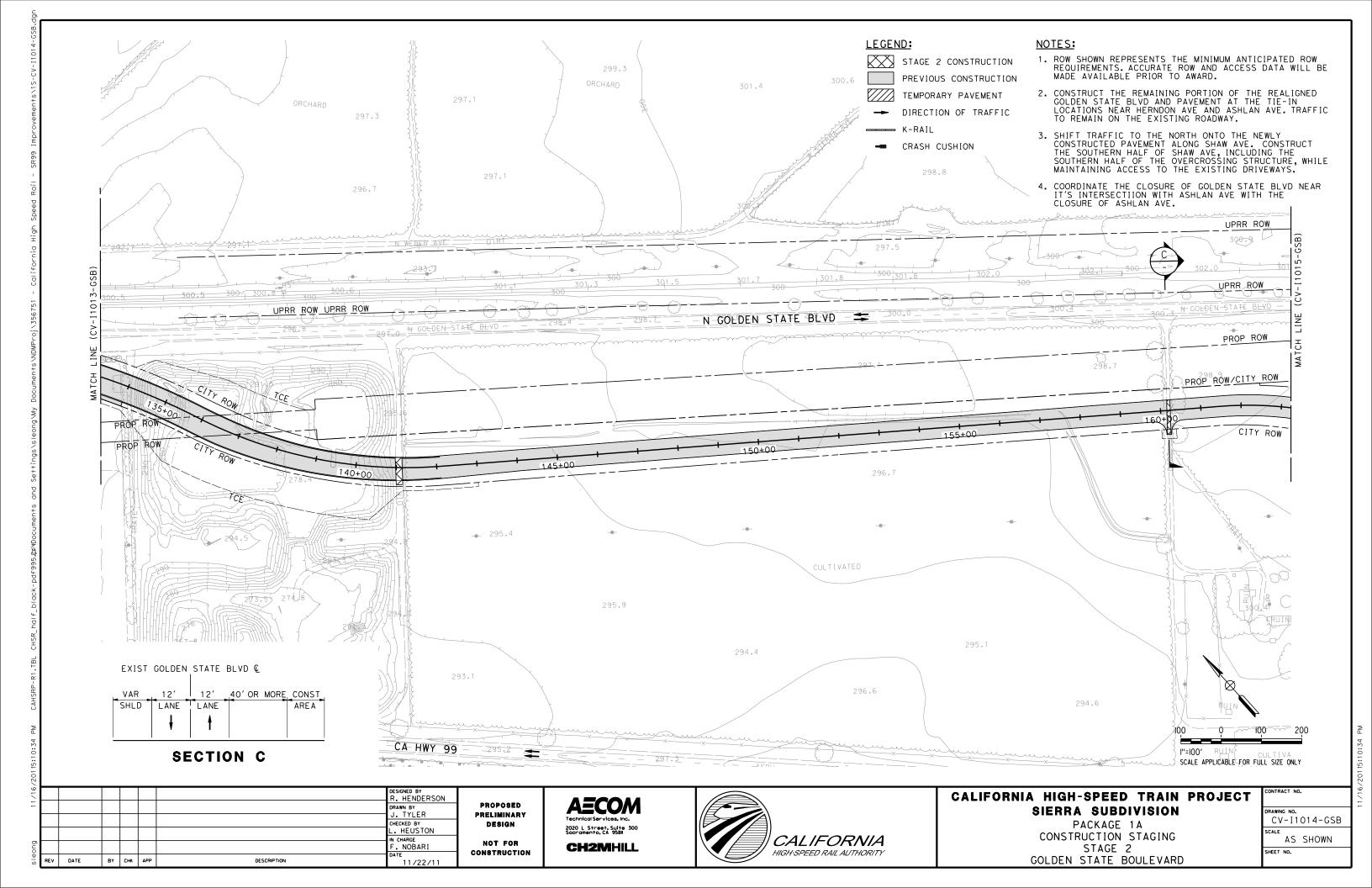
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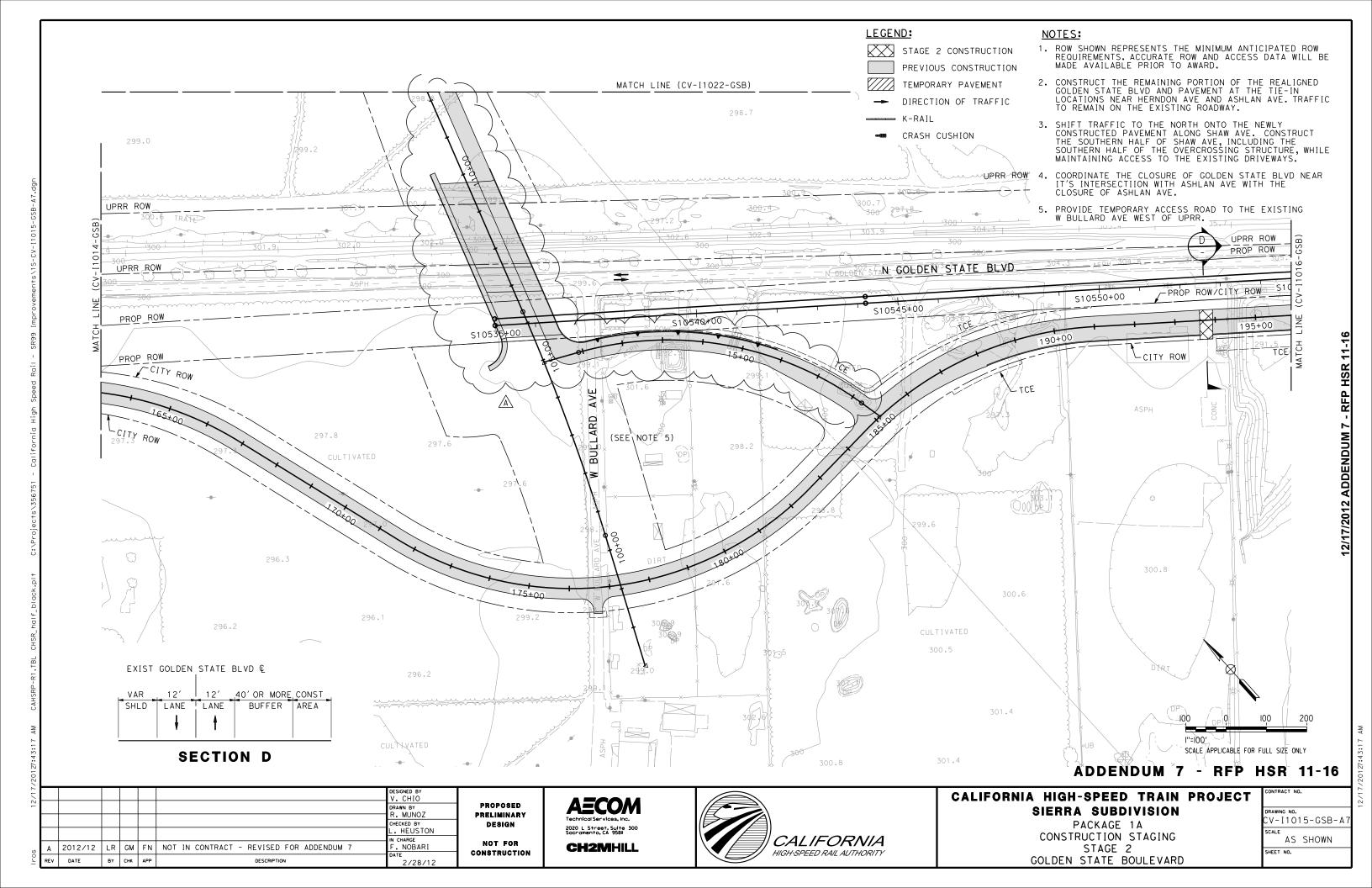
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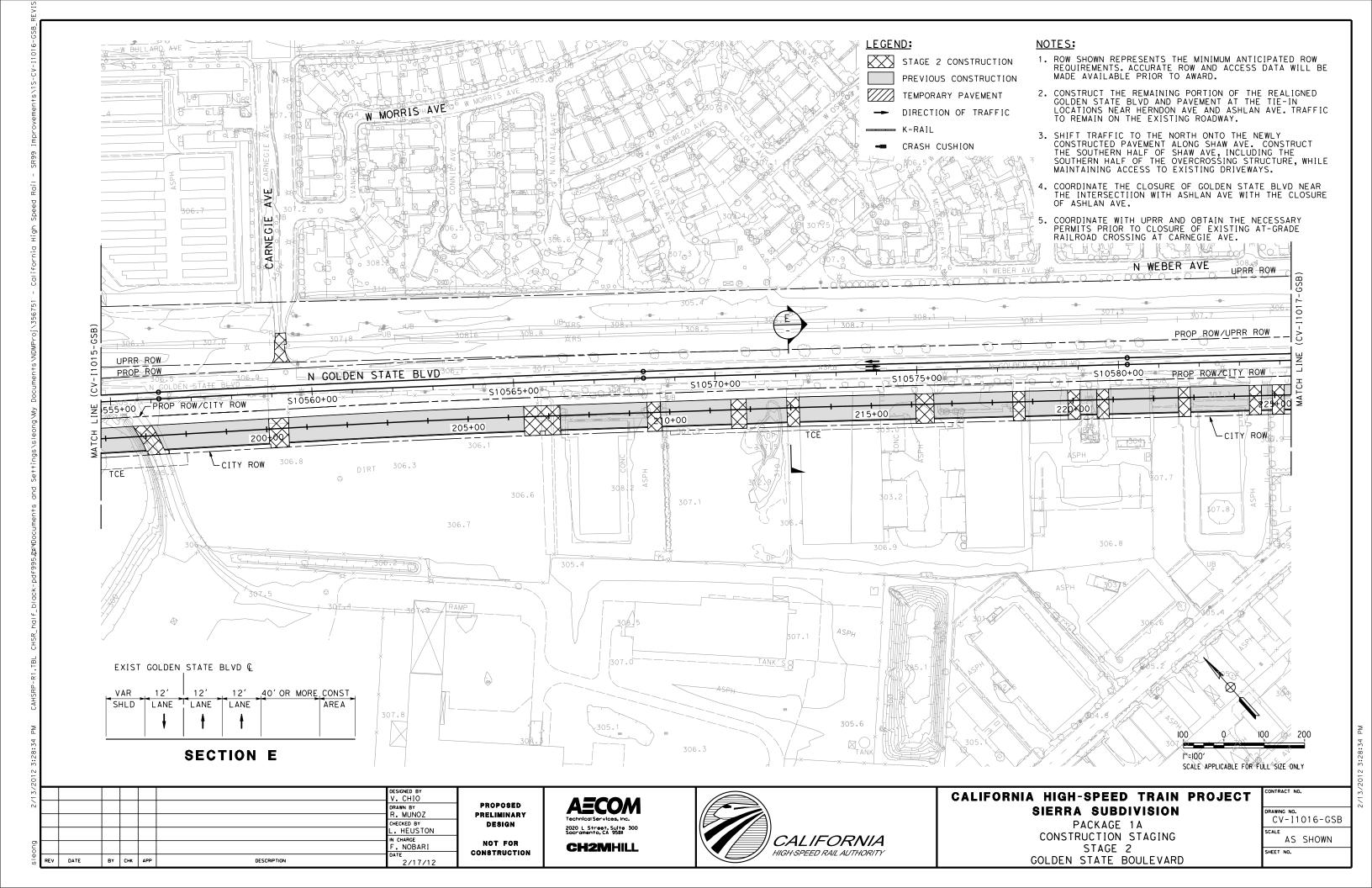
GOLDEN STATE BOULEVARD

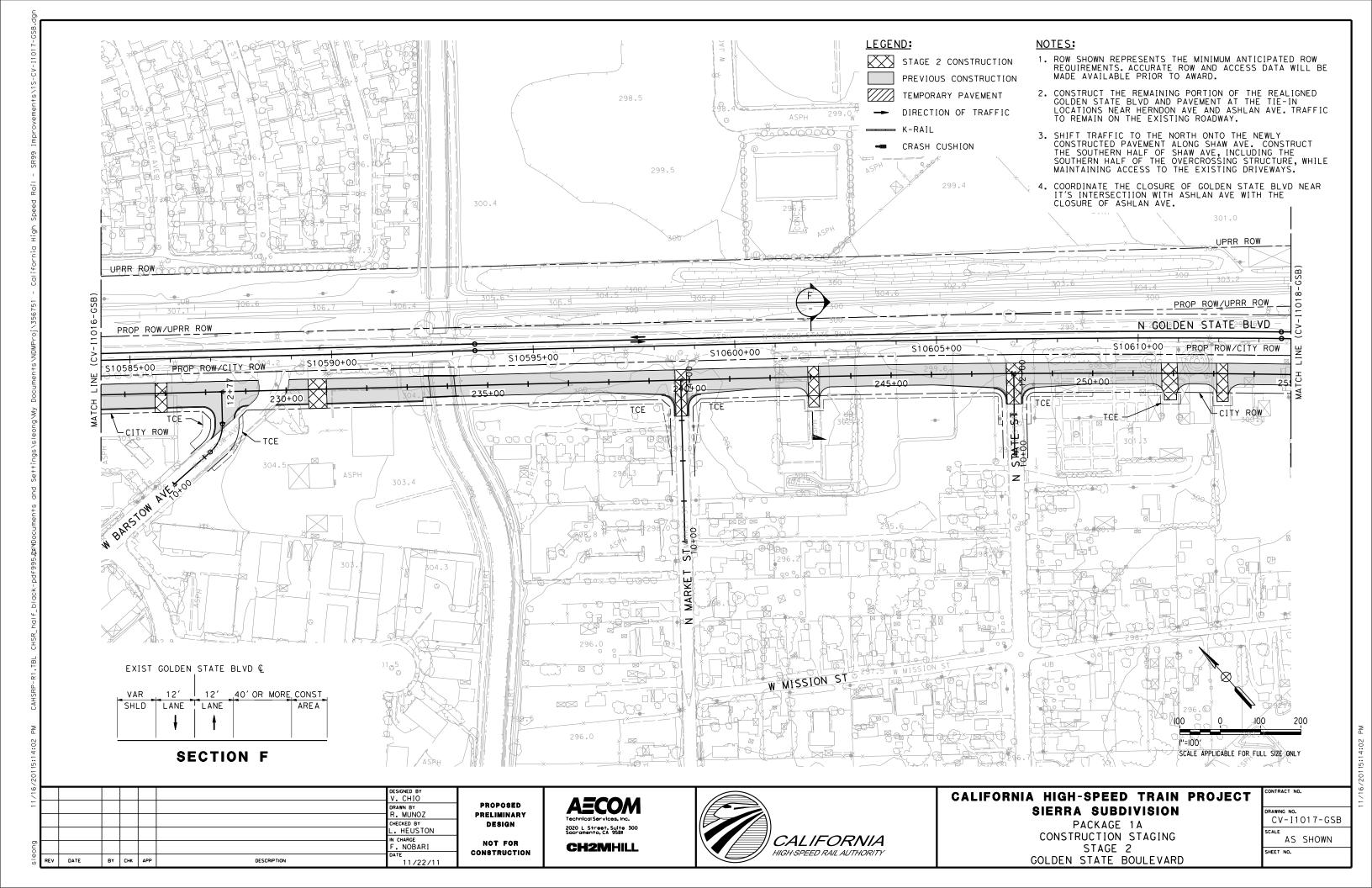


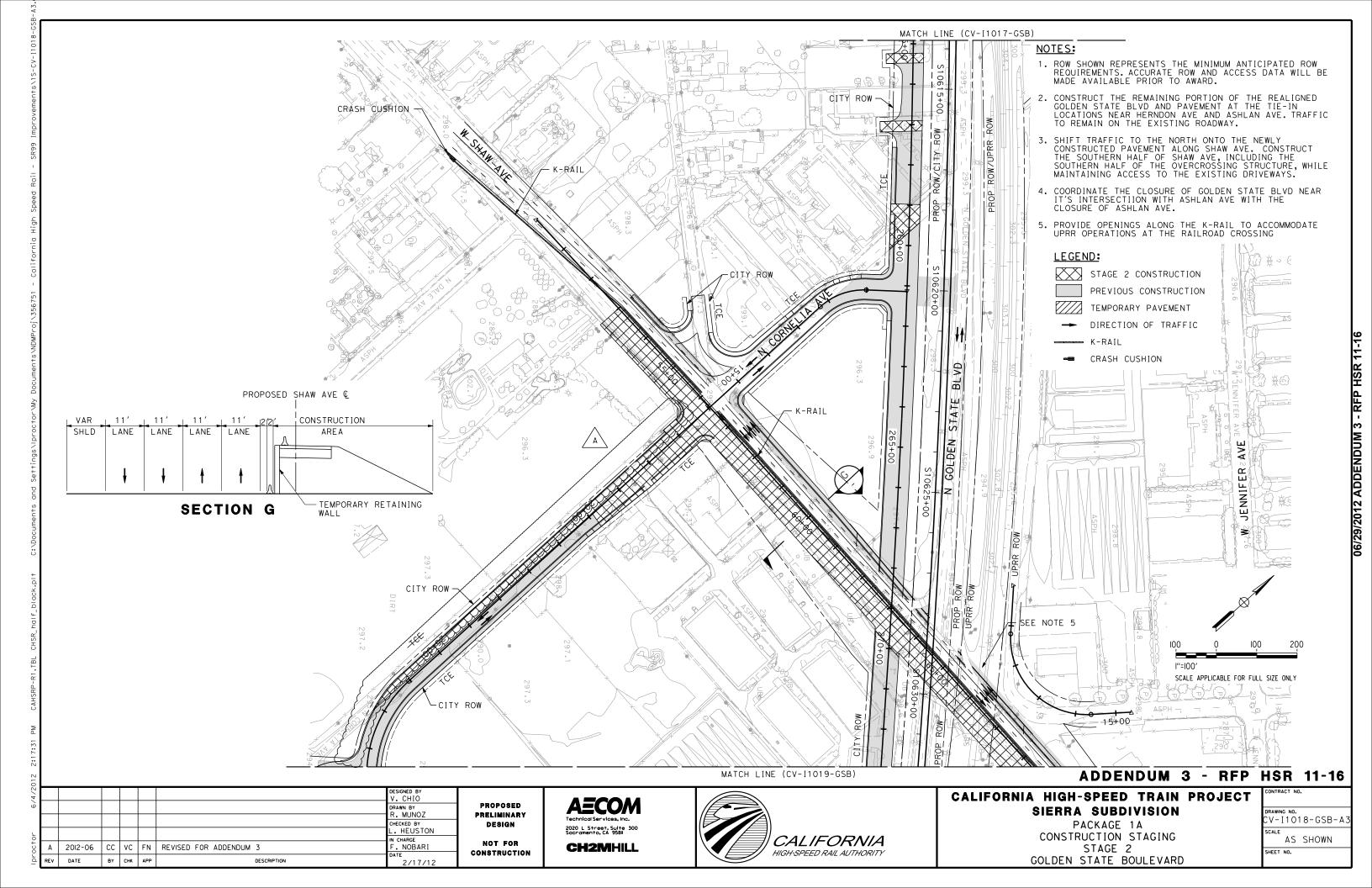


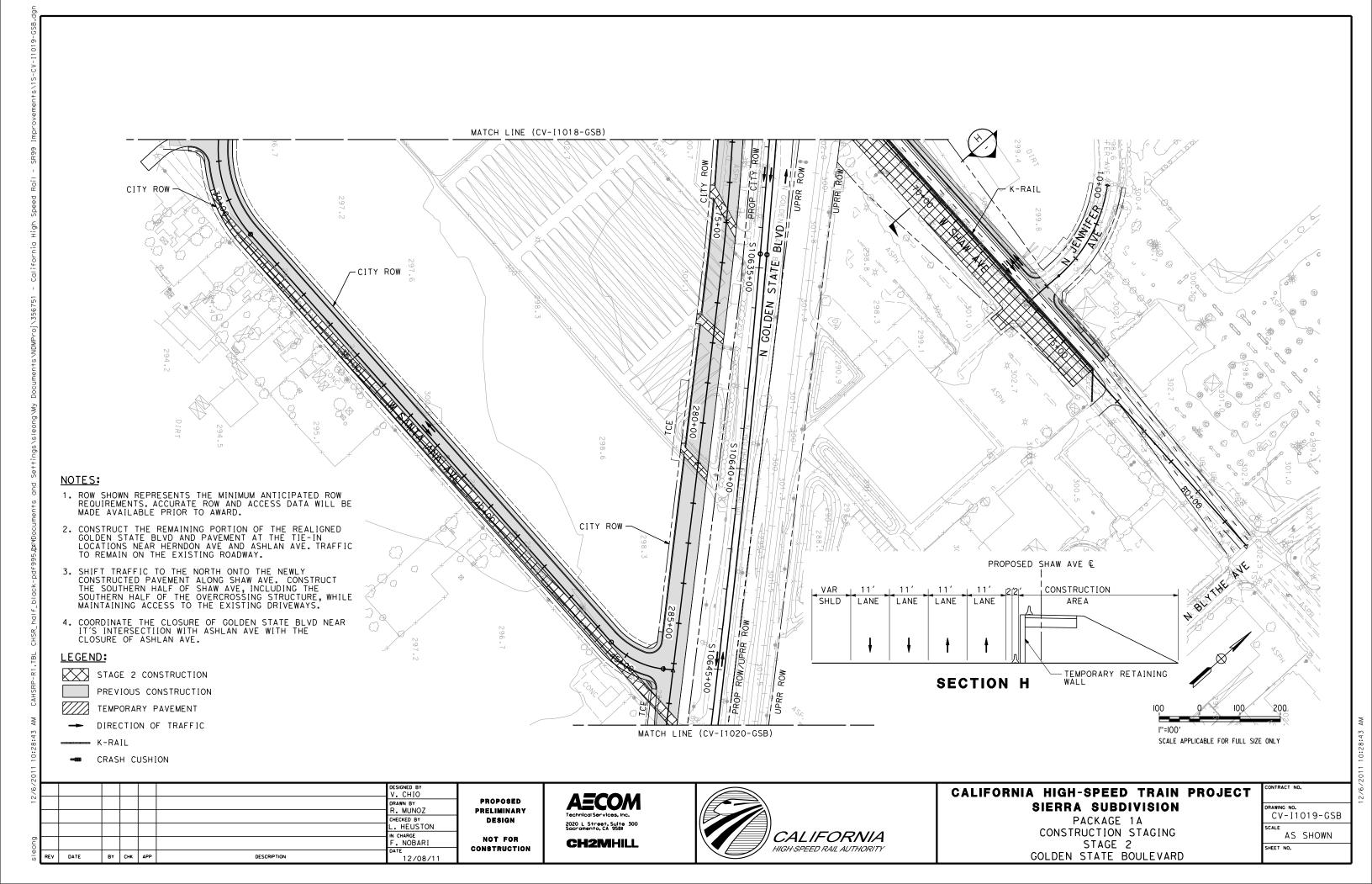


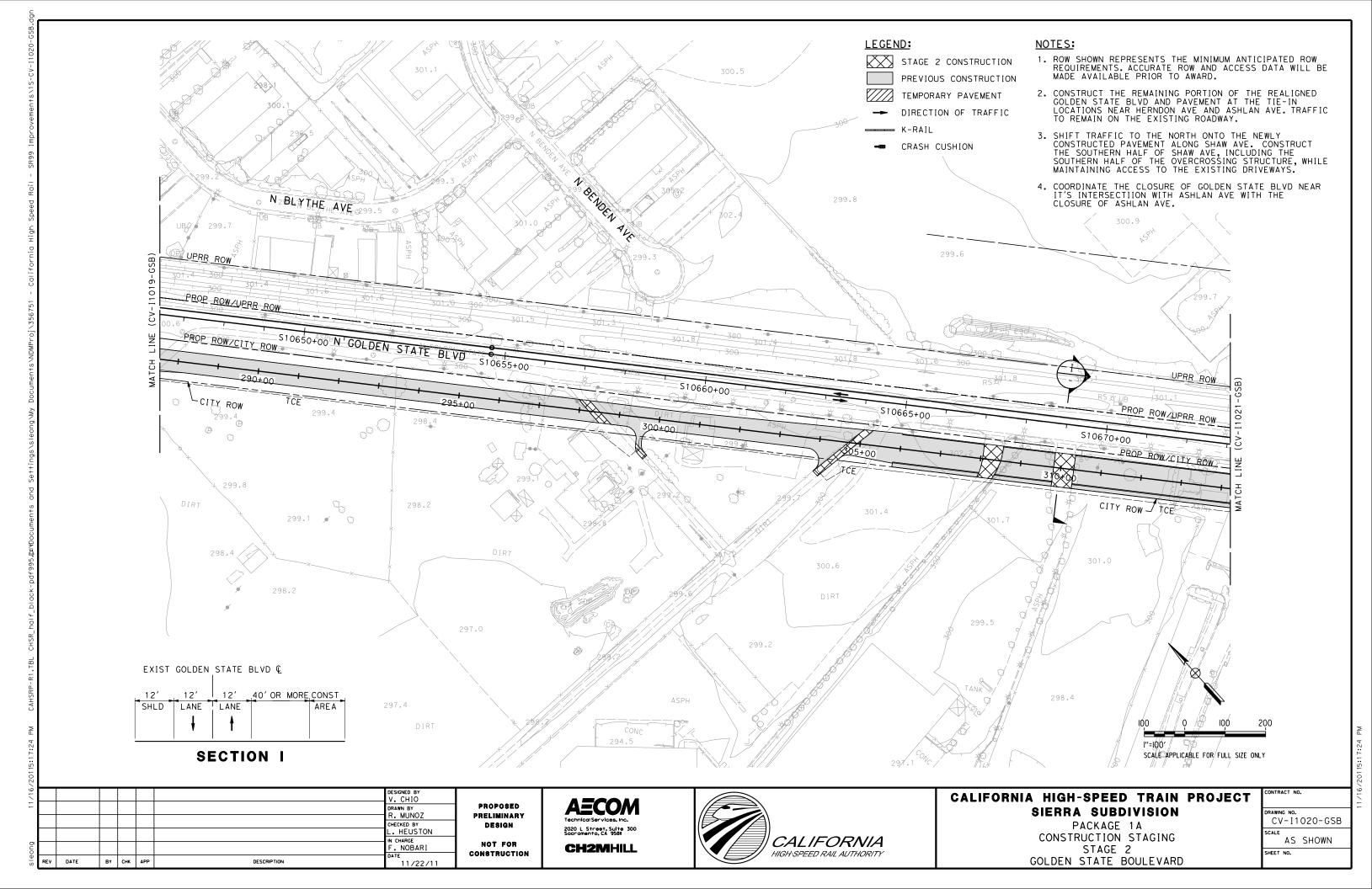


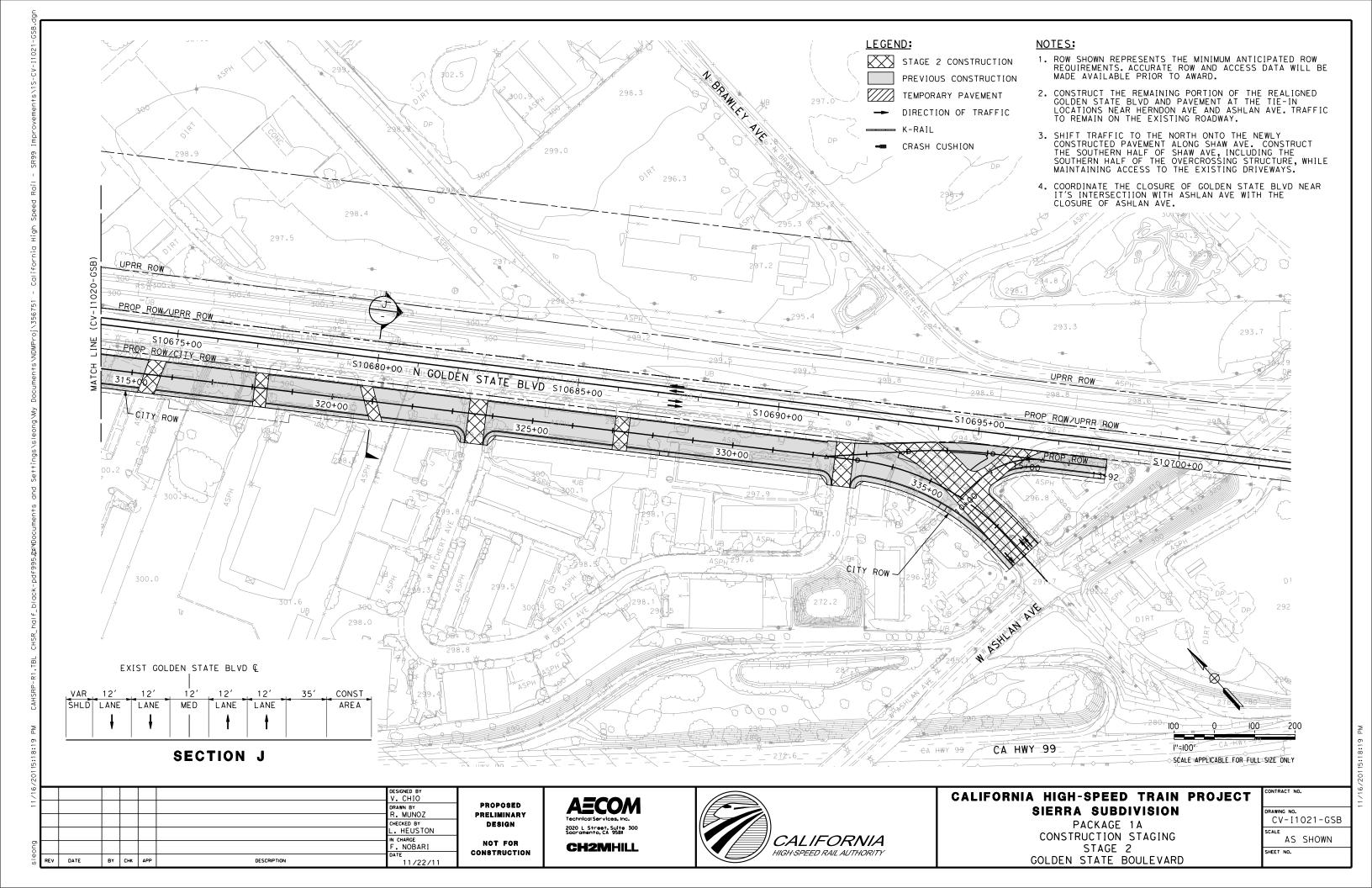


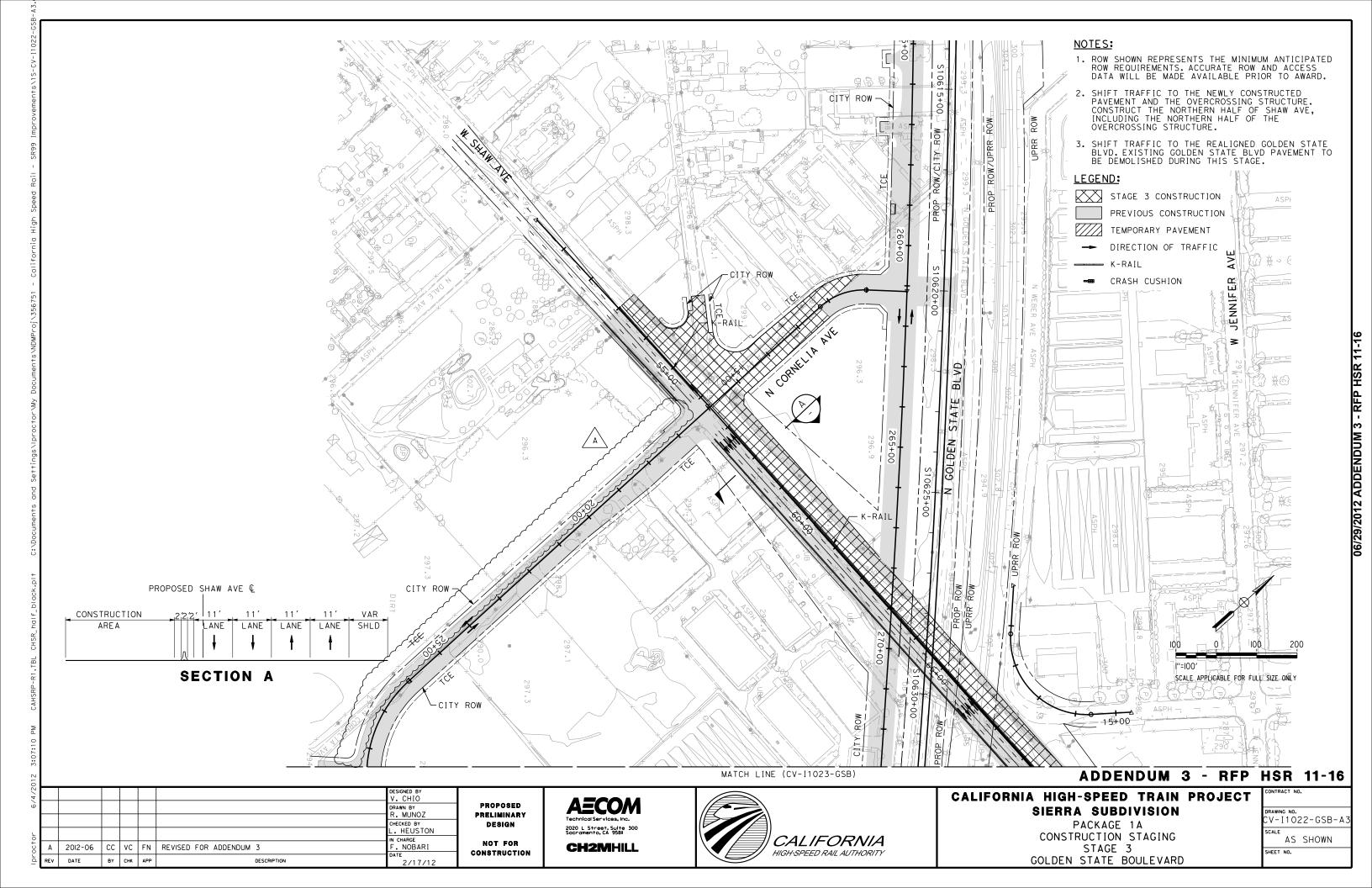


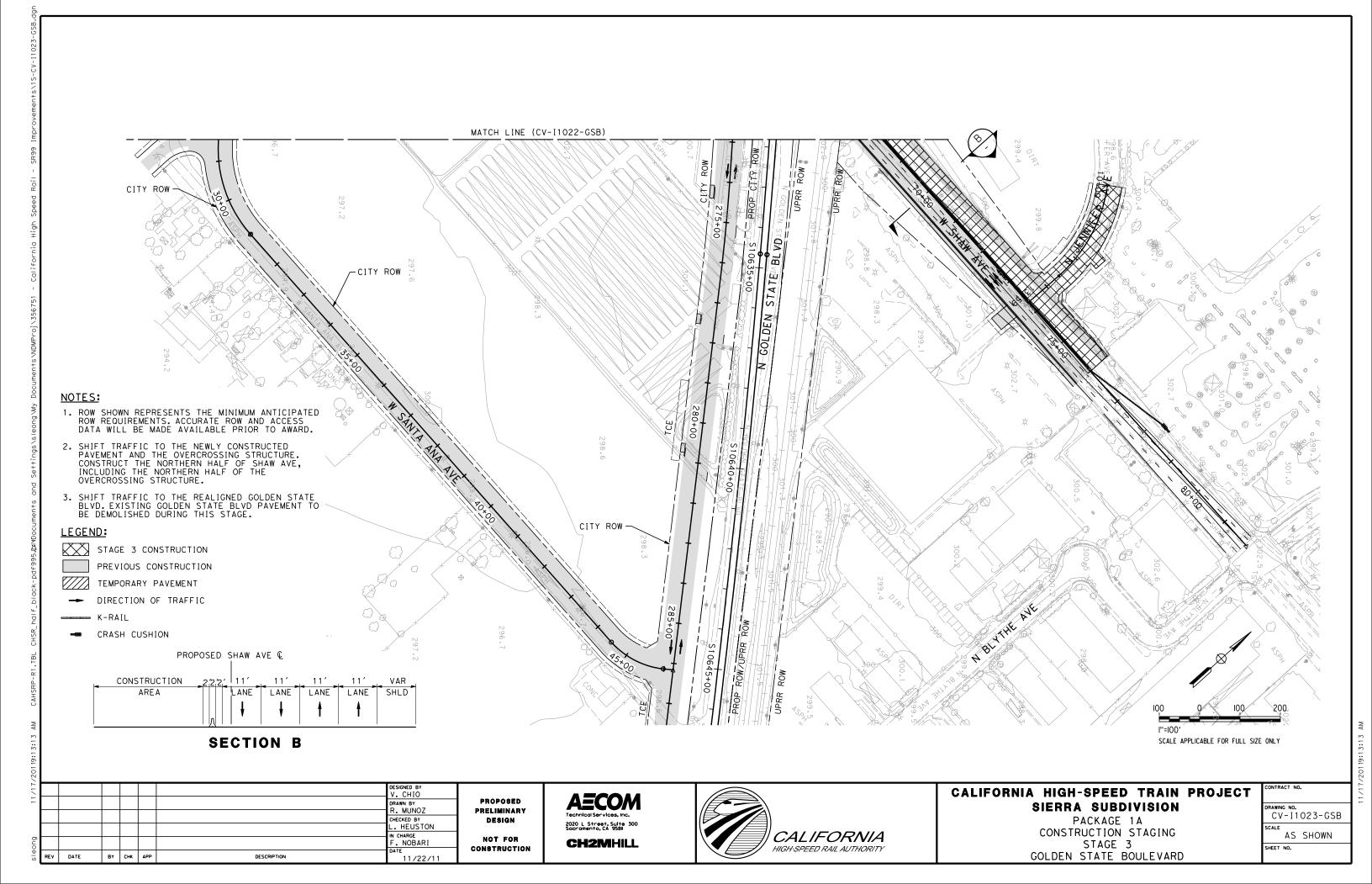


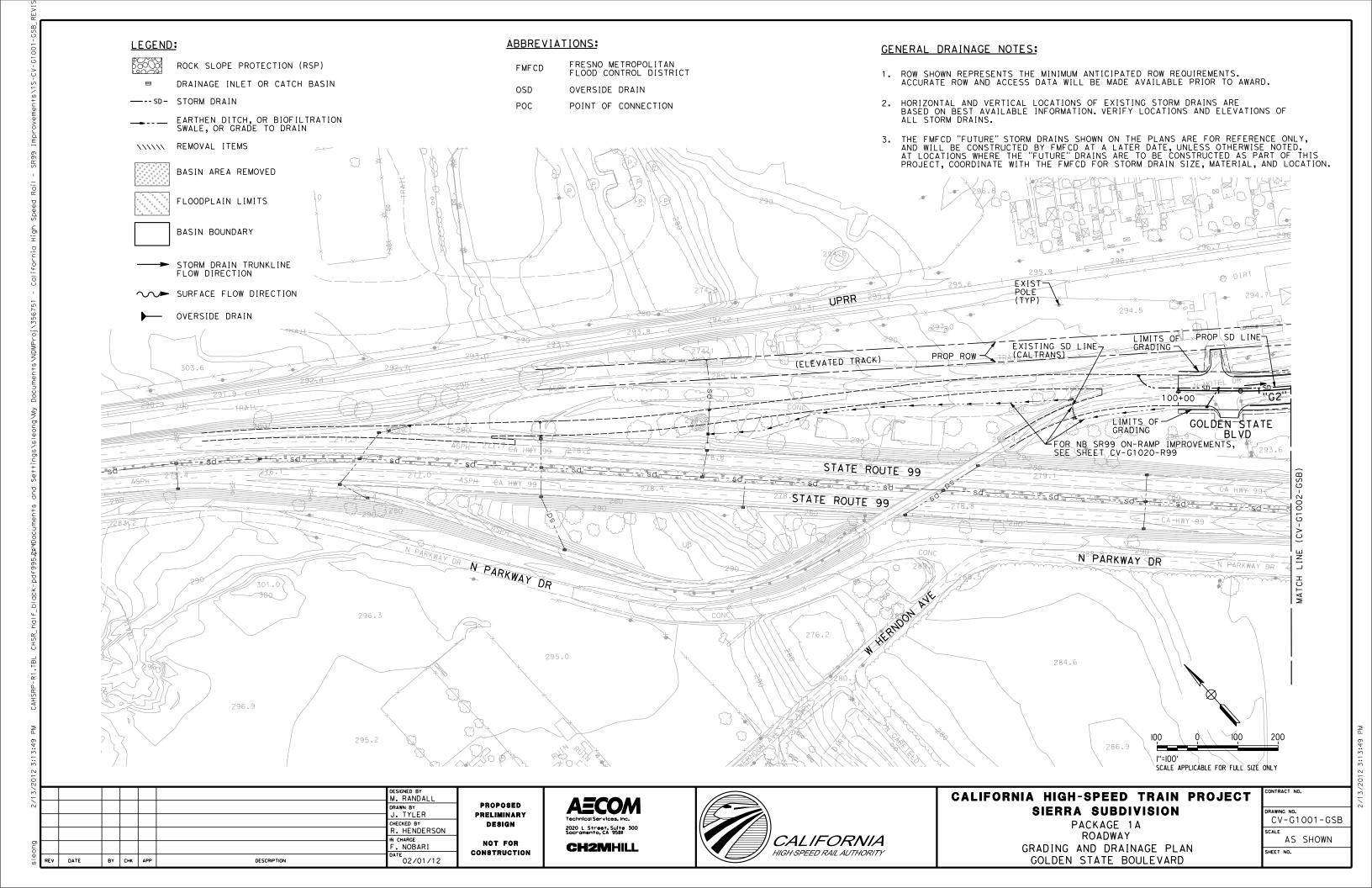


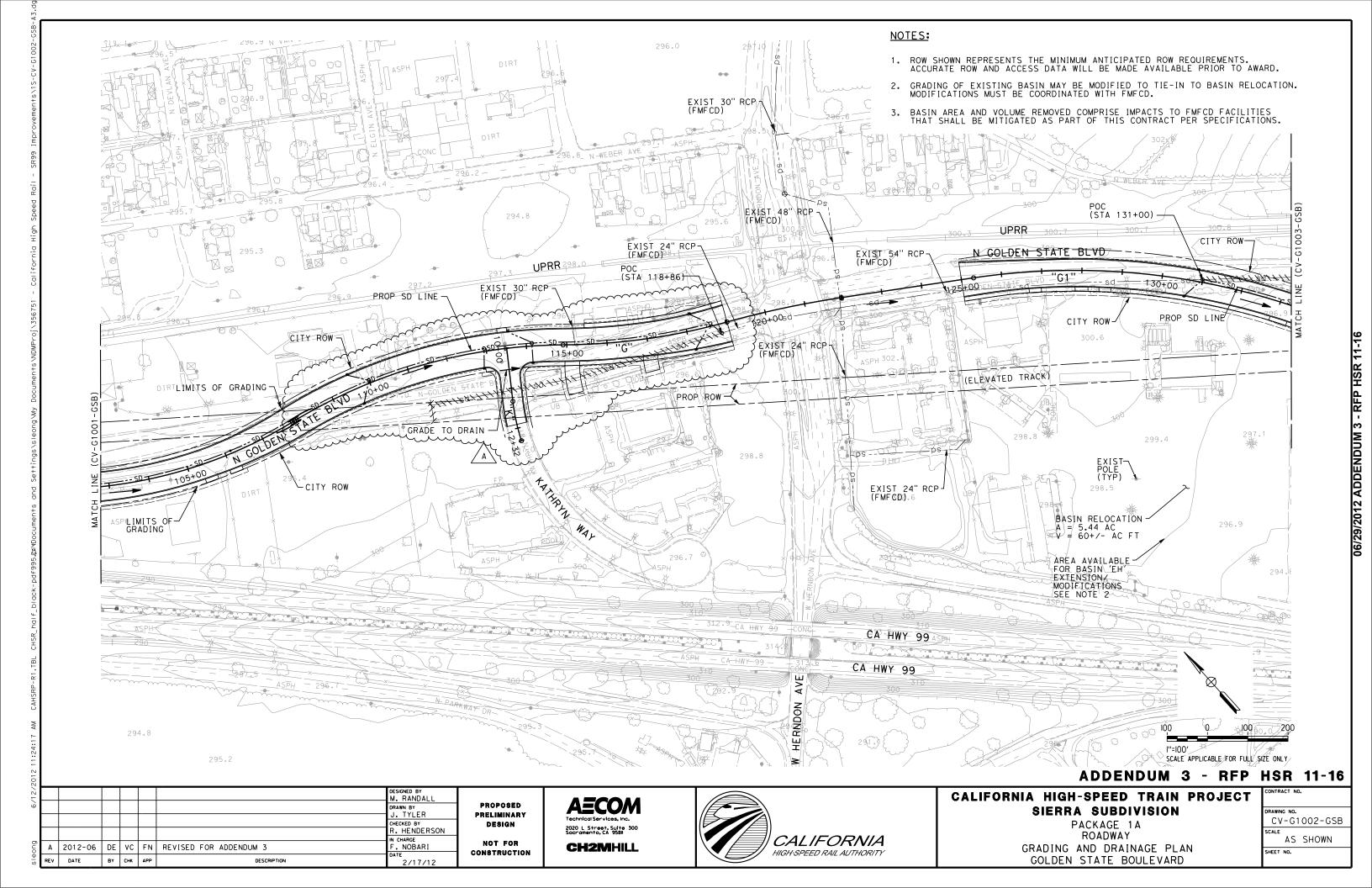


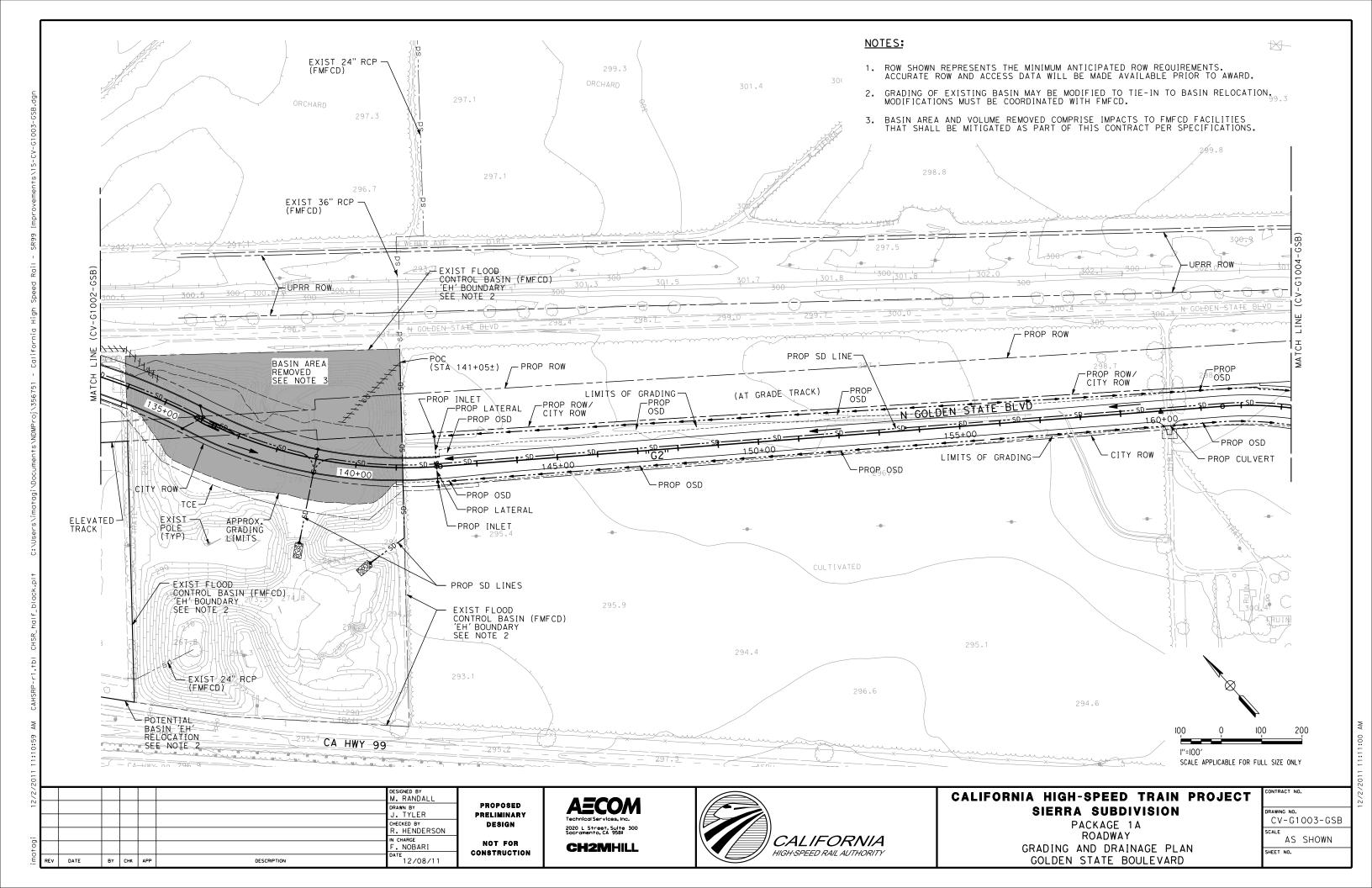


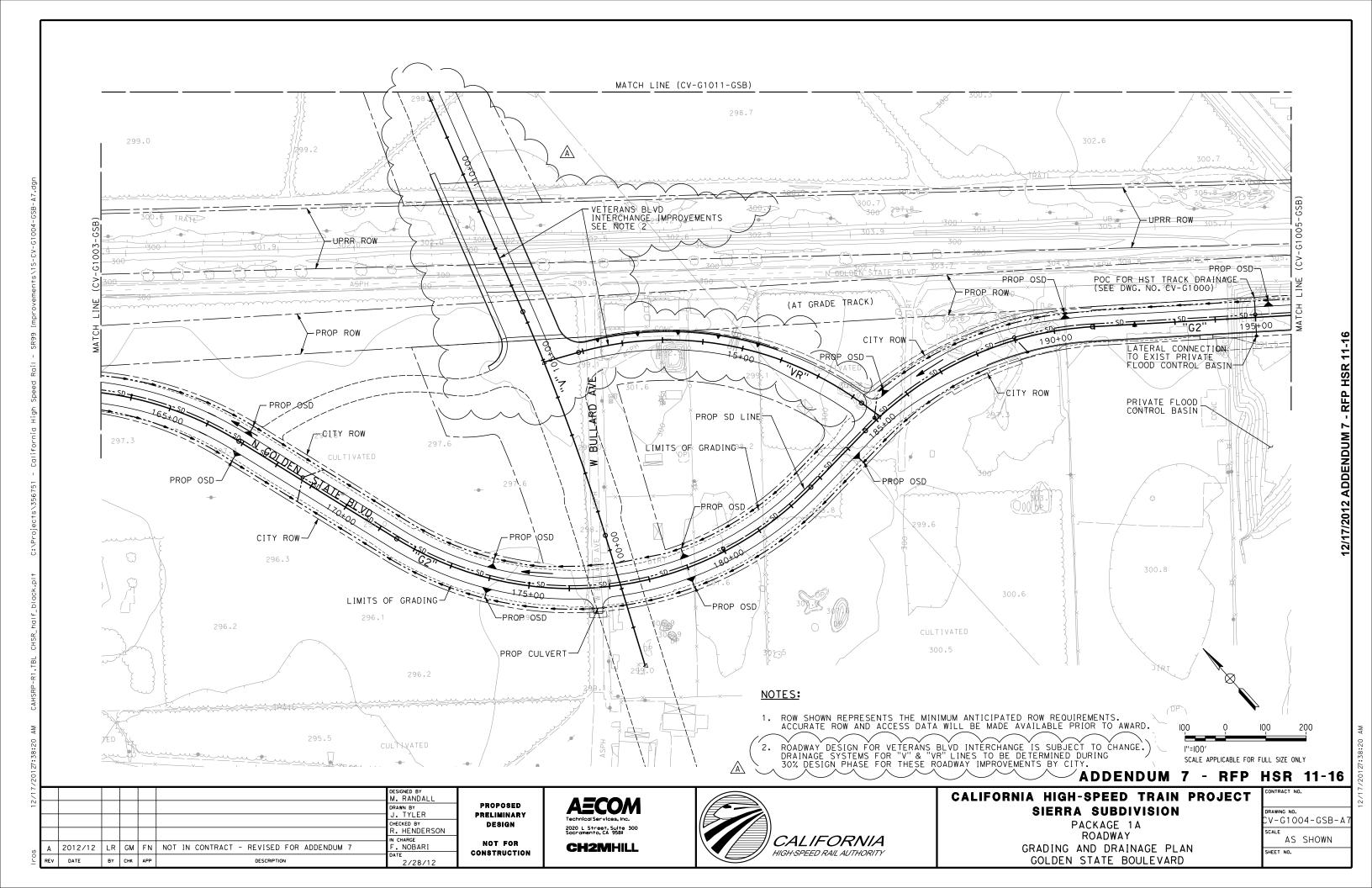


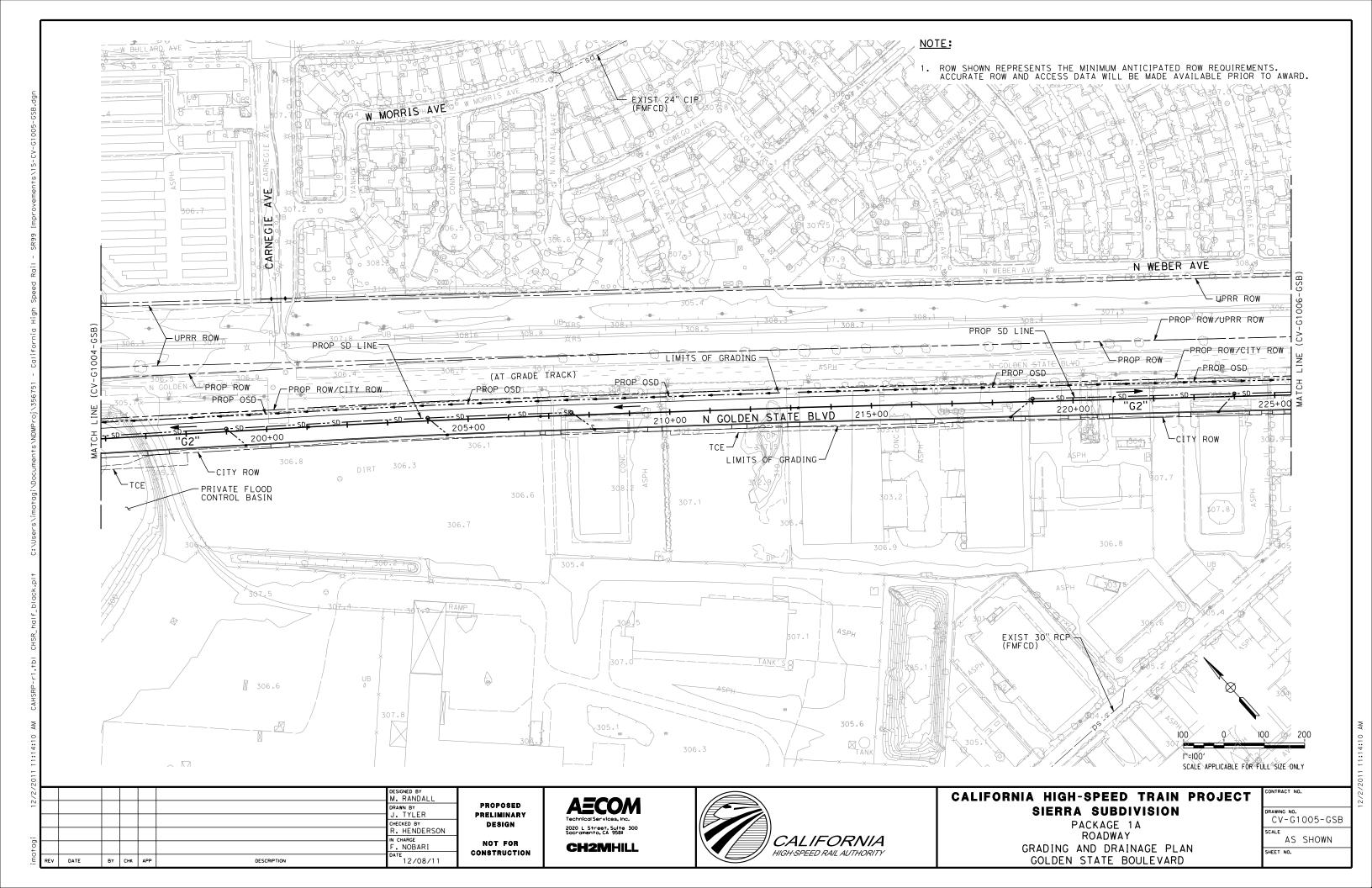


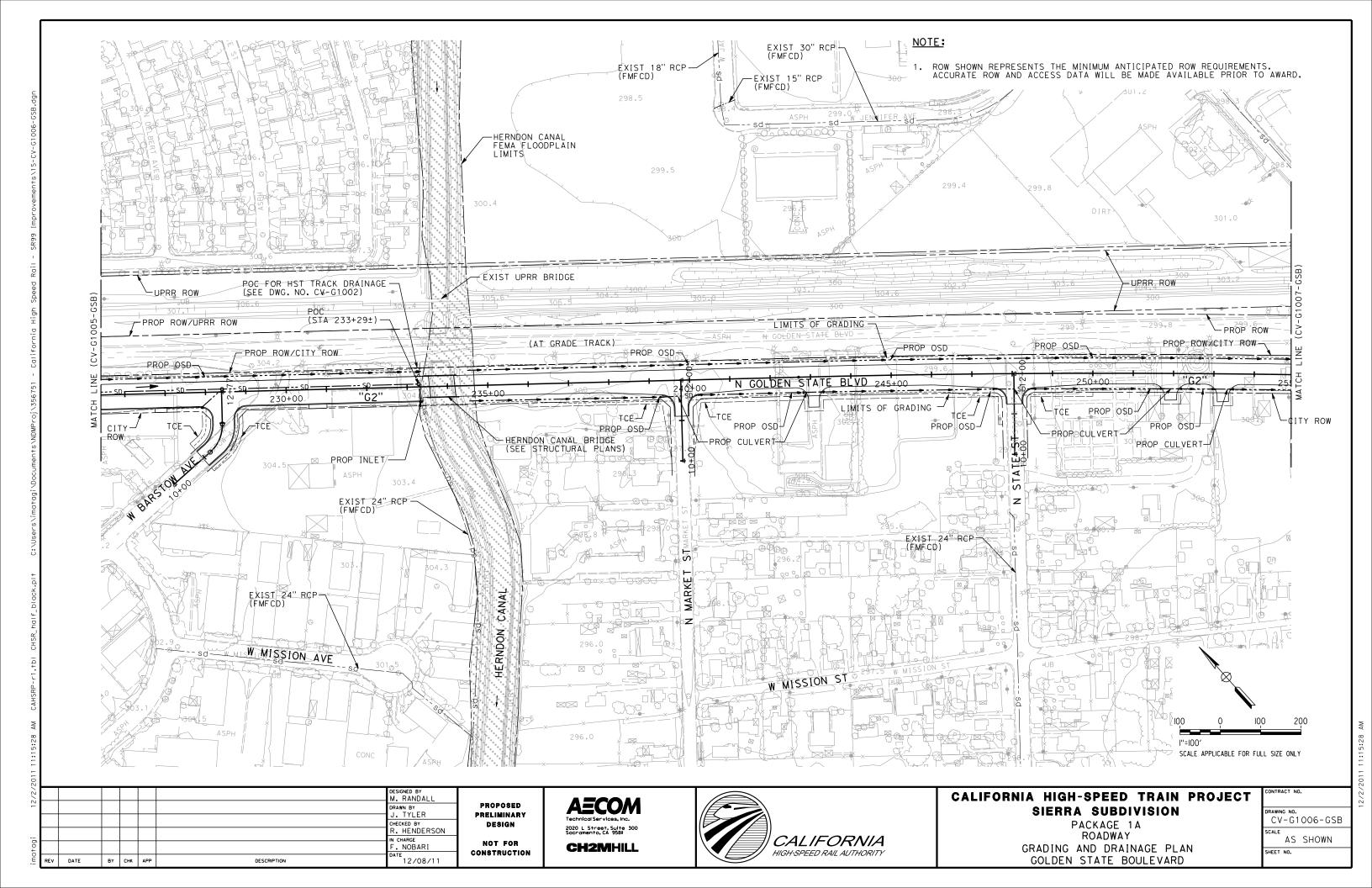


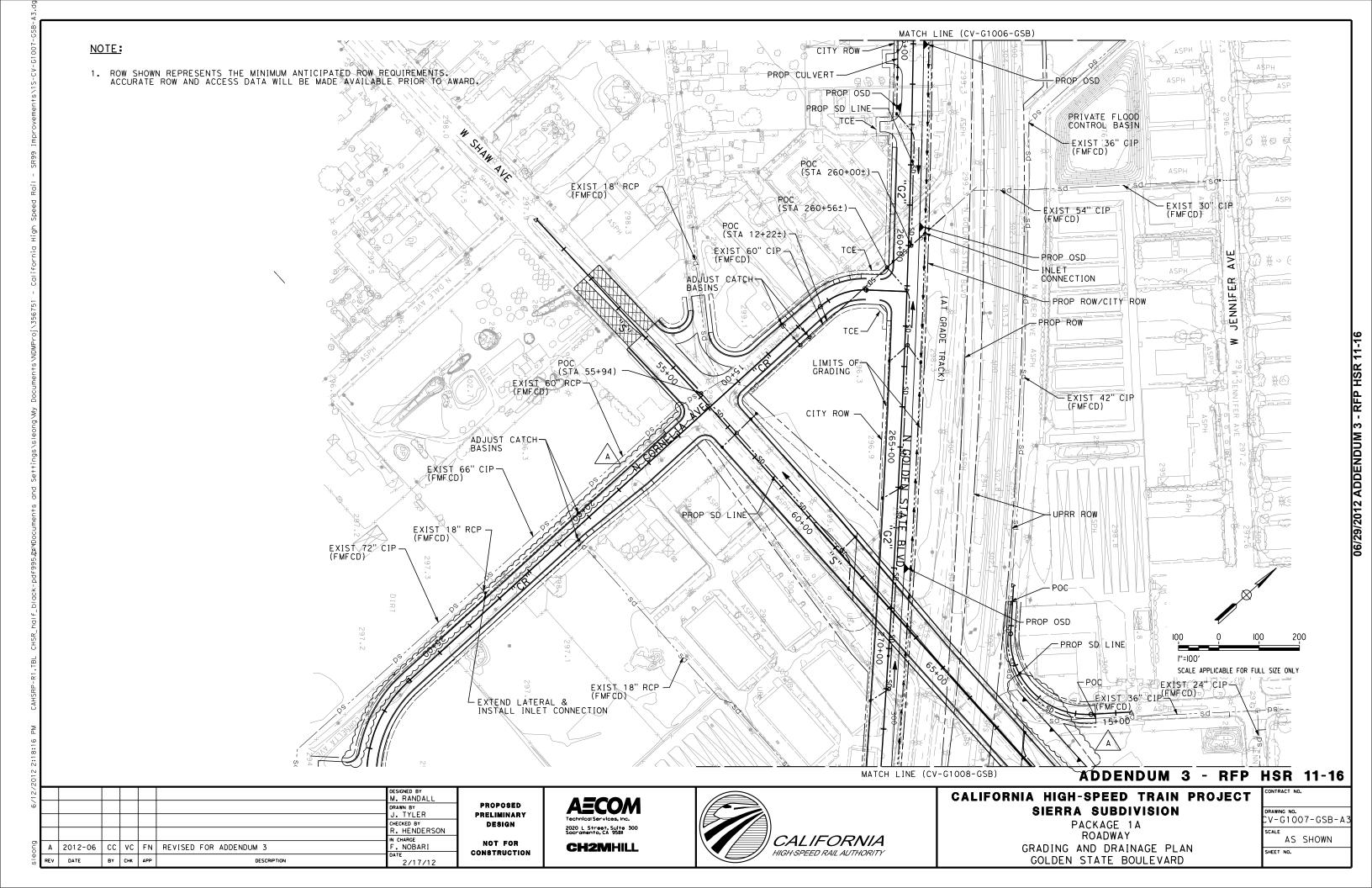




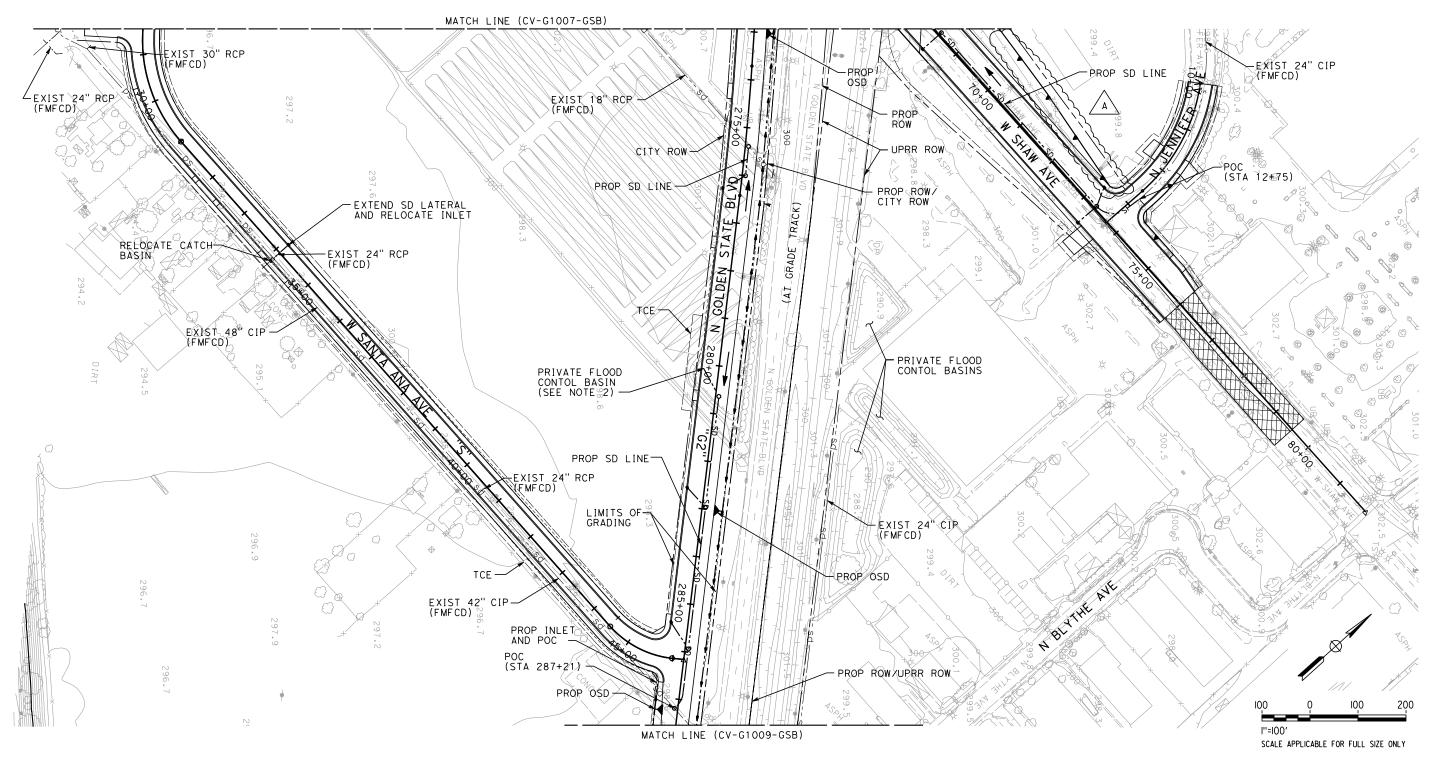








- 1. ROW SHOWN REPRESENTS THE MINIMUM ANTICIPATED ROW REQUIREMENTS. ACCURATE ROW AND ACCESS DATA WILL BE MADE AVAILABLE PRIOR TO AWARD.
- 2. PROVIDE DIRECT CONNECTION TO PRIVATE STORM DRAIN AT BASIN.



ADDENDUM 3 - RFP HSR 11-16

	i	i I	IN TIENDENSON	
			DRAWN BY J. TYLER CHECKED BY R. HENDERSON	
			DESIGNED BY	

PROPOSED
PRELIMINARY
DESIGN
NOT FOR
CONSTRUCTION

AECOM
Technical Services, inc.
2020 L Street, Suite 300
Sacramento, CA 9581
CH2MHILL

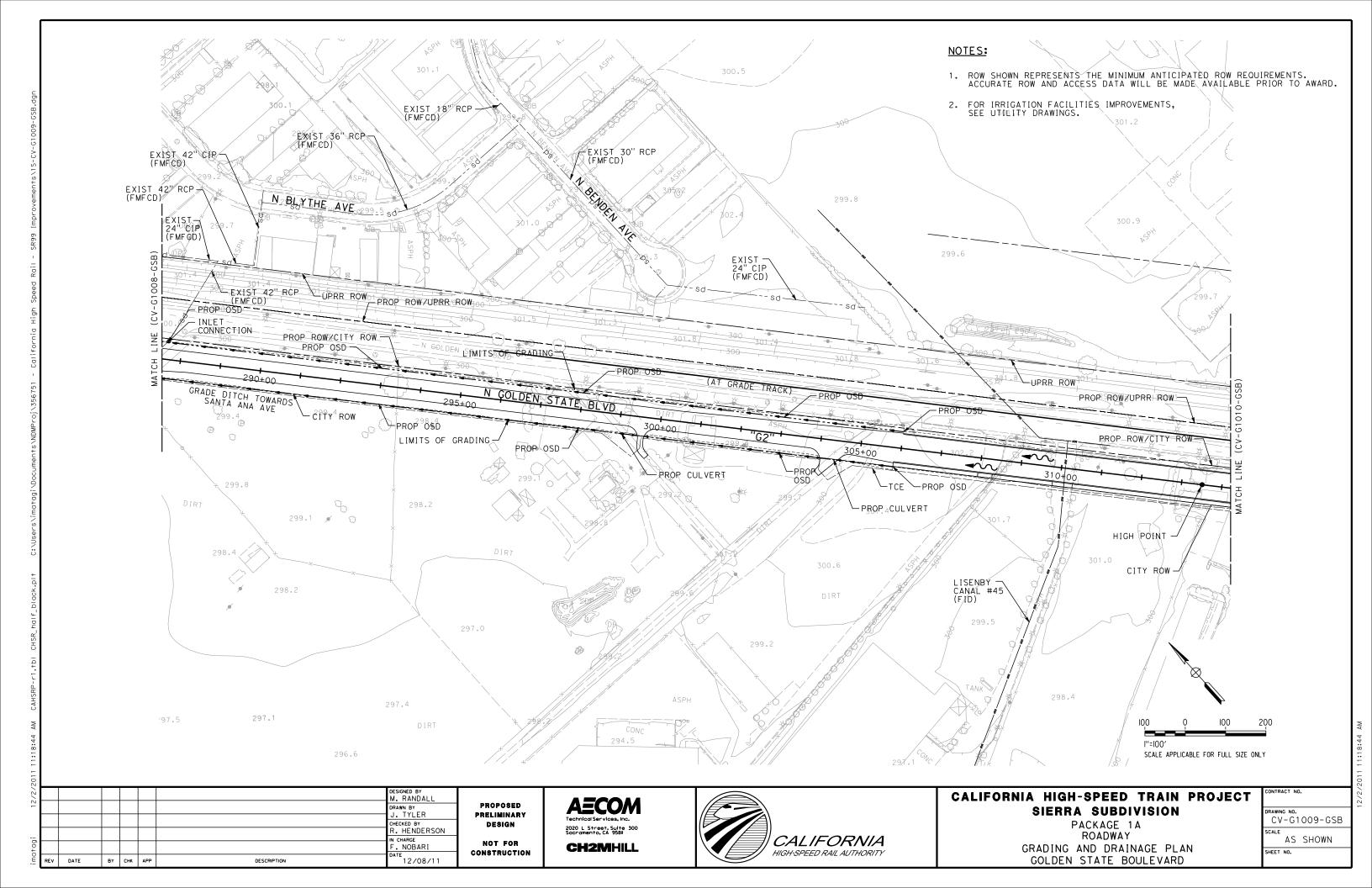


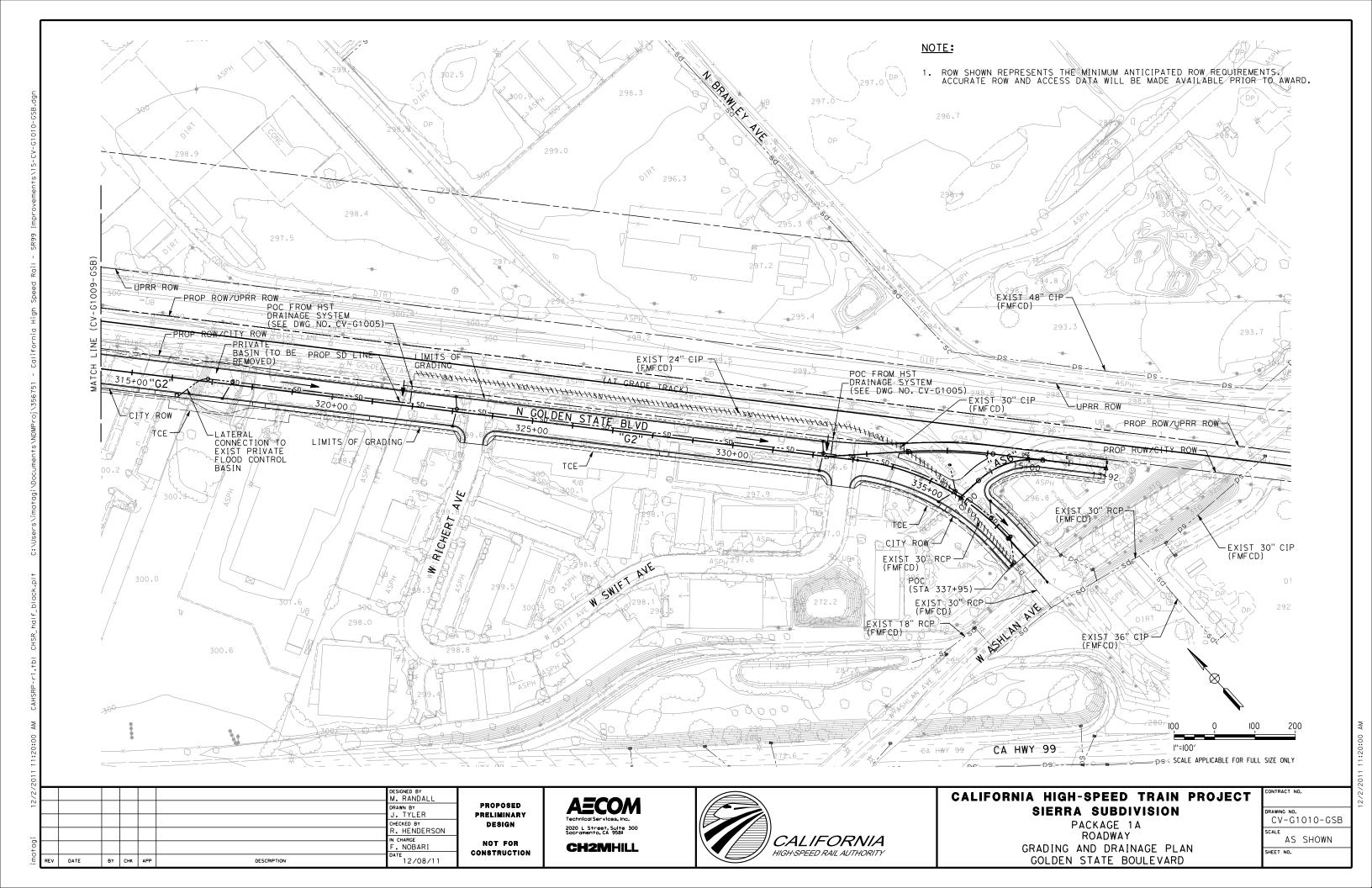
CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION PACKAGE 1A

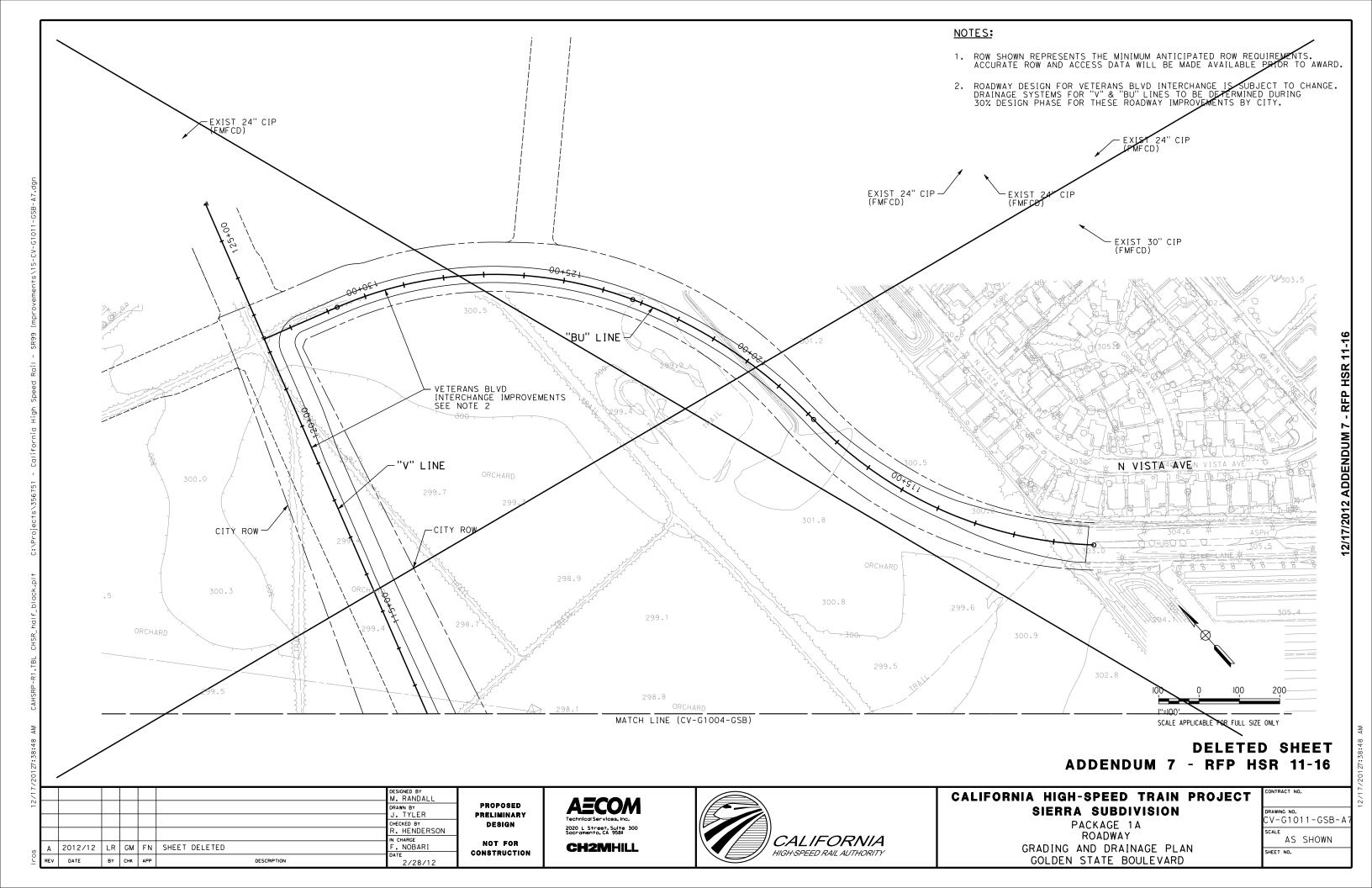
PACKAGE 1A ROADWAY GRADING AND DRAINAGE PLAN GOLDEN STATE BOULEVARD

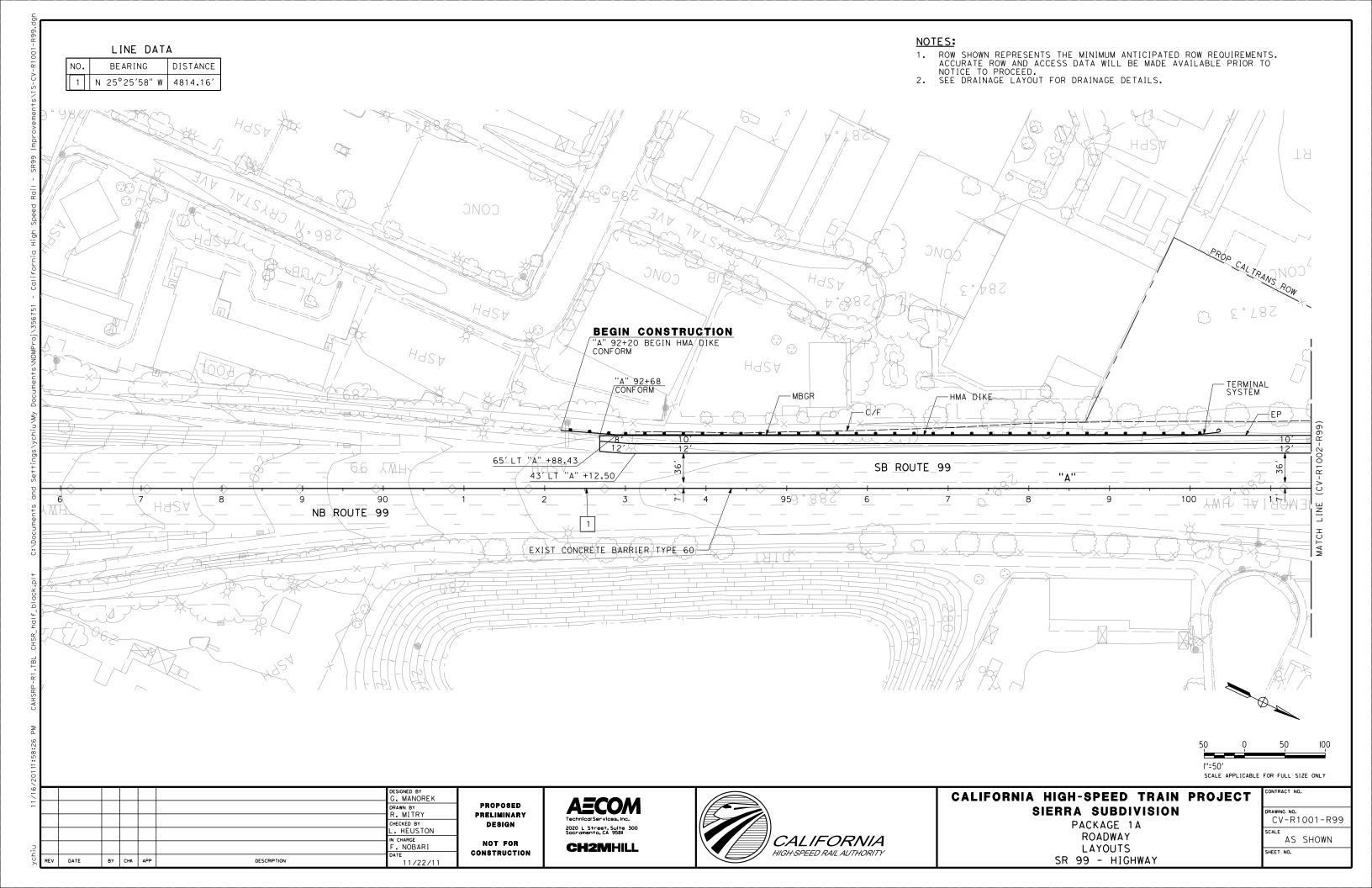
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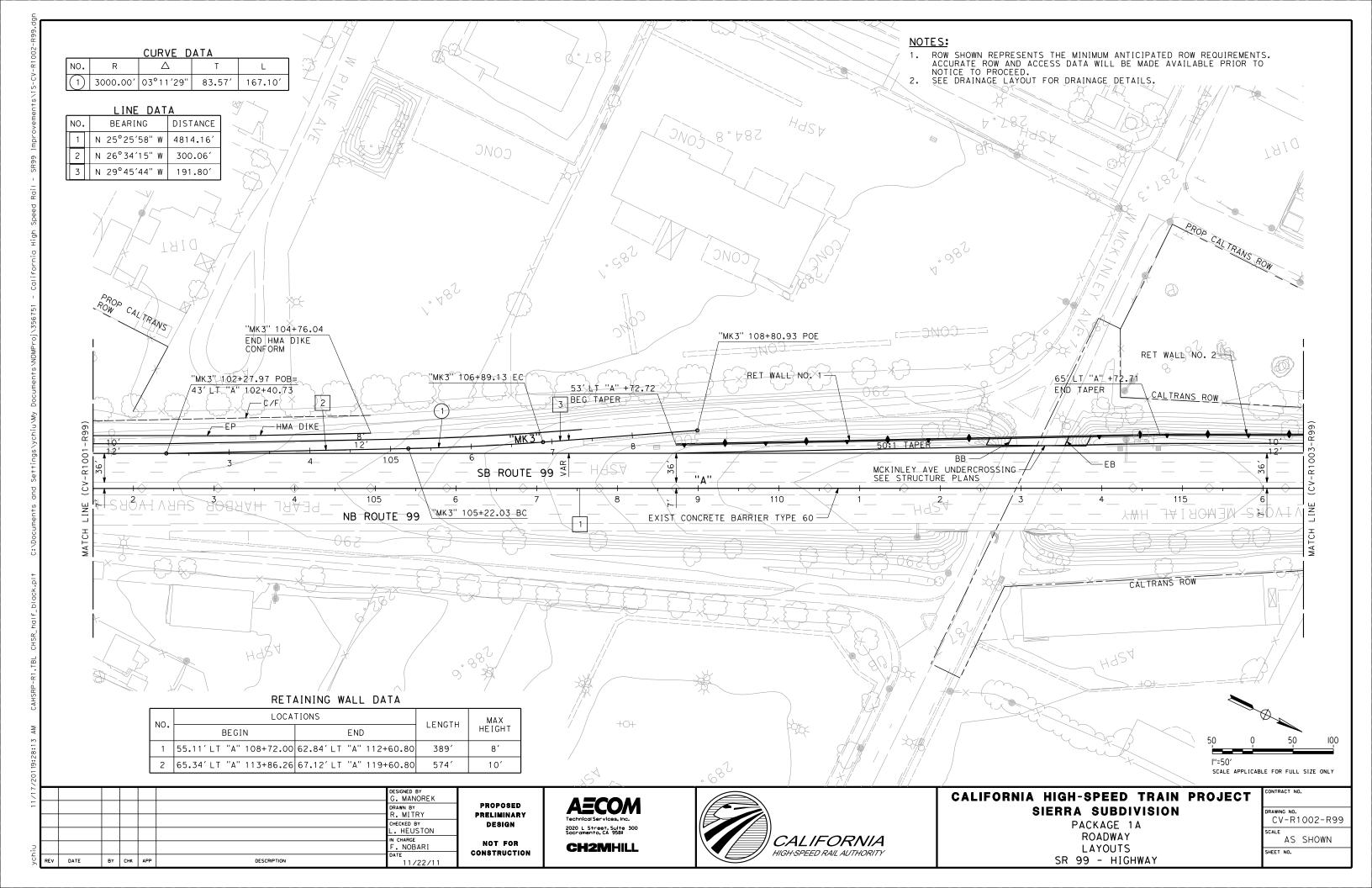
06/29/2012 ADDENDUM 3 - RFP HSR 11-16

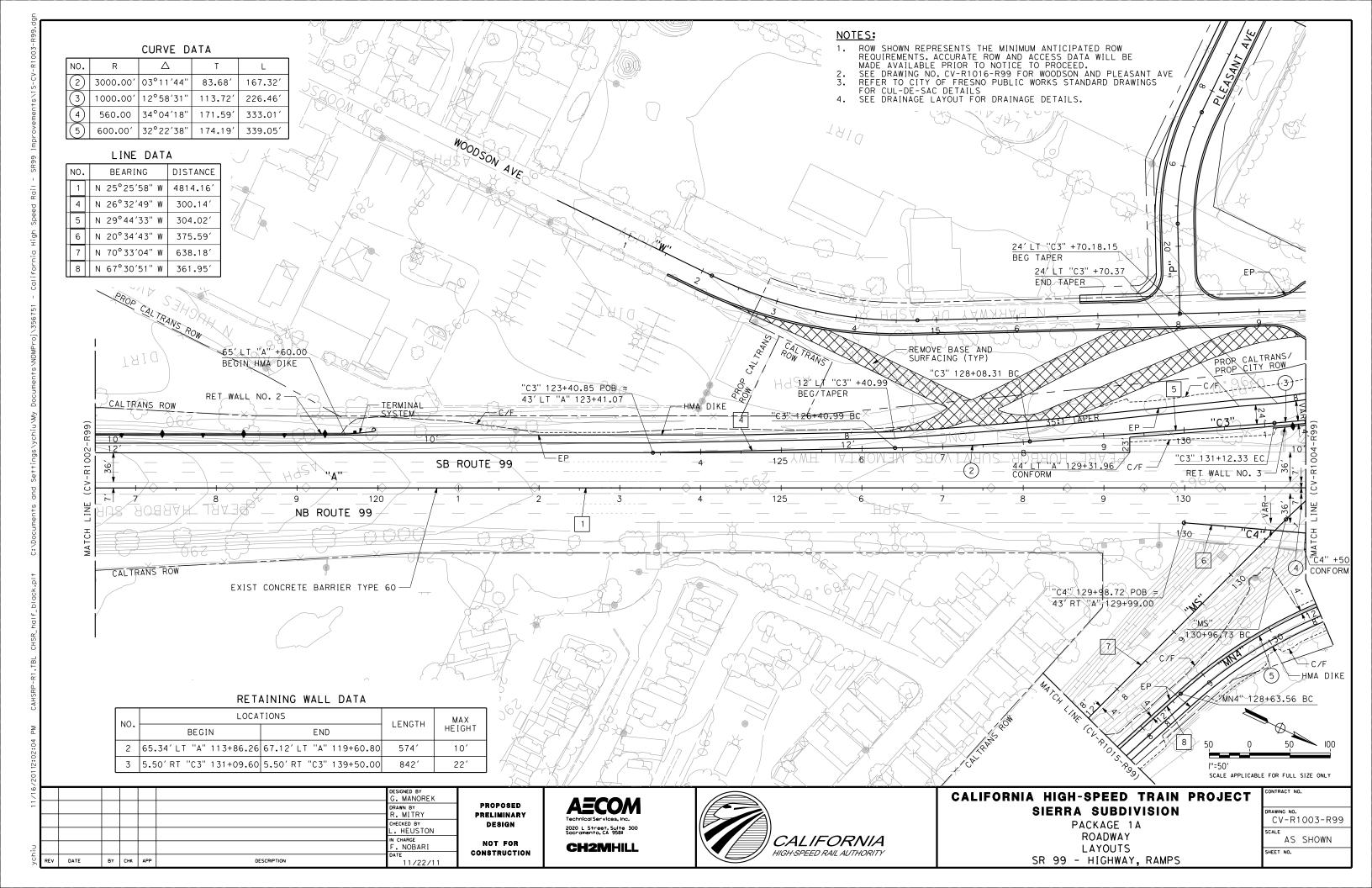


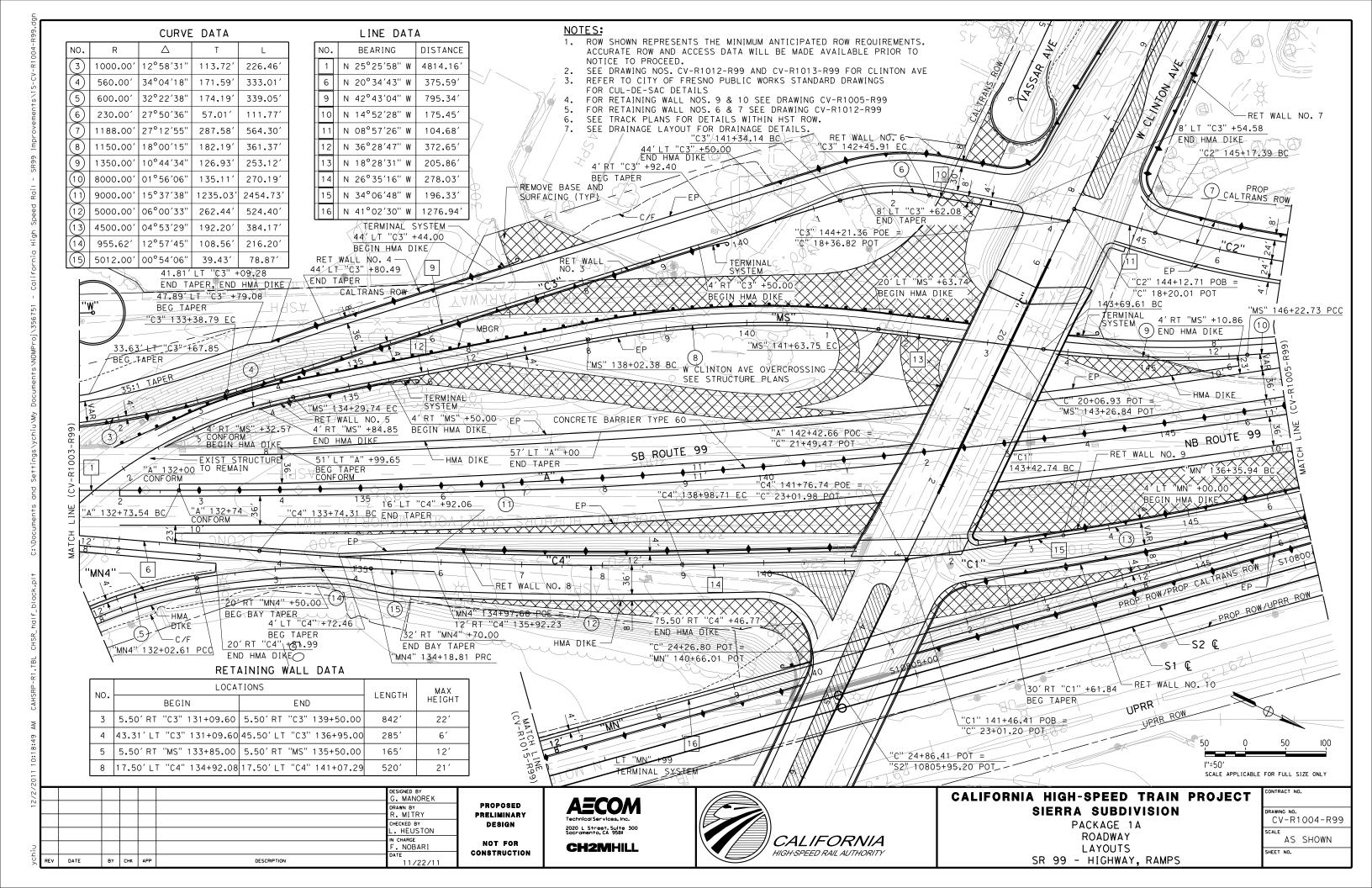


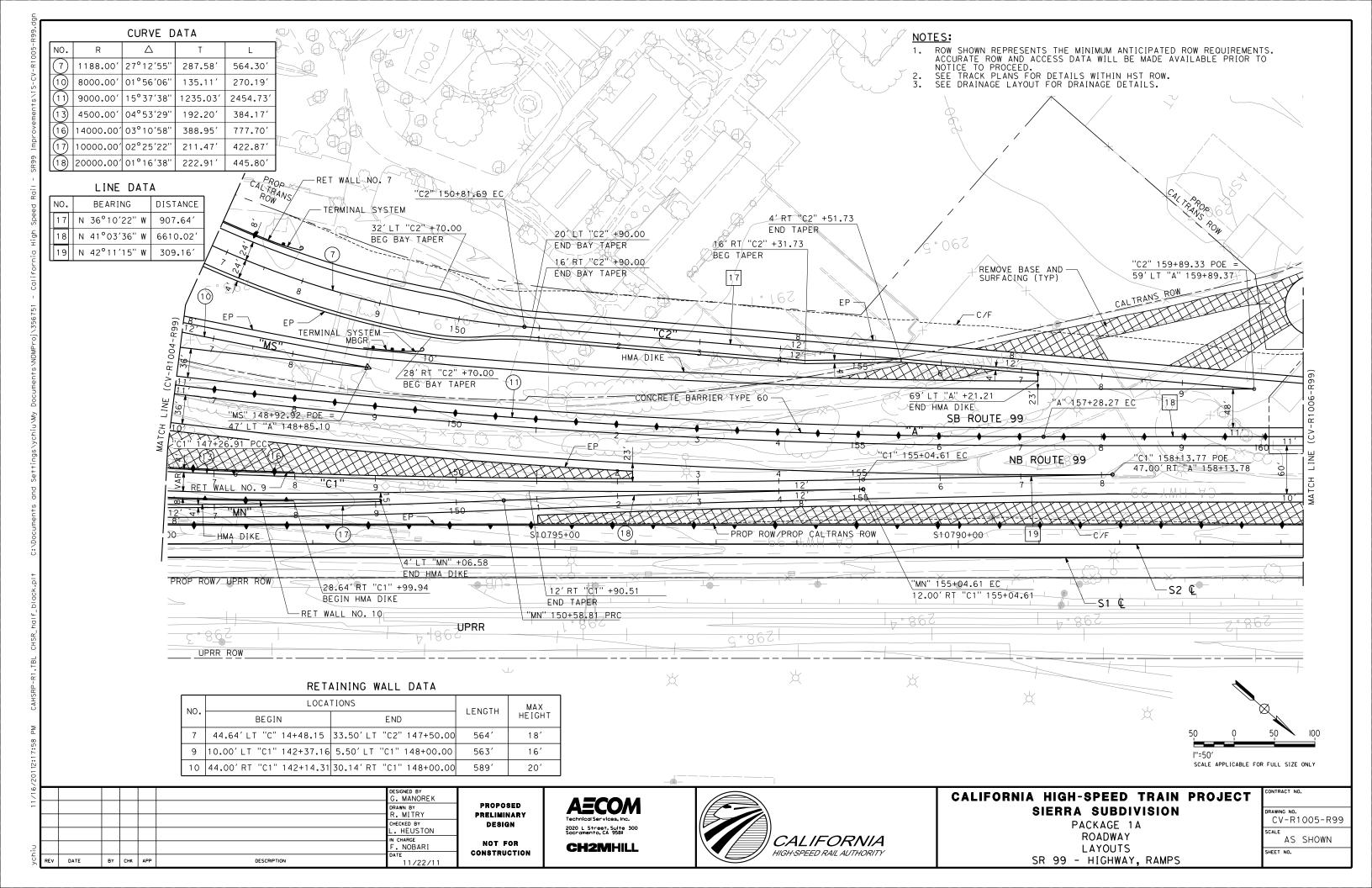


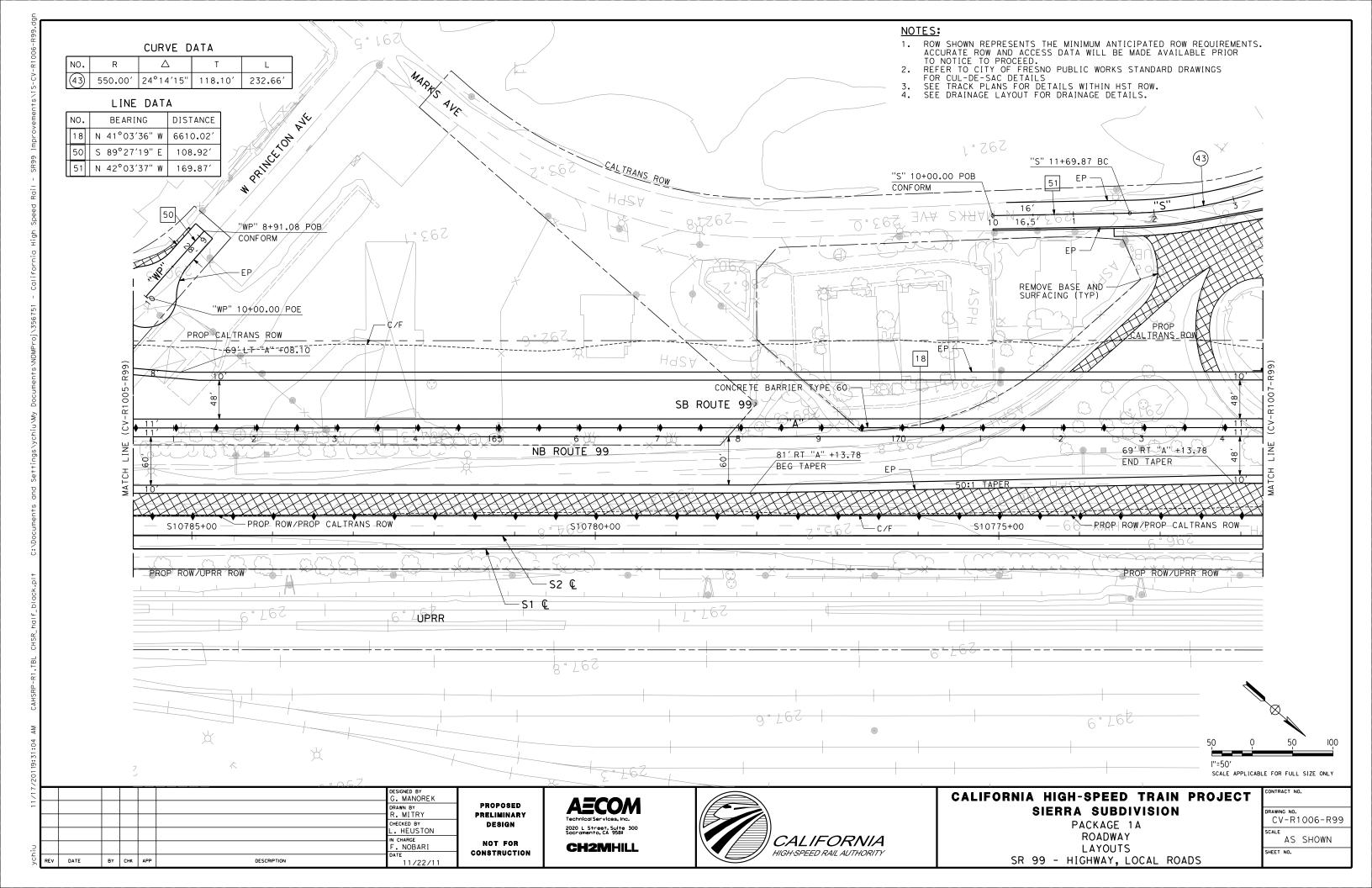


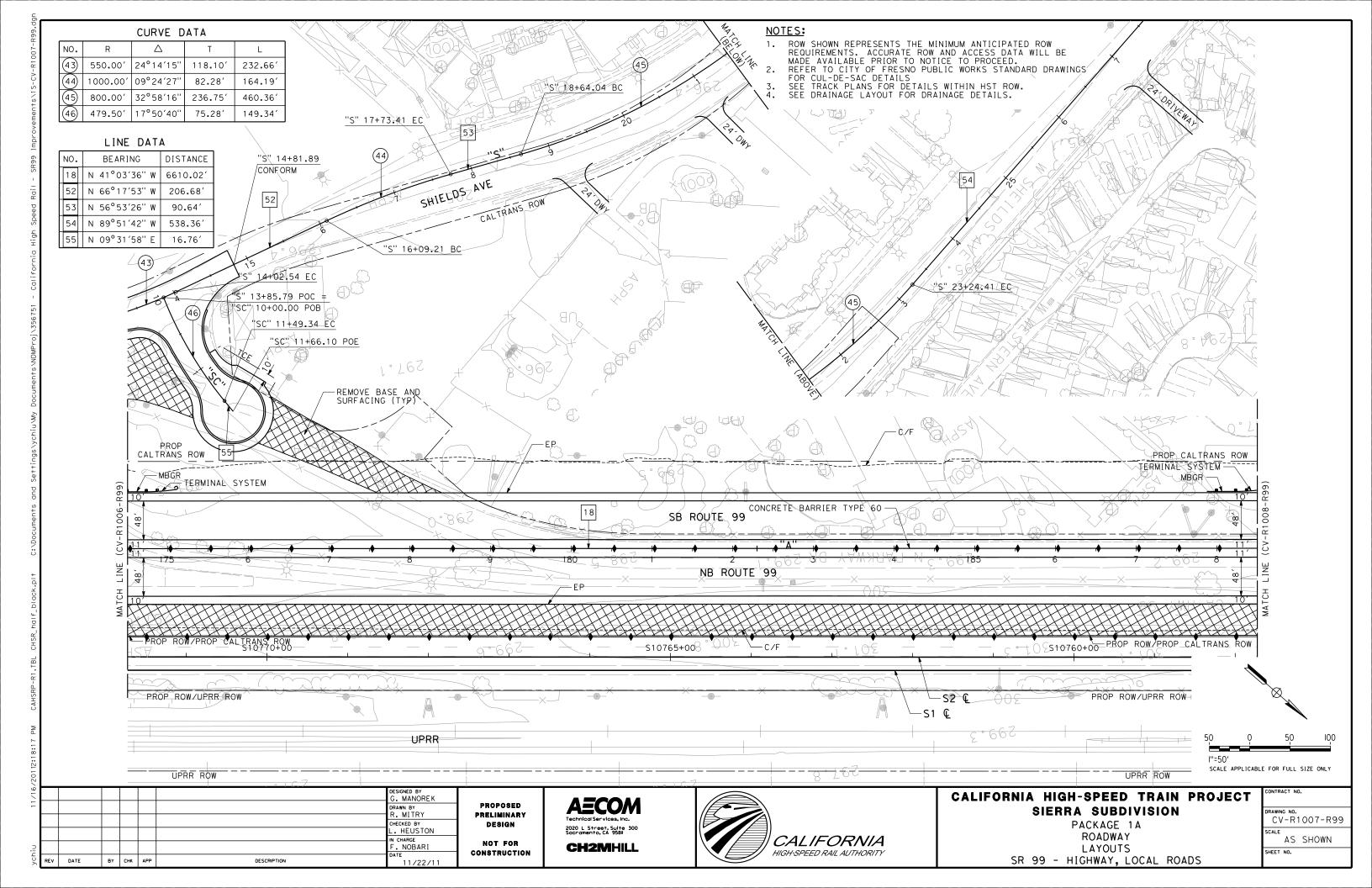


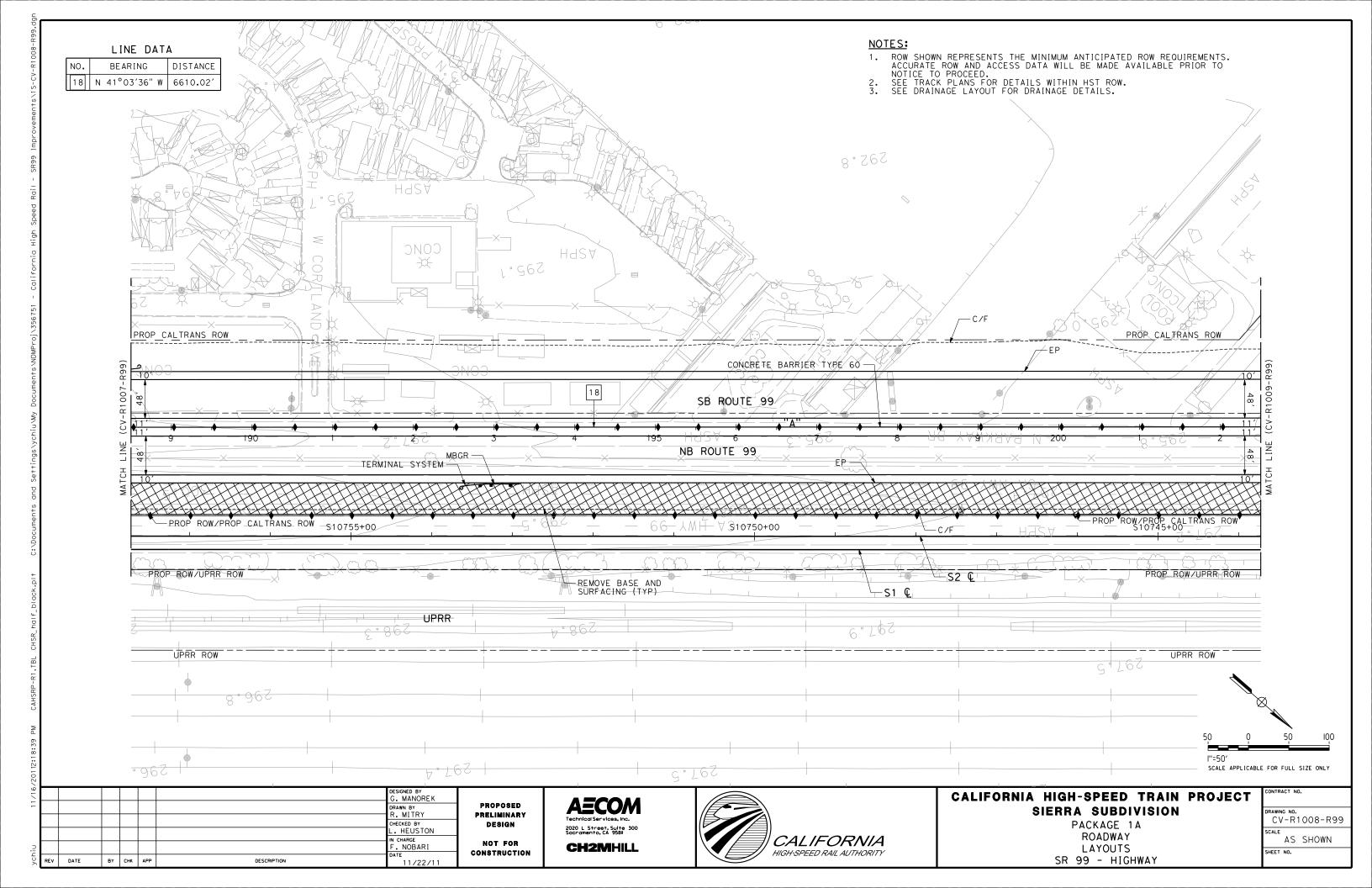


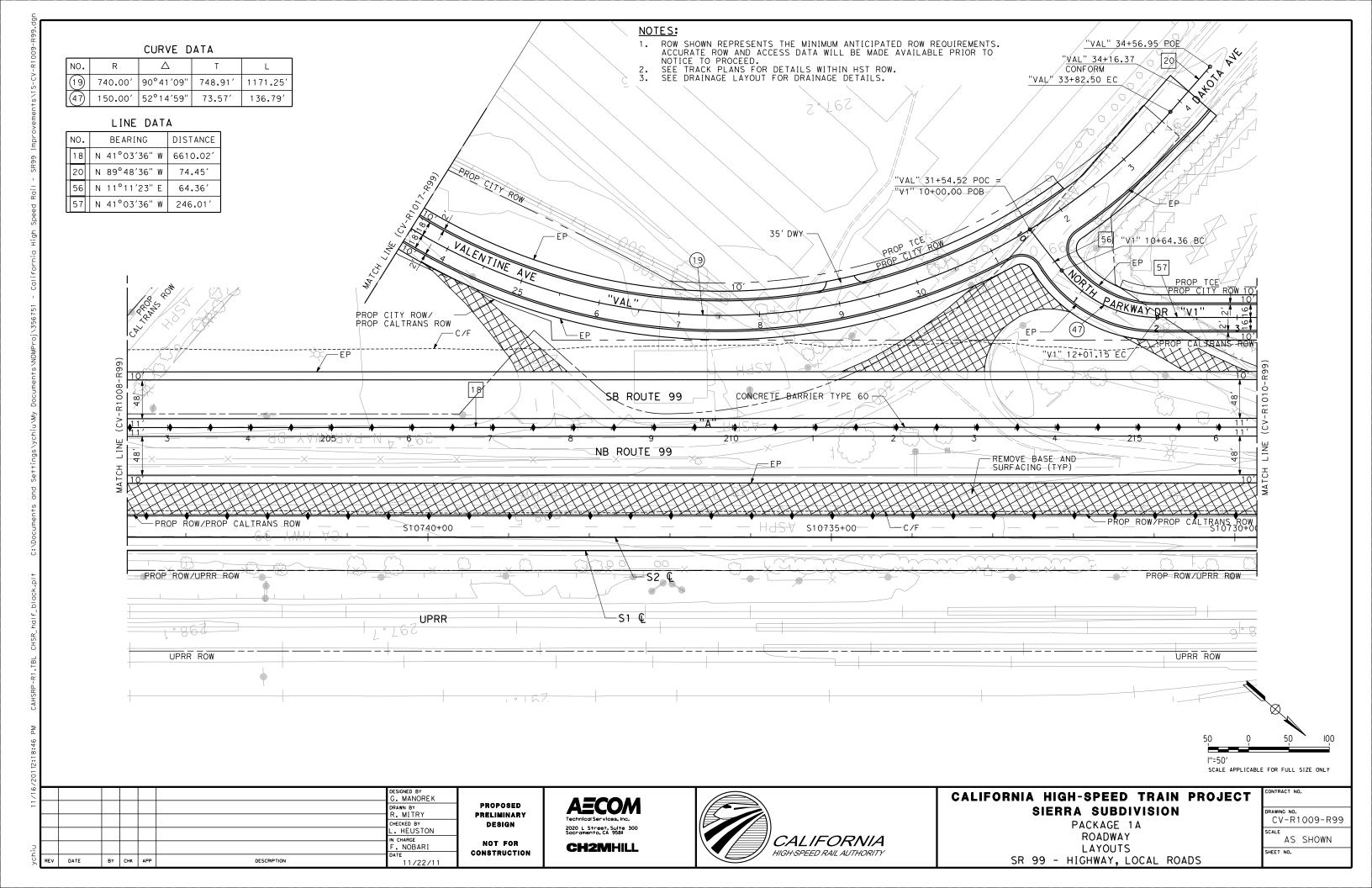


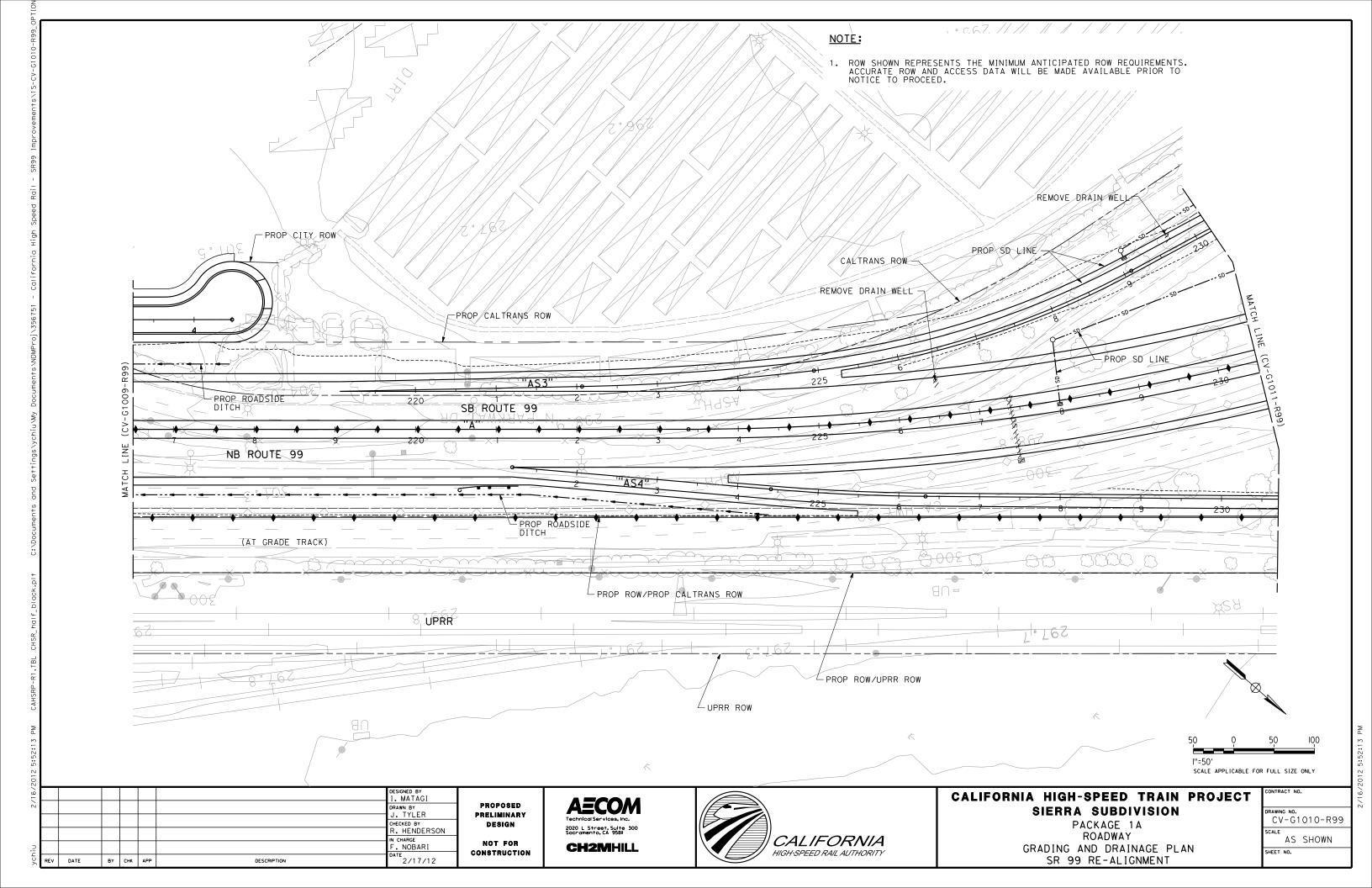


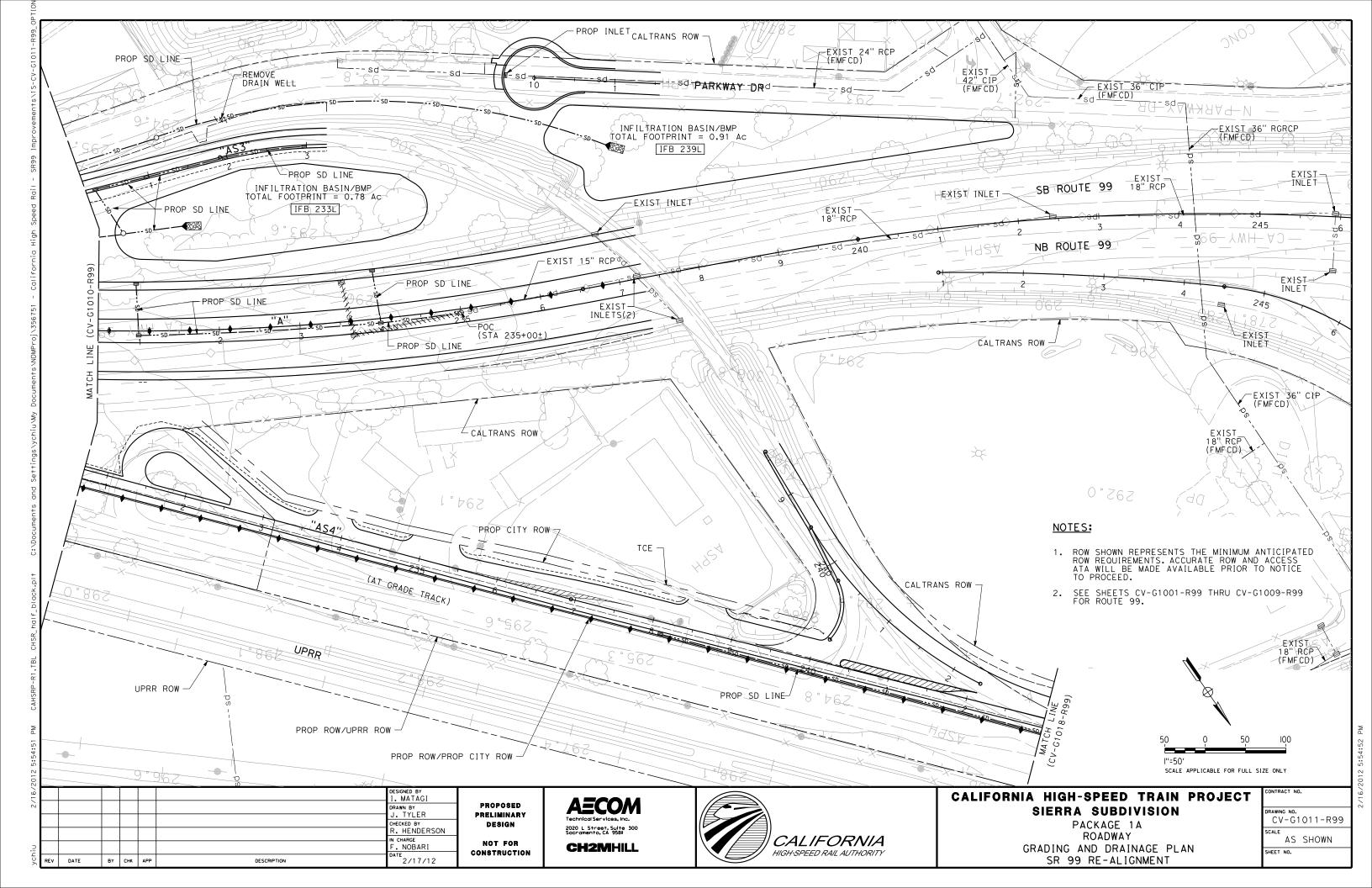


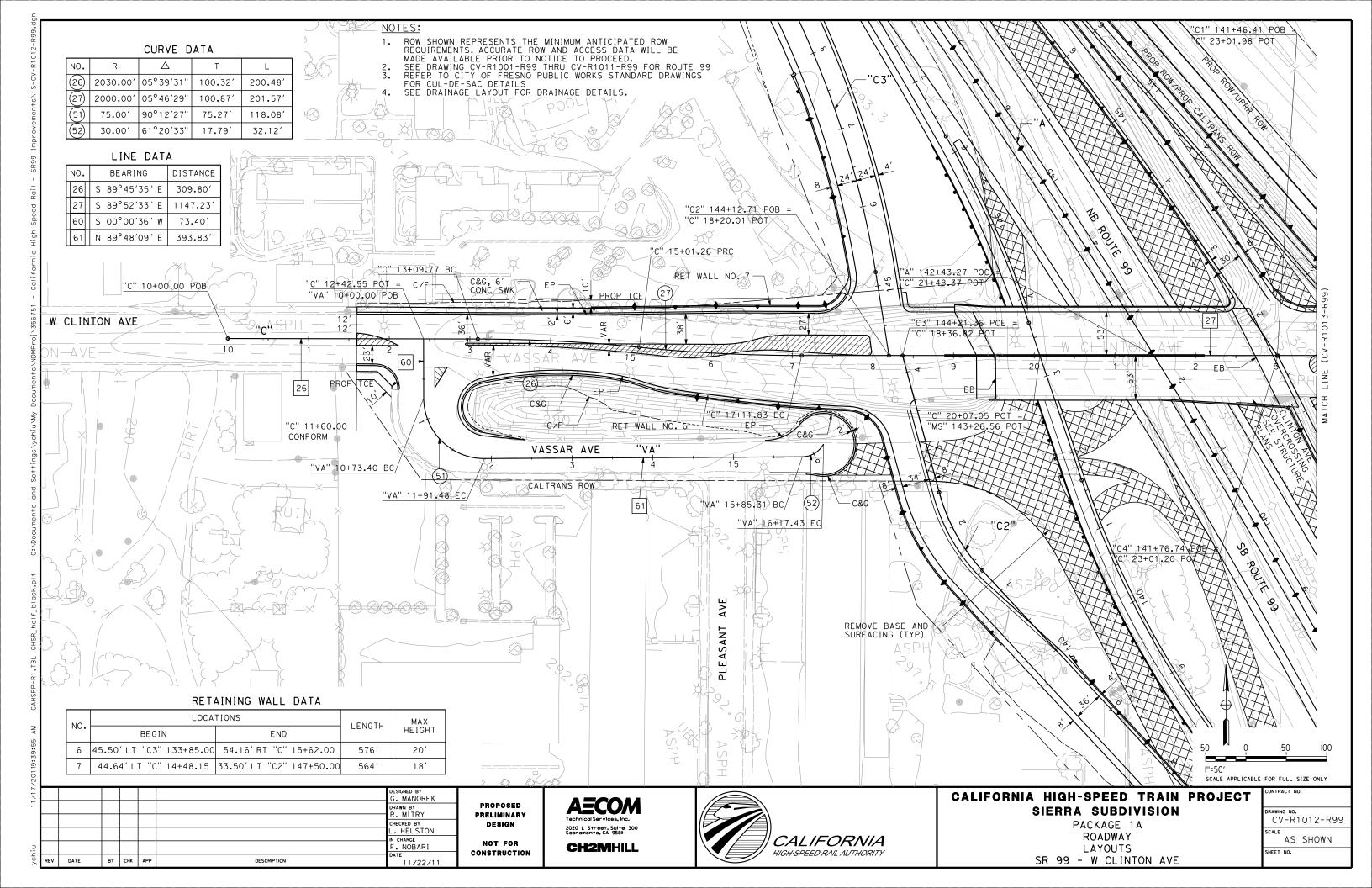


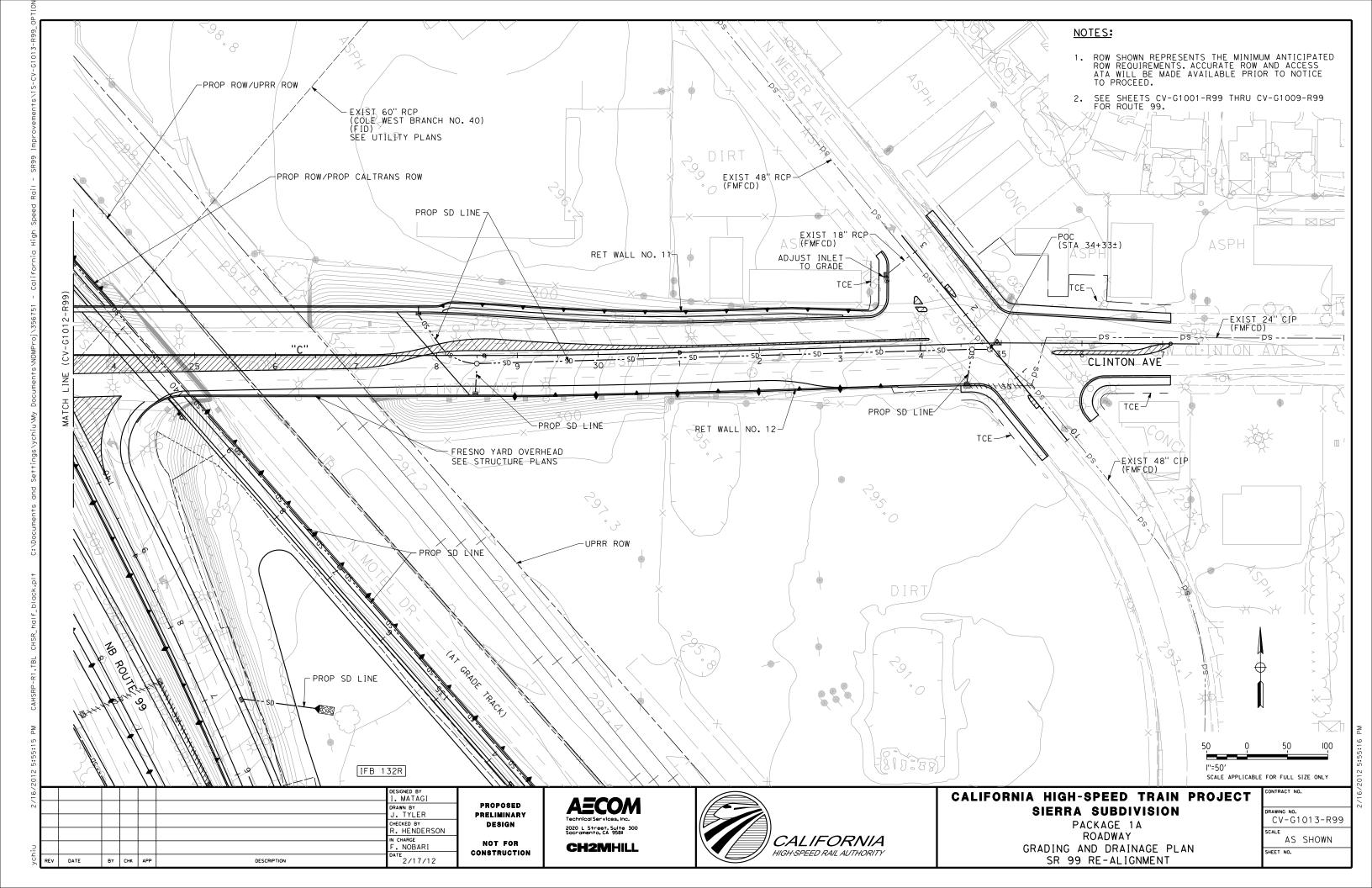


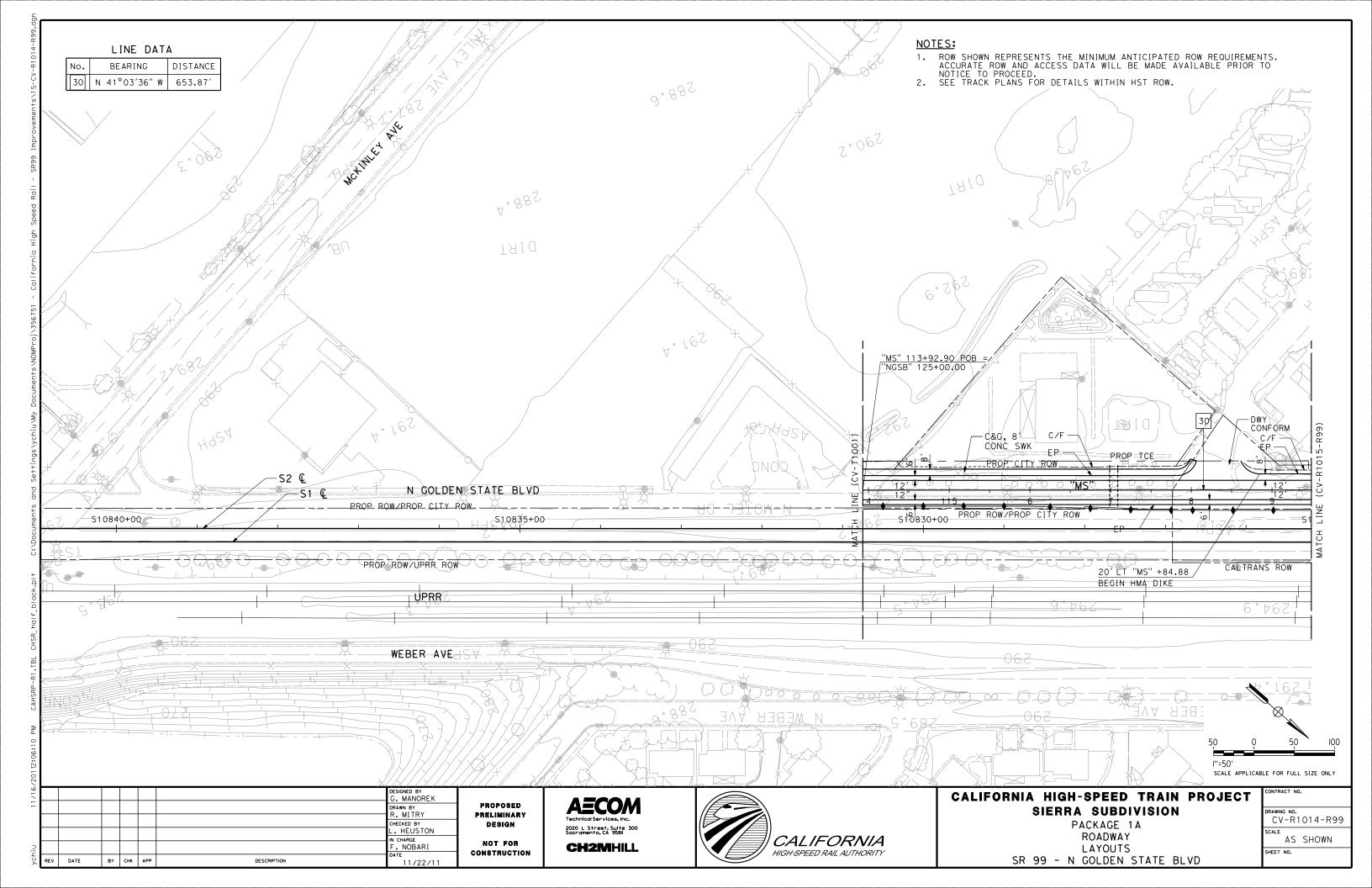


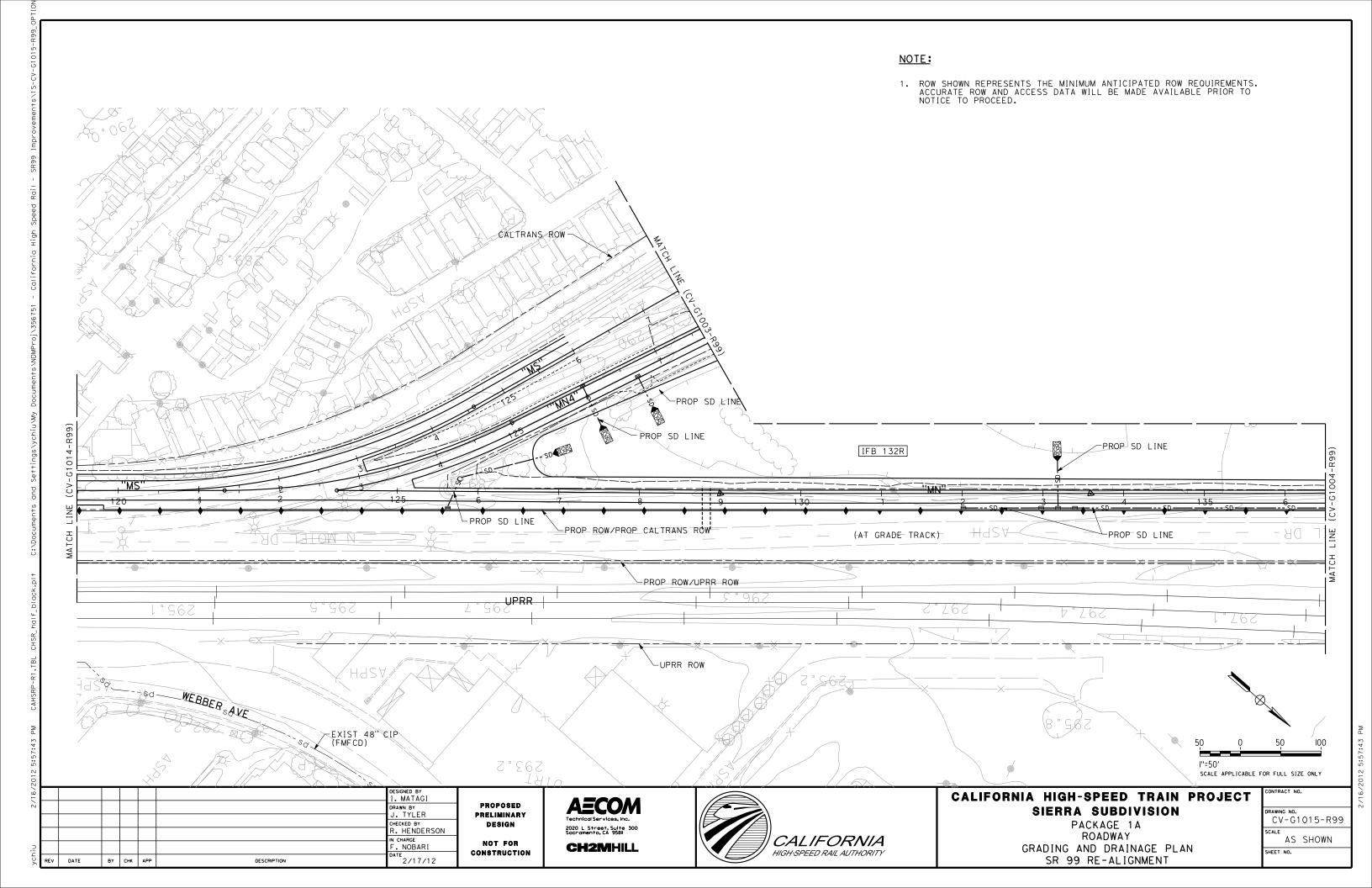


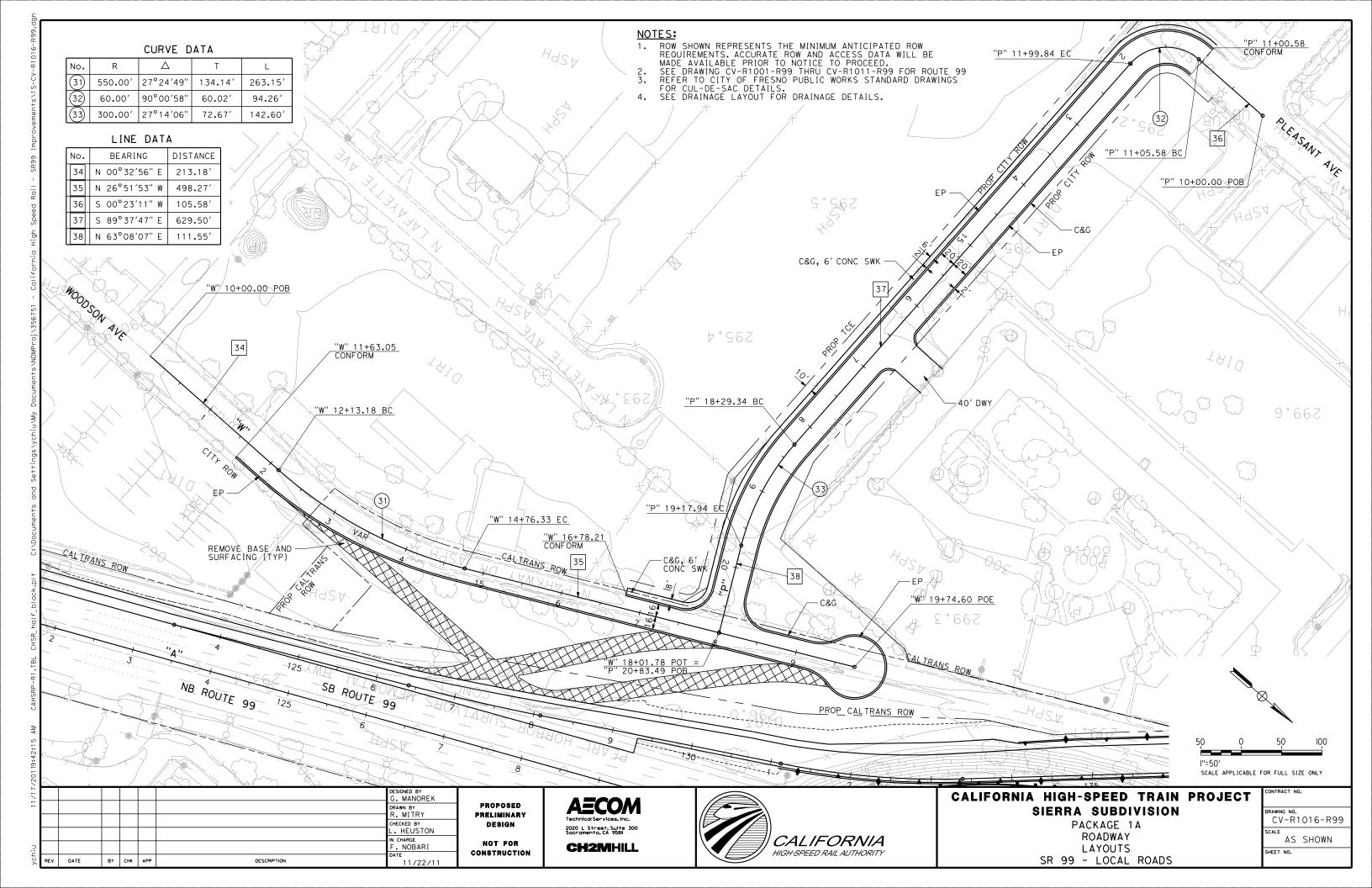


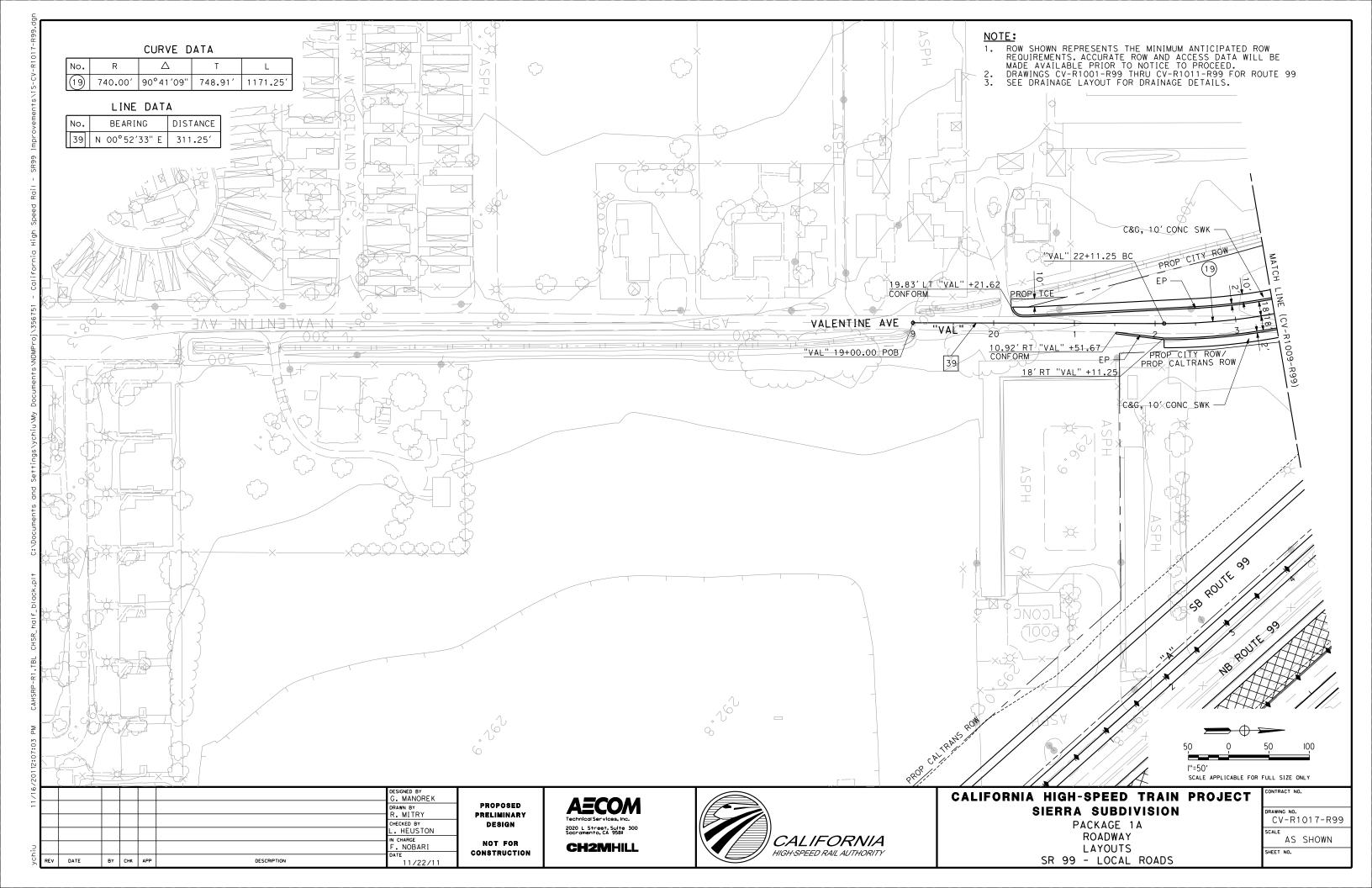


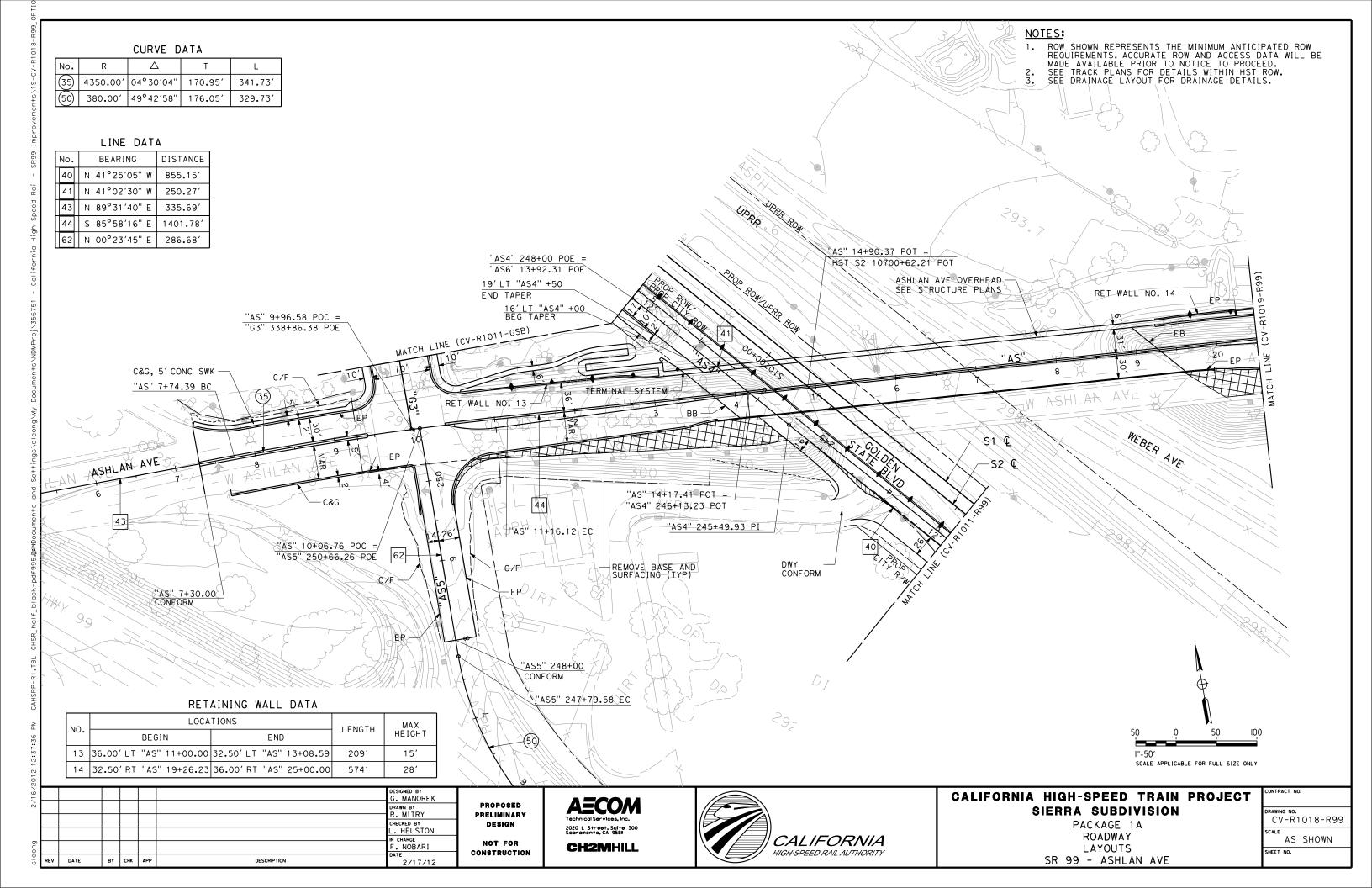


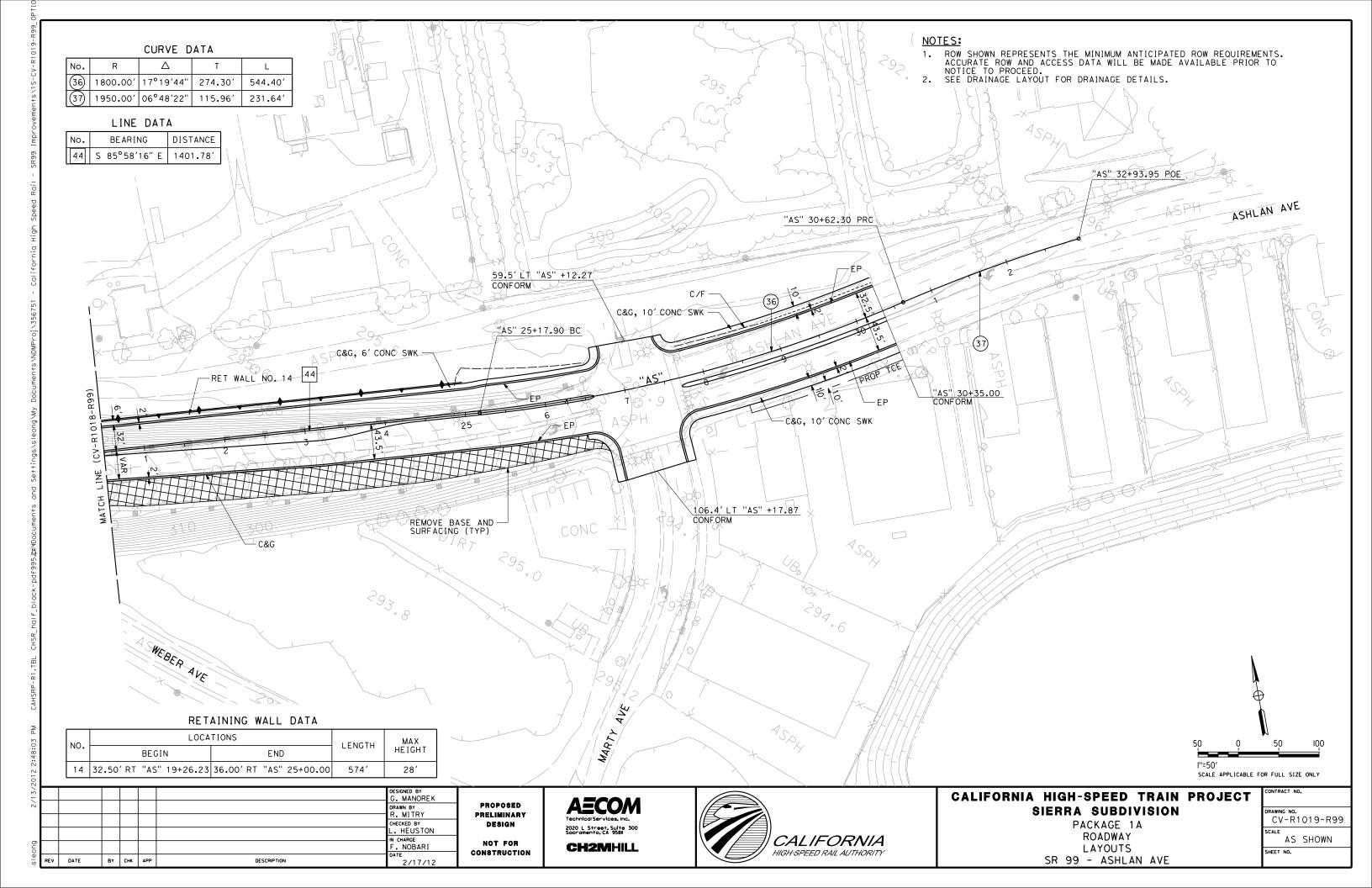


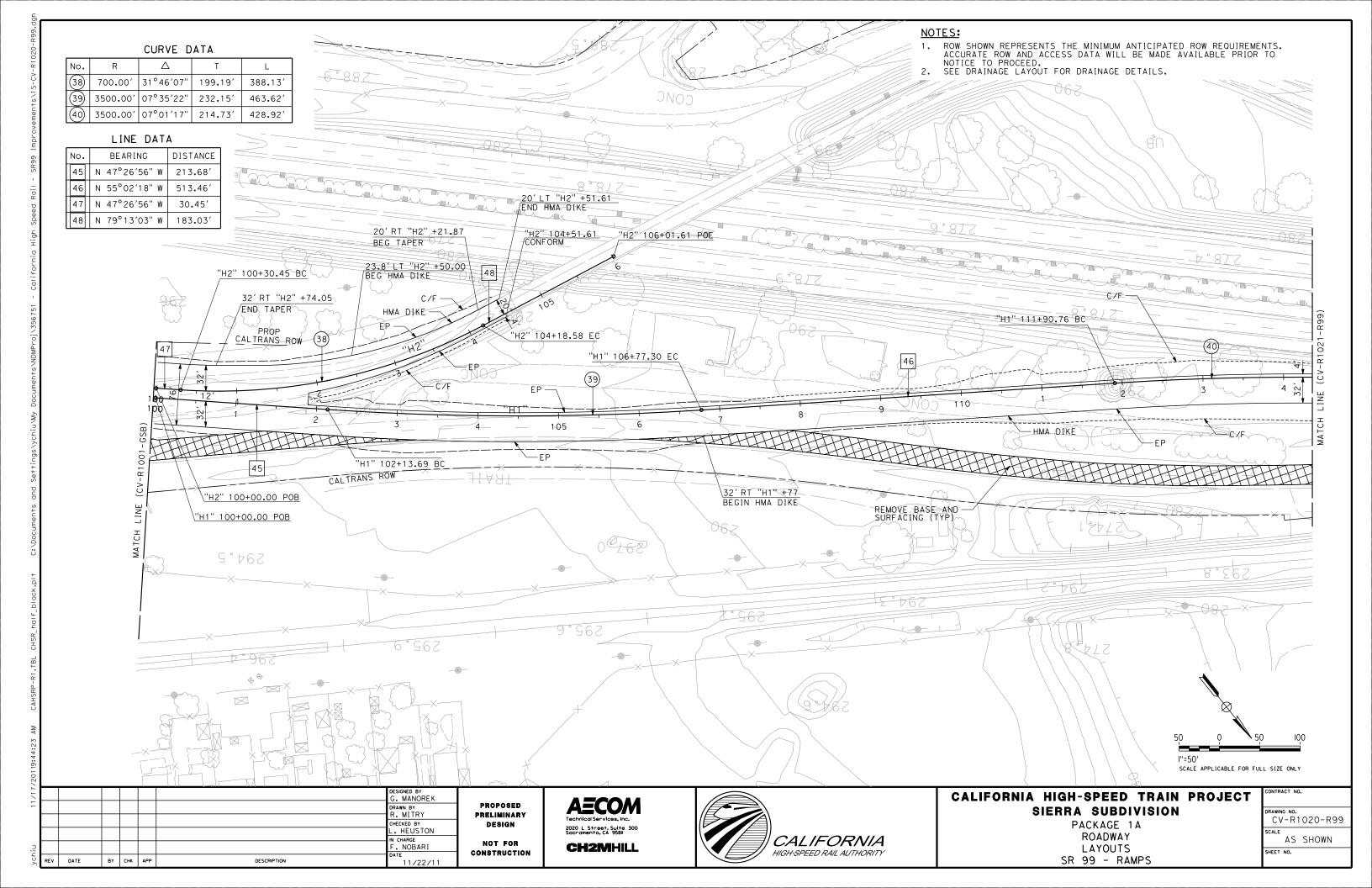


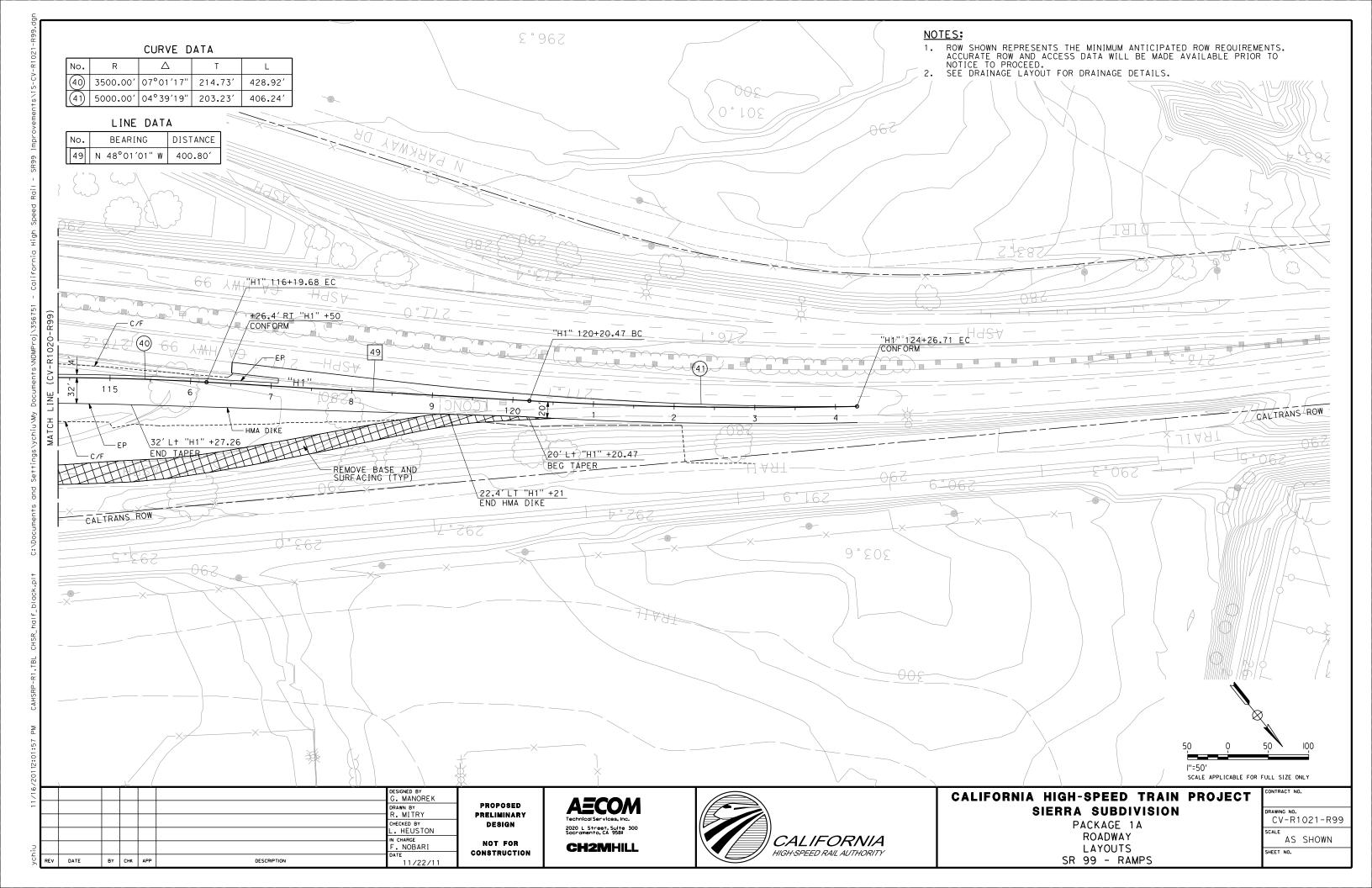


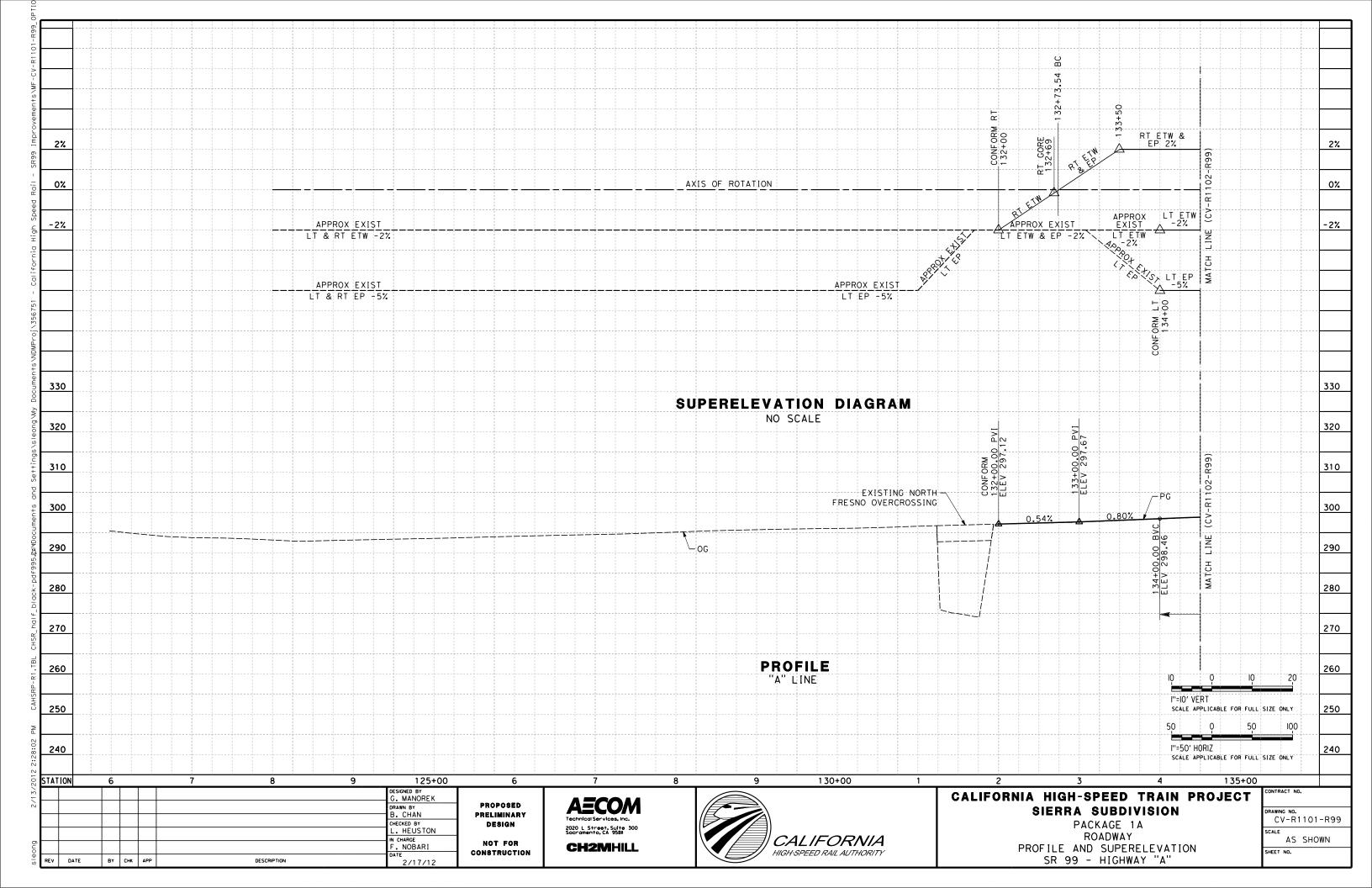


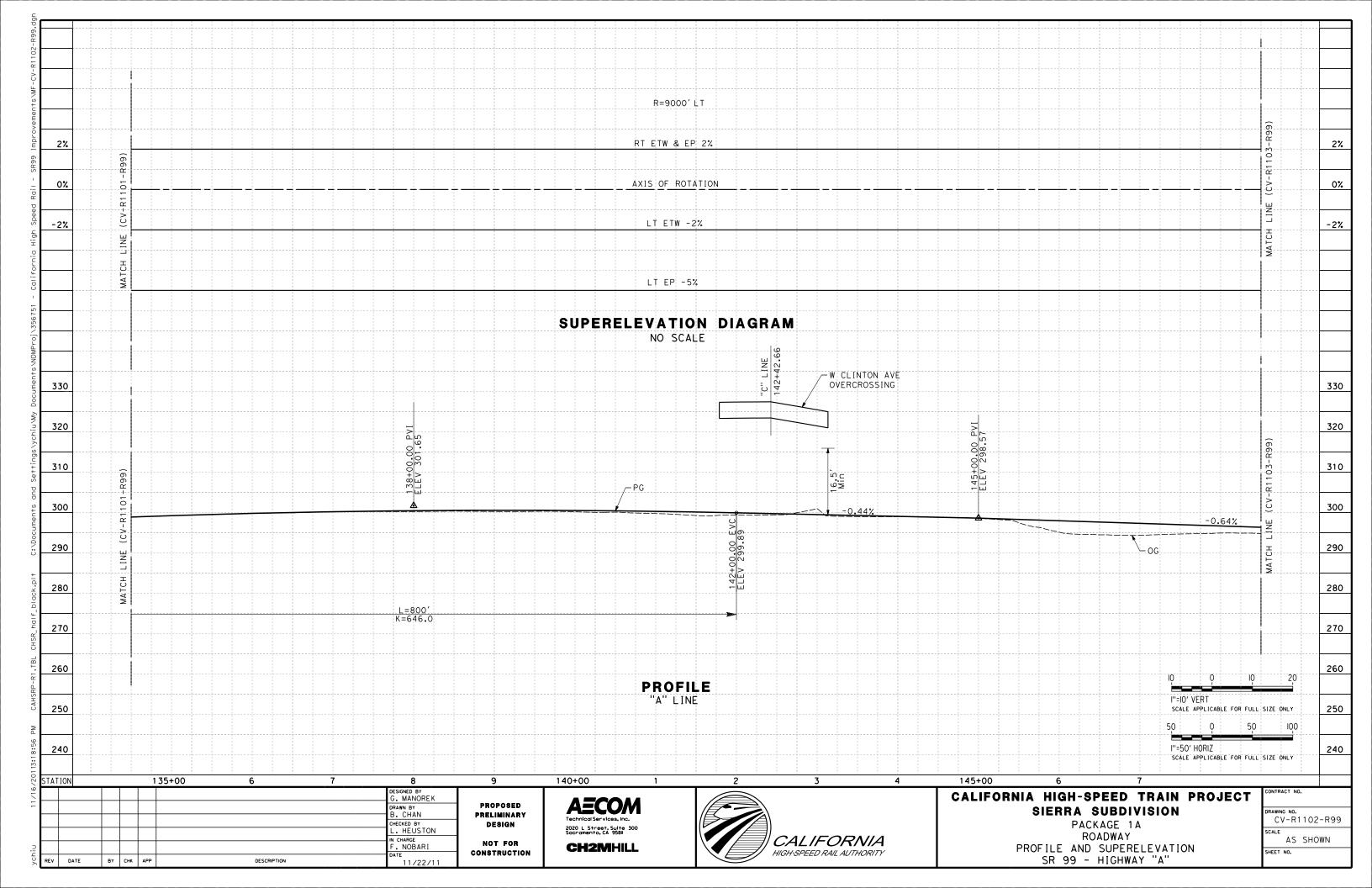


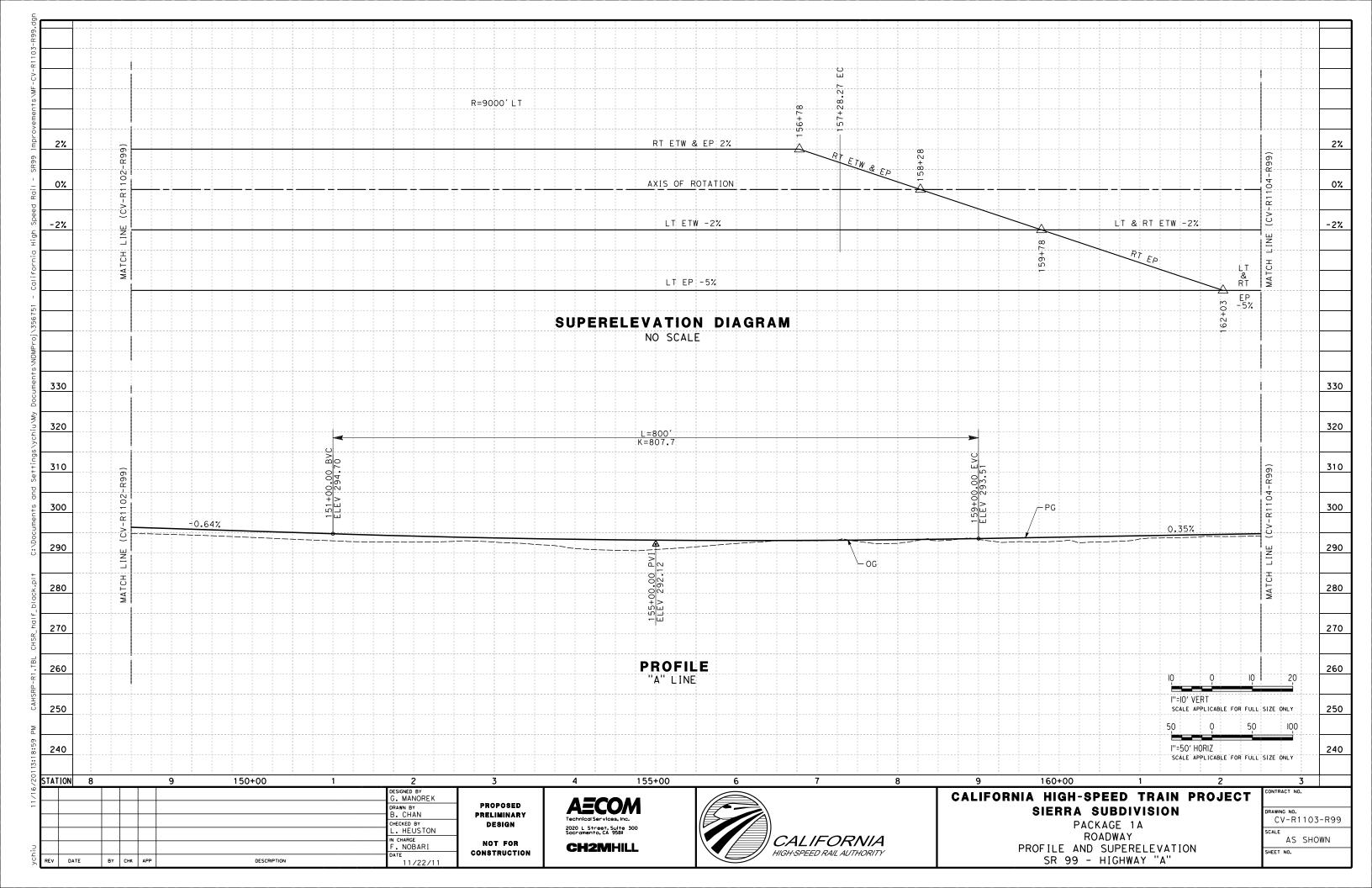


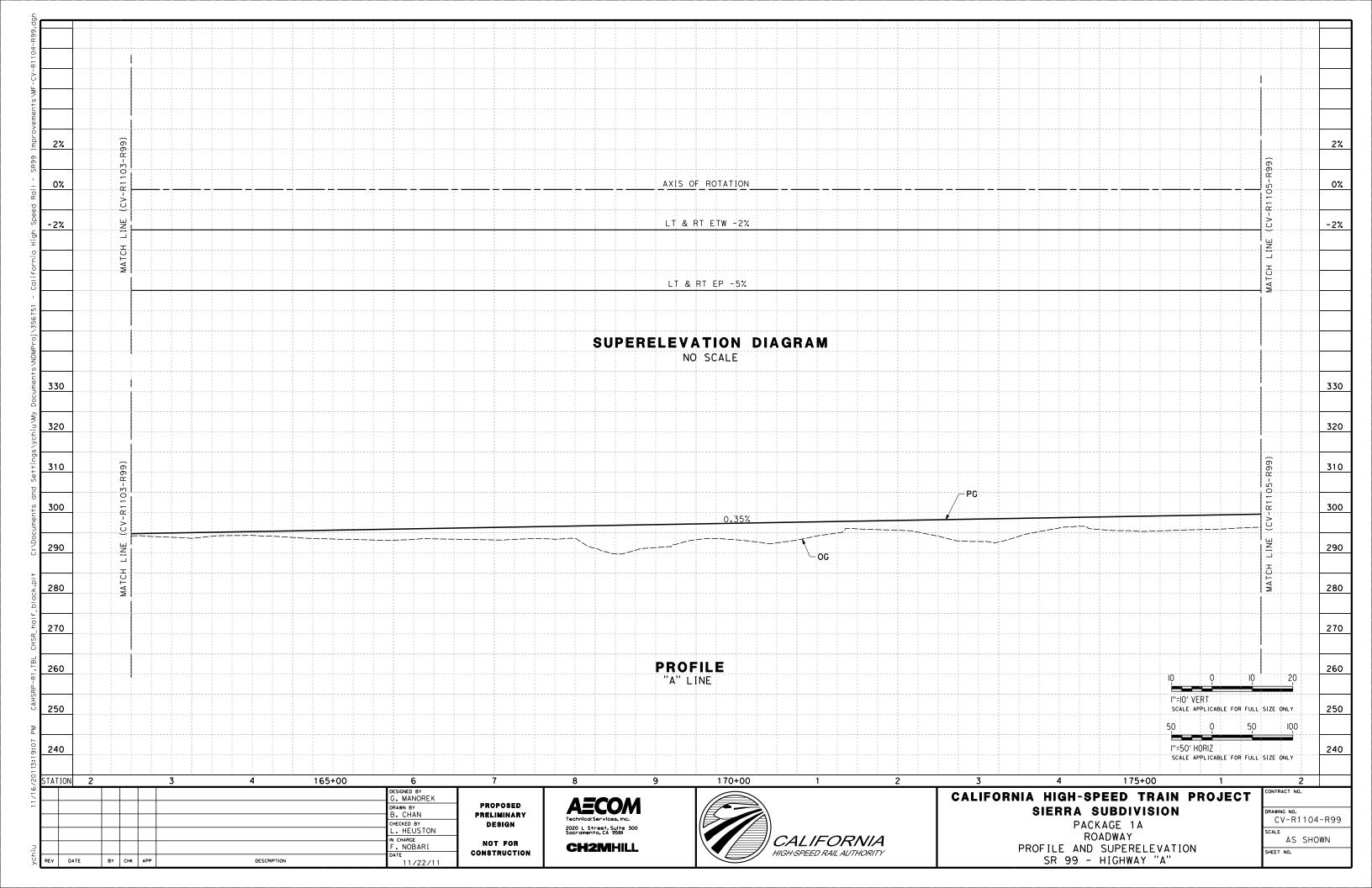


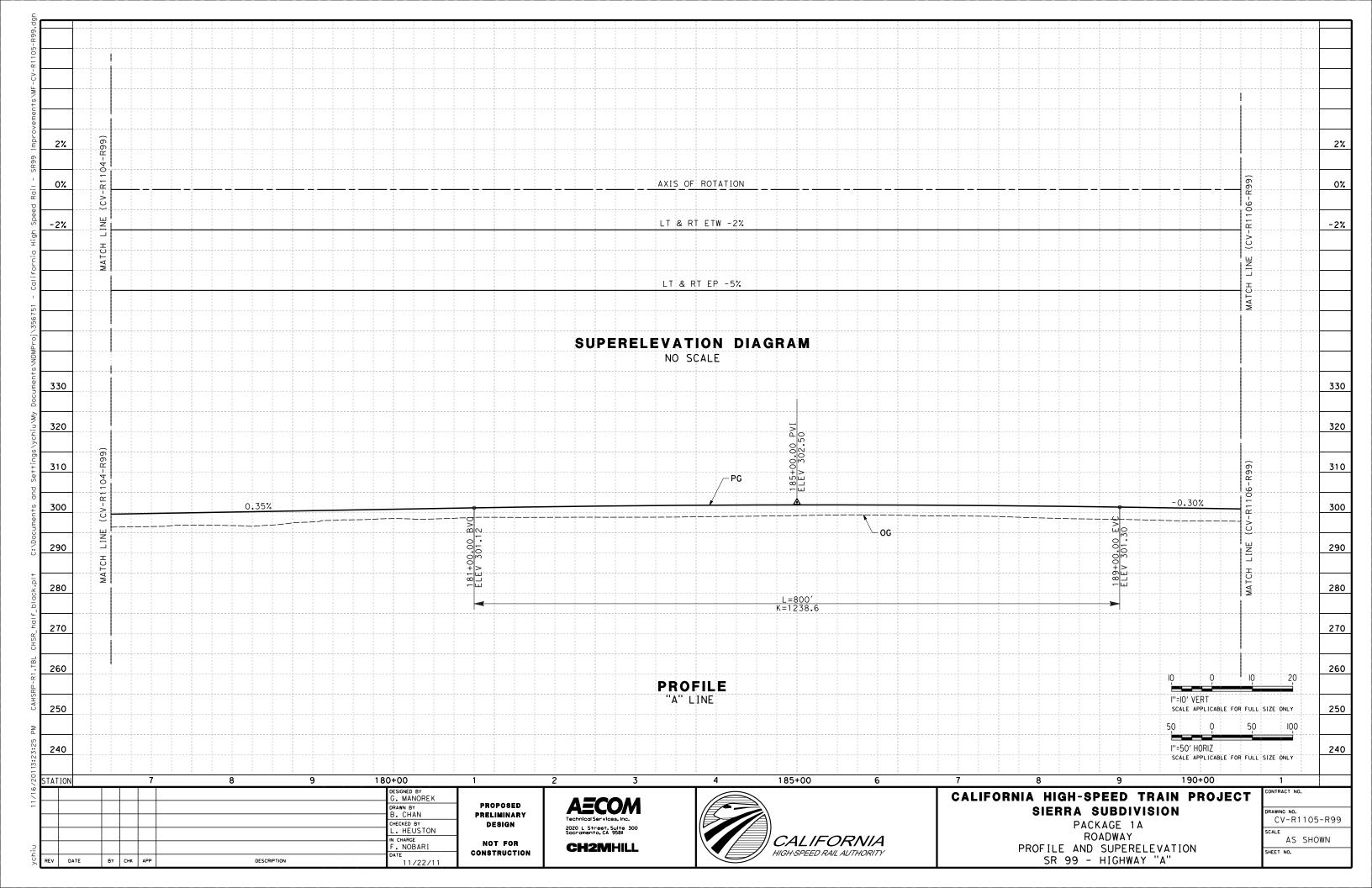


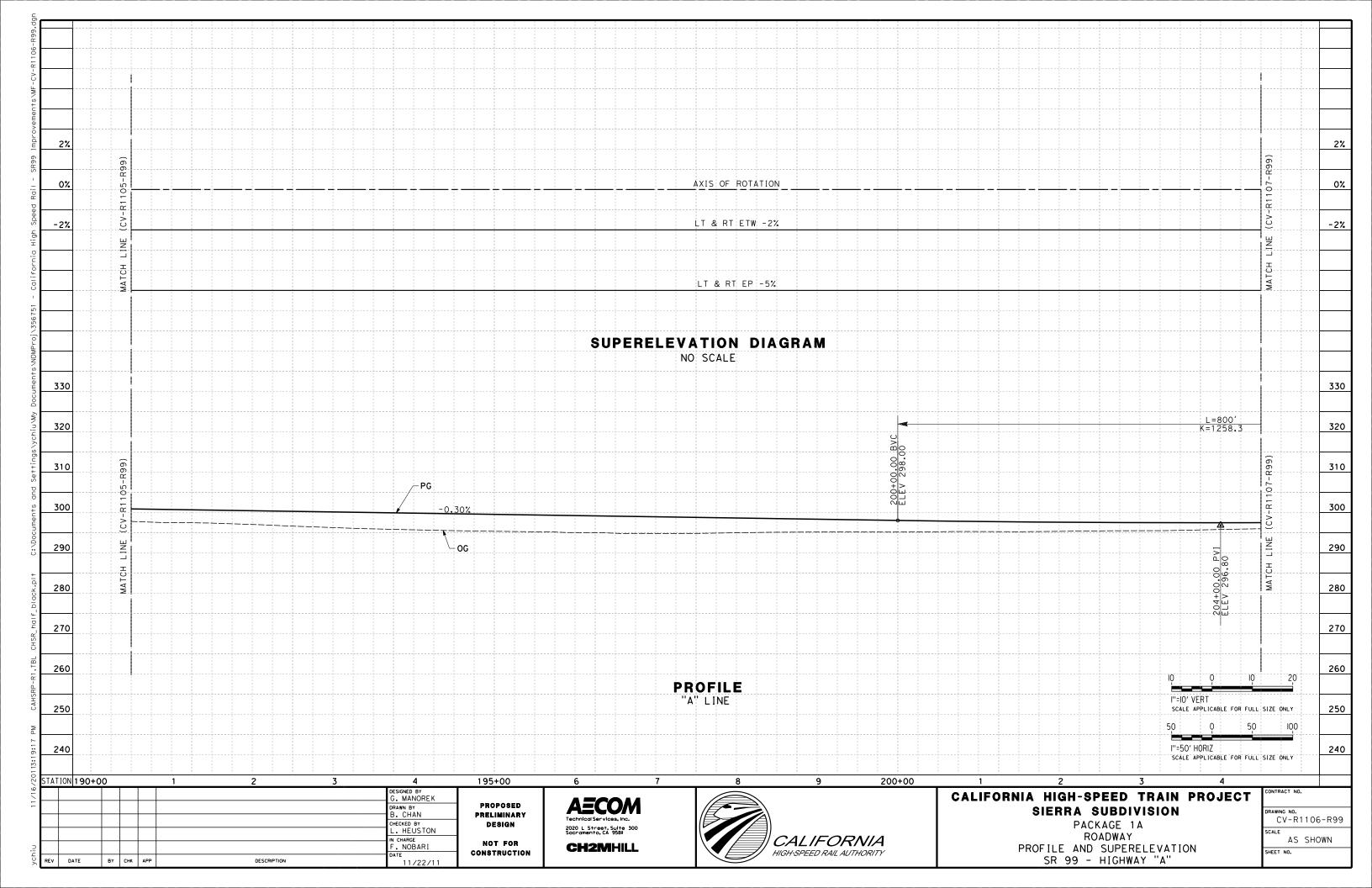


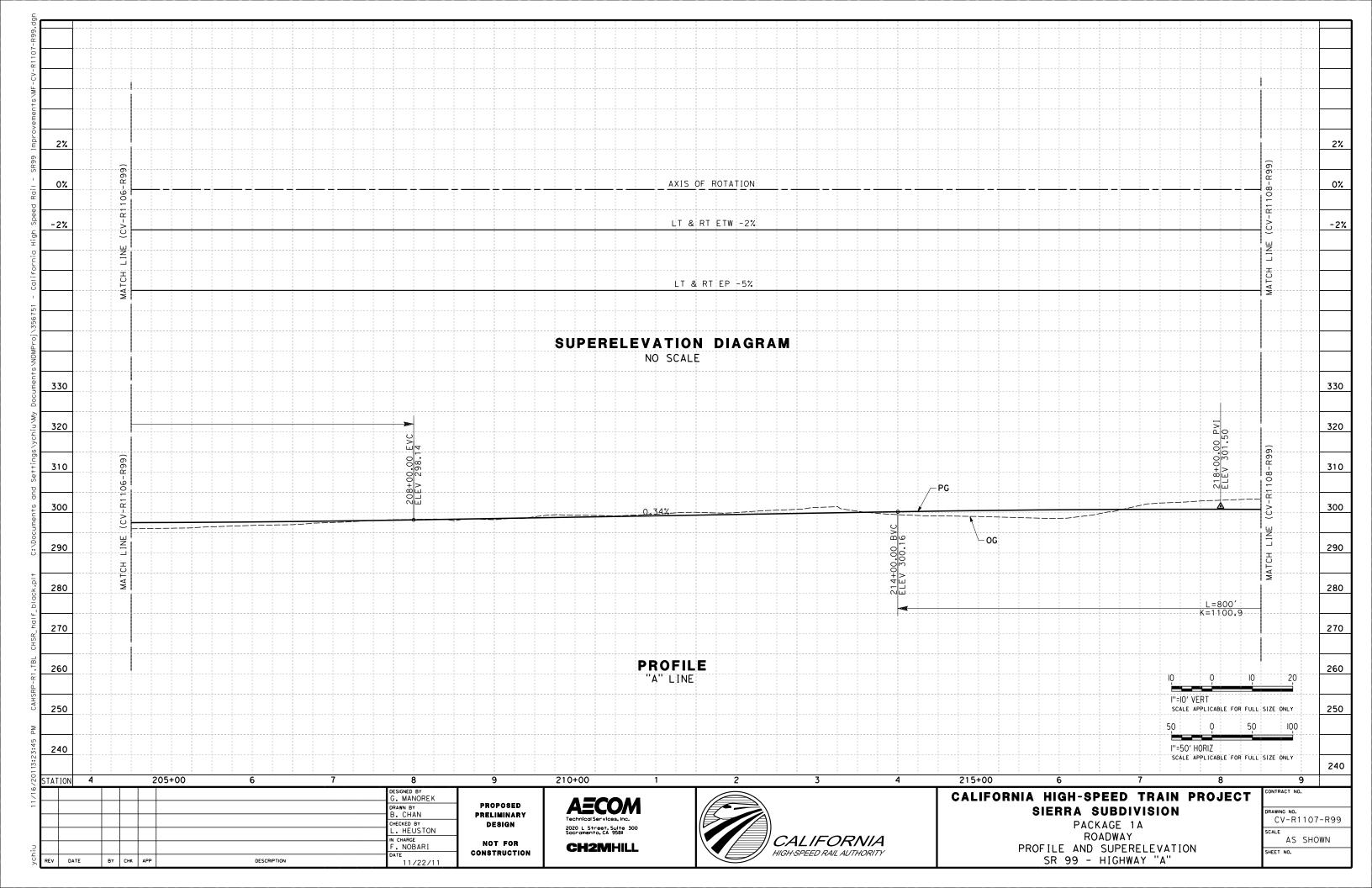


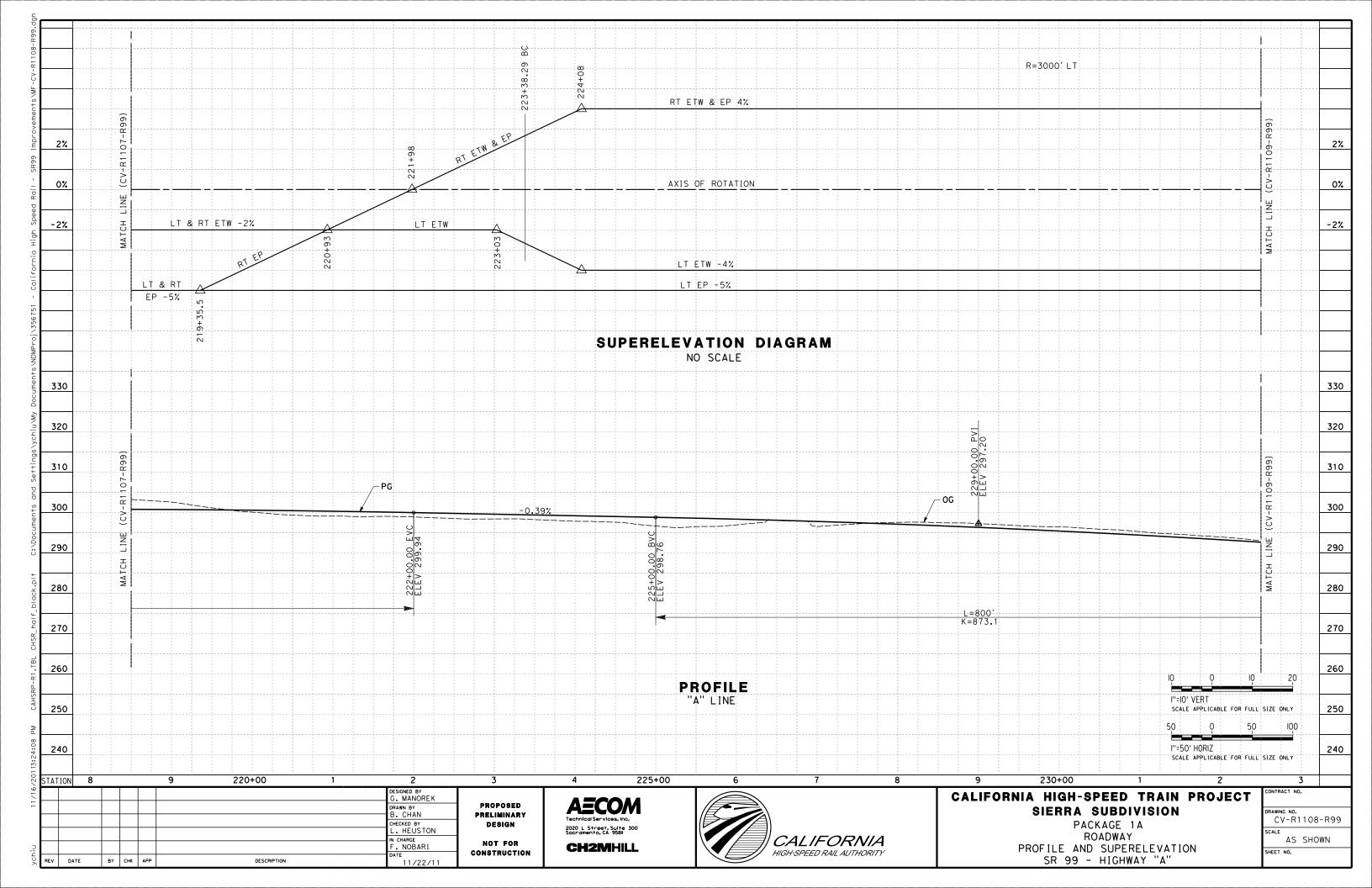


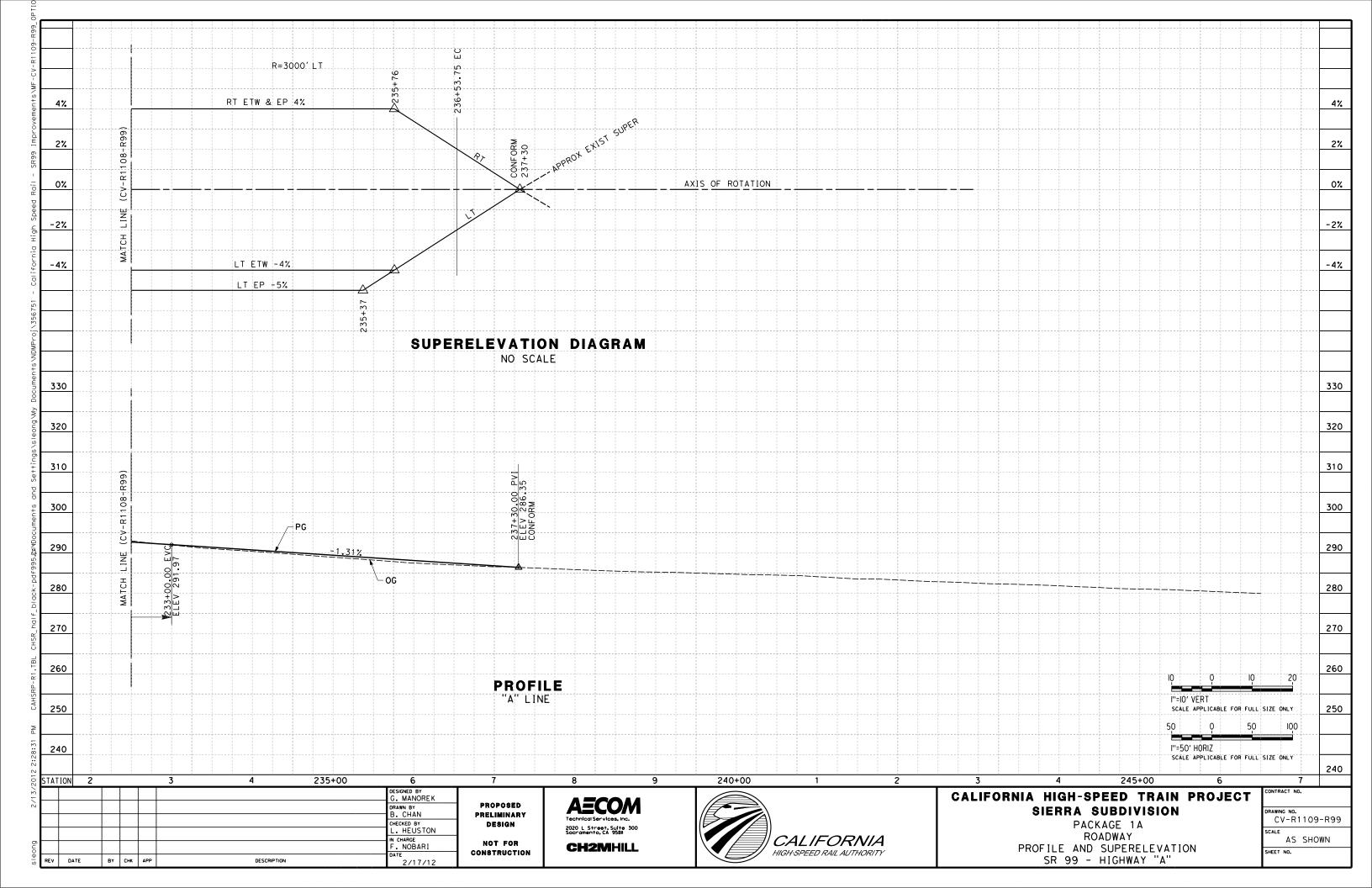


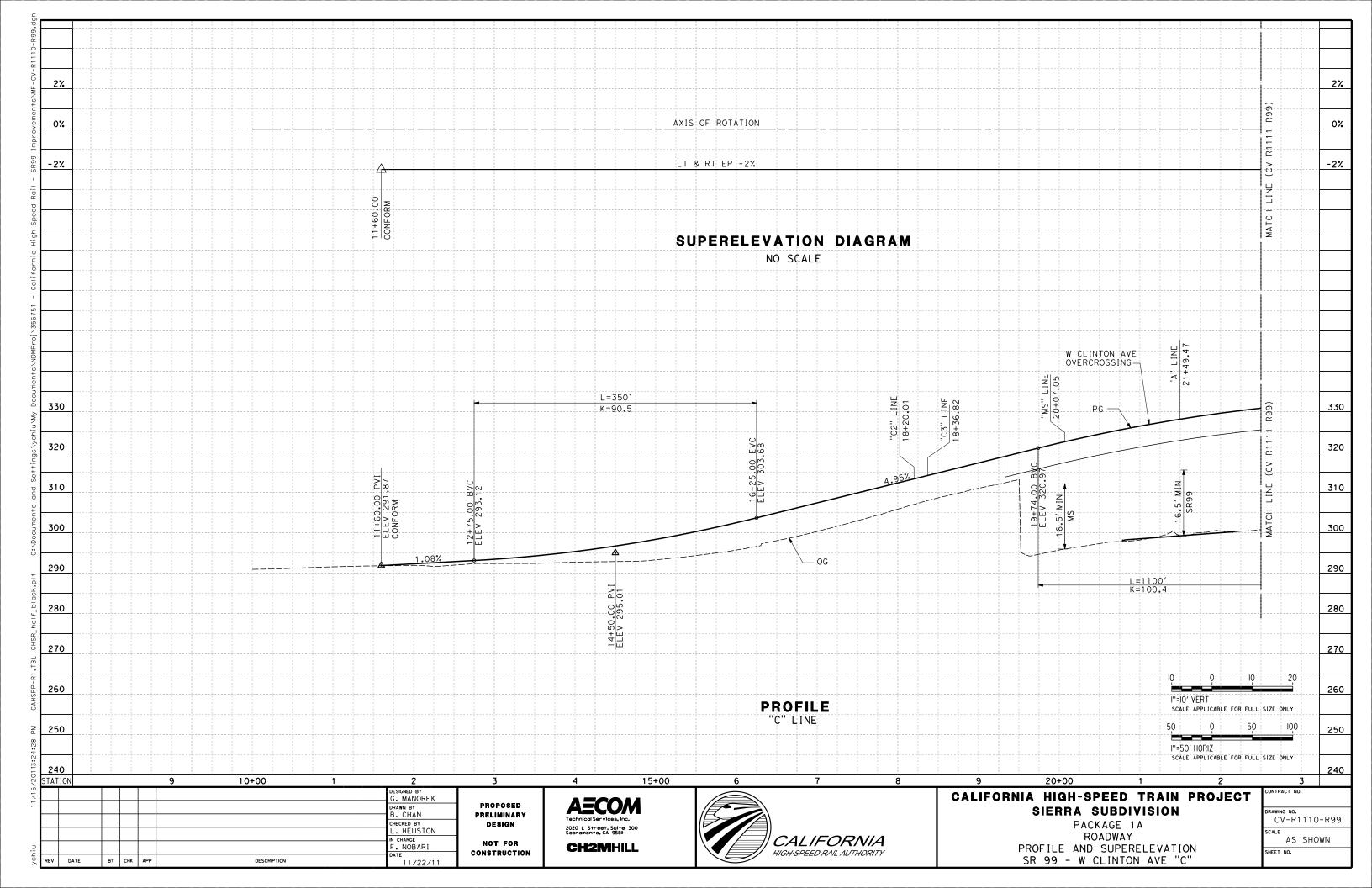


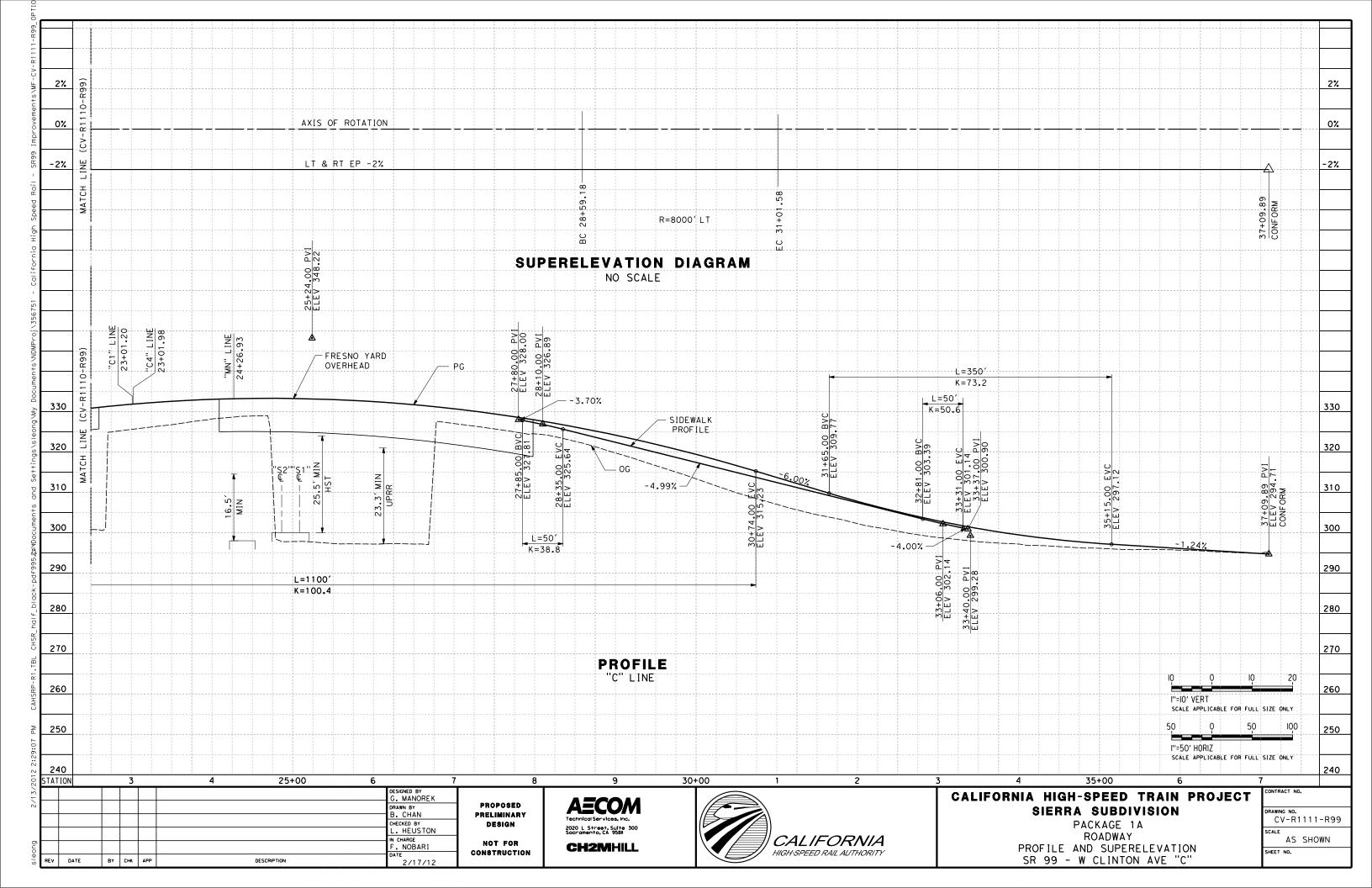


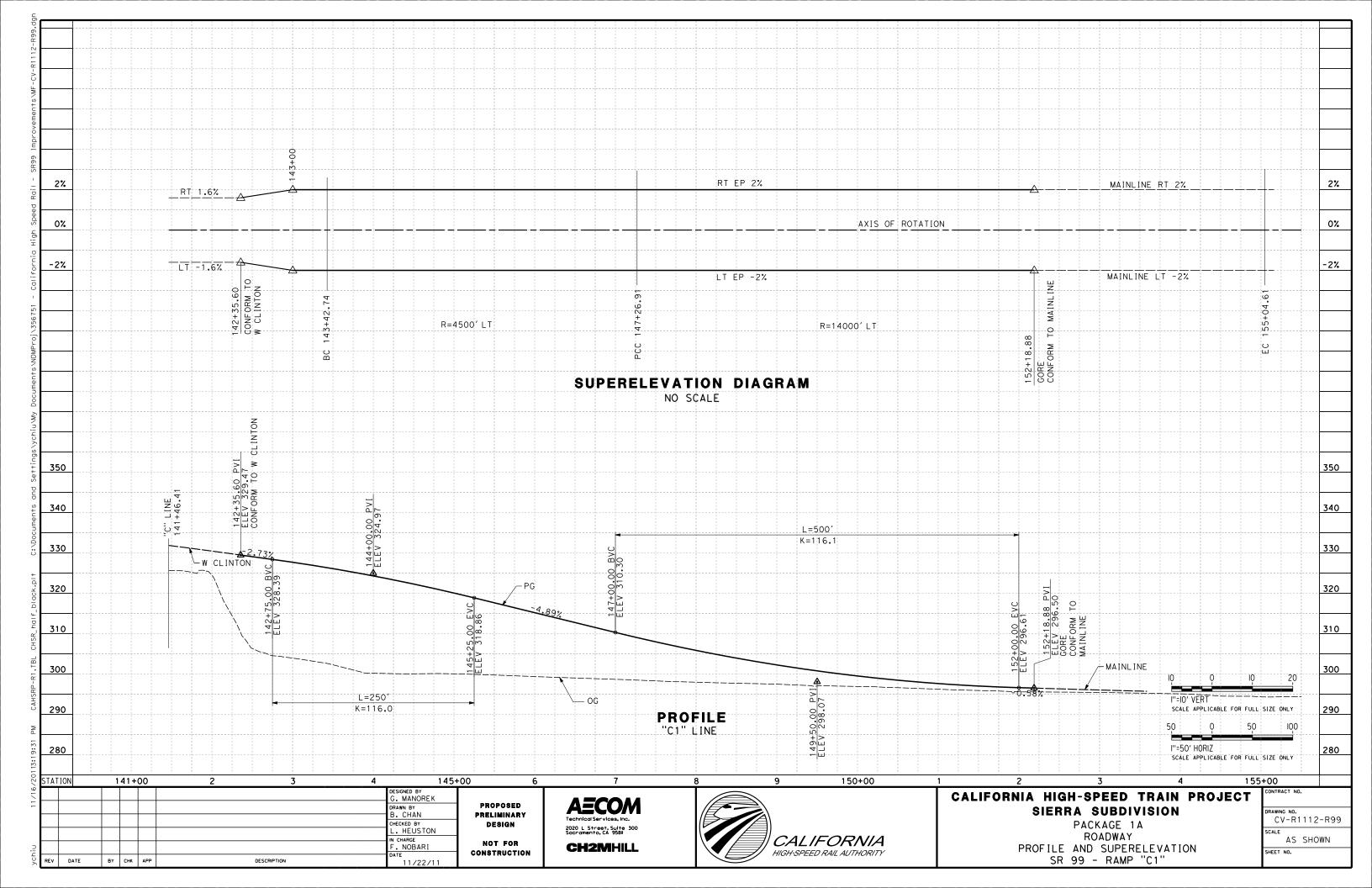


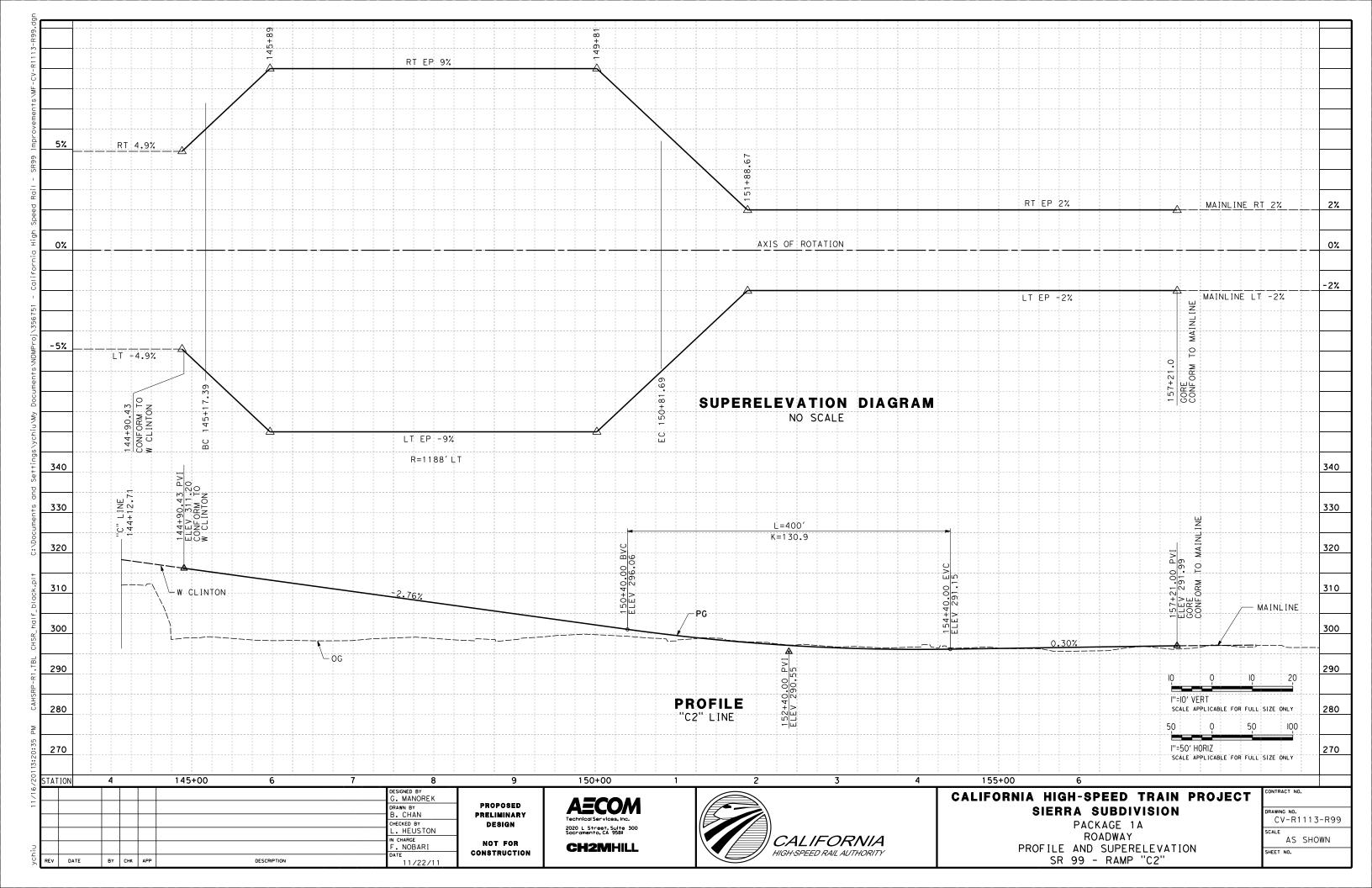


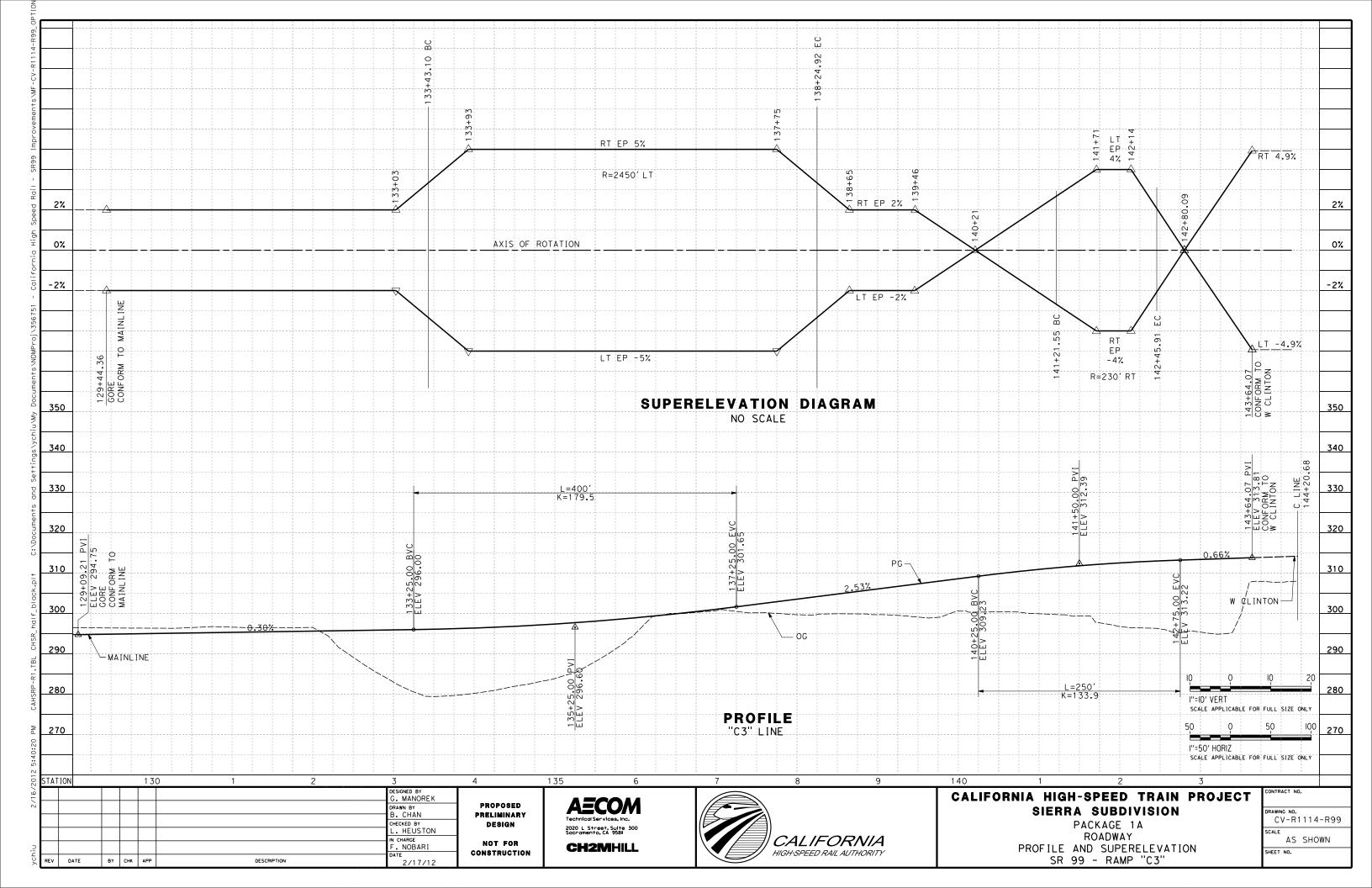


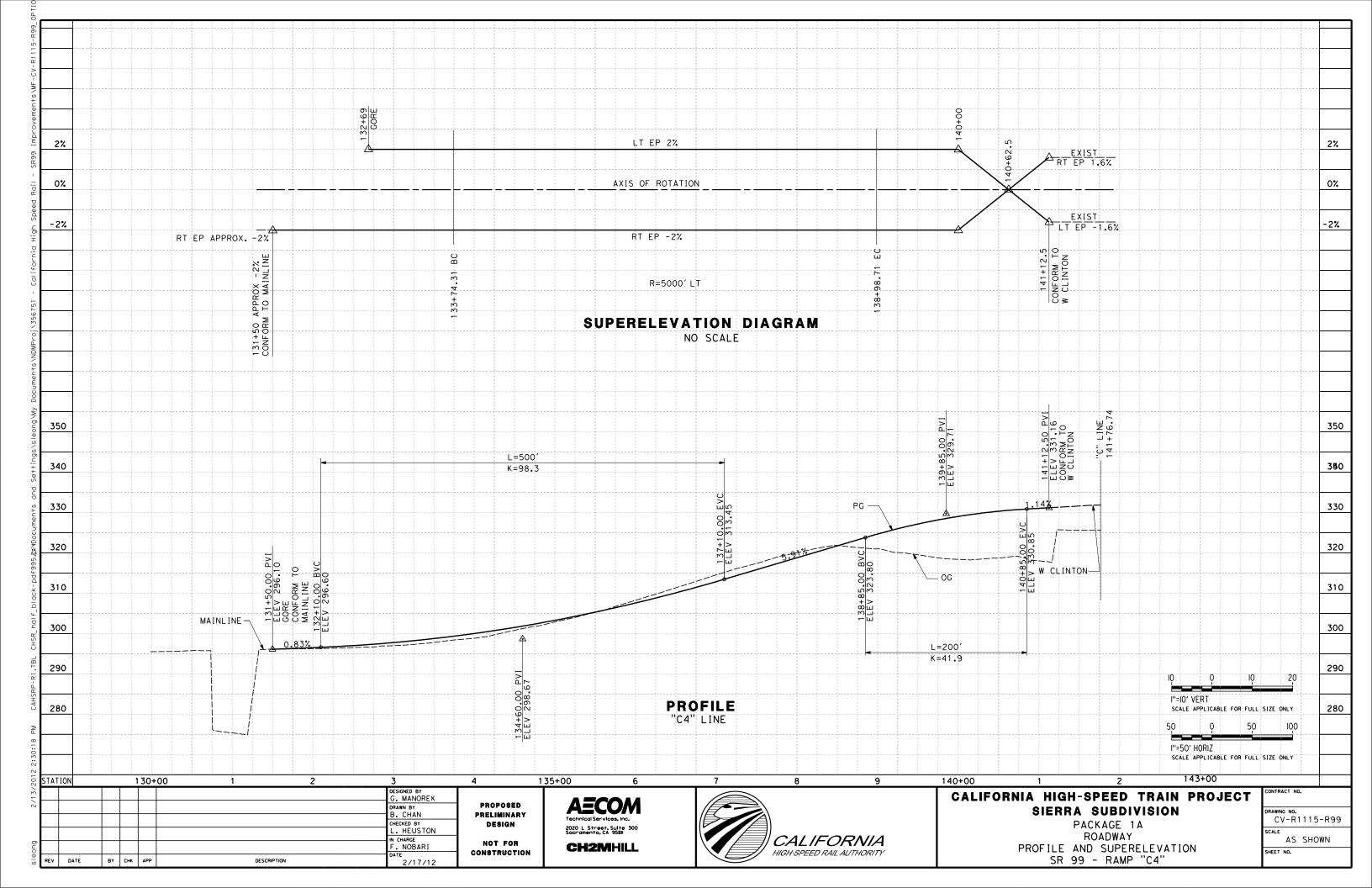


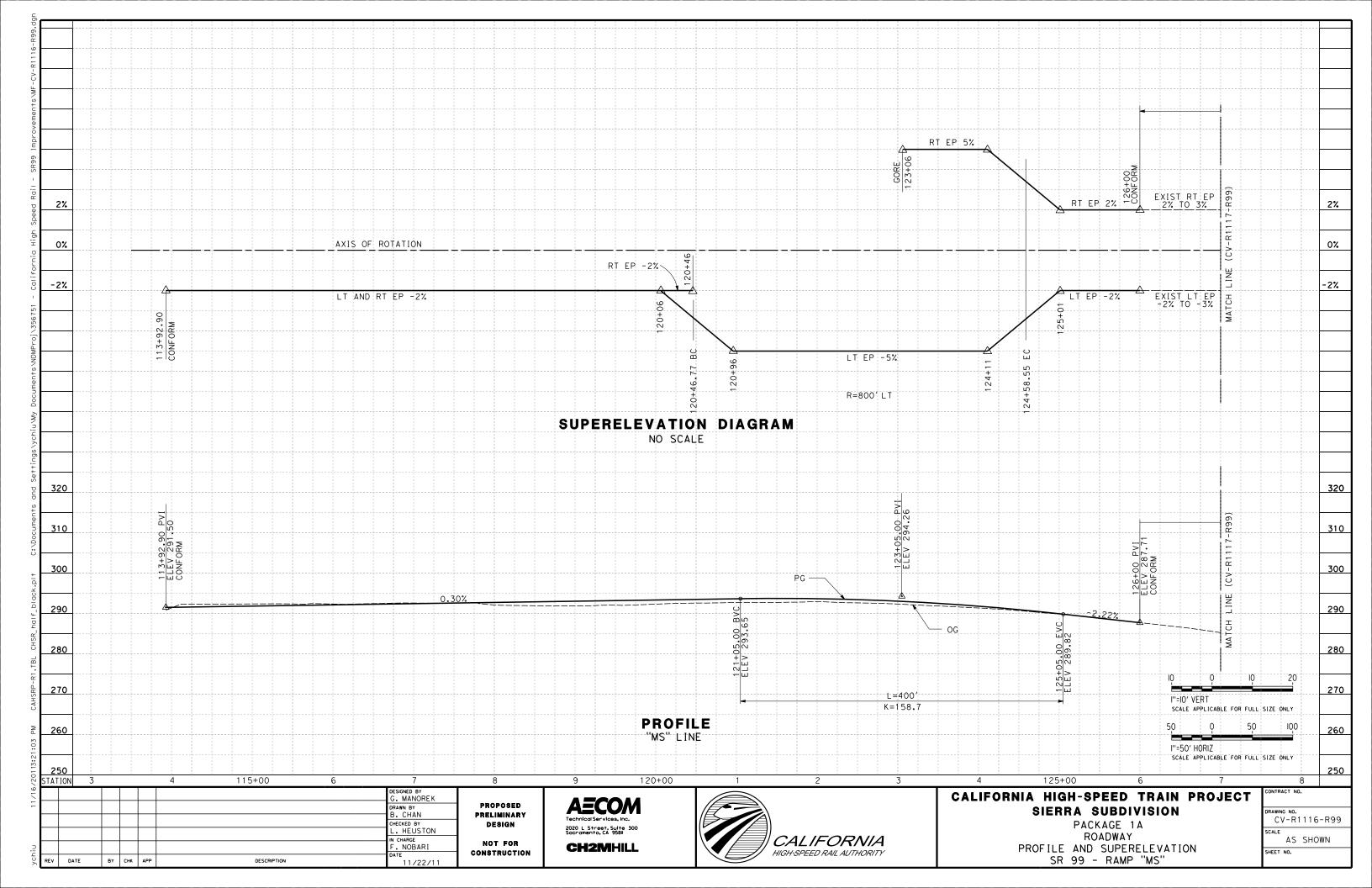


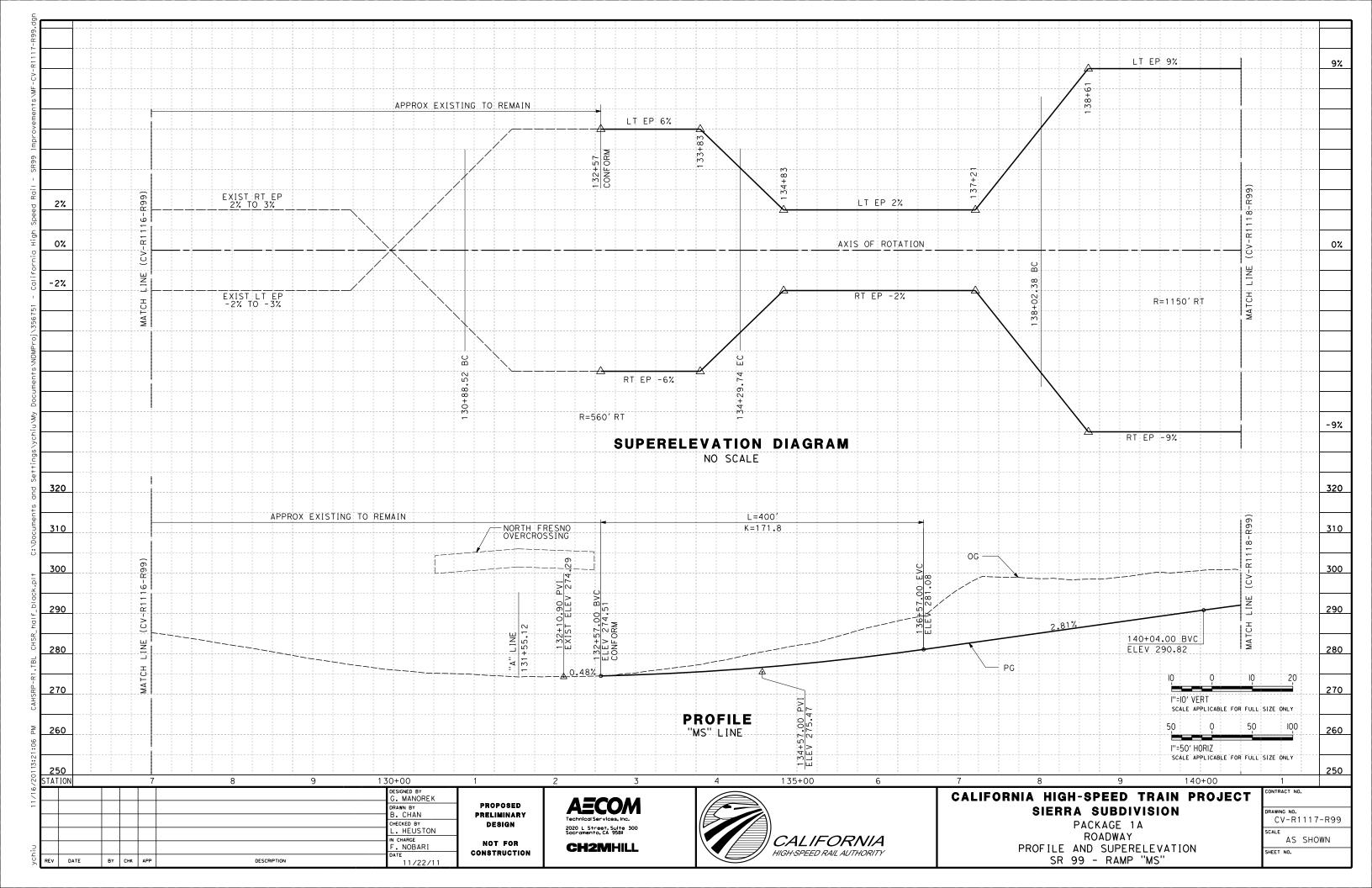


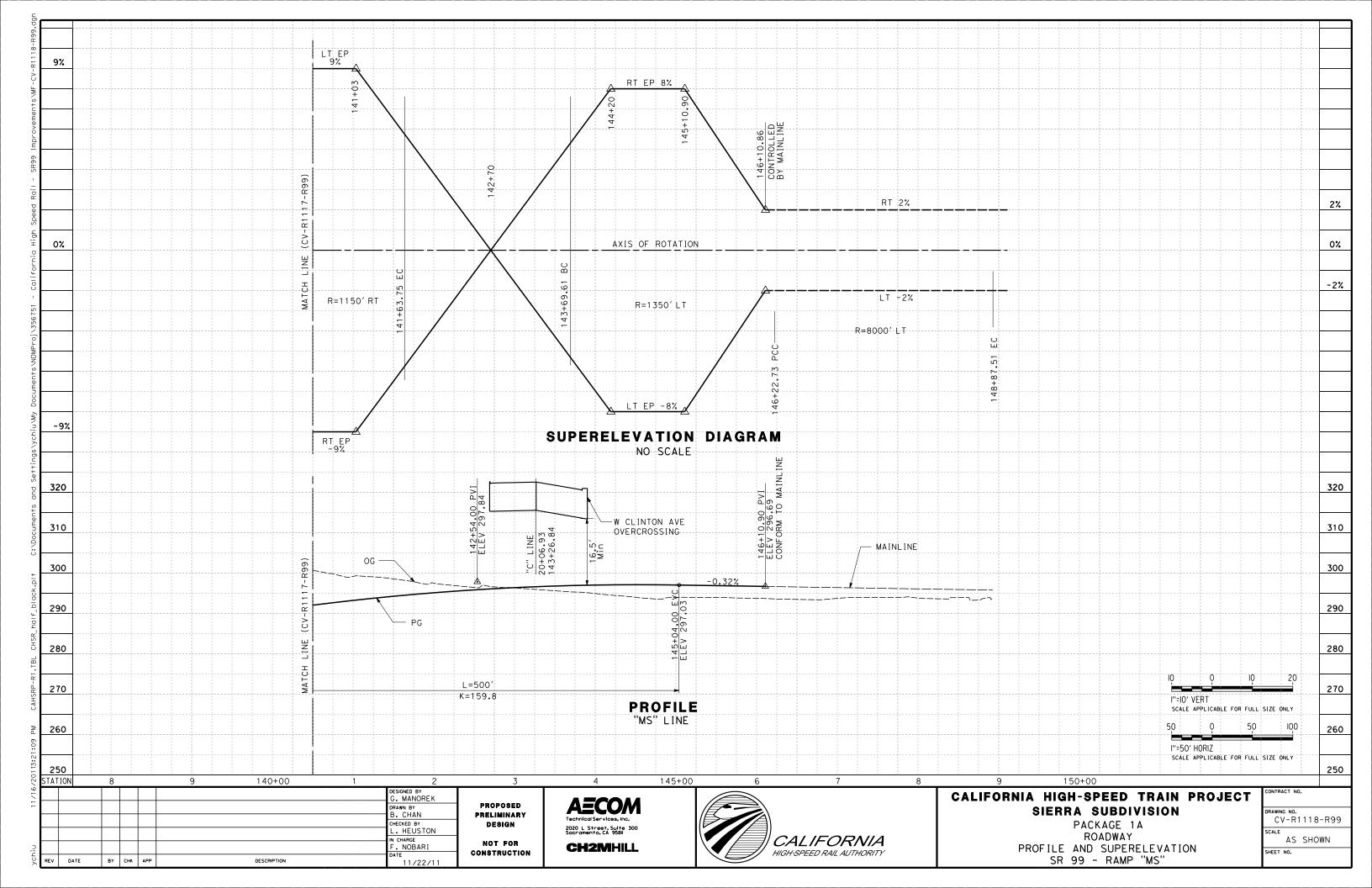


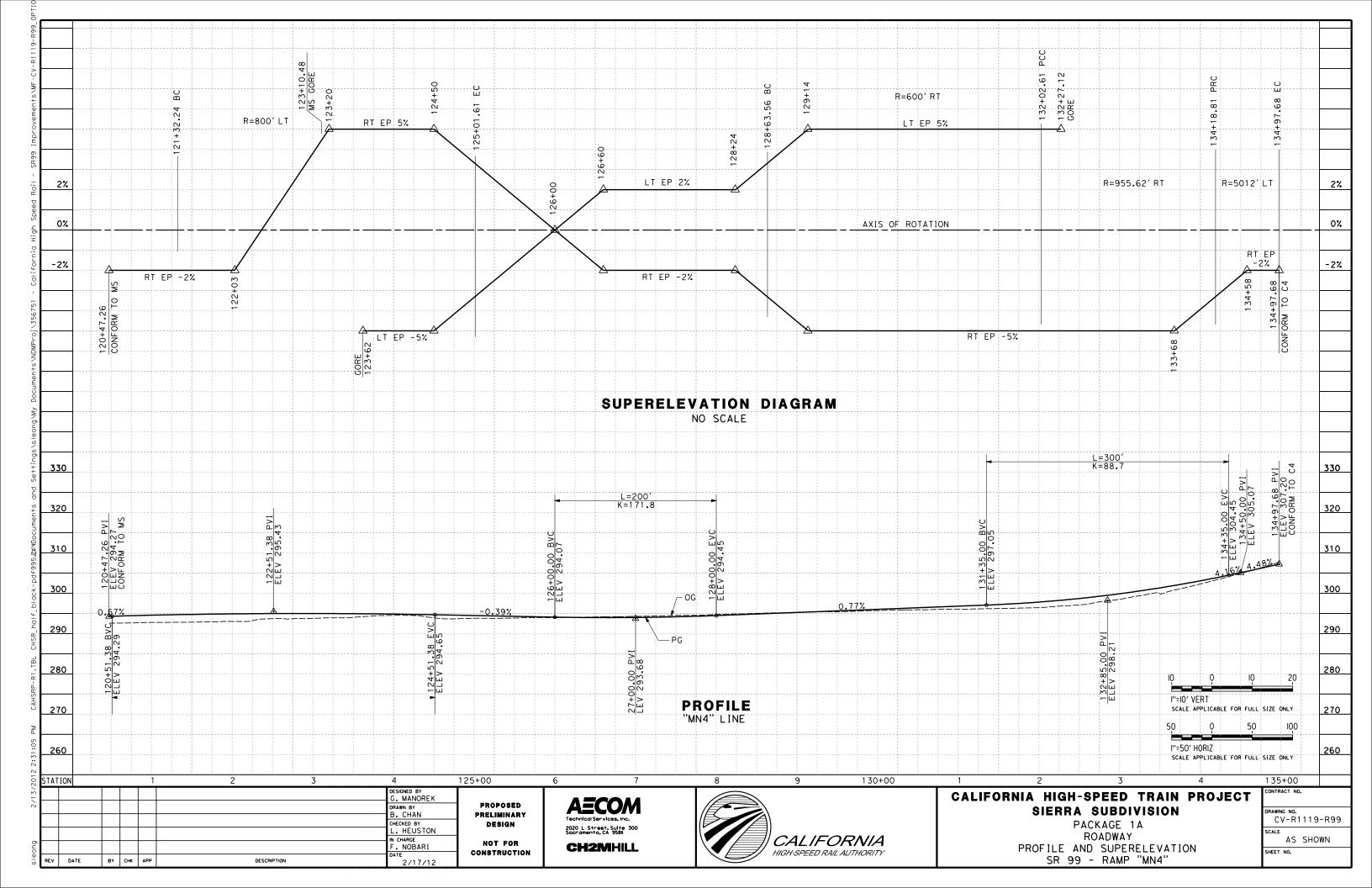


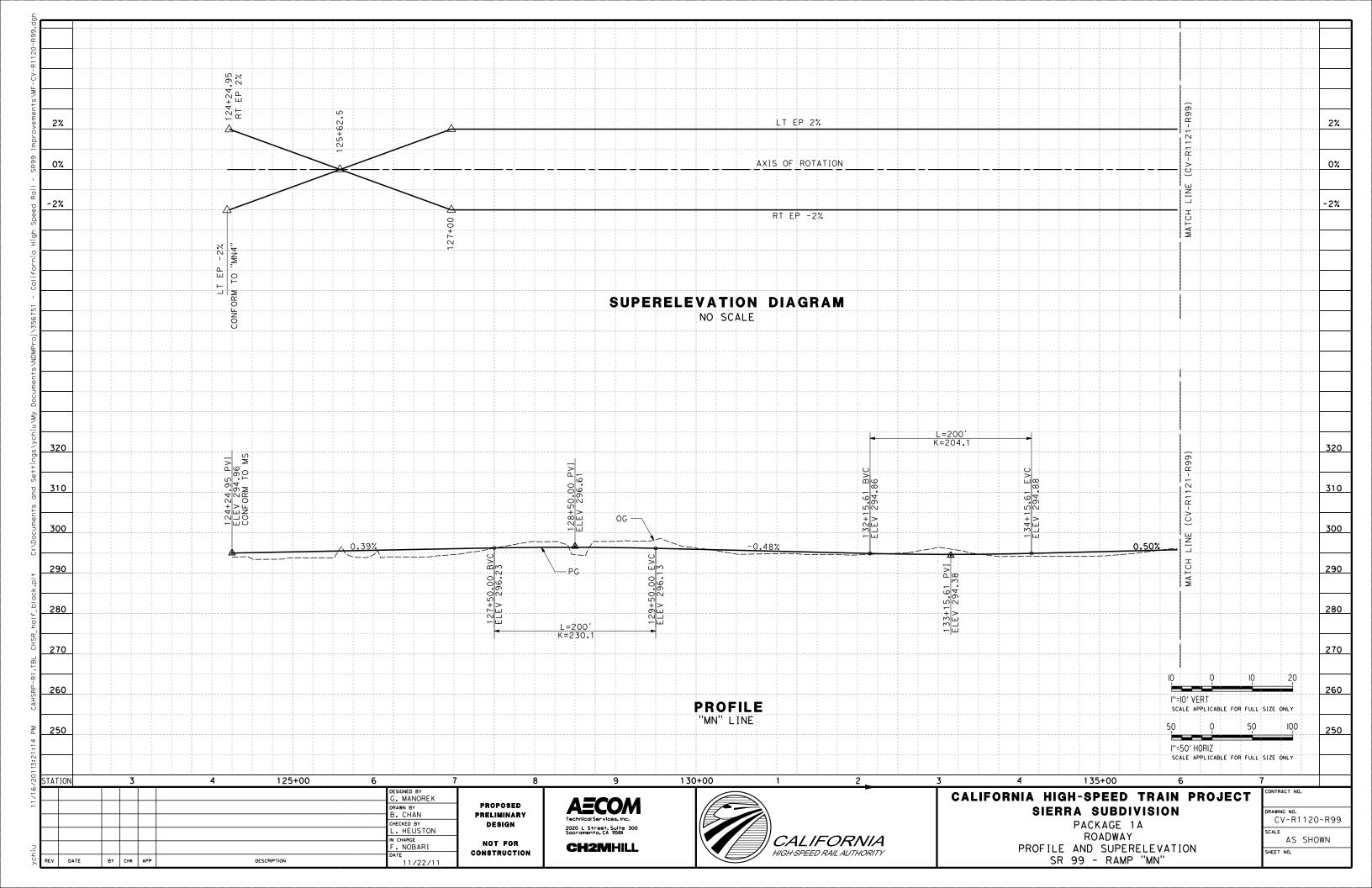


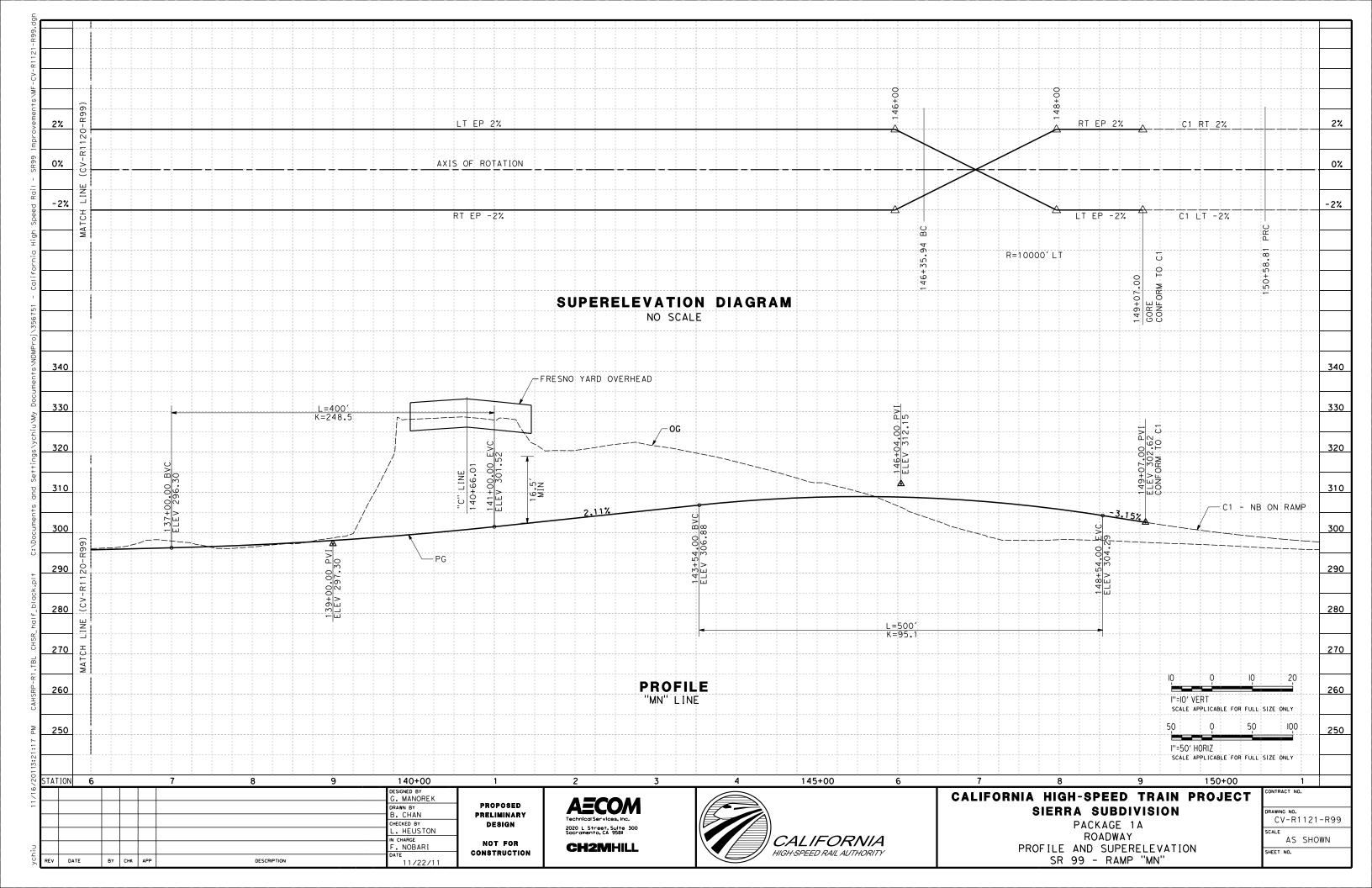


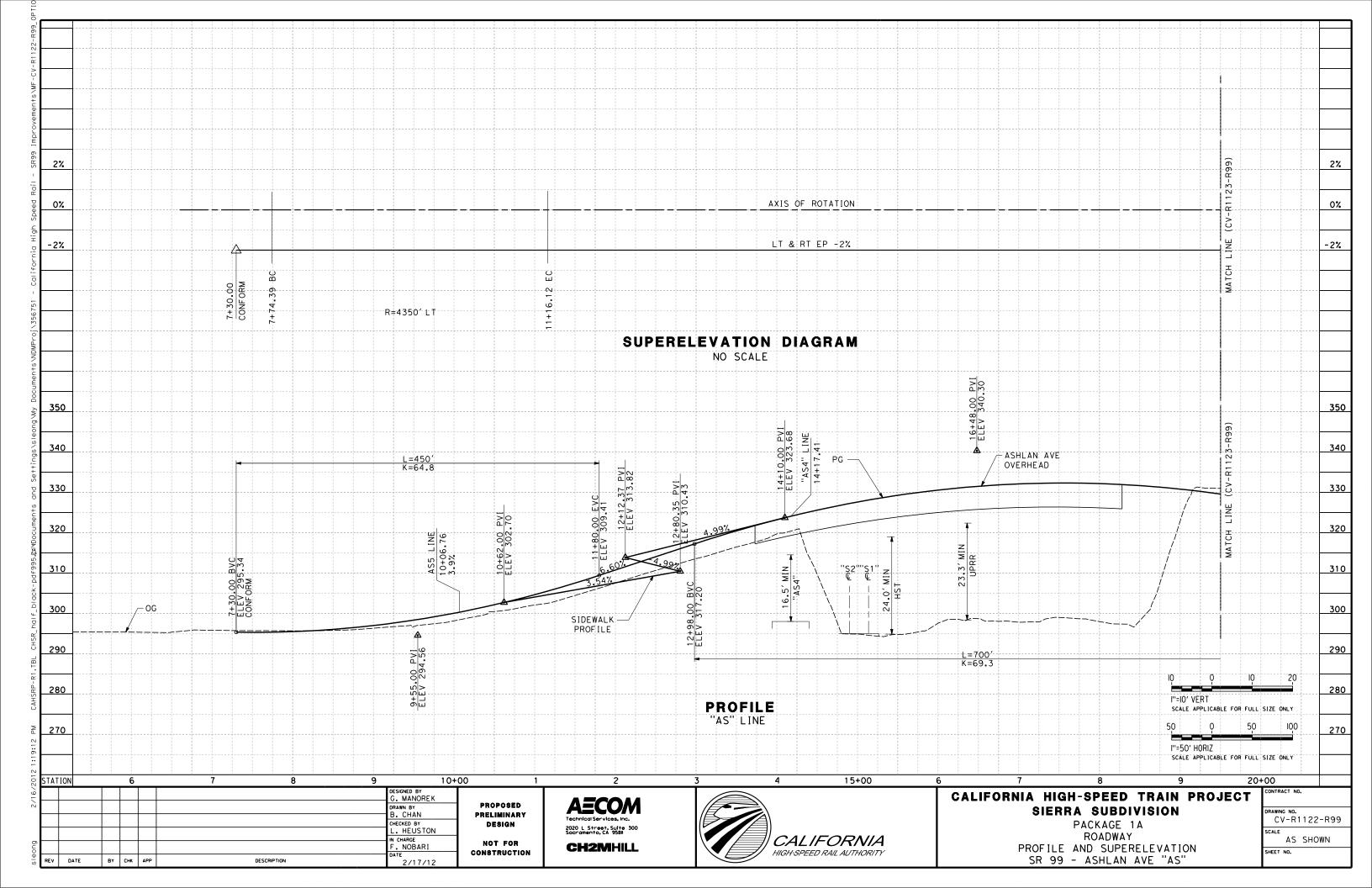


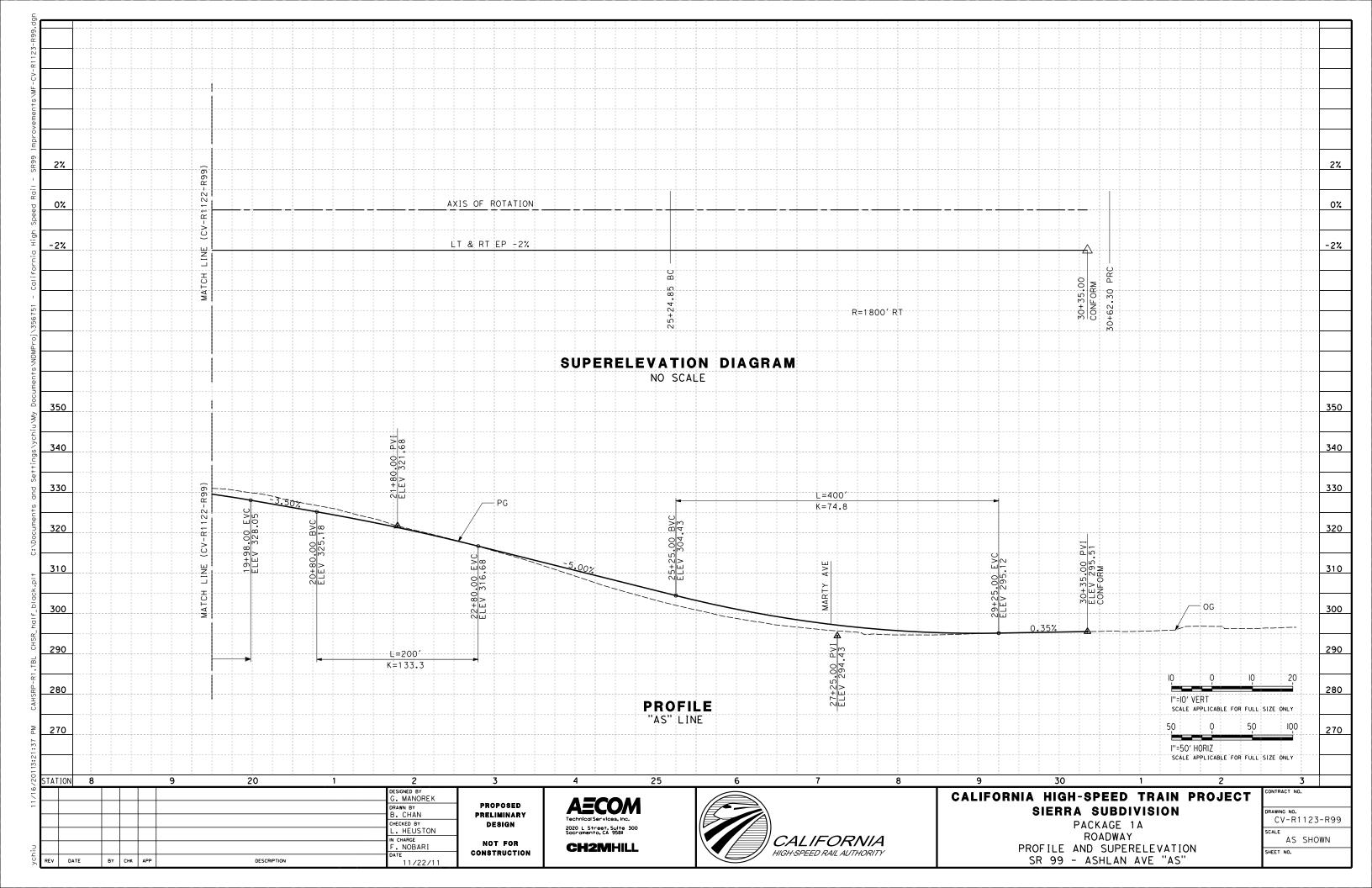


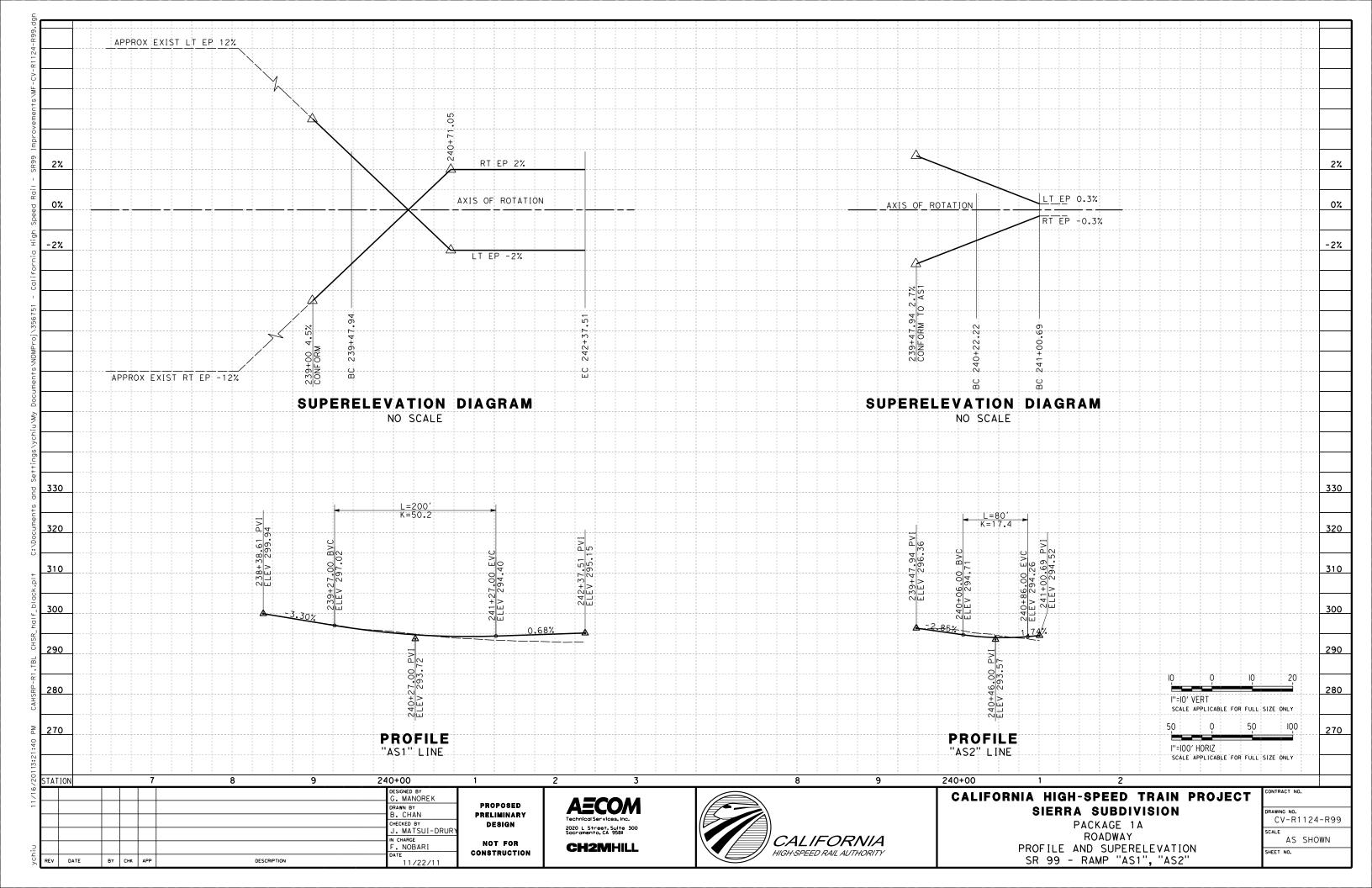


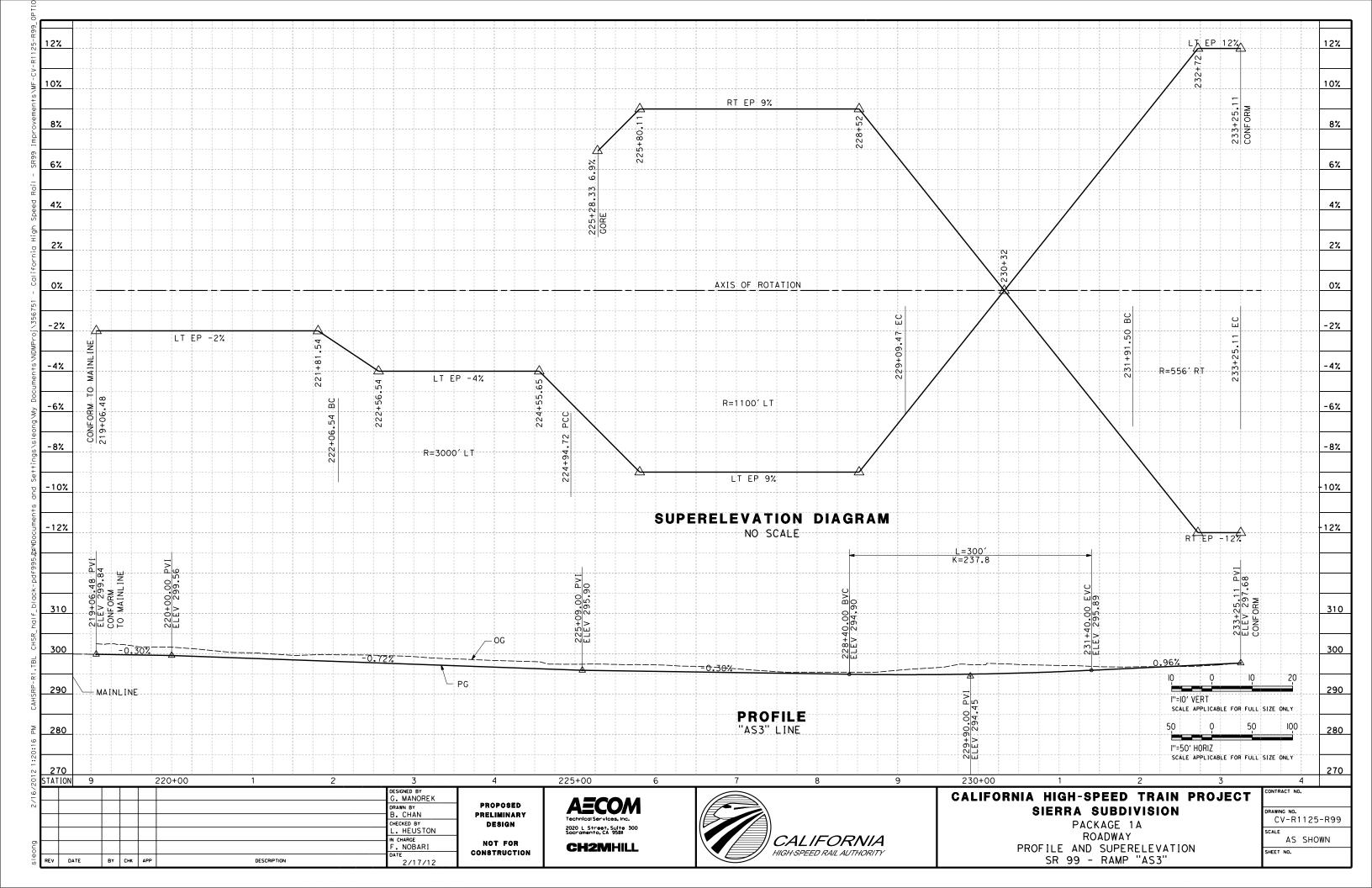


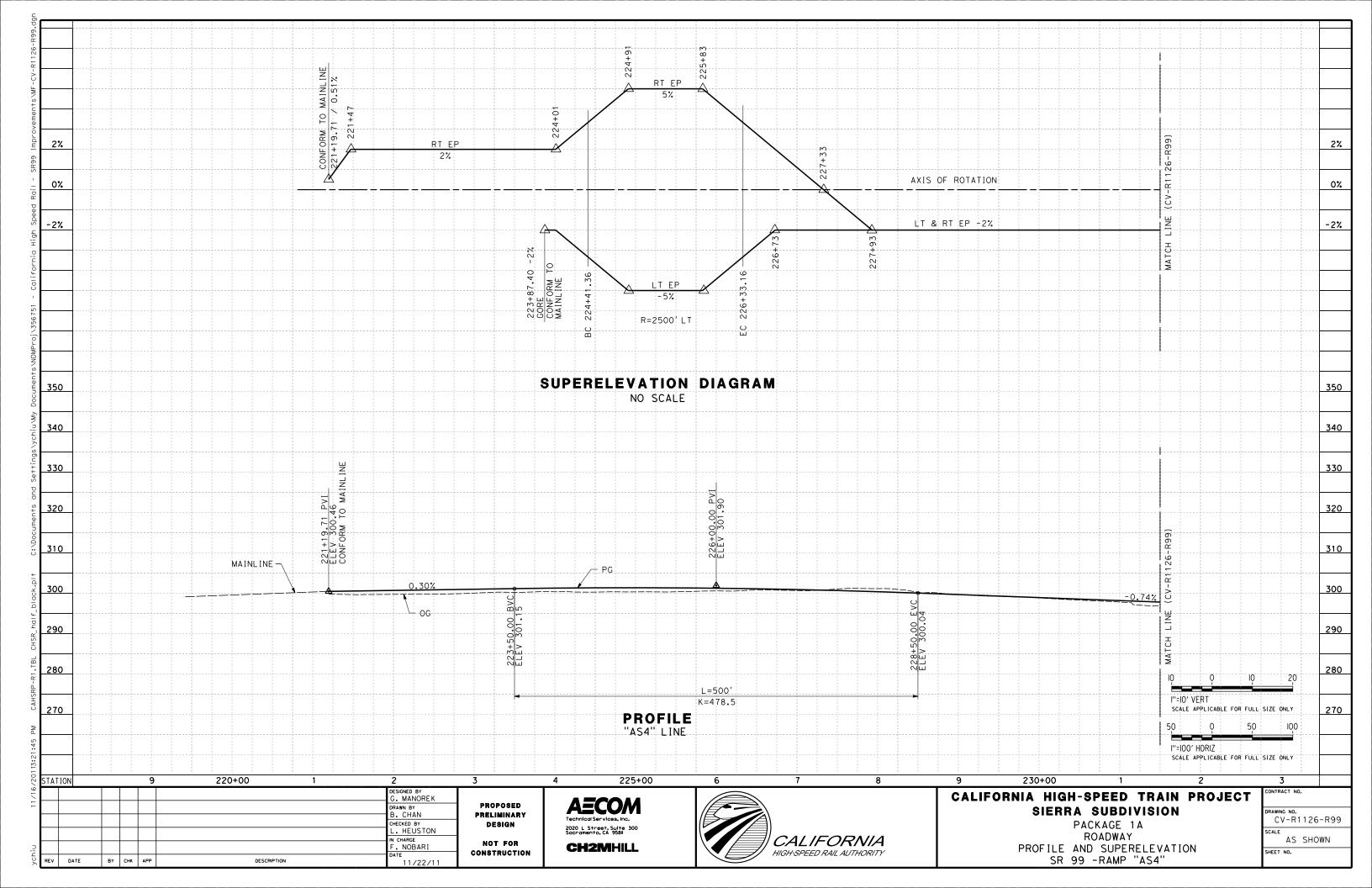


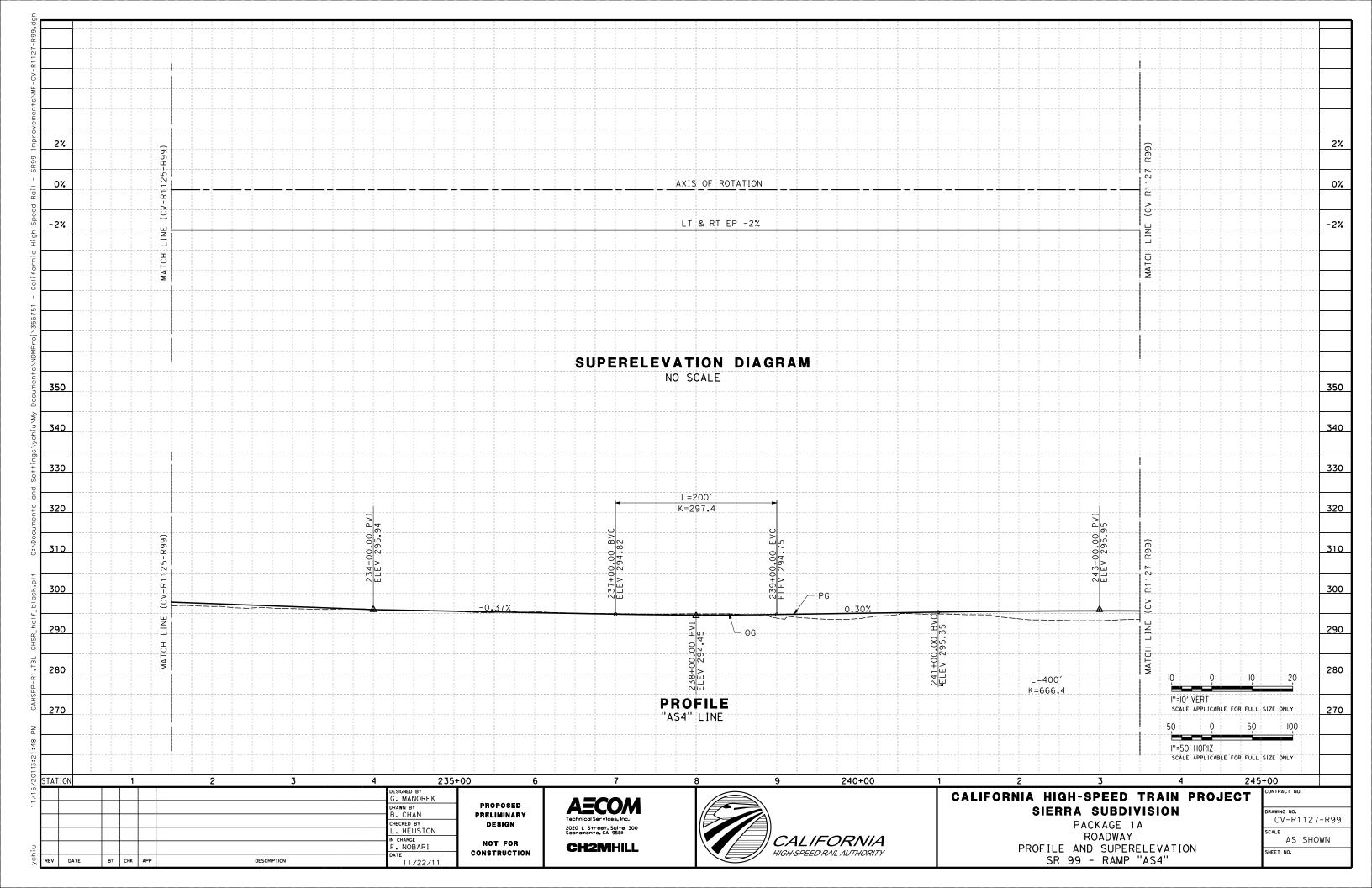


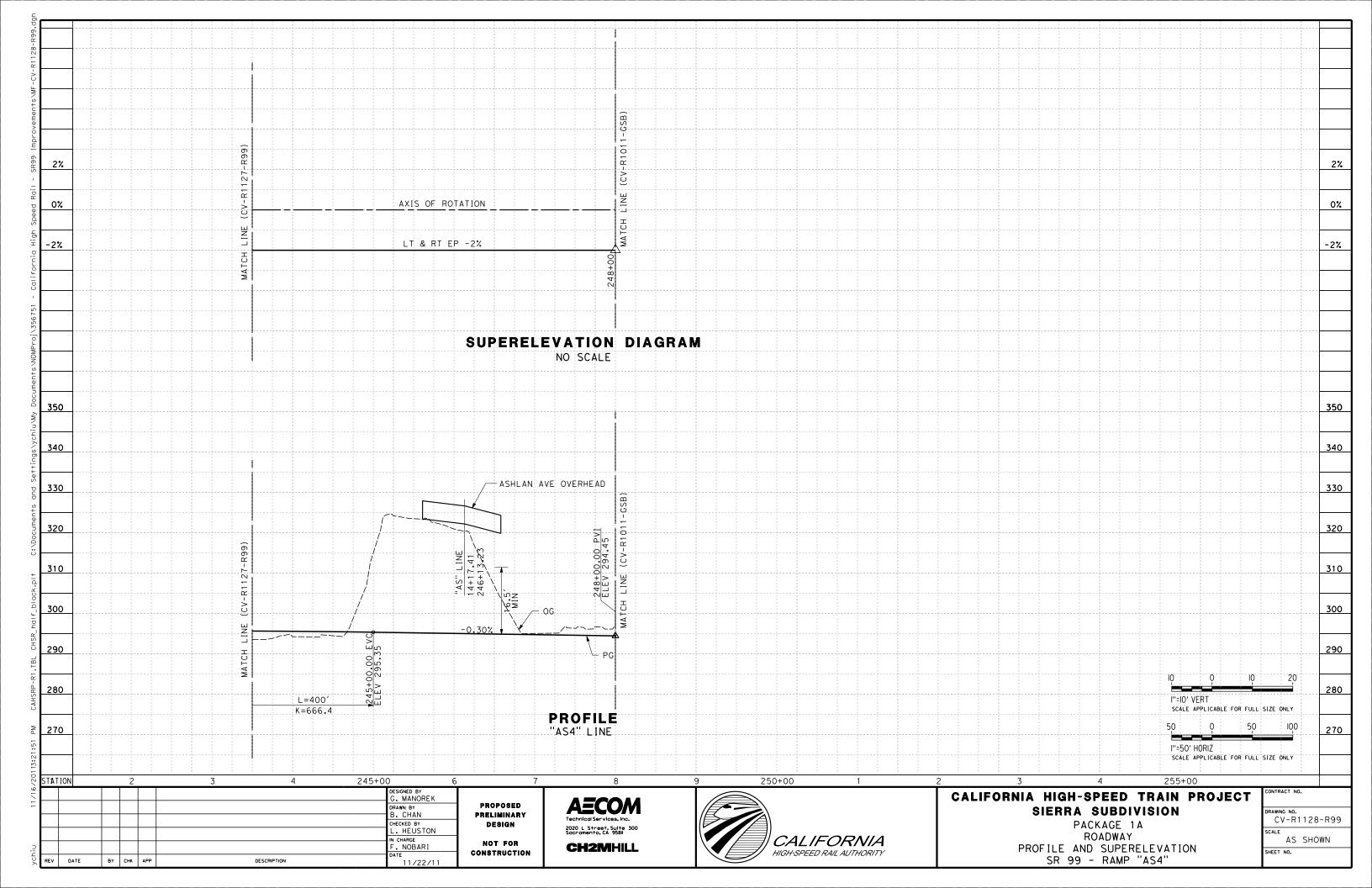


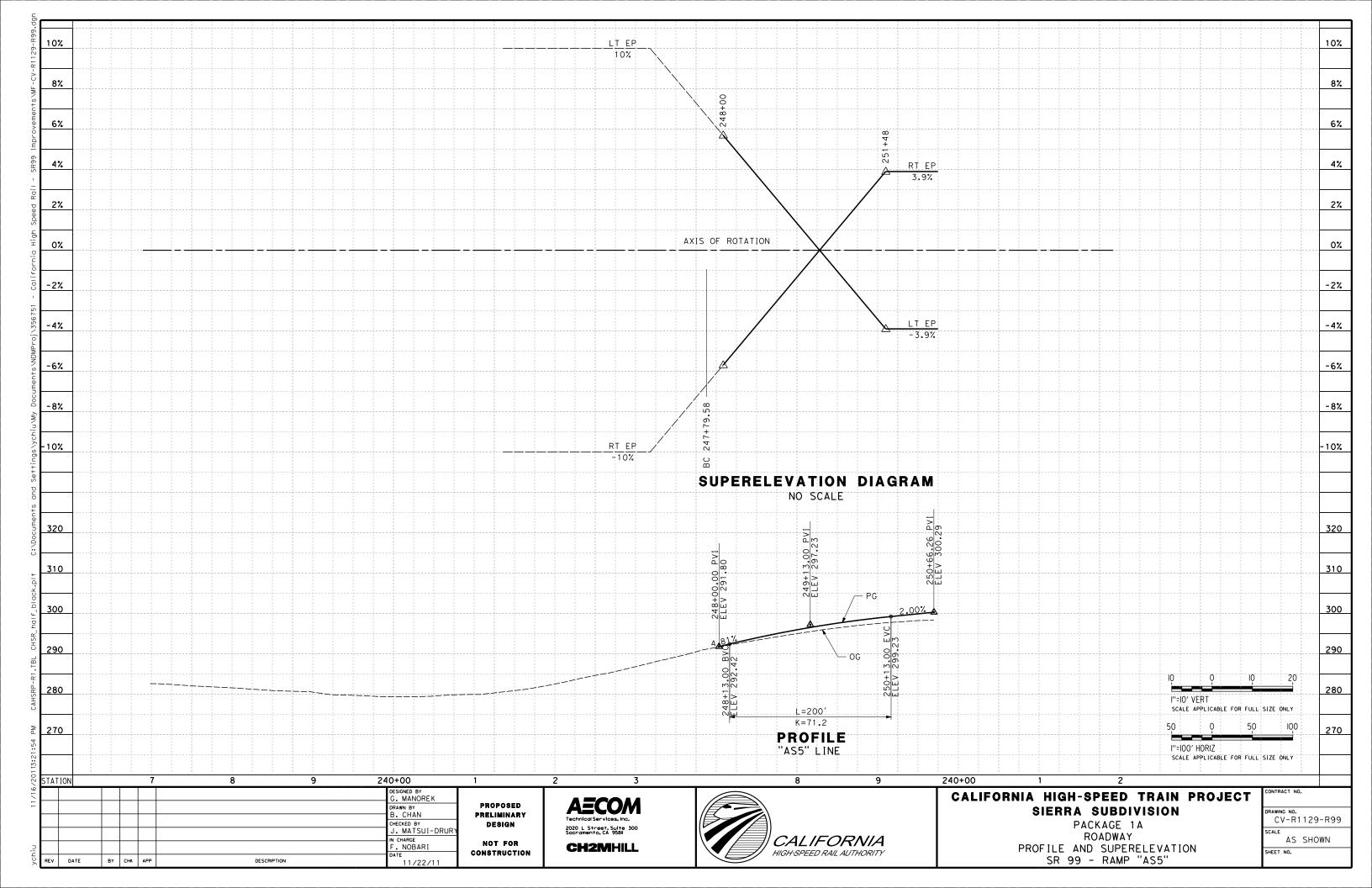


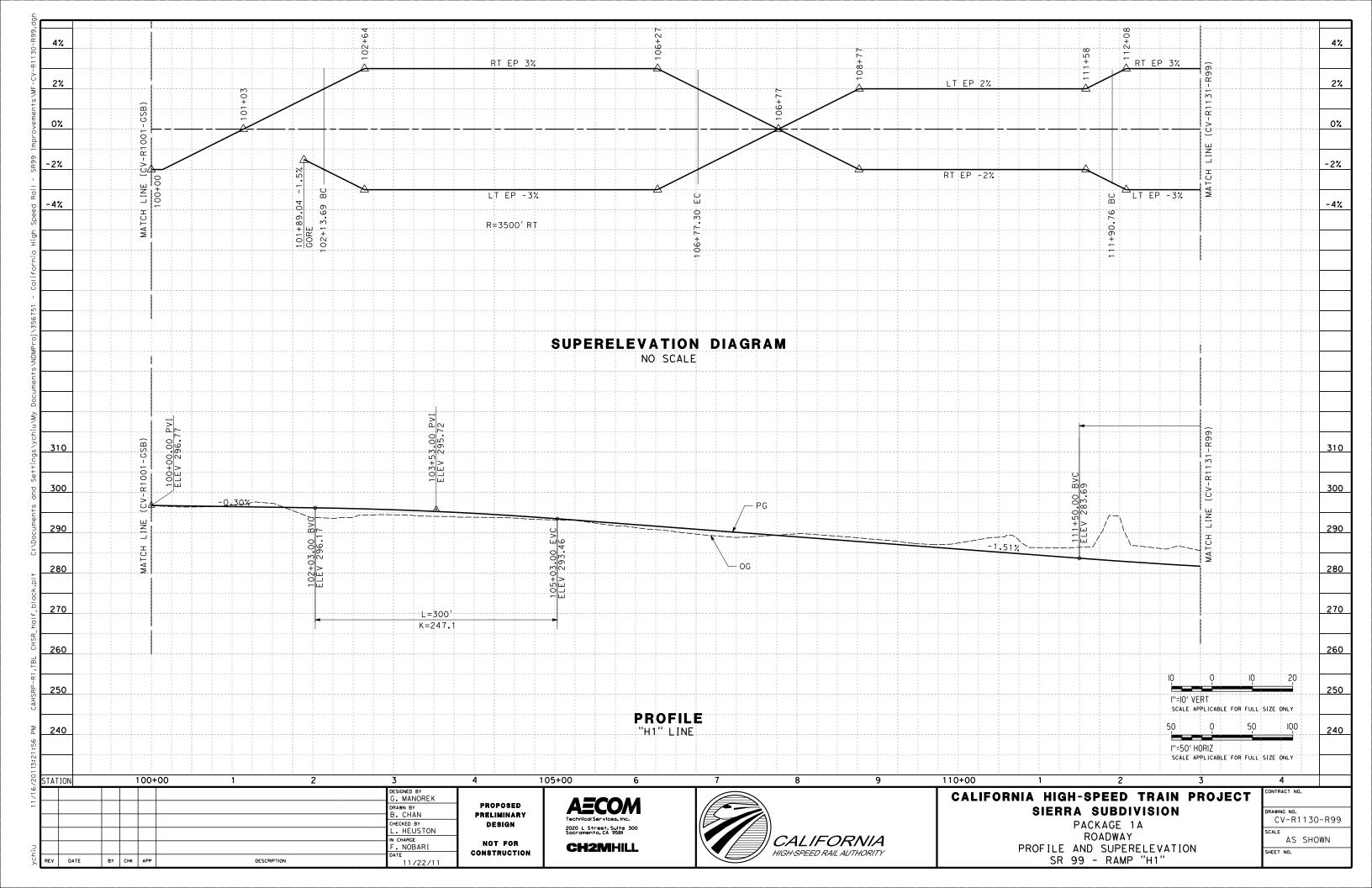


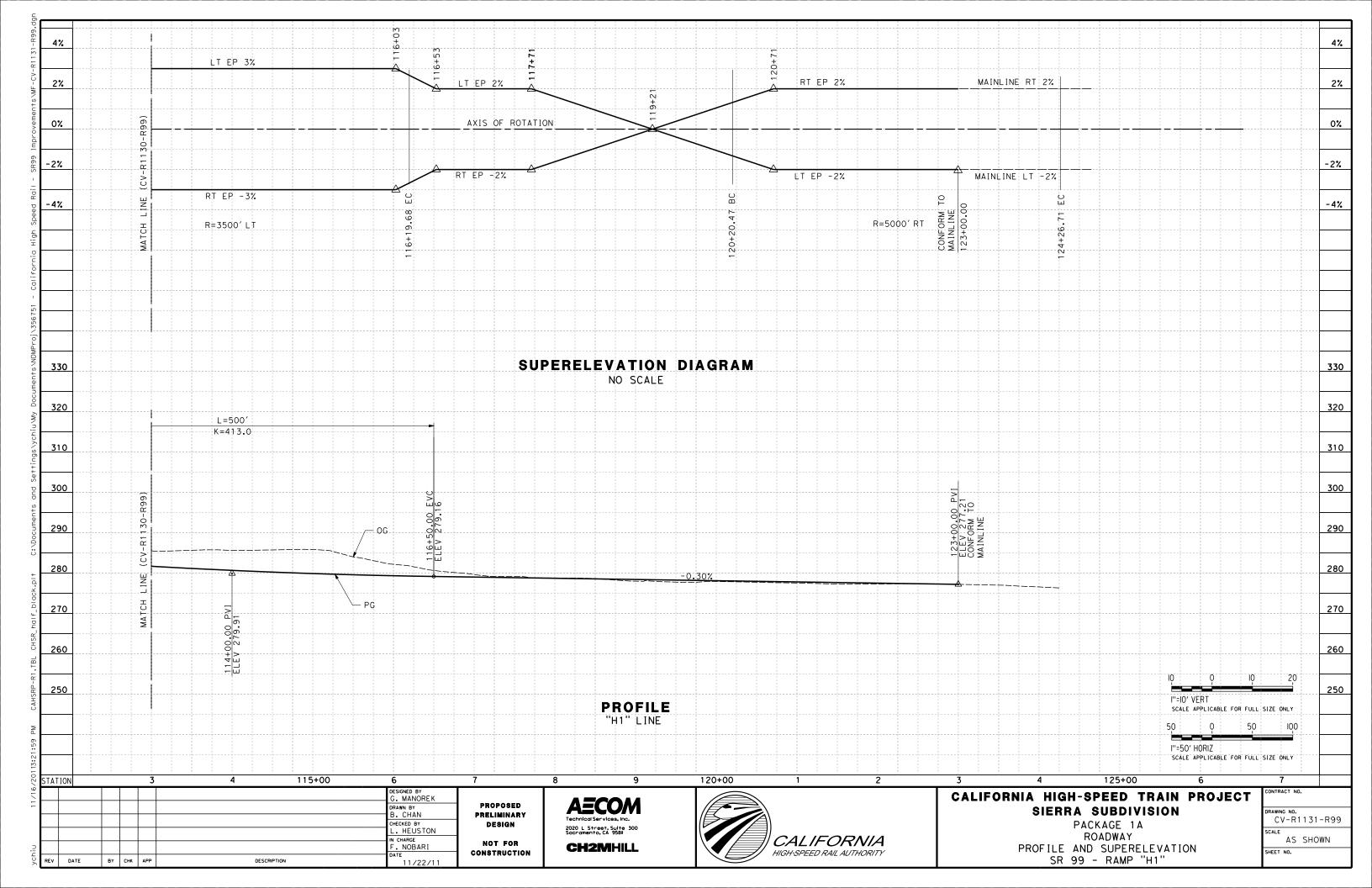


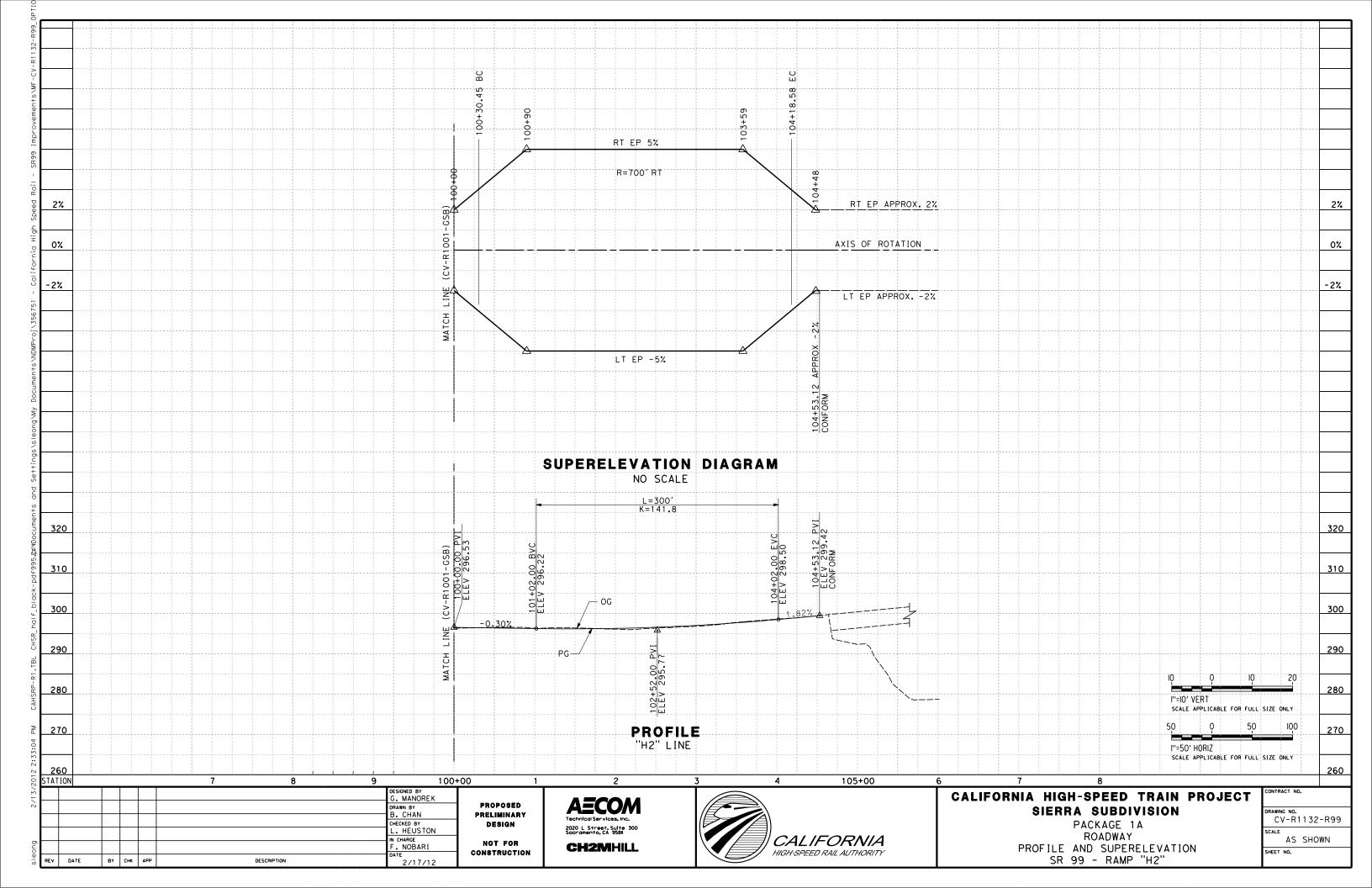


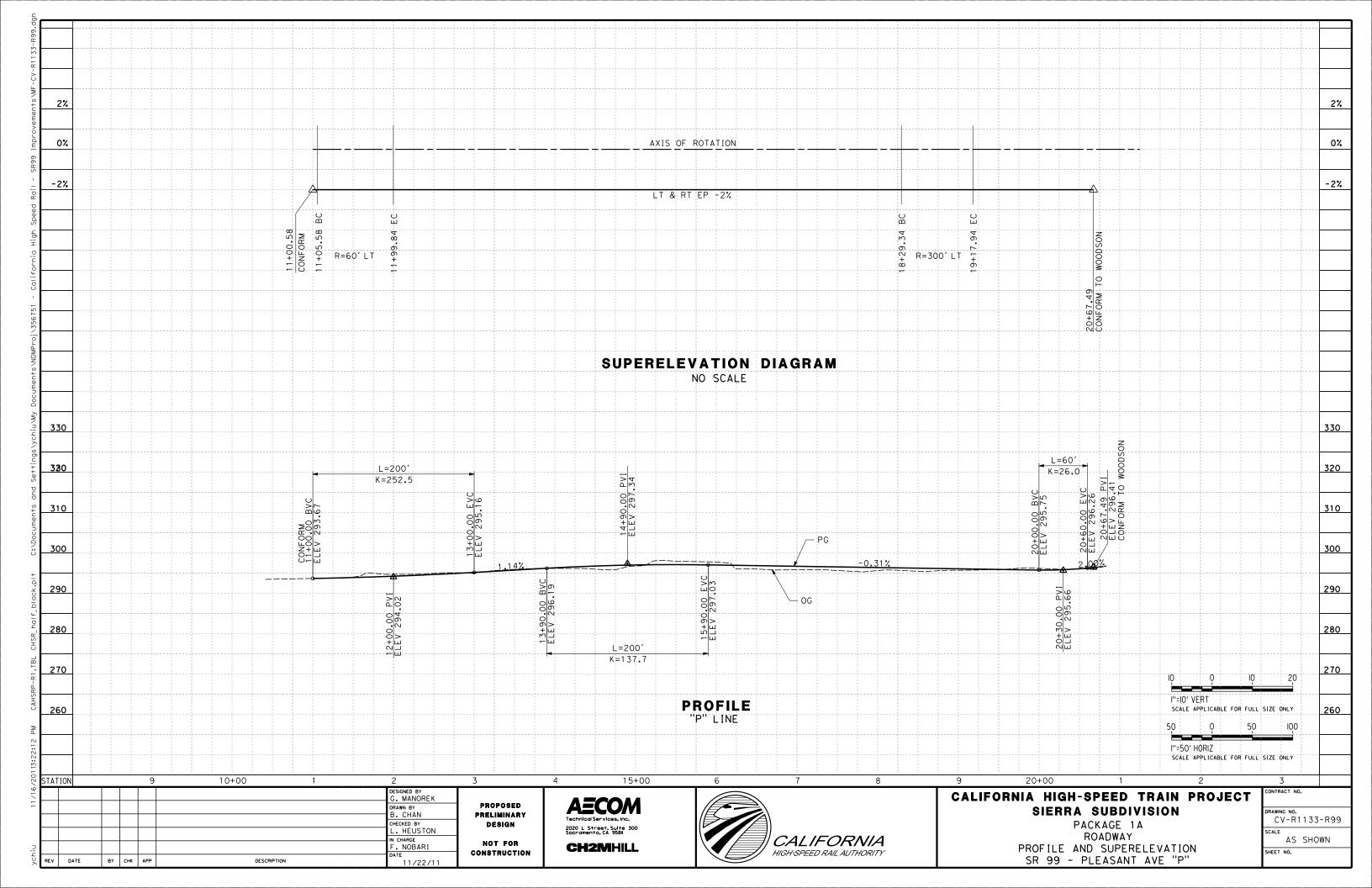


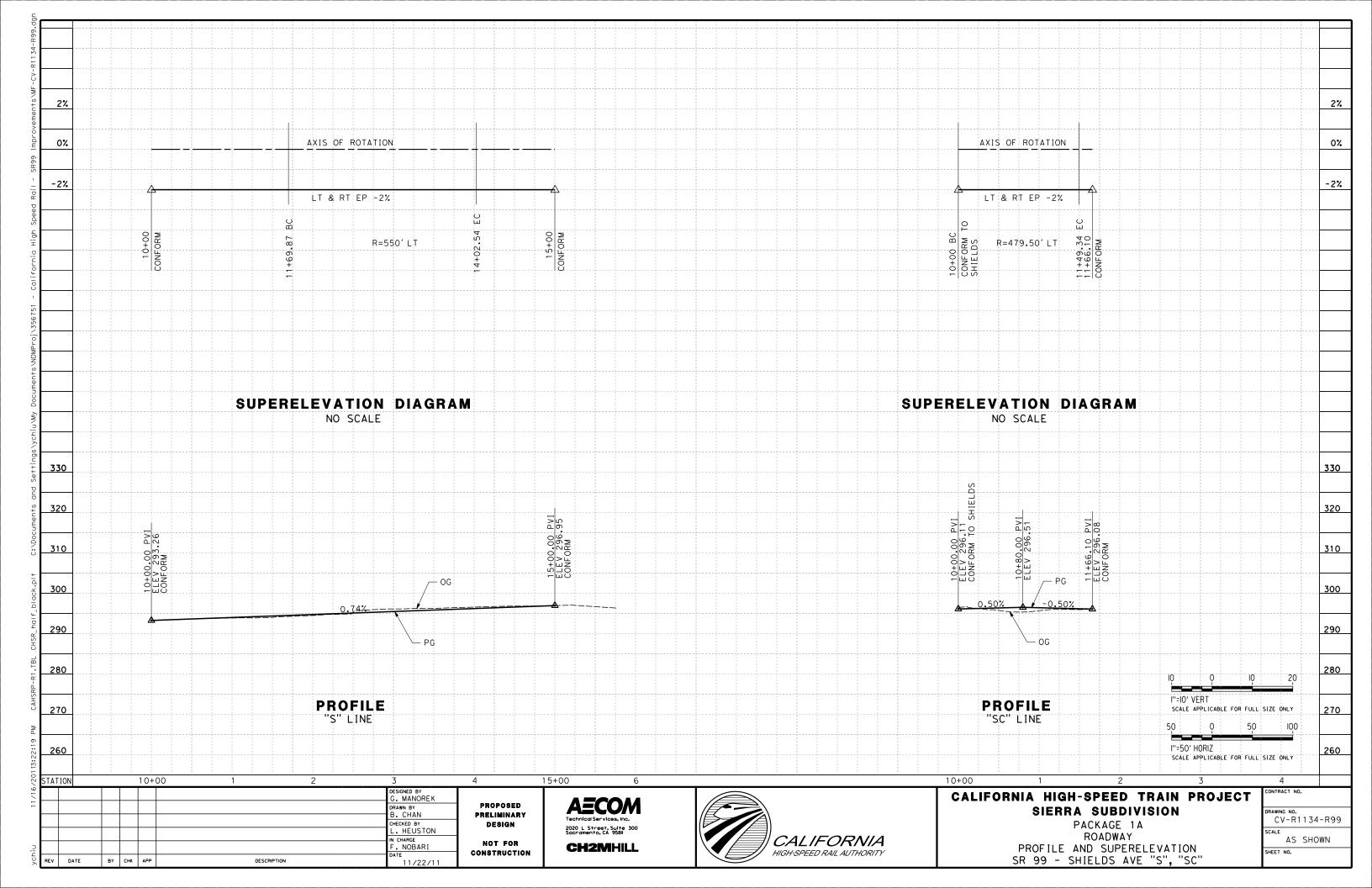


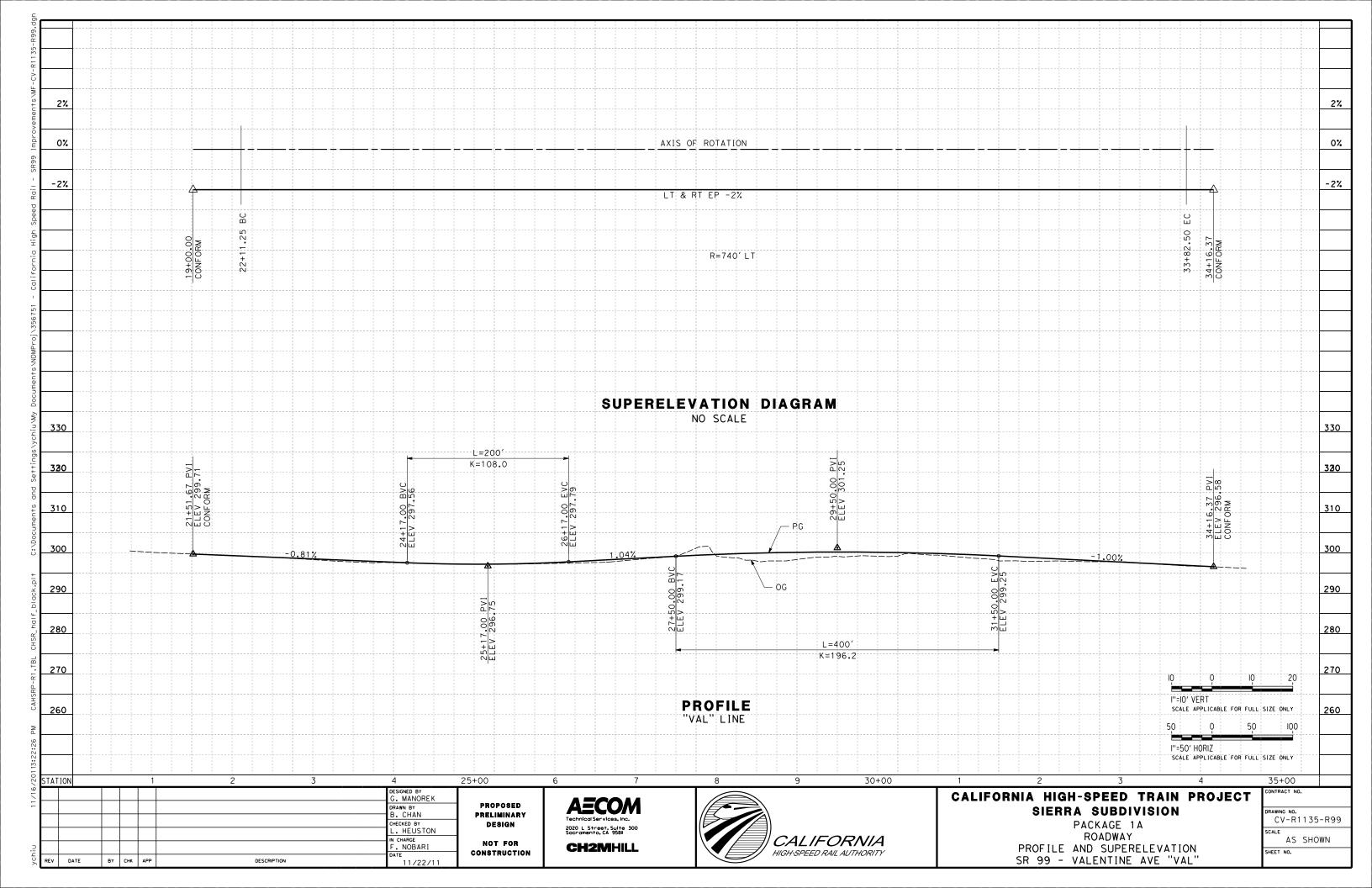


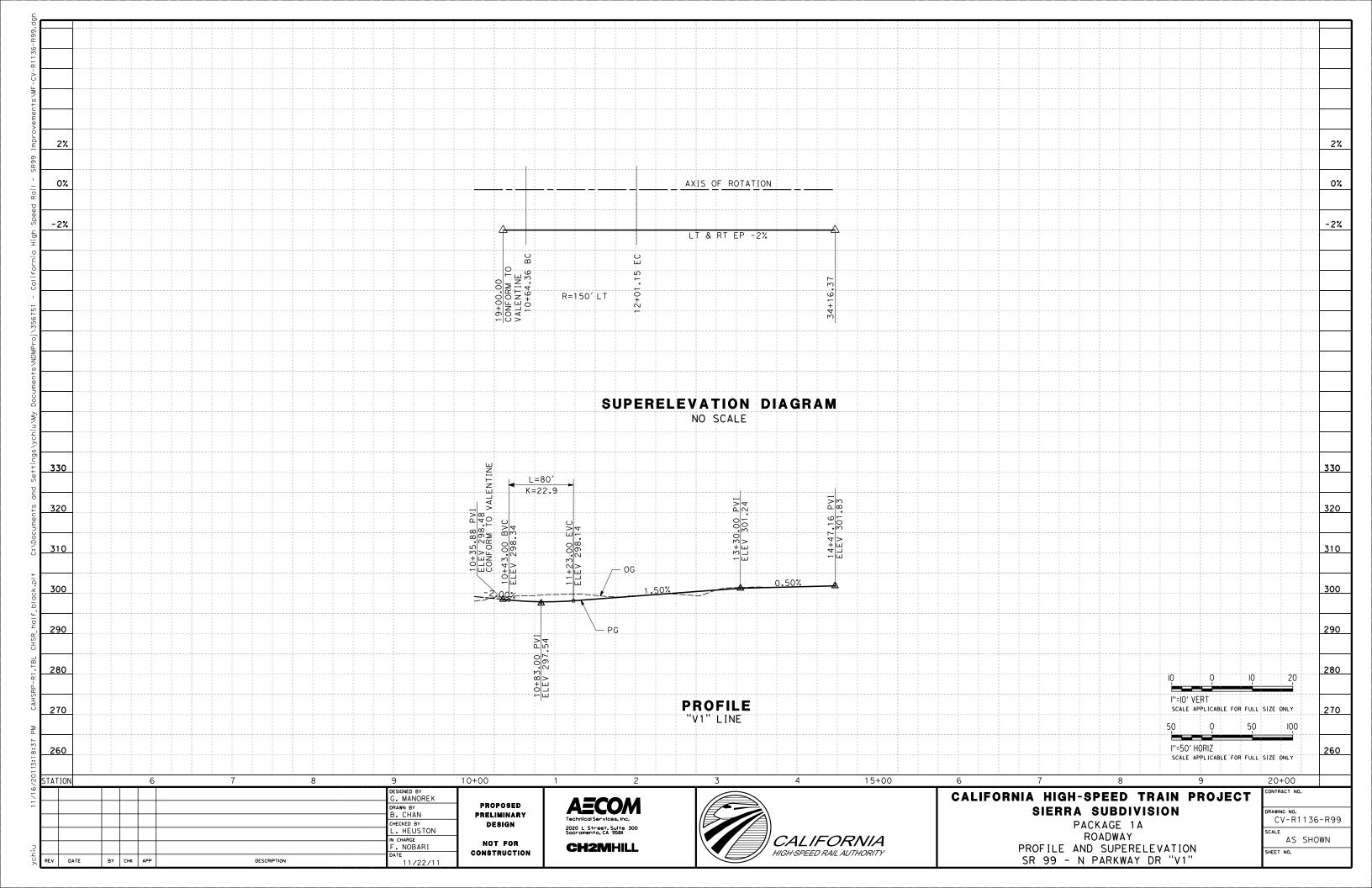


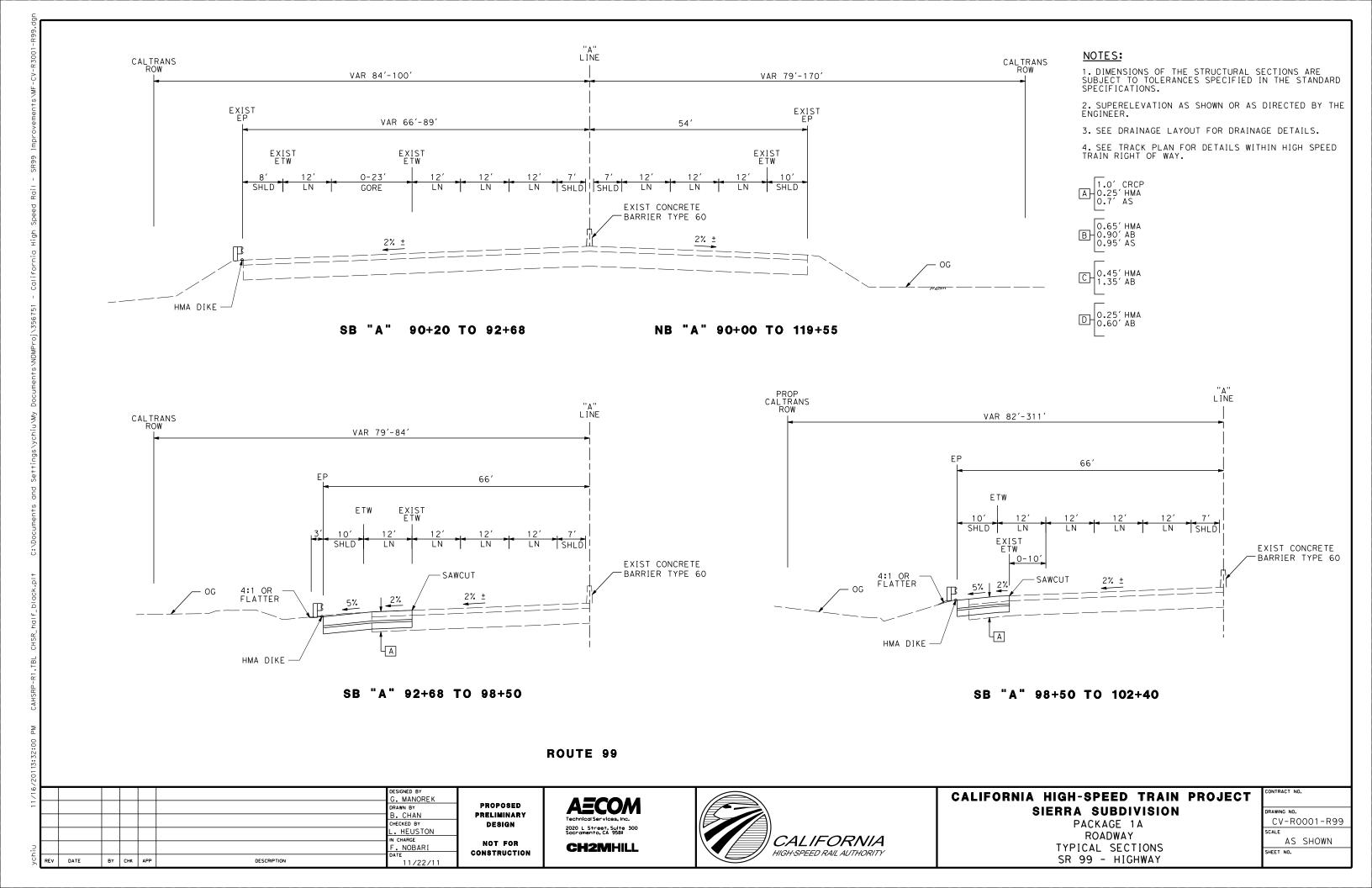


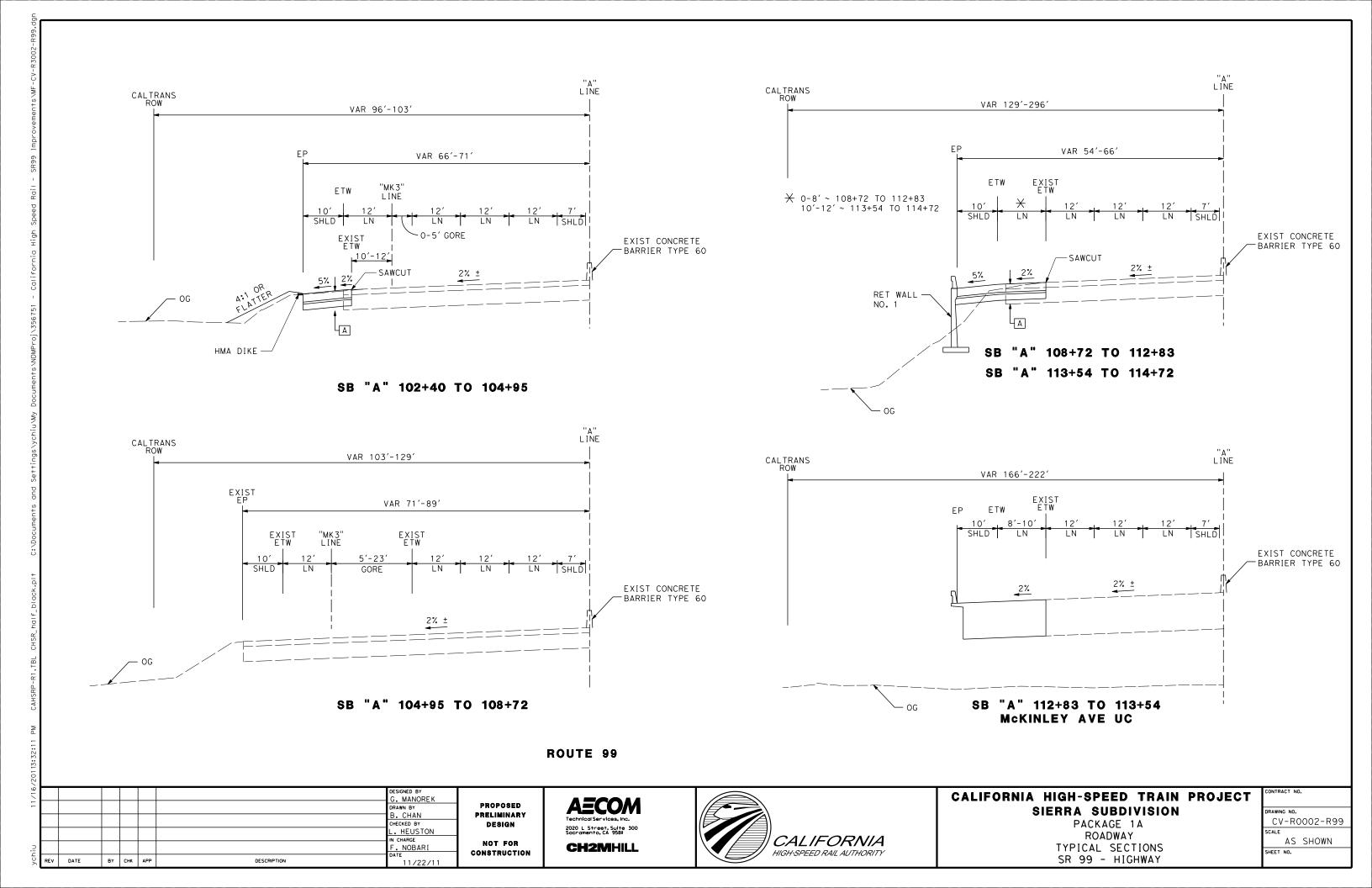


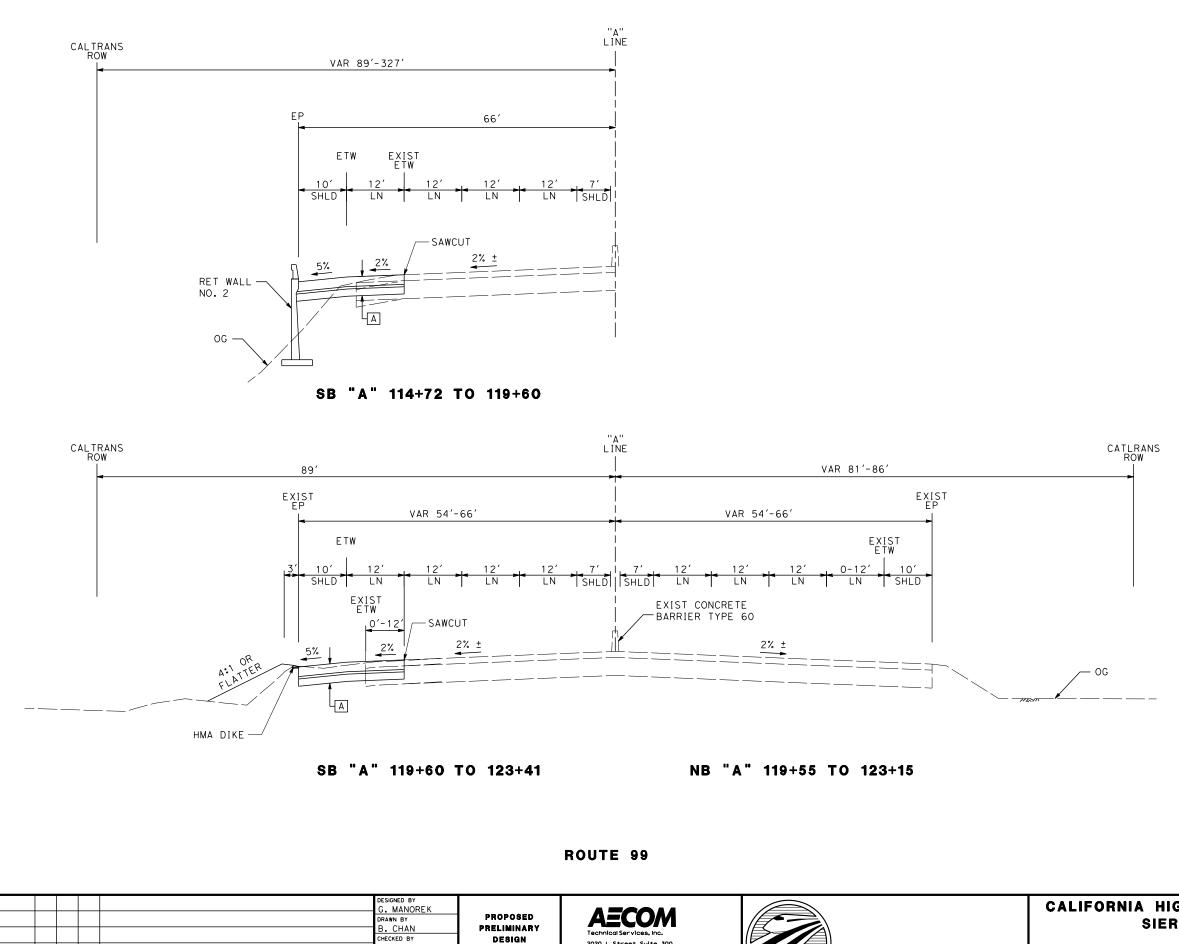












PRELIMINARY

DESIGN

NOT FOR

CONSTRUCTION

. HEUSTON

NOBARI

11/22/11

BY CHK APP

DESCRIPTION

2020 L Street, Suite 300 Sacramento, CA 958II

CH2MHILL

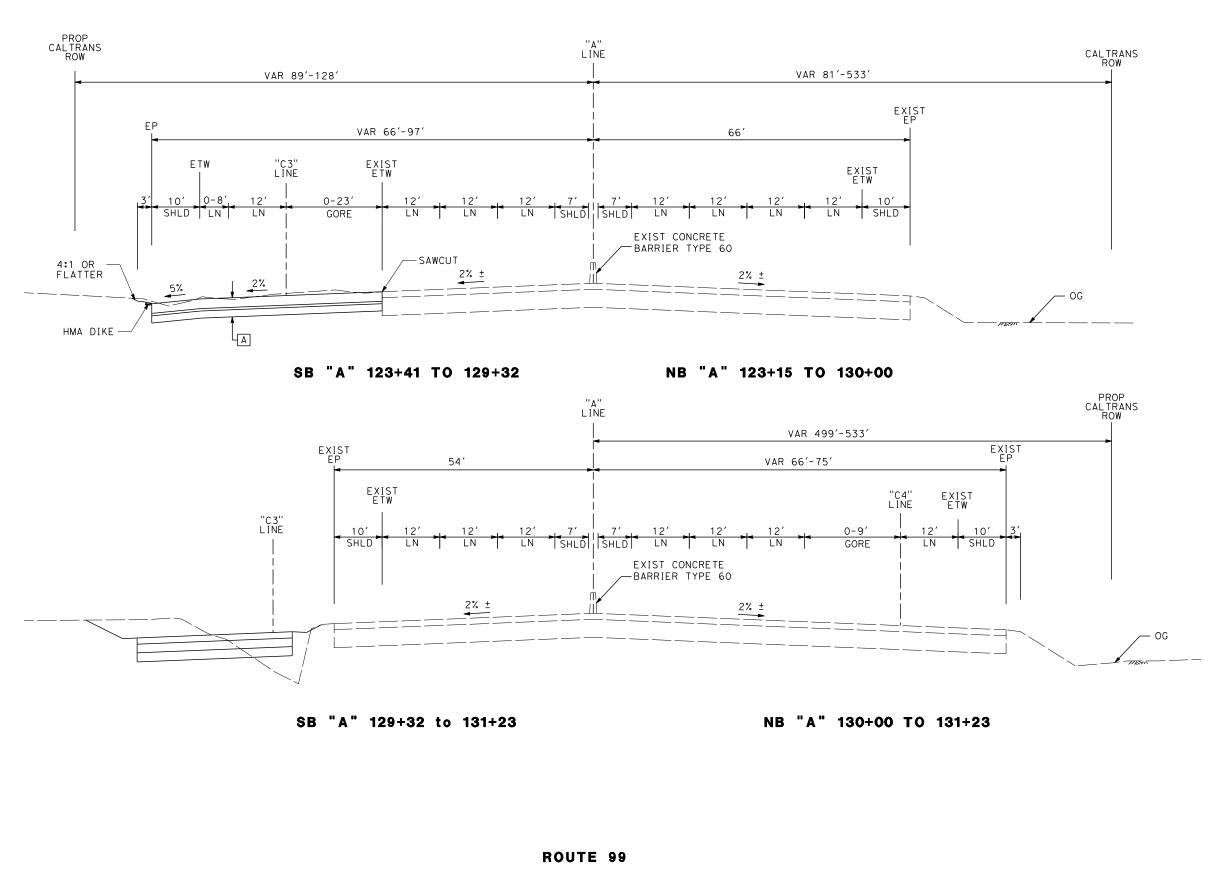
CALIFORNIA

HIGH-SPEED RAIL AUTHORITY

CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A ROADWAY TYPICAL SECTIONS SR 99 - HIGHWAY

CV-R0003-R99 AS SHOWN



DESIGNED BY
G. MANOREK
DRAWN BY
B. CHAN
CHECKED BY
L. HEUSTON
IN CHARGE
F. NOBARI
DATE
REV DATE BY CHK APP
DESCRIPTION

DESCRIPTION

DATE
11/22/11

PROPOSED
PRELIMINARY
DESIGN
NOT FOR
CONSTRUCTION

AECOM
Technical Services, Inc.
2020 L Street, Suite 300
Sacramento, CA 95811
CH2MHILL



CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A ROADWAY TYPICAL SECTIONS SR 99 - HIGHWAY DRAWING NO.

CV-R0004-R99

SCALE

AS SHOWN

SHEET NO.

PROPOSED PRELIMINARY DESIGN NOT FOR CONSTRUCTION

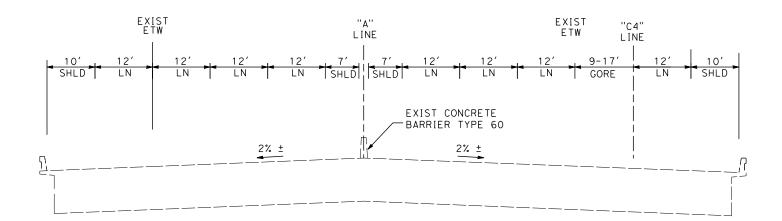
2020 L Street, Suite 300 Sacramento, CA 958II **CH2MHILL**



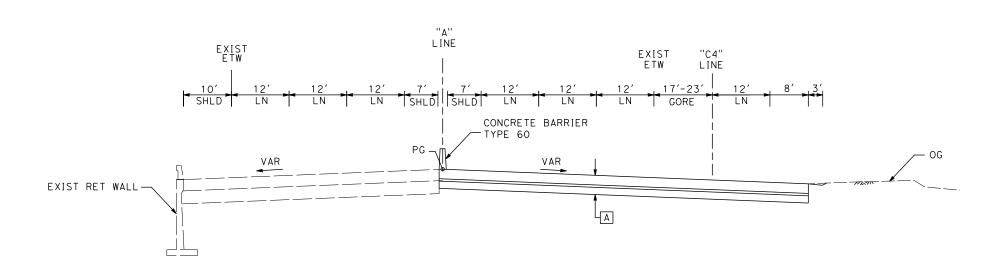
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PACKAGE 1A ROADWAY TYPICAL SECTIONS SR 99 - HIGHWAY

CV-R0005-R99 AS SHOWN



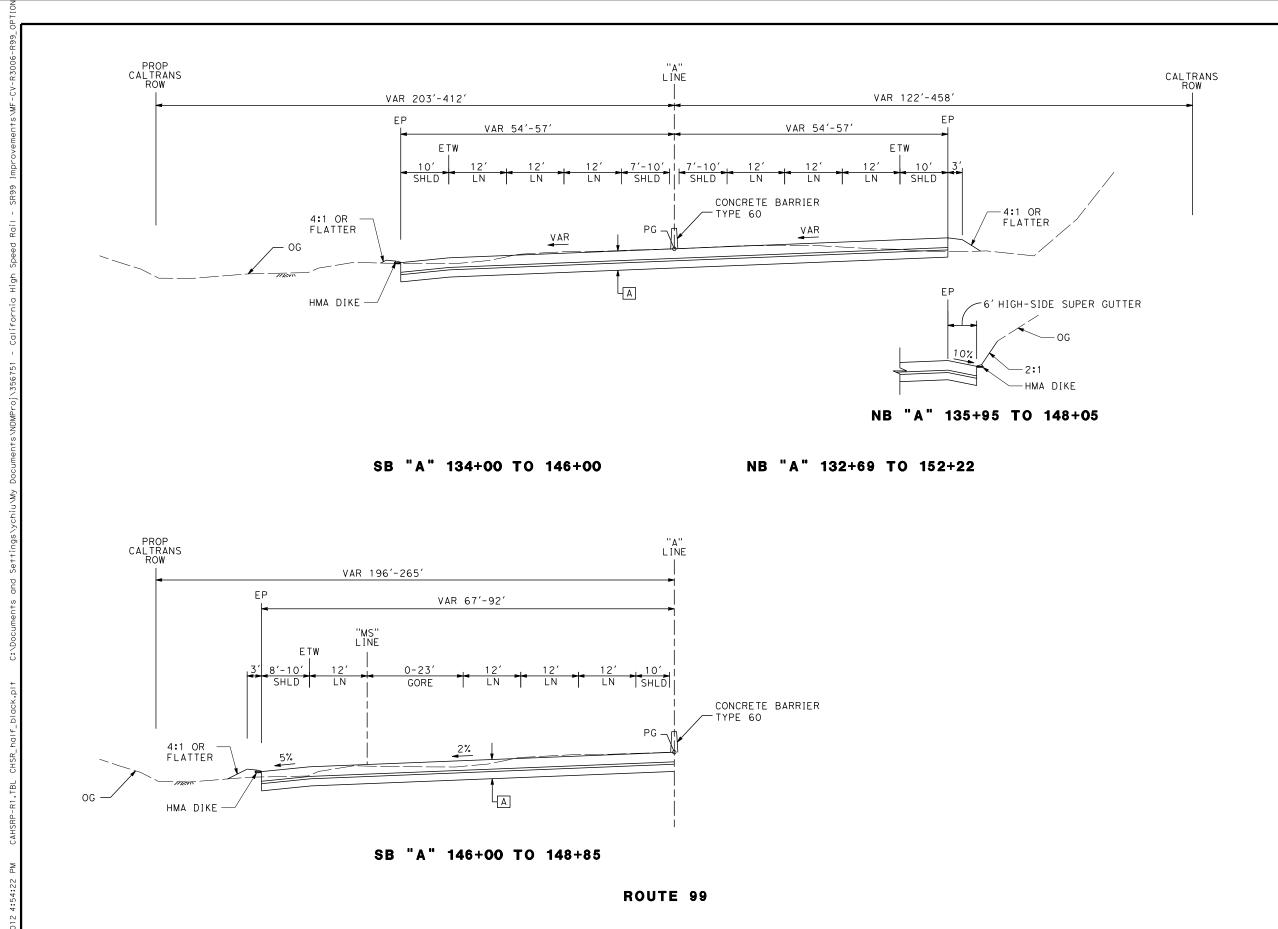
"A" 131+23 TO 132+00 NORTH FRESNO OC



SB "A" 132+00 TO 134+00

NB "A" 132+00 TO 132+69

ROUTE 99





PROPOSED
PRELIMINARY
DESIGN
NOT FOR
CONSTRUCTION

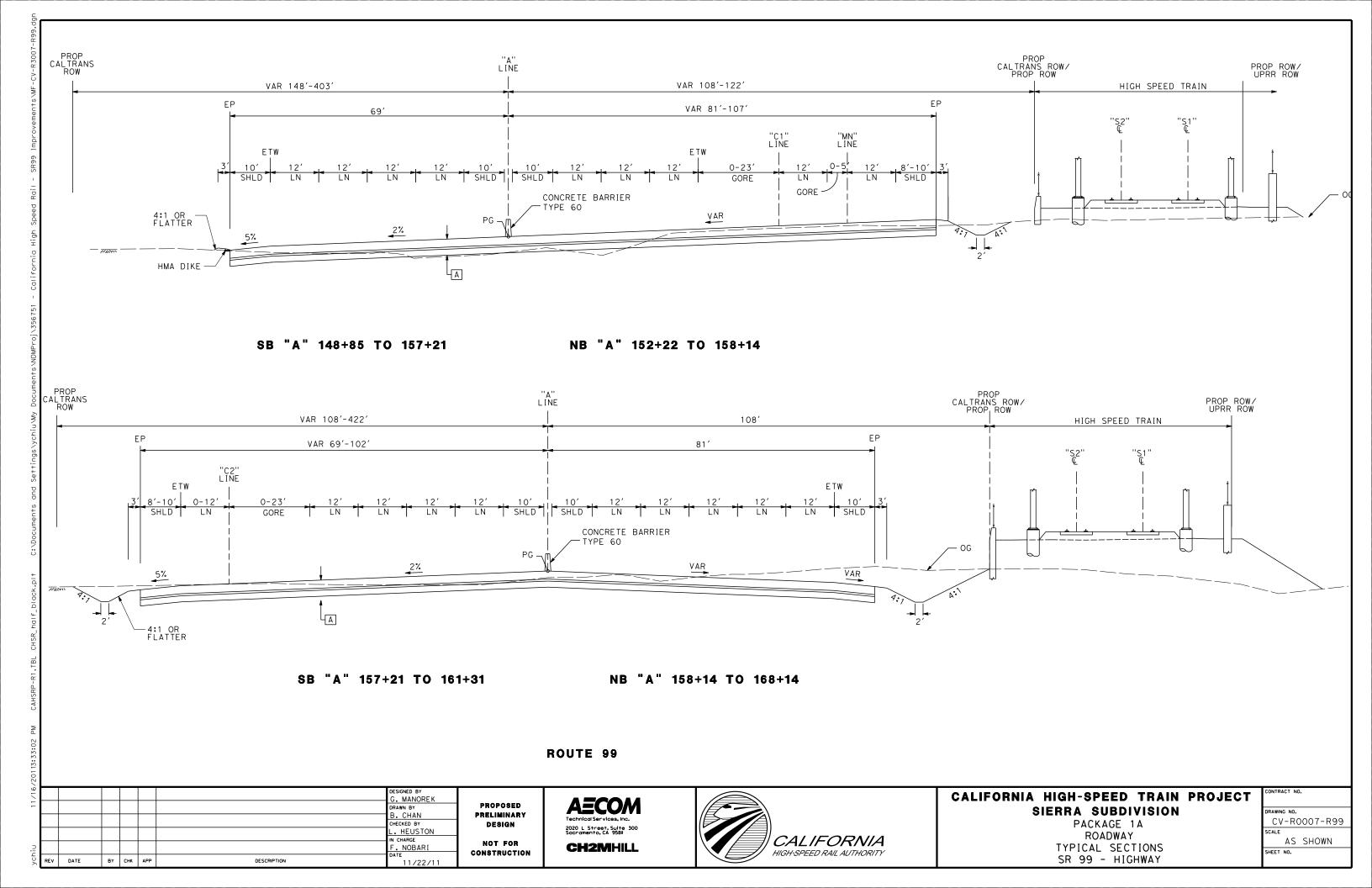
AECOM
Technical Services, Inc.
2020 L Street, Suite 300
Sacramento, CA 958II
CH2MHILL

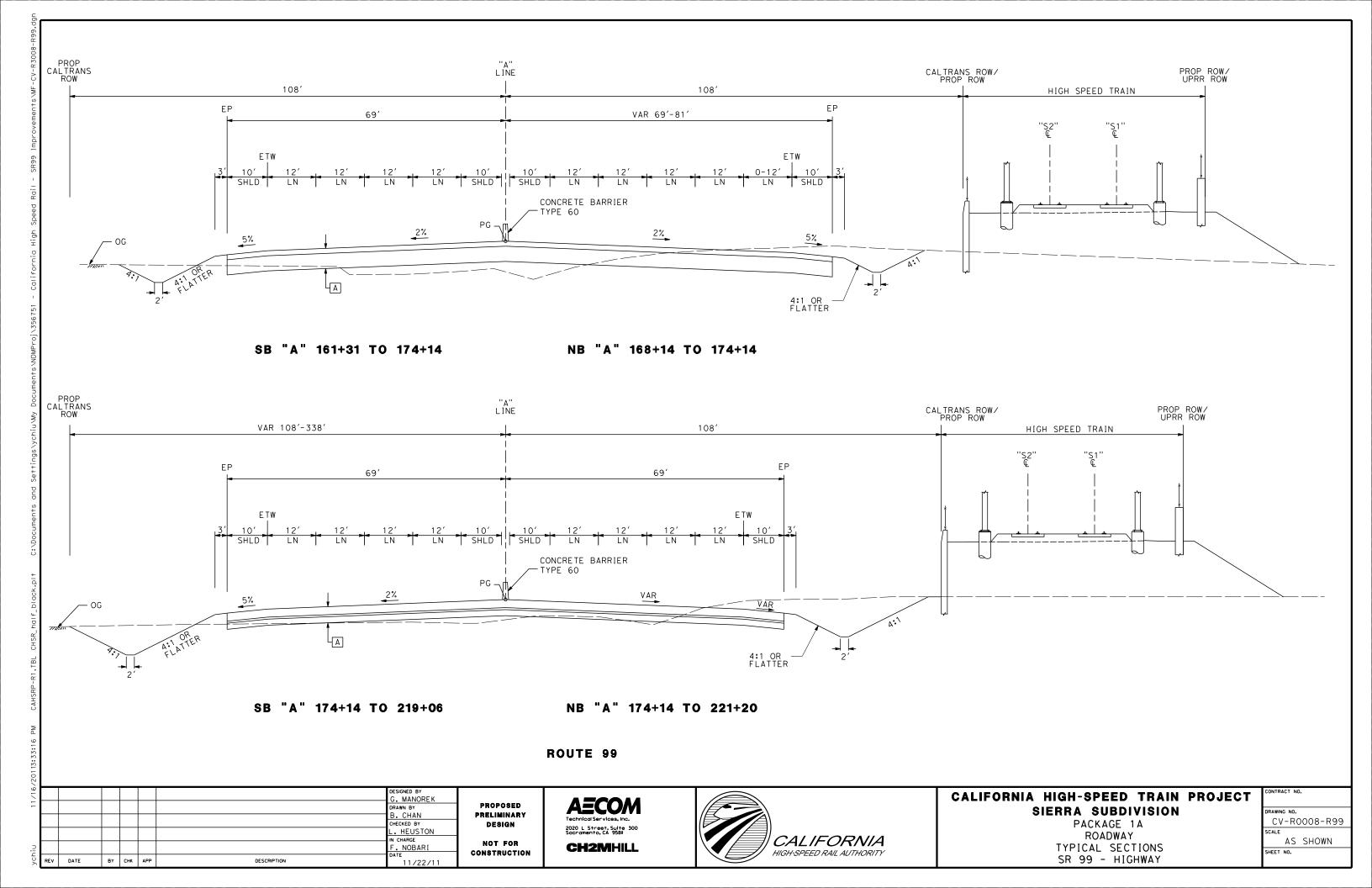


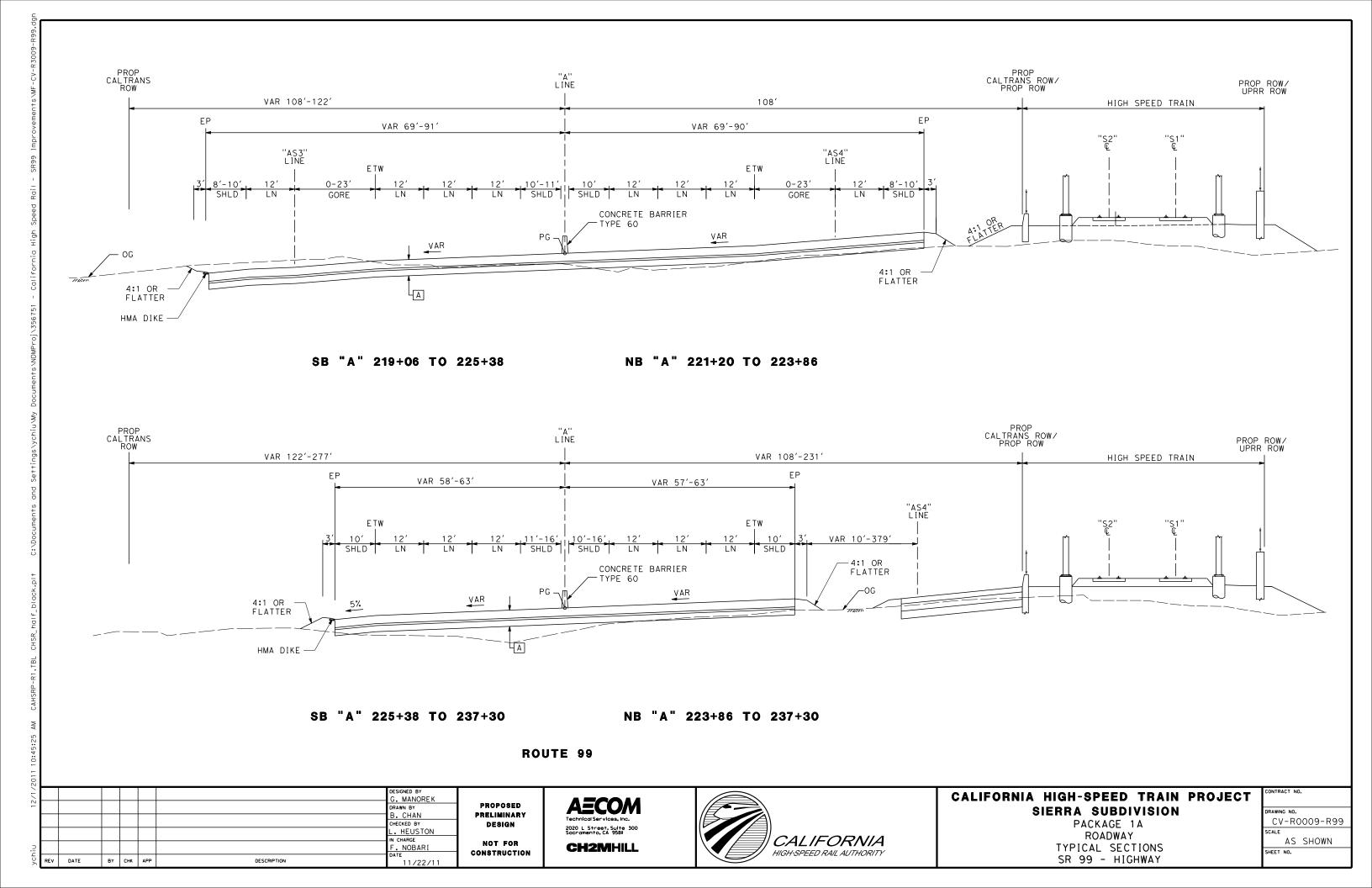
CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

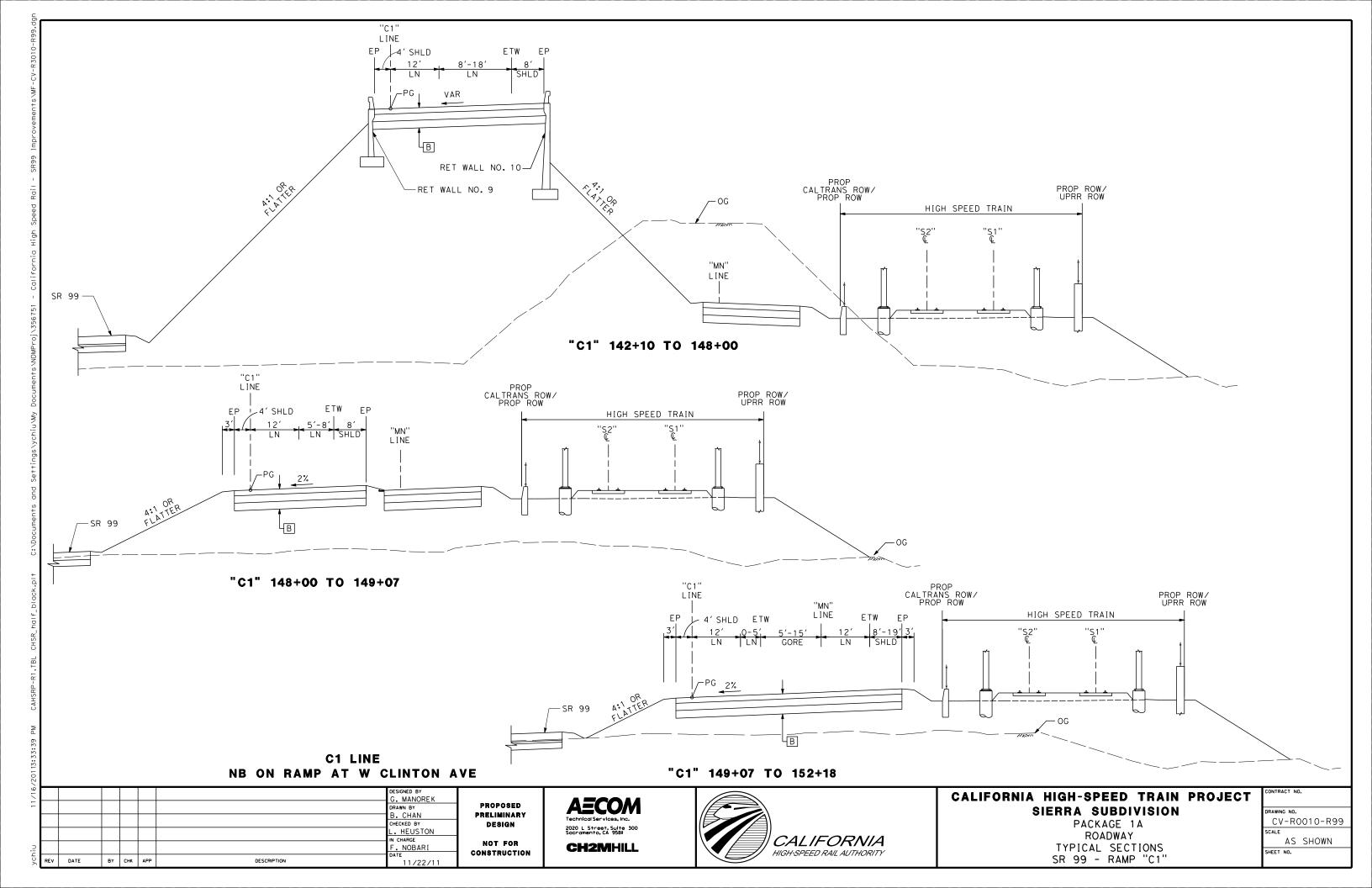
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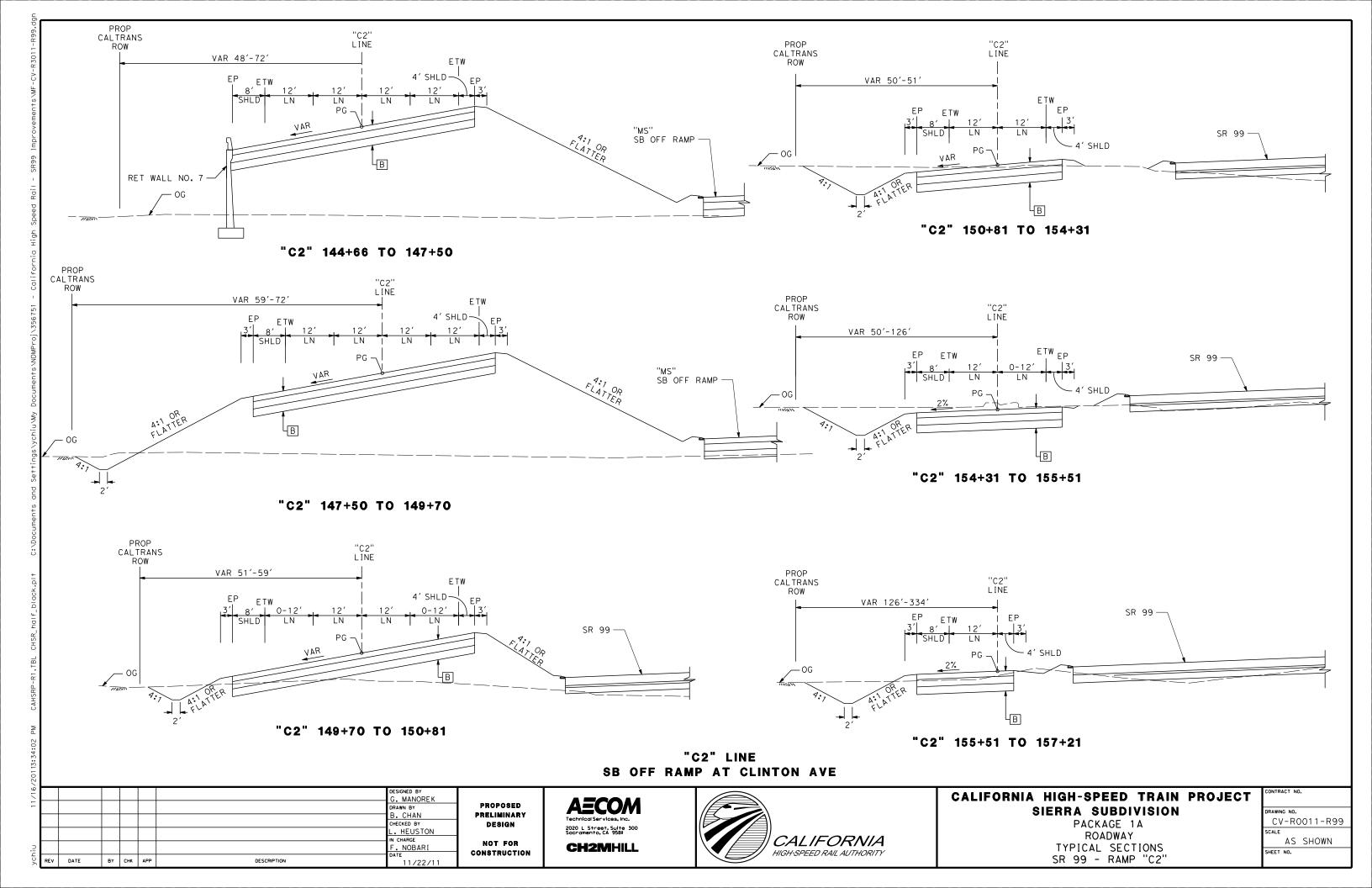
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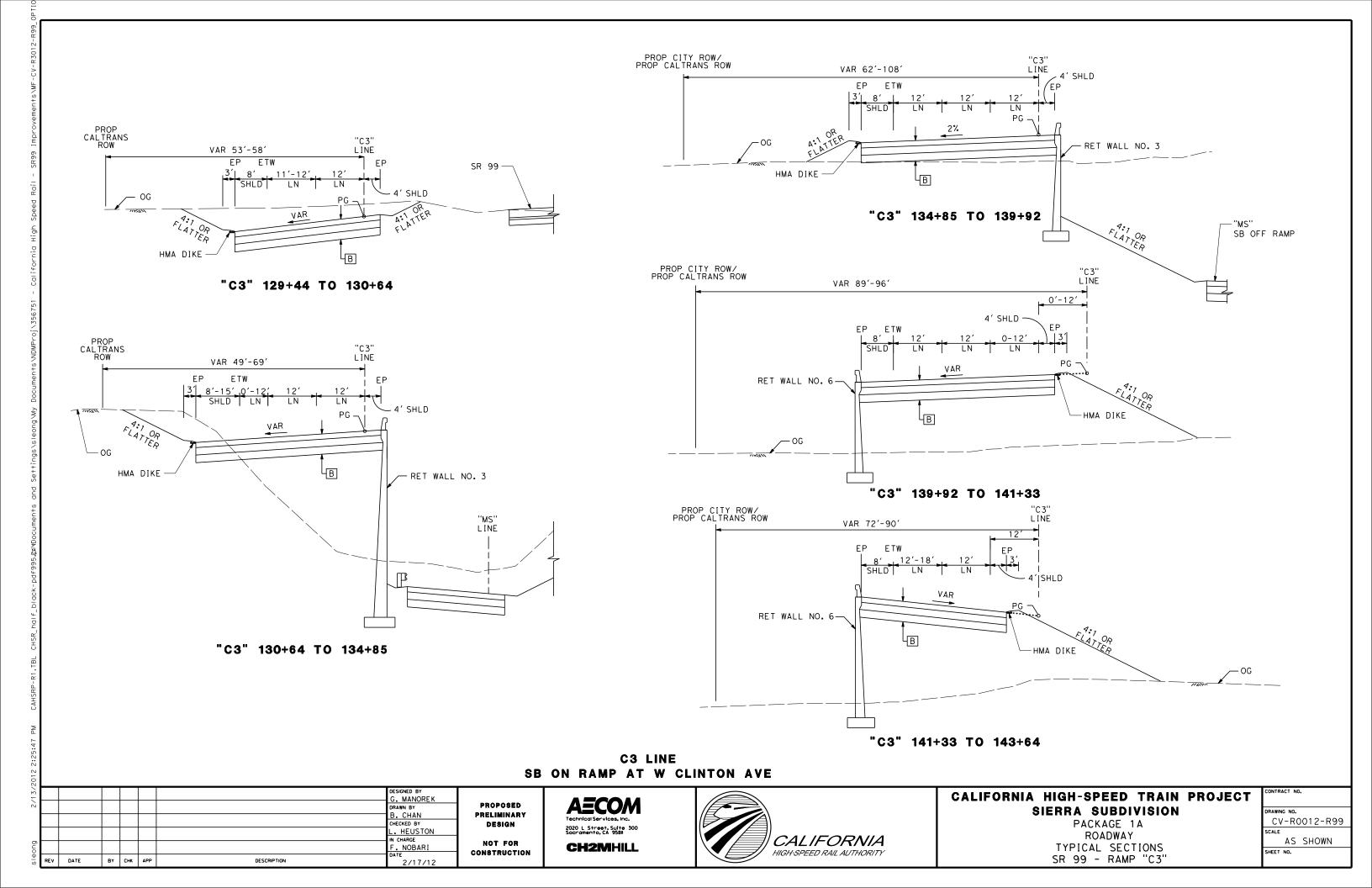


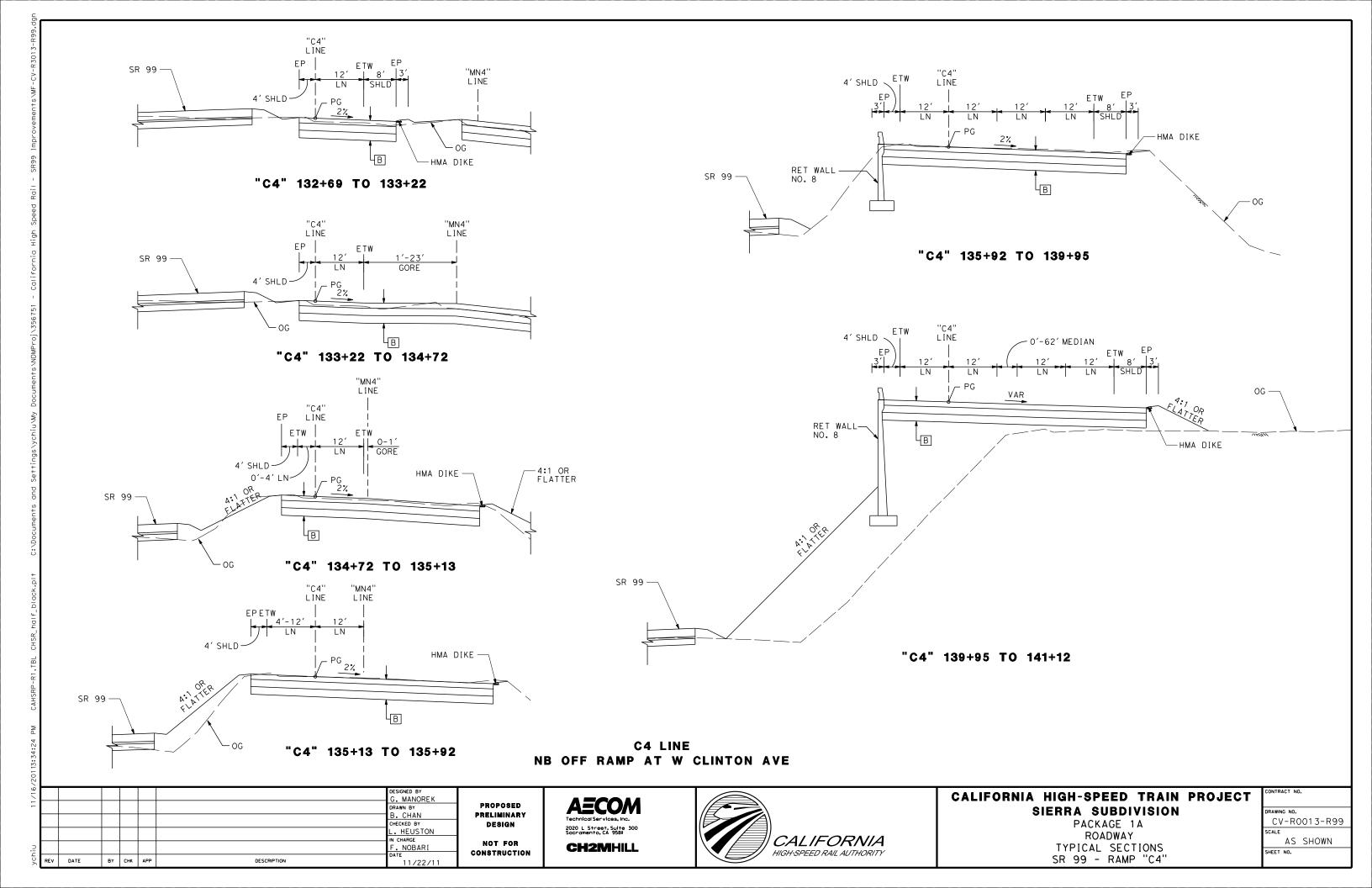


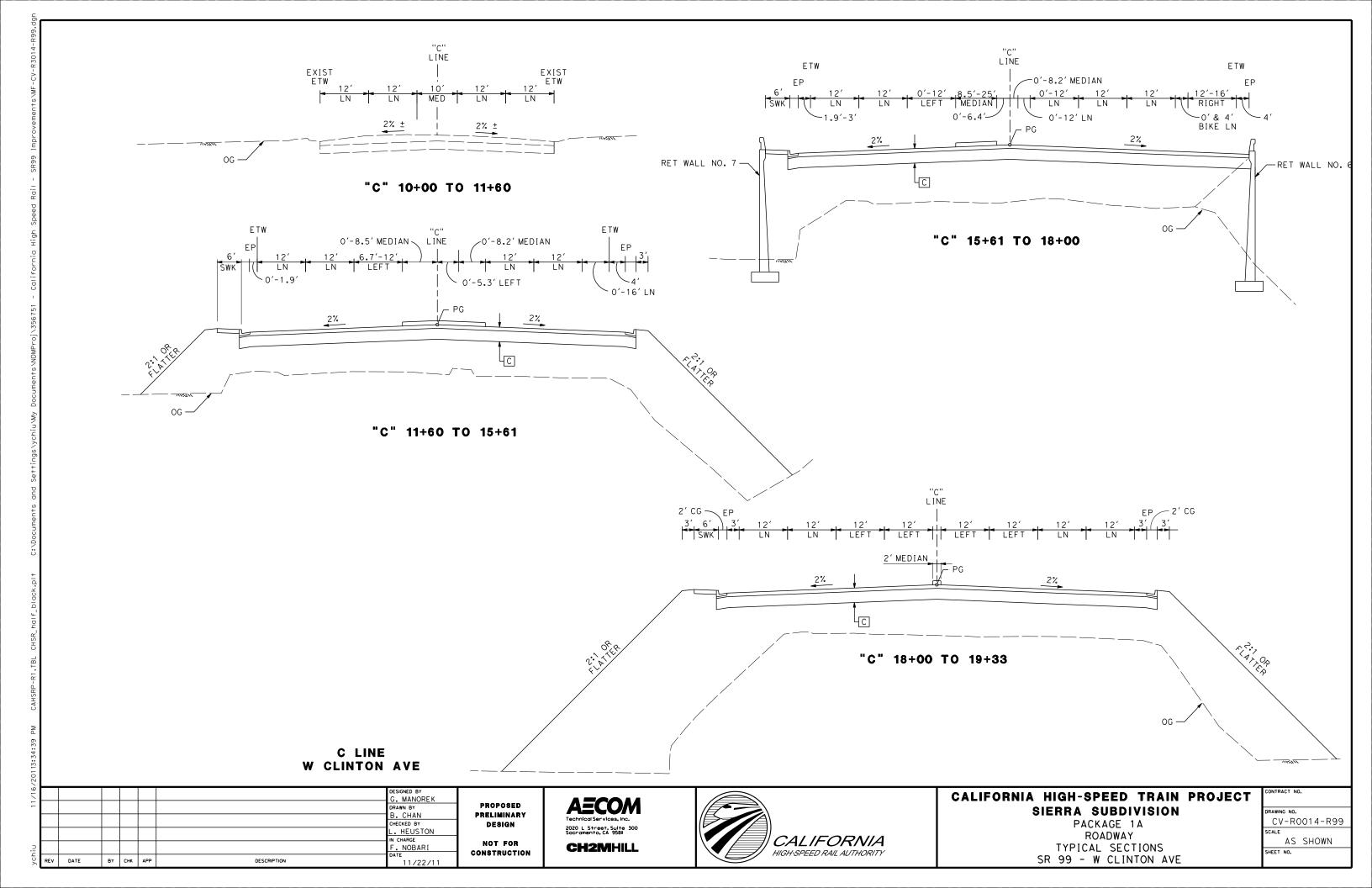


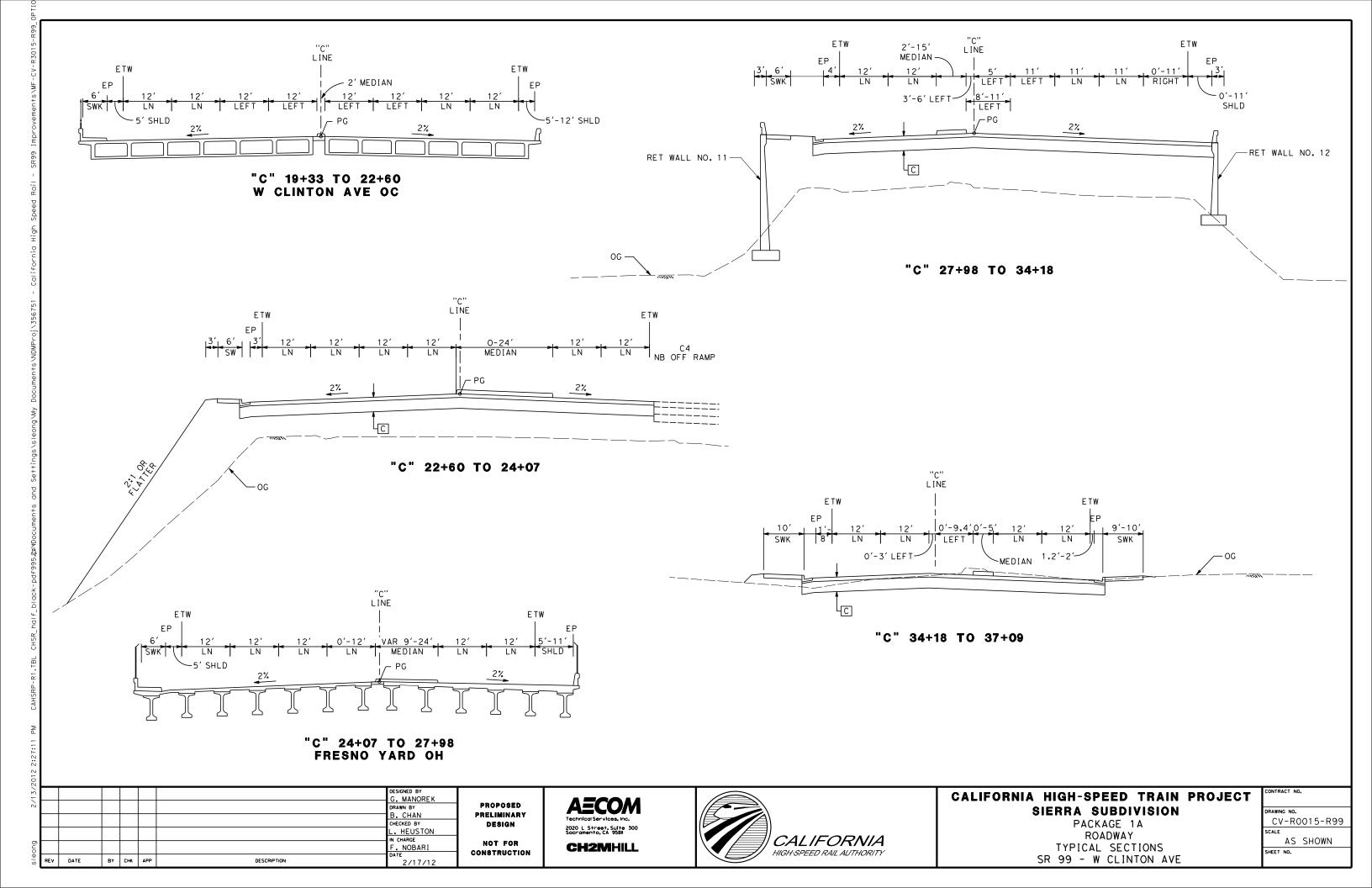


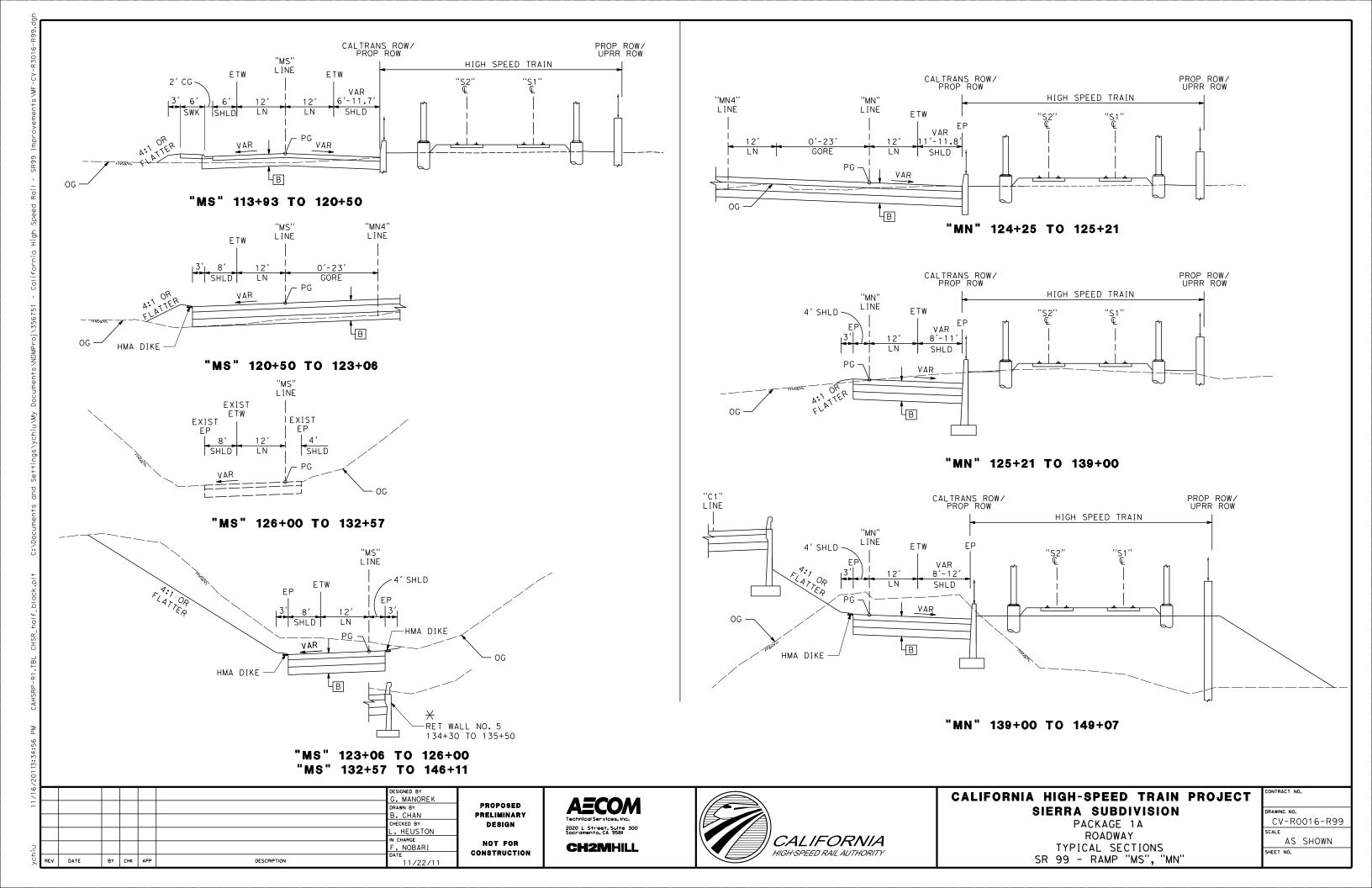


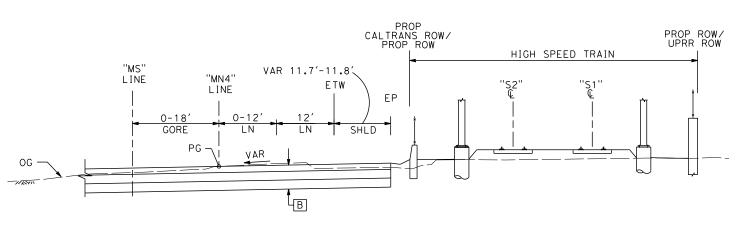




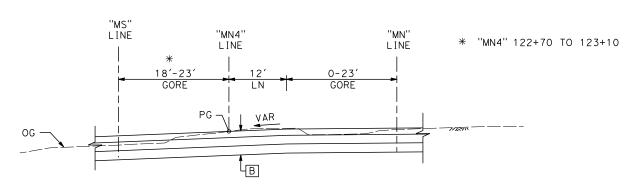




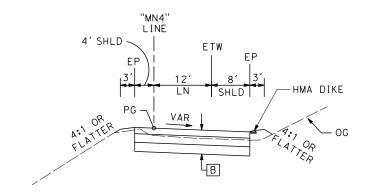




"MN4" 120+47 TO 122+70

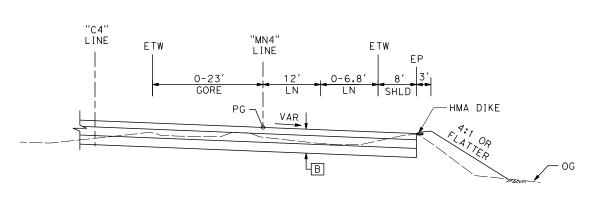


"MN4" 122+70 TO 123+62

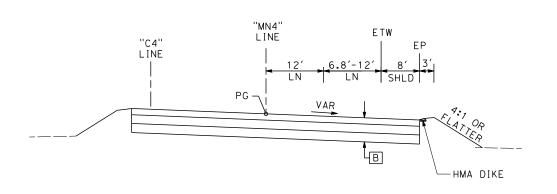


"MN4" 123+62 TO 132+27

MN4 LINE GOLDEN STATE BLVD NB OFF RAMP



"MN4" 132+27 TO 134+18



"MN4" 134+18 TO 134+97



PROPOSED PRELIMINARY DESIGN NOT FOR

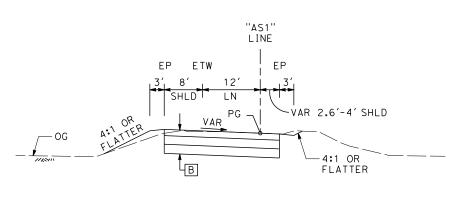
2020 L Street, Suite 300 Sacramento, CA 958II **CH2MHILL**



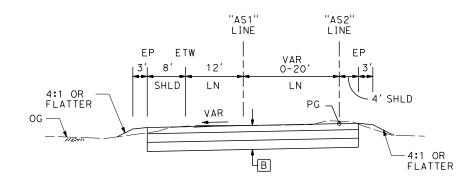
CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A ROADWAY TYPICAL SECTIONS SR 99 - RAMP "MN4"

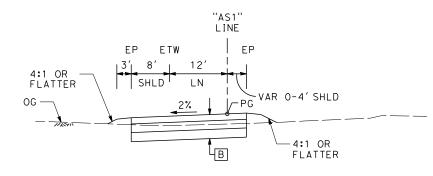
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"AS1" 238+39 TO 239+48

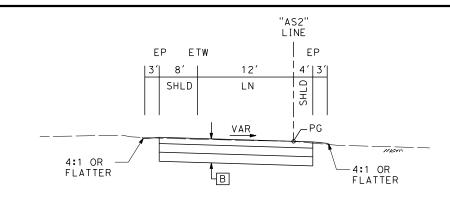


"AS1" 239+48 TO 240+47



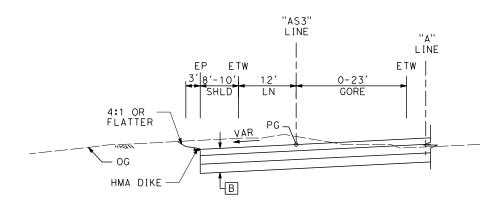
"AS1" 240+47 TO 242+38

AS1 LINE SB ON RAMP AT ASHLAN AVE

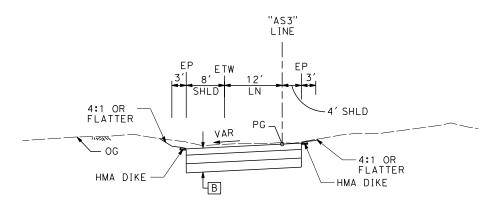


"AS2" 240+40 TO 241+01

AS2 LINE SB ON RAMP AT ASHLAN AVE



"AS3" 219+06 TO 225+34



"AS3" 225+34 TO 233+28

AS3 LINE SB ON RAMP AT ASHLAN AVE

			CHECKED BY L. HEUSTON IN CHARGE F. NOBARI	-
			CHECKED BY	1
			DESIGNED BY G. MANOREK DRAWN BY B. CHAN	-

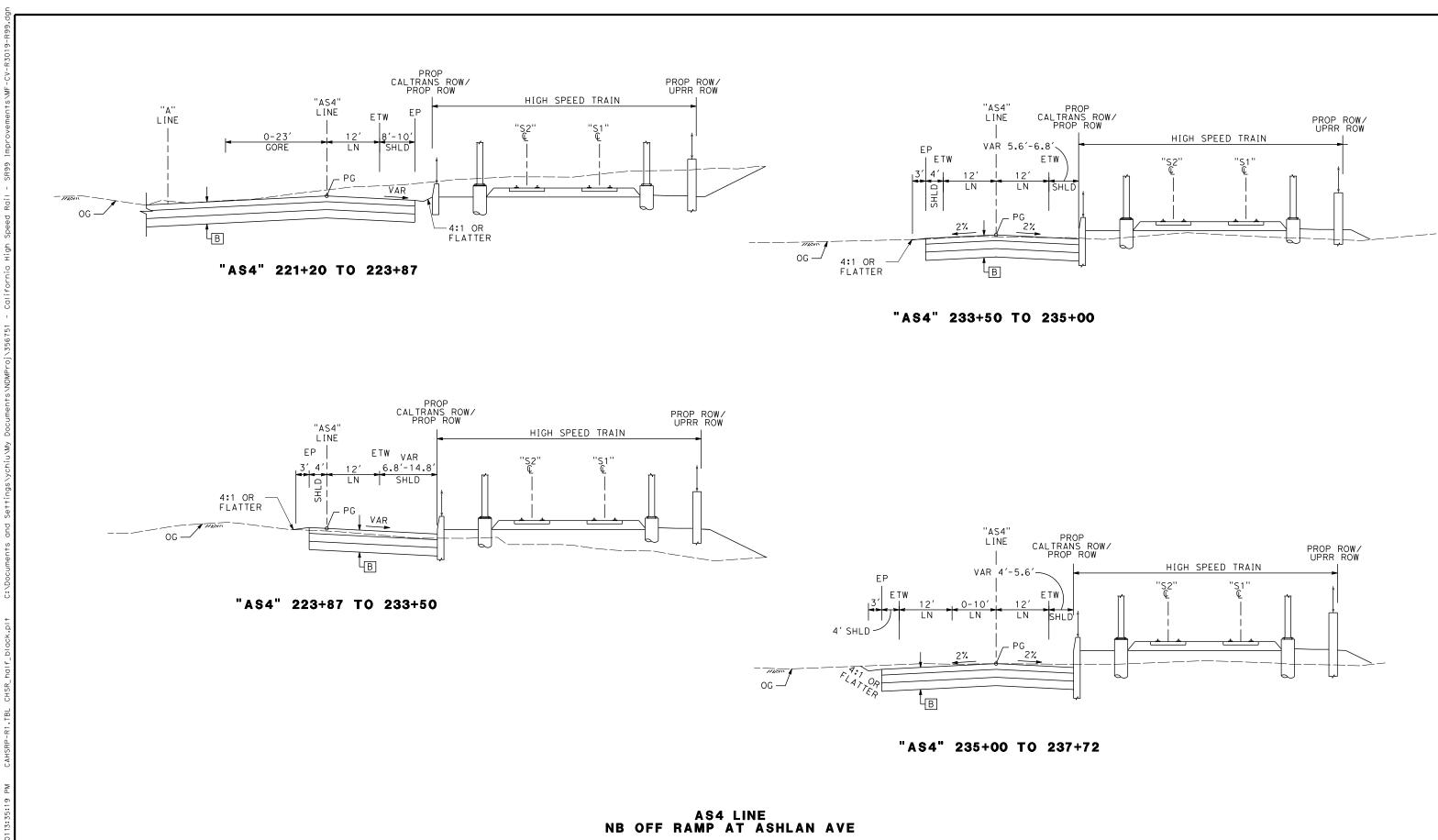
PROPOSED PRELIMINARY DESIGN NOT FOR CONSTRUCTION 2020 L Street, Suite 300 Sacramento, CA 95811 **CH2MHILL**



CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A ROADWAY TYPICAL SECTIONS SR 99 - RAMP "AS1", "AS2", "AS3"

CONTRACT NO.						
DRAWING NO.						
CV-R0018-R99						
SCALE						
AS SHOWN						
SHEET NO.						





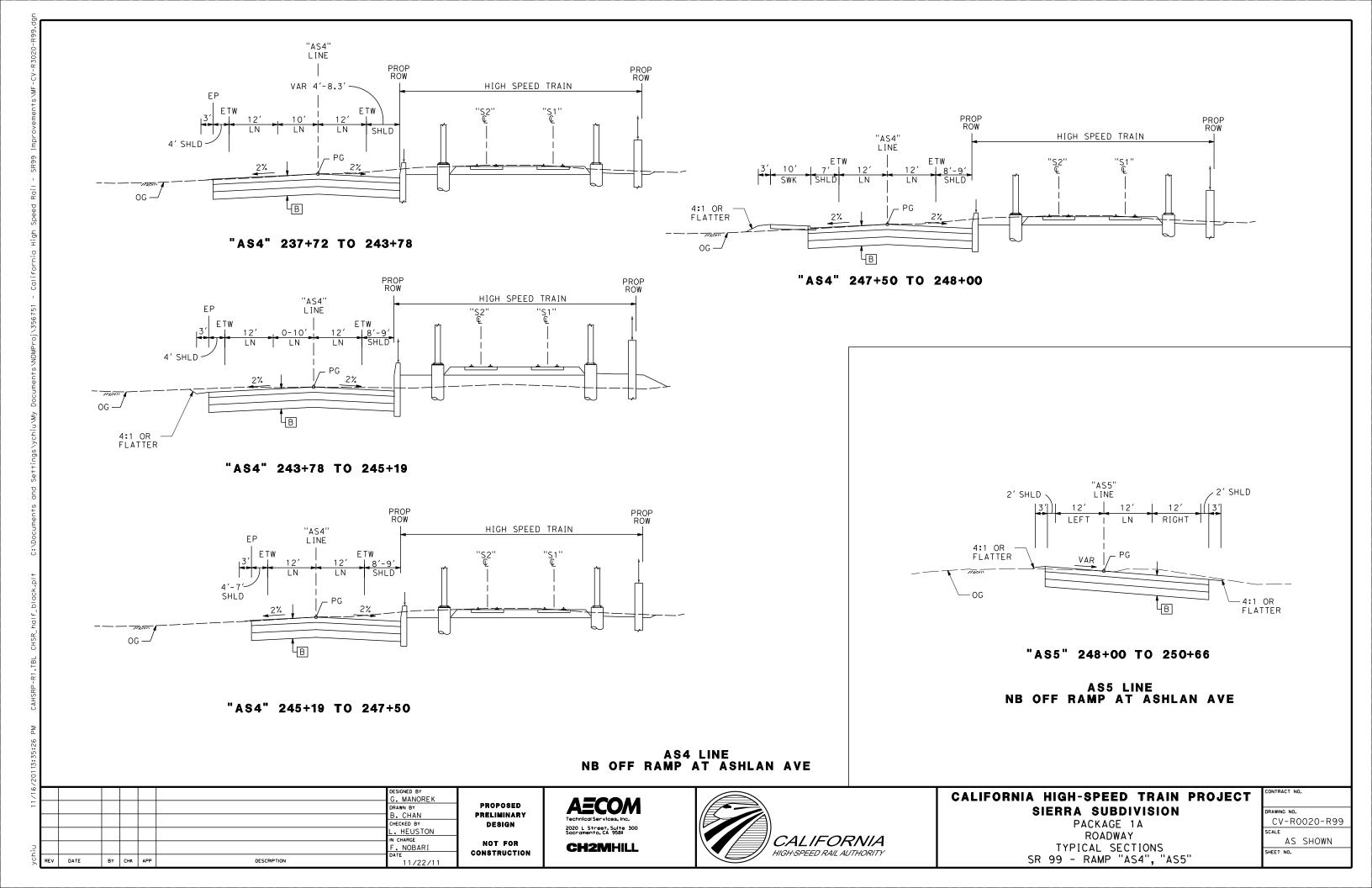
PROPOSED PRELIMINARY DESIGN NOT FOR

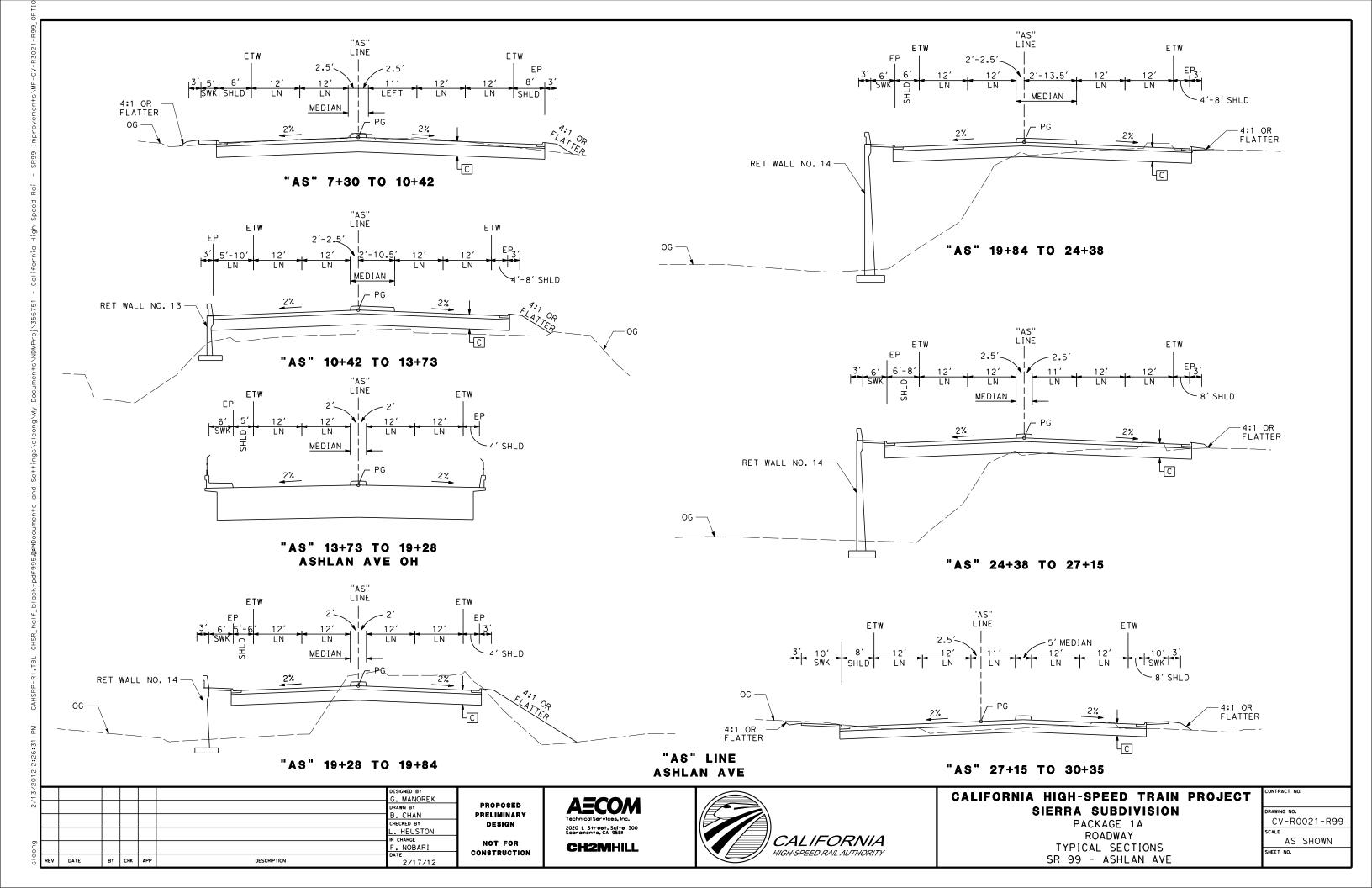
2020 L Street, Suite 300 Sacramento, CA 958II **CH2MHILL**

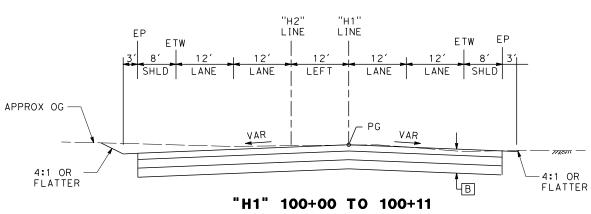


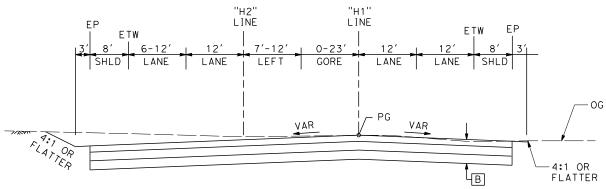
CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A ROADWAY TYPICAL SECTIONS SR 99 - RAMP "AS4" CV-R0019-R99 AS SHOWN

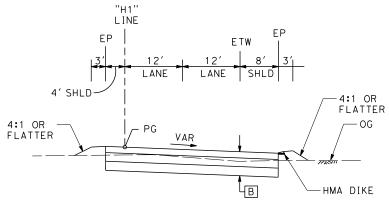






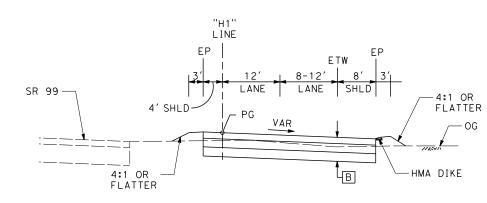


"H1" 100+11 TO 101+89

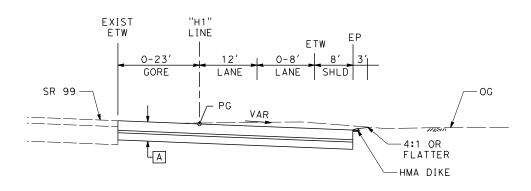


"H1" 101+89 TO 115+27

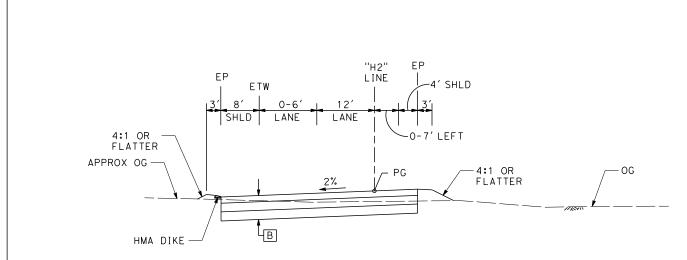
H1 LINE NB ON RAMP AT HERNDON AVE



"H1" 115+27 TO 117+08



"H1" 117+08 TO 124+27



"H2" 101+95 TO 104+51

H2 LINE SB OFF RAMP AT HERNDON AVE

						DESIGNED BY G. MANORE K DRAWN BY B. CHAN CHECKED BY	P
REV	DATE	BY	СНК	APP	DESCRIPTION	L. HEUSTON IN CHARGE F. NOBARI DATE 11/22/11	C

PROPOSED
PRELIMINARY
DESIGN
NOT FOR
CONSTRUCTION

AECOM
Technical Services, inc.
2020 L Street, Suite 300
Sacramento, CA 9581
CH2MHILL



CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A ROADWAY TYPICAL SECTIONS SR 99 - RAMP "H1", "H2" DRAWING NO.

CV-R0022-R99

SCALE

AS SHOWN

SHEET NO.

PROPOSED PRELIMINARY DESIGN NOT FOR CONSTRUCTION

G. MANOREK

. HEUSTON

N CHARGE F. NOBARI

DRAWN BY
B. CHAN
CHECKED BY

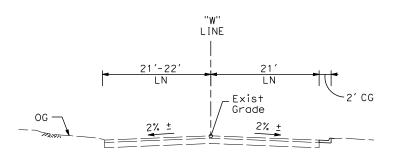
AECOM 2020 L Street, Suite 300 Sacramento, CA 958II **CH2MHILL**



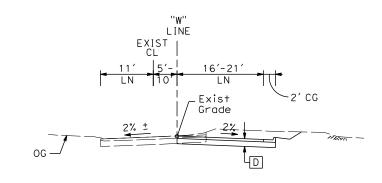
CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A ROADWAY TYPICAL SECTIONS SR 99 - LOCAL ROAD "W"

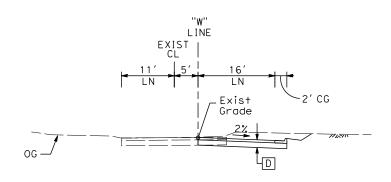
RAWING NO. CV-R0023-R99 AS SHOWN



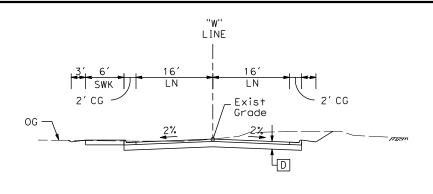
"W" 11+63 TO 12+40



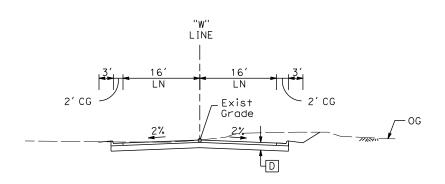
"W" 12+40 TO 14+65



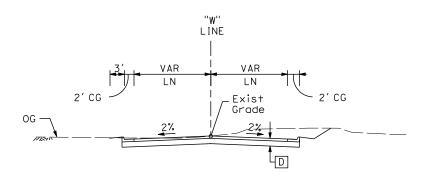
"W" 14+65 TO 16+78



"W" 16+78 TO 17+46

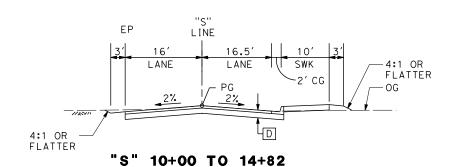


"W" 17+46 TO 19+14

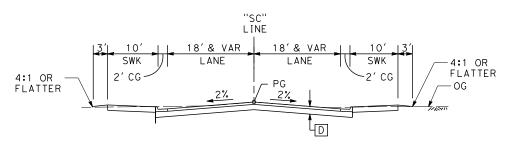


"W" 19+14 TO 19+75

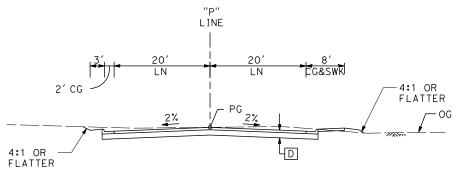
"W"LINE WOODSON AVE



S LINE Shields ave

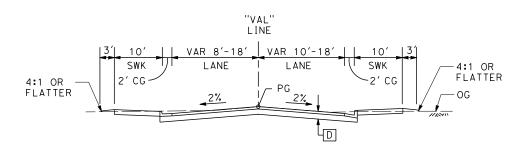


"SC" 10+00 TO 11+66 SC LINE

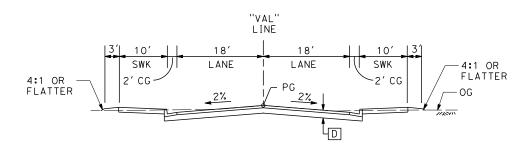


"P" 11+00 TO 20+30

P LINE Pleasant ave

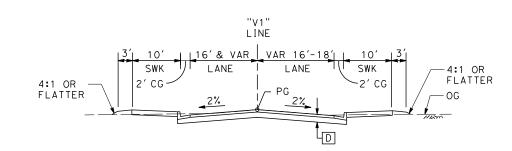


"VAL" 20+36 TO 22+51 "VAL" 31+17 TO 34+16



"VAL" 22+51 TO 31+17

VAL LINE VALENTINE AVE



"V1" 10+00 TO 14+47

V1 LINE NORTH PARKWAY DR

						DESIGNED BY G. MANORE K DRAWN BY B. CHAN CHECKED BY	P
REV	DATE	BY	СНК	APP	DESCRIPTION	L. HEUSTON IN CHARGE F. NOBARI DATE 11/22/11	C

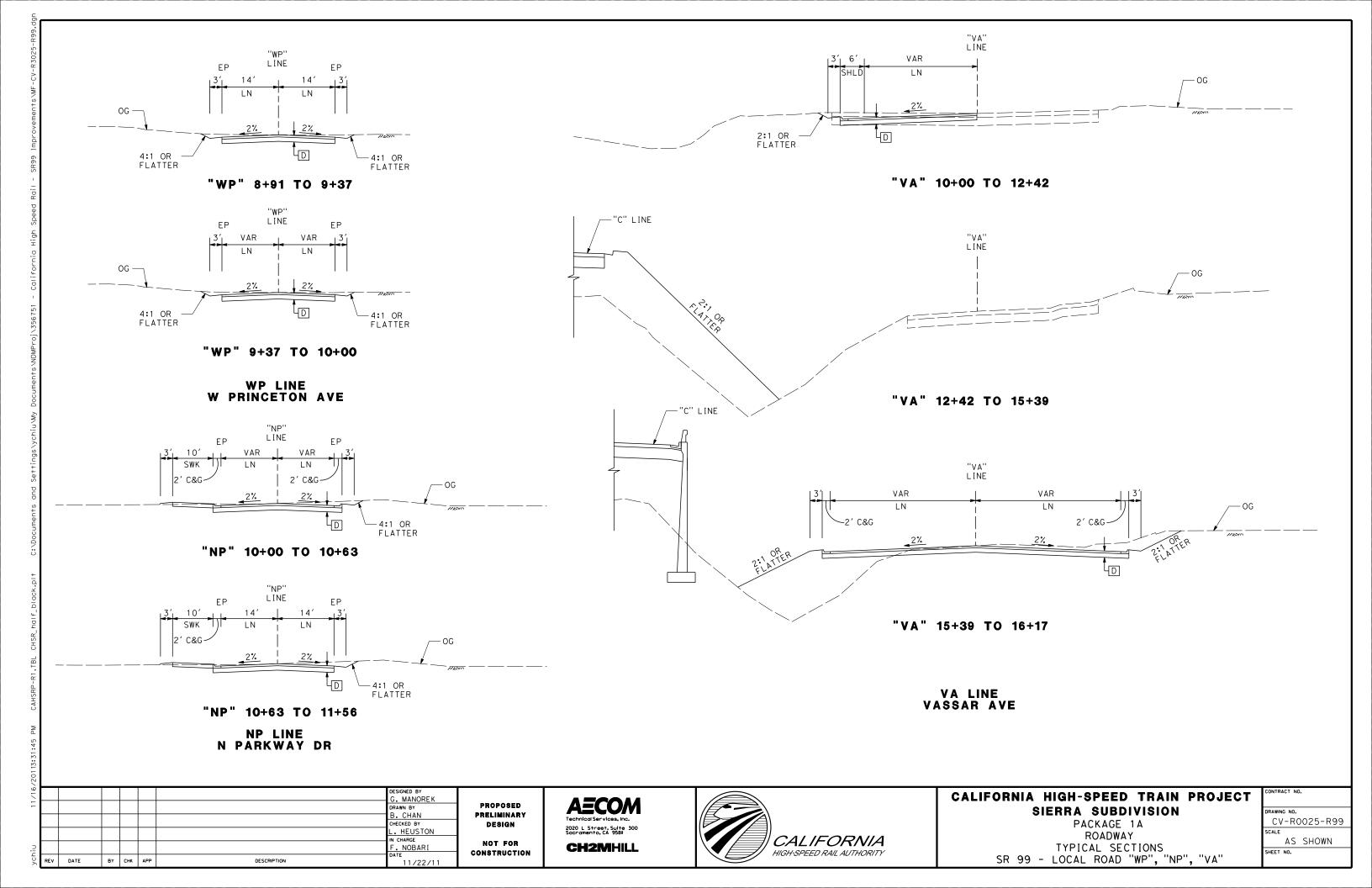
PROPOSED PRELIMINARY DESIGN NOT FOR ONSTRUCTION 2020 L Street, Suite 300 Sacramento, CA 95811 **CH2MHILL**

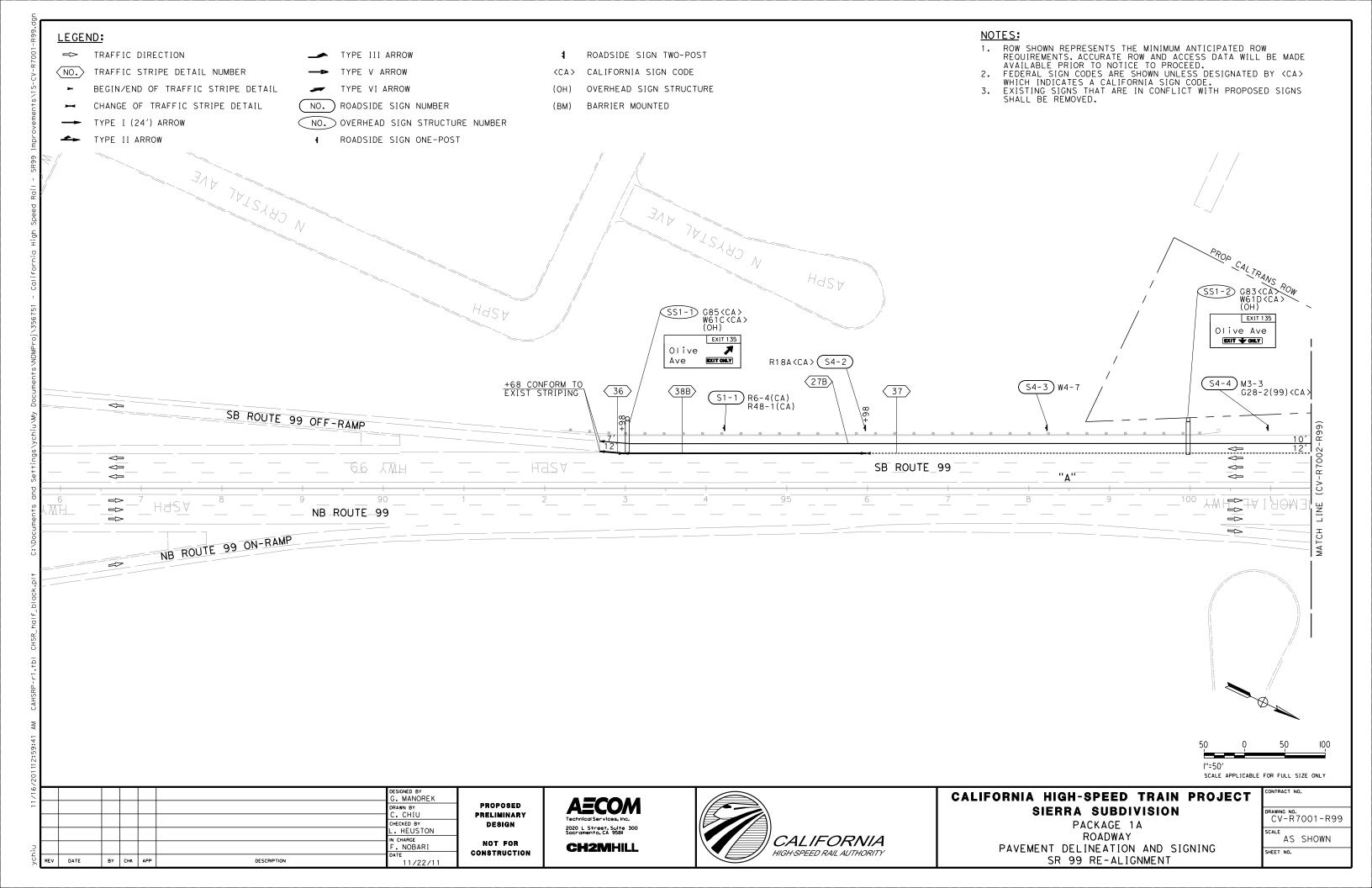


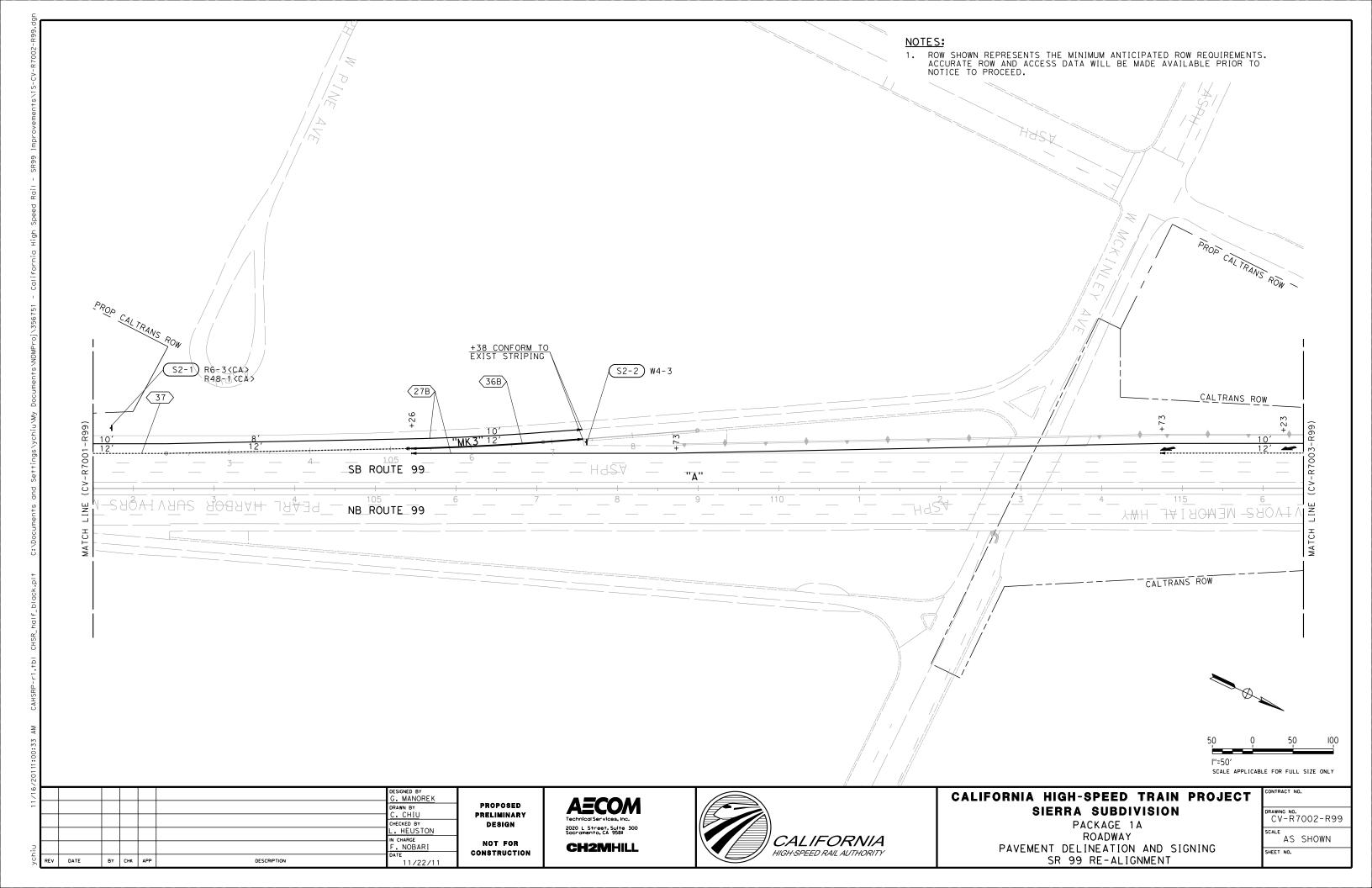
CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

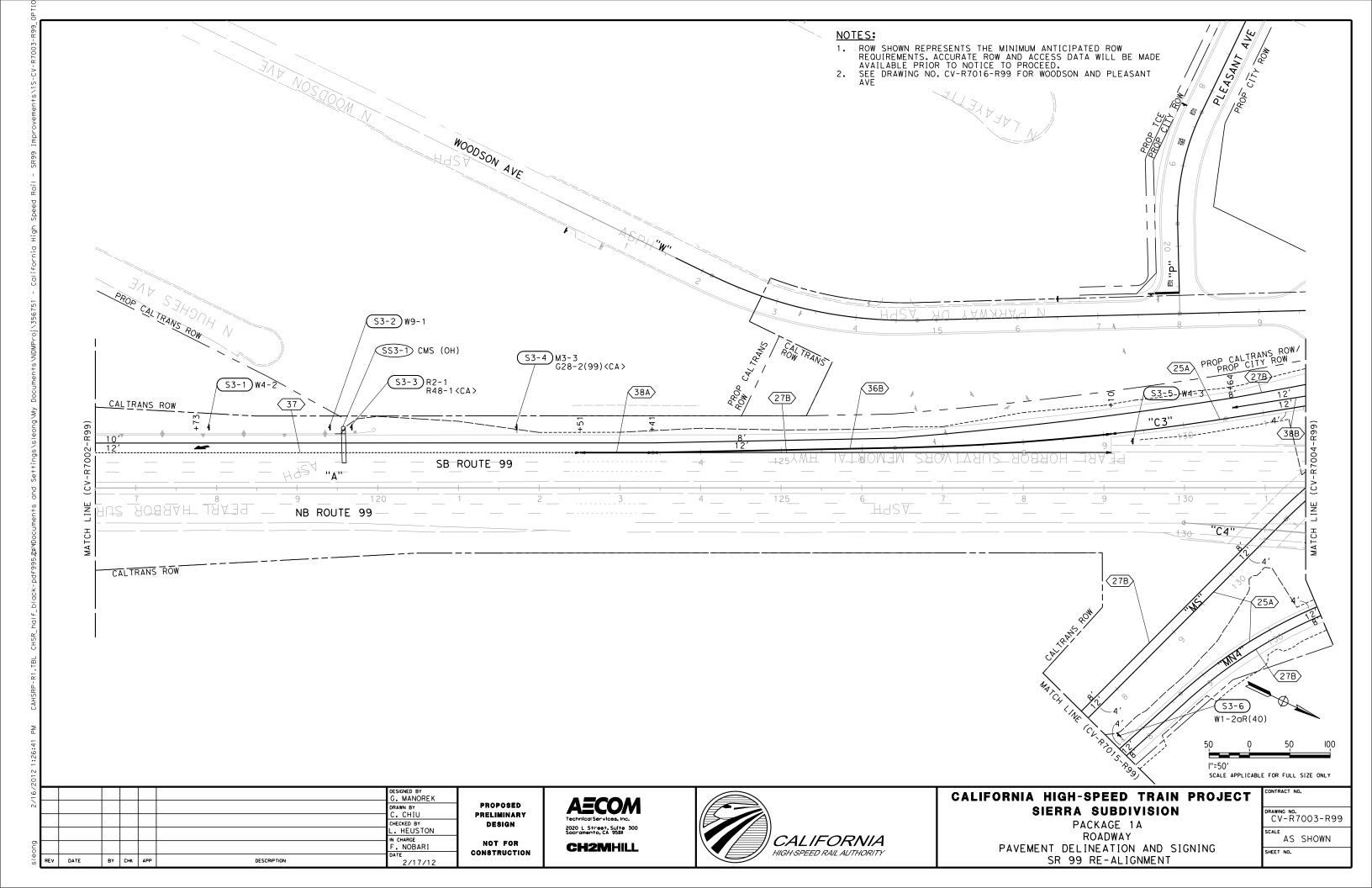
PACKAGE 1A ROADWAY TYPICAL SECTIONS
SR 99 - LOCAL ROAD "S", "SC", "P", "VAL", "V1"

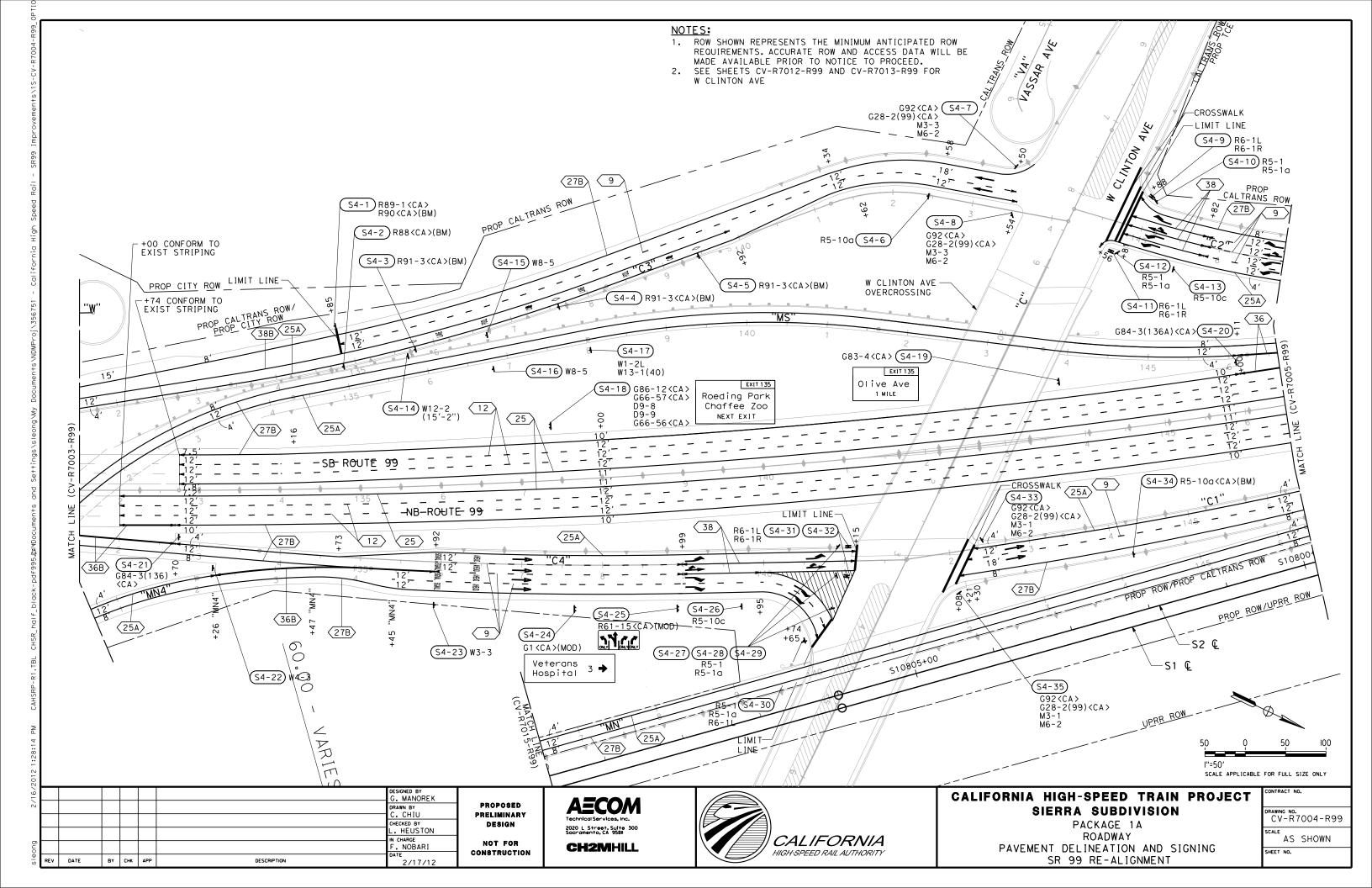
CONTRACT N	0.					
DRAWING NO.						
CV-R0024-R99						
SCALE						
AS	SHOWN					
SHEET NO.						

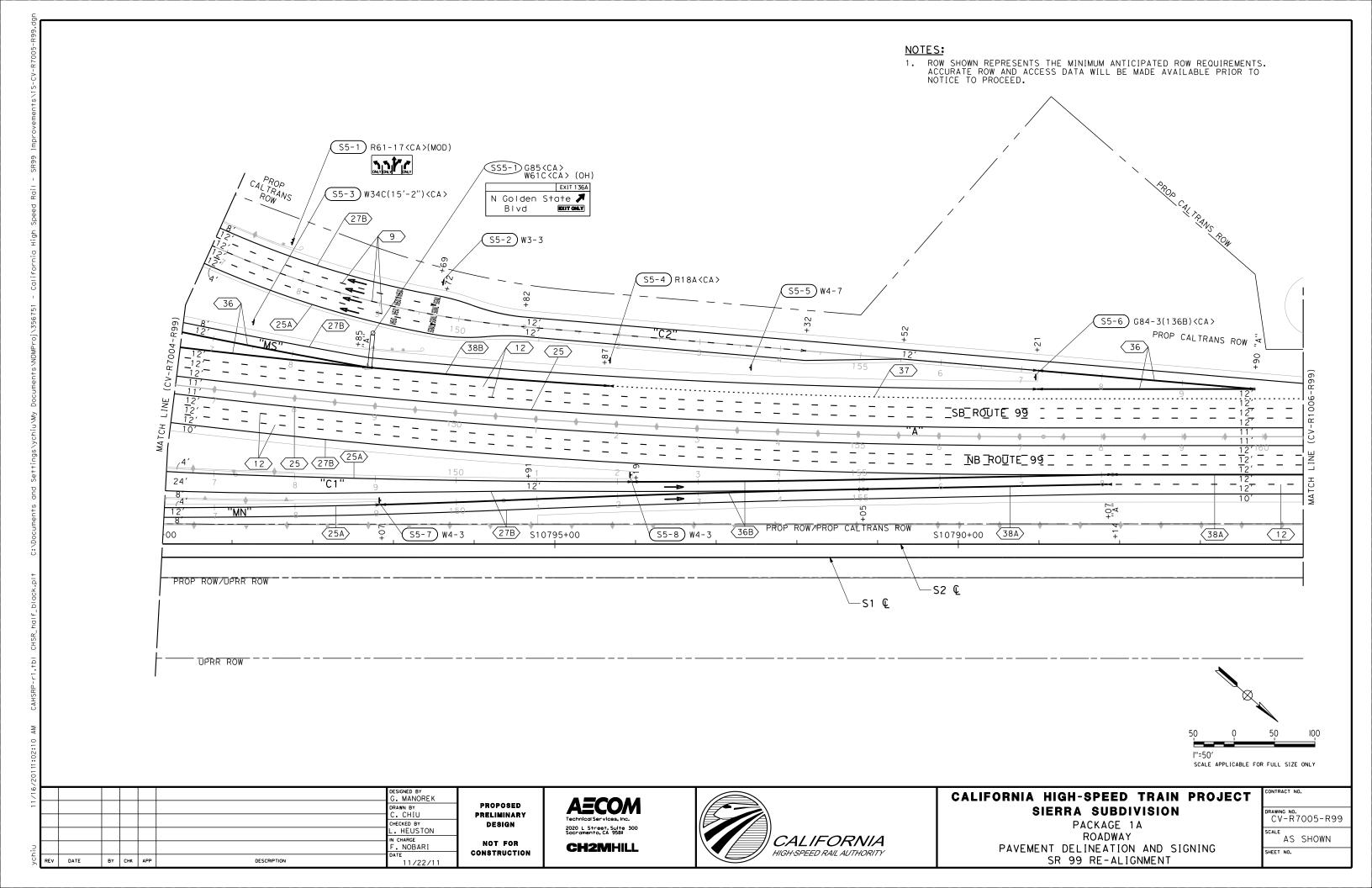


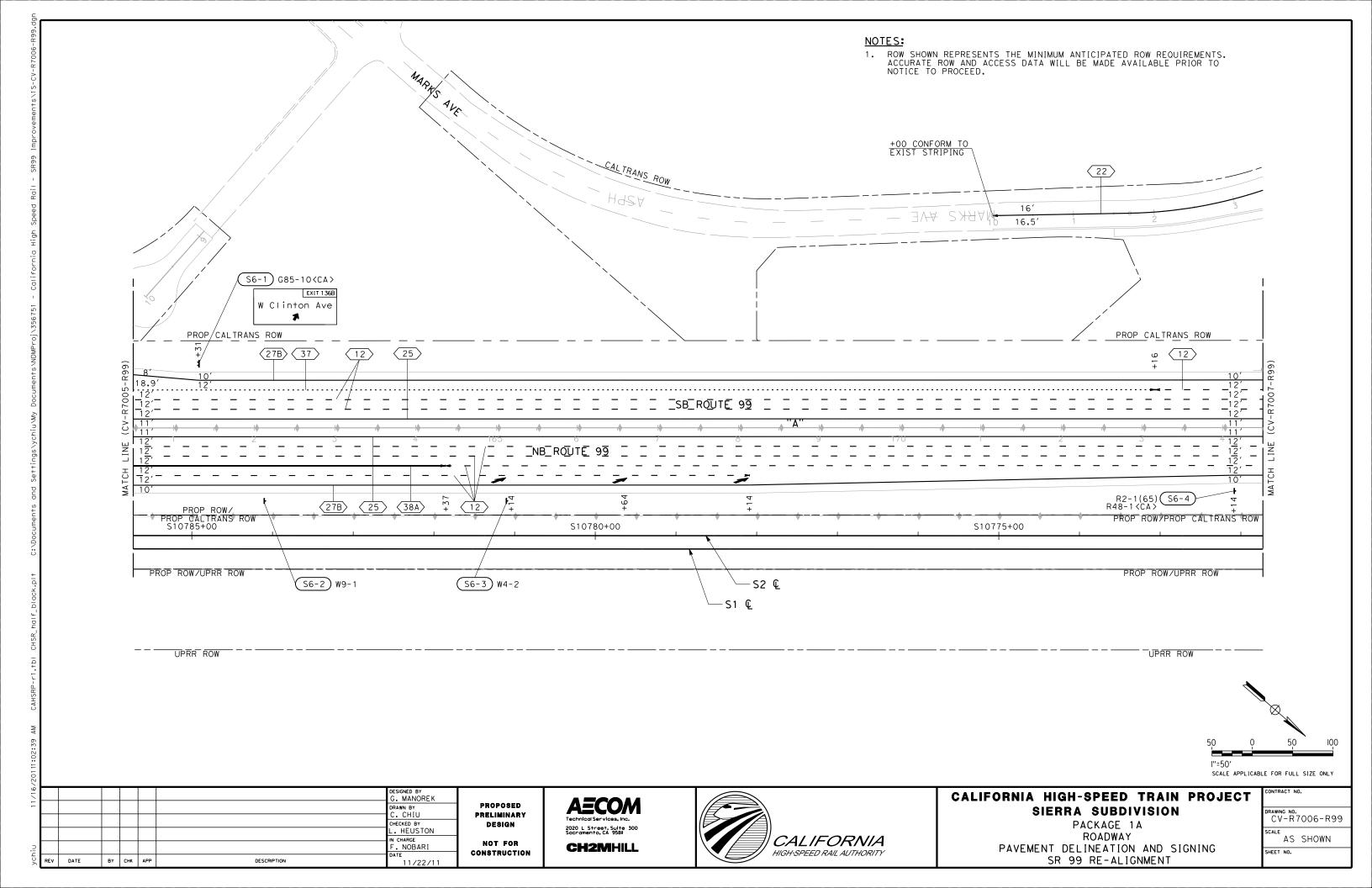


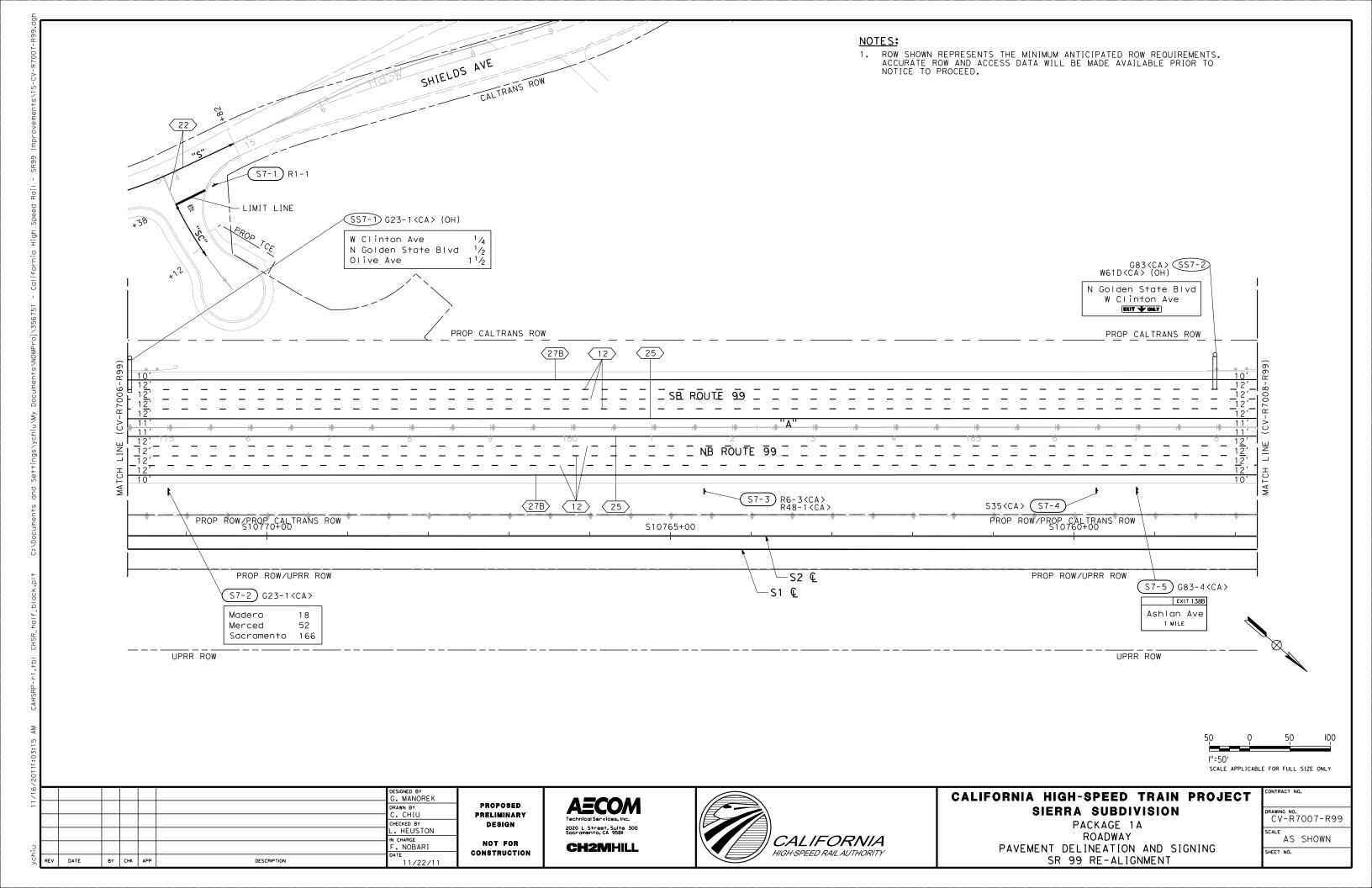


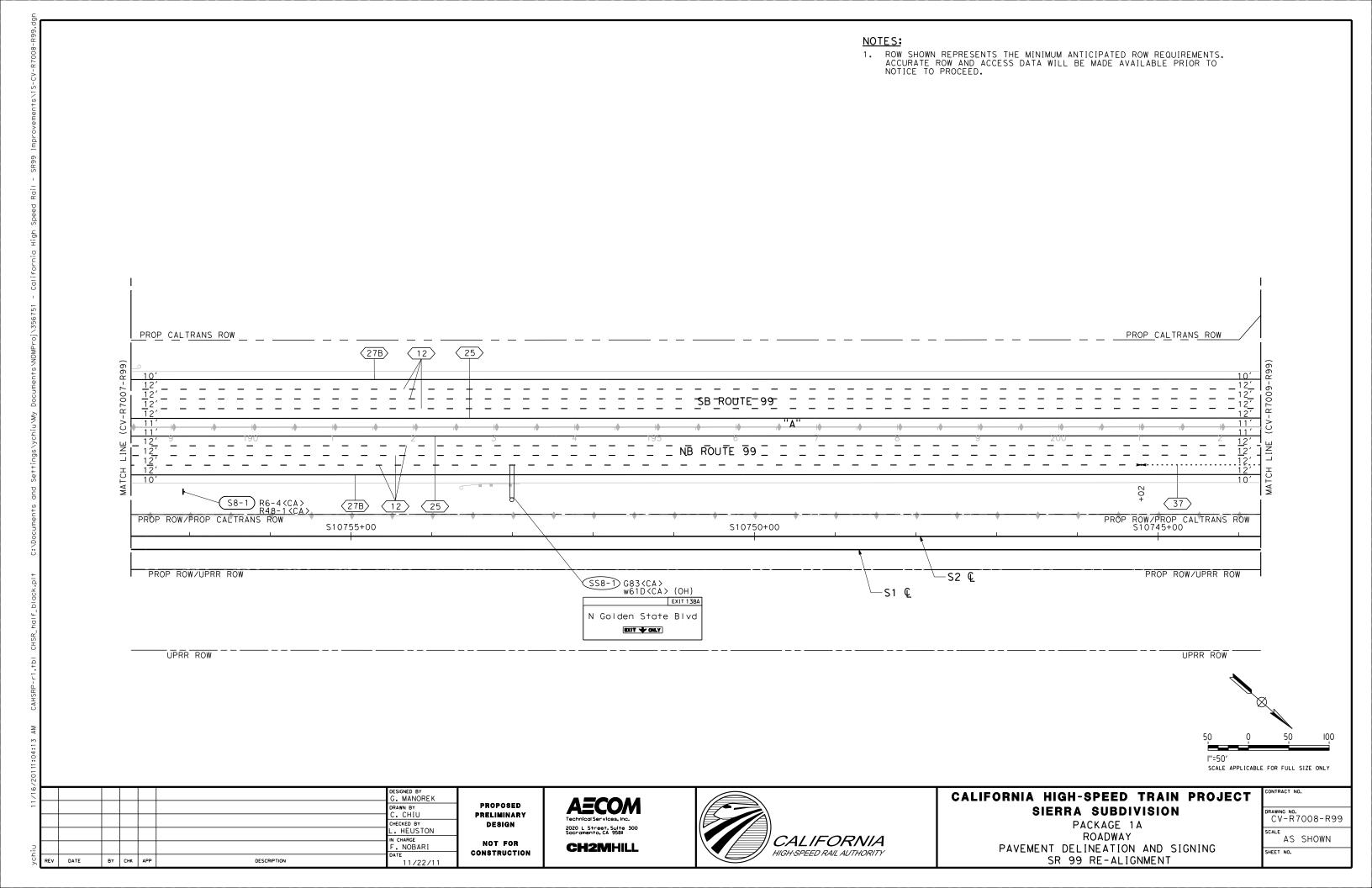


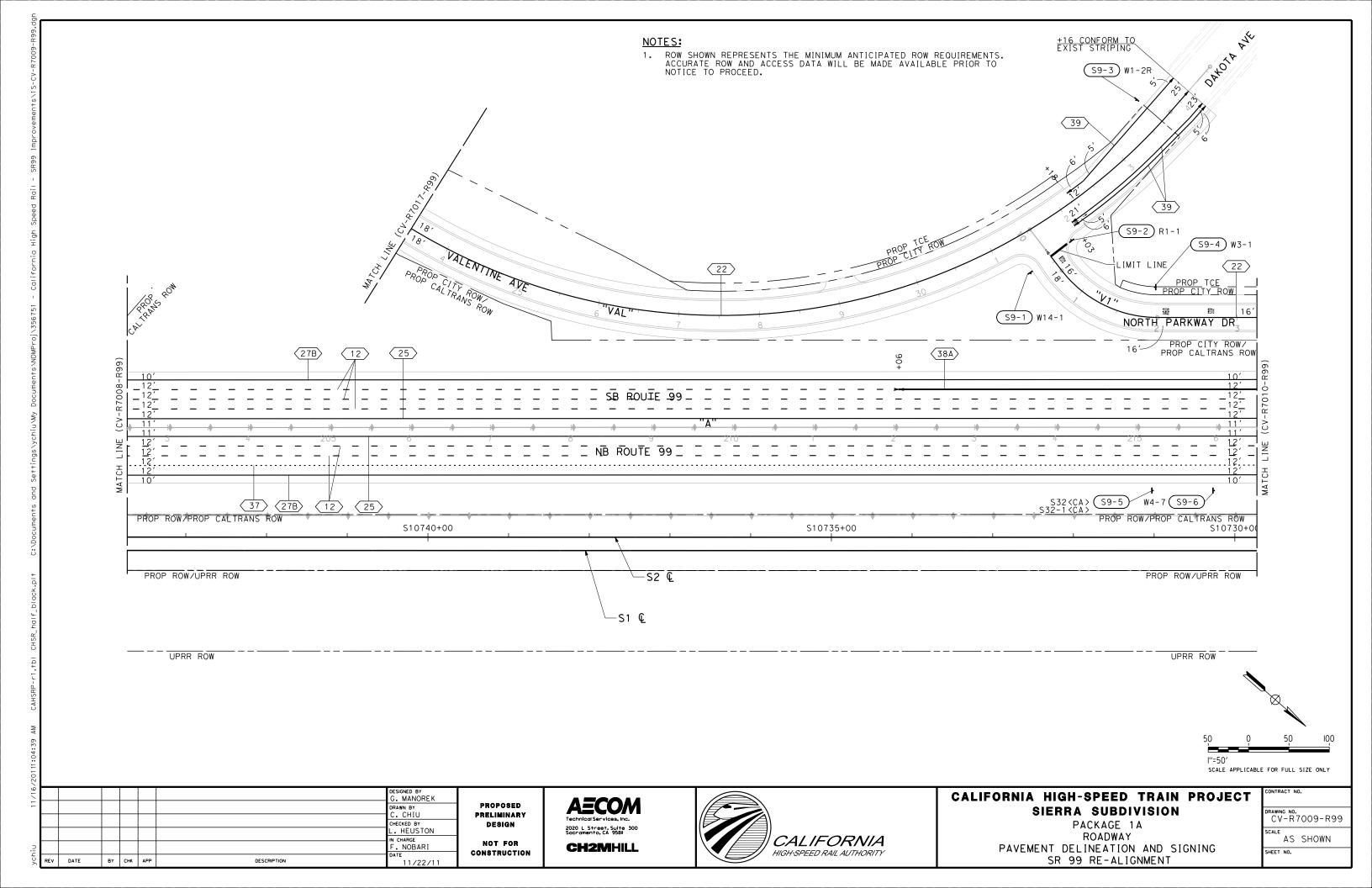


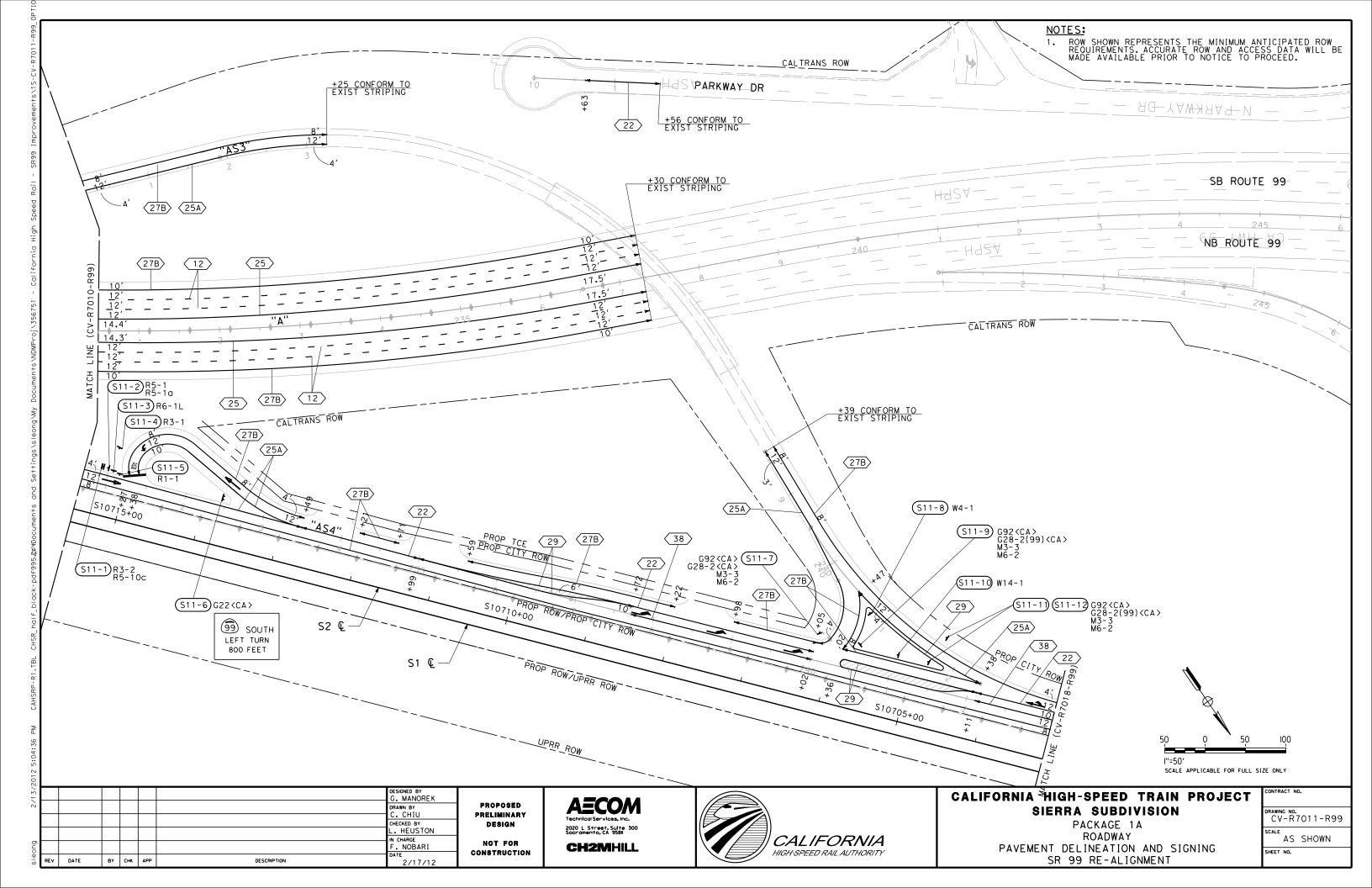


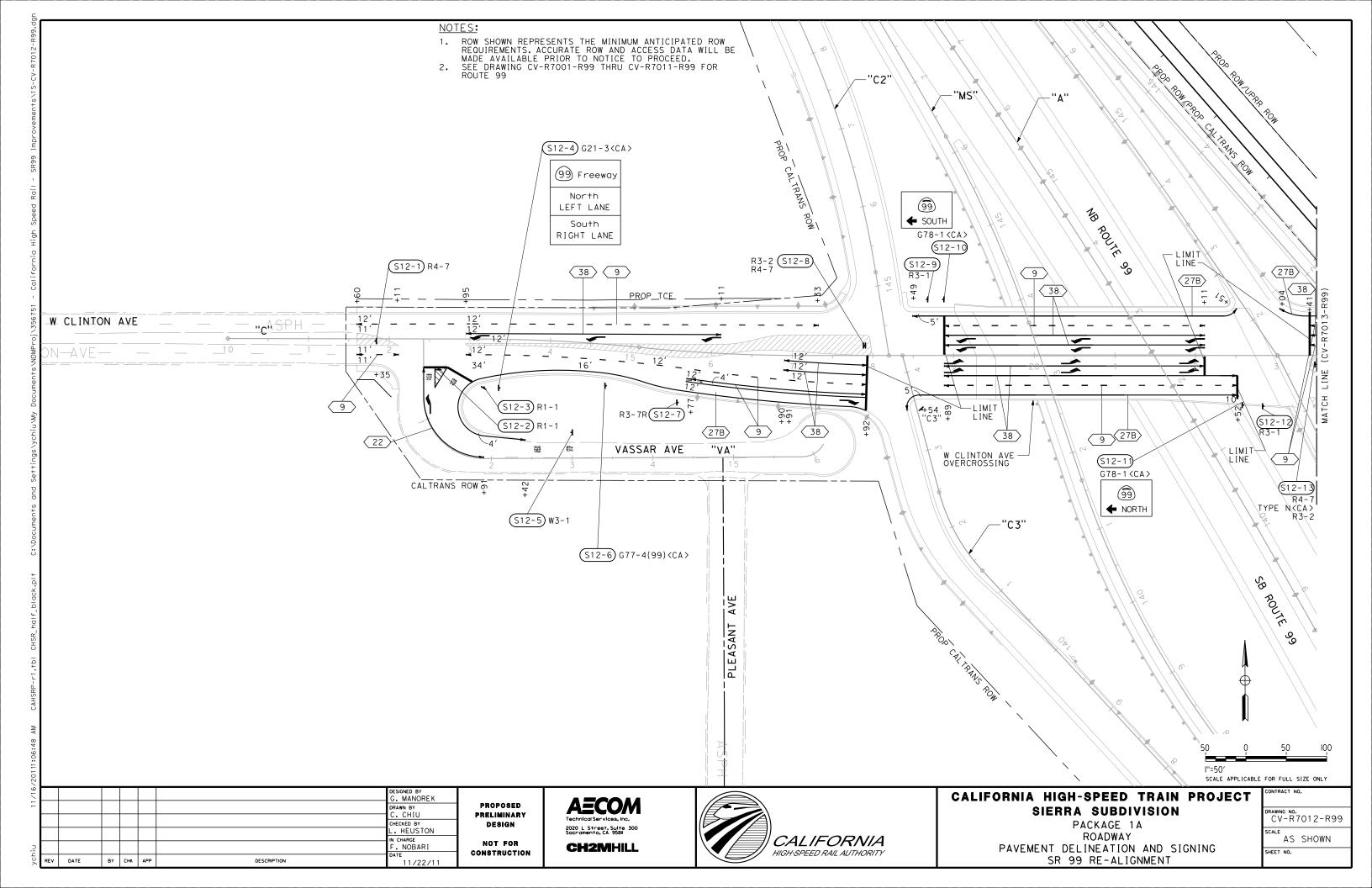


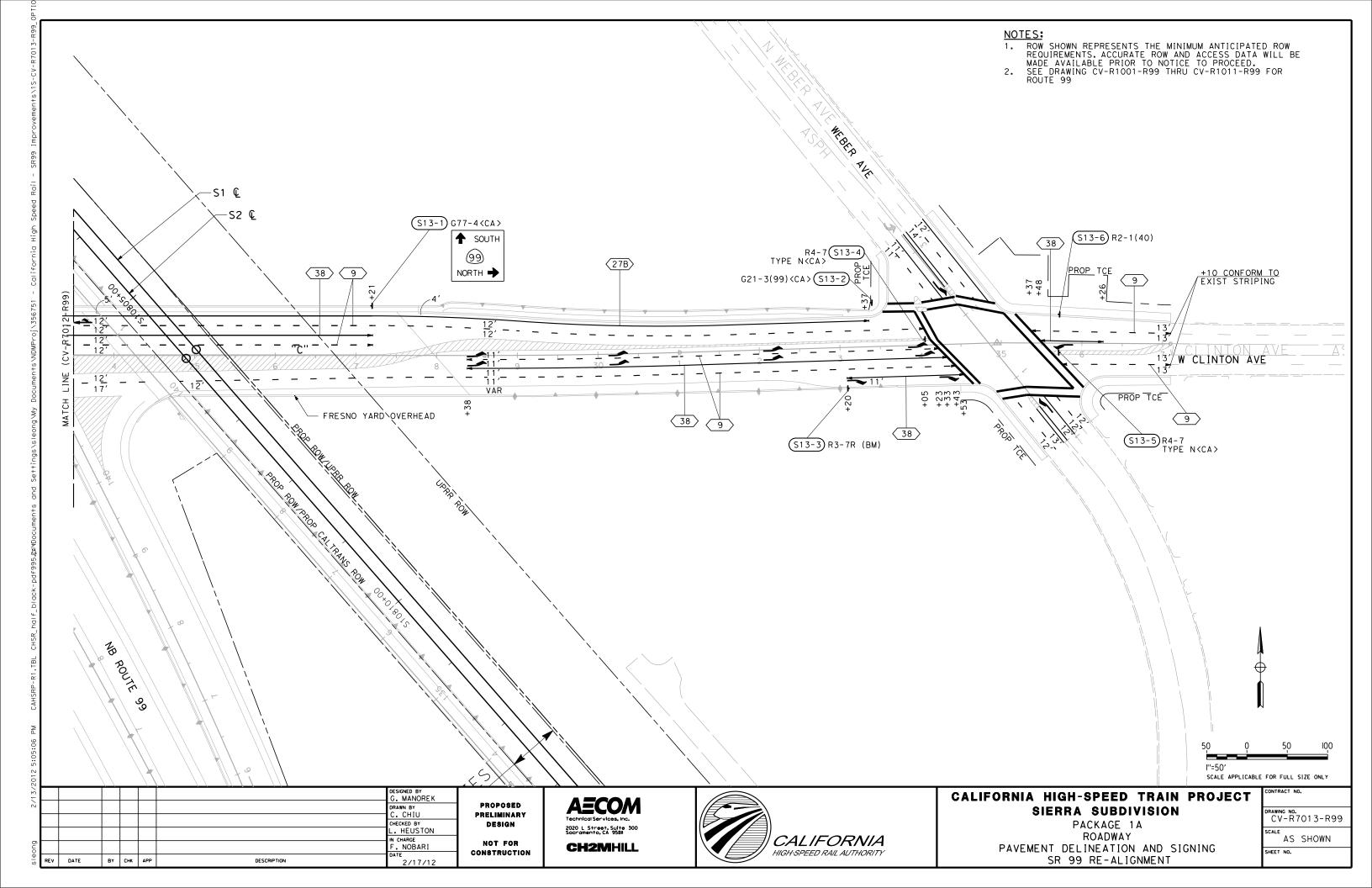


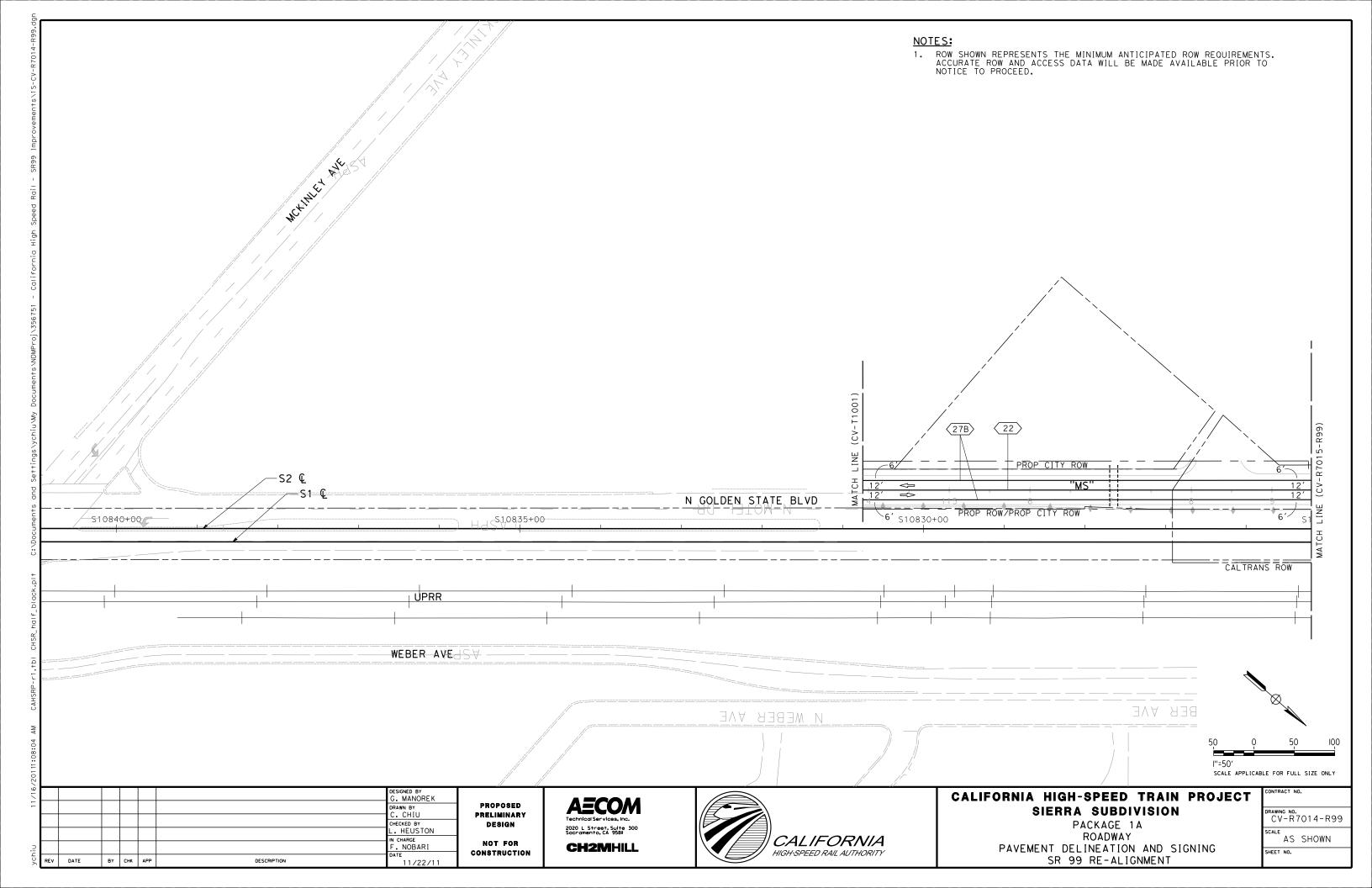


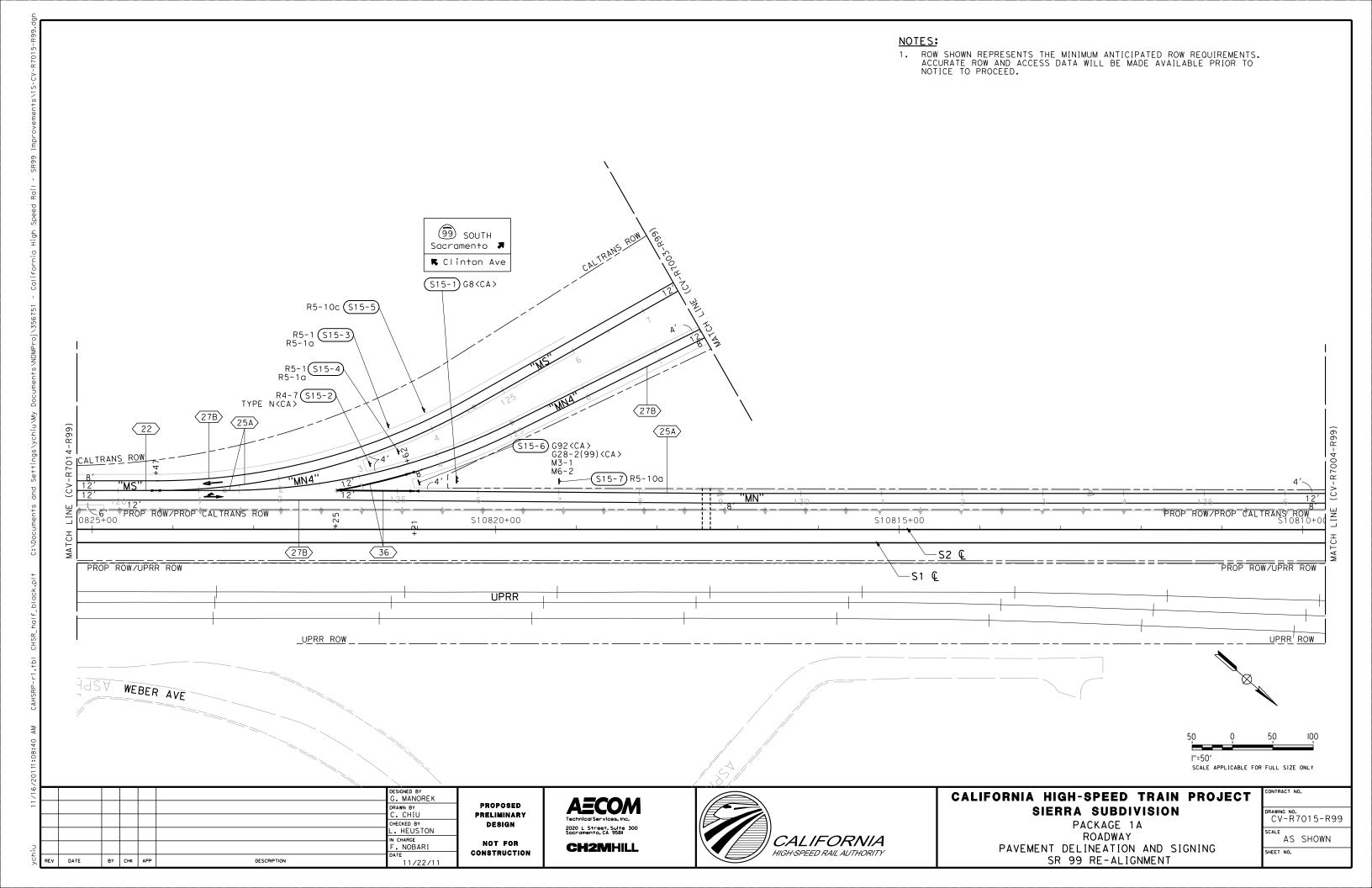


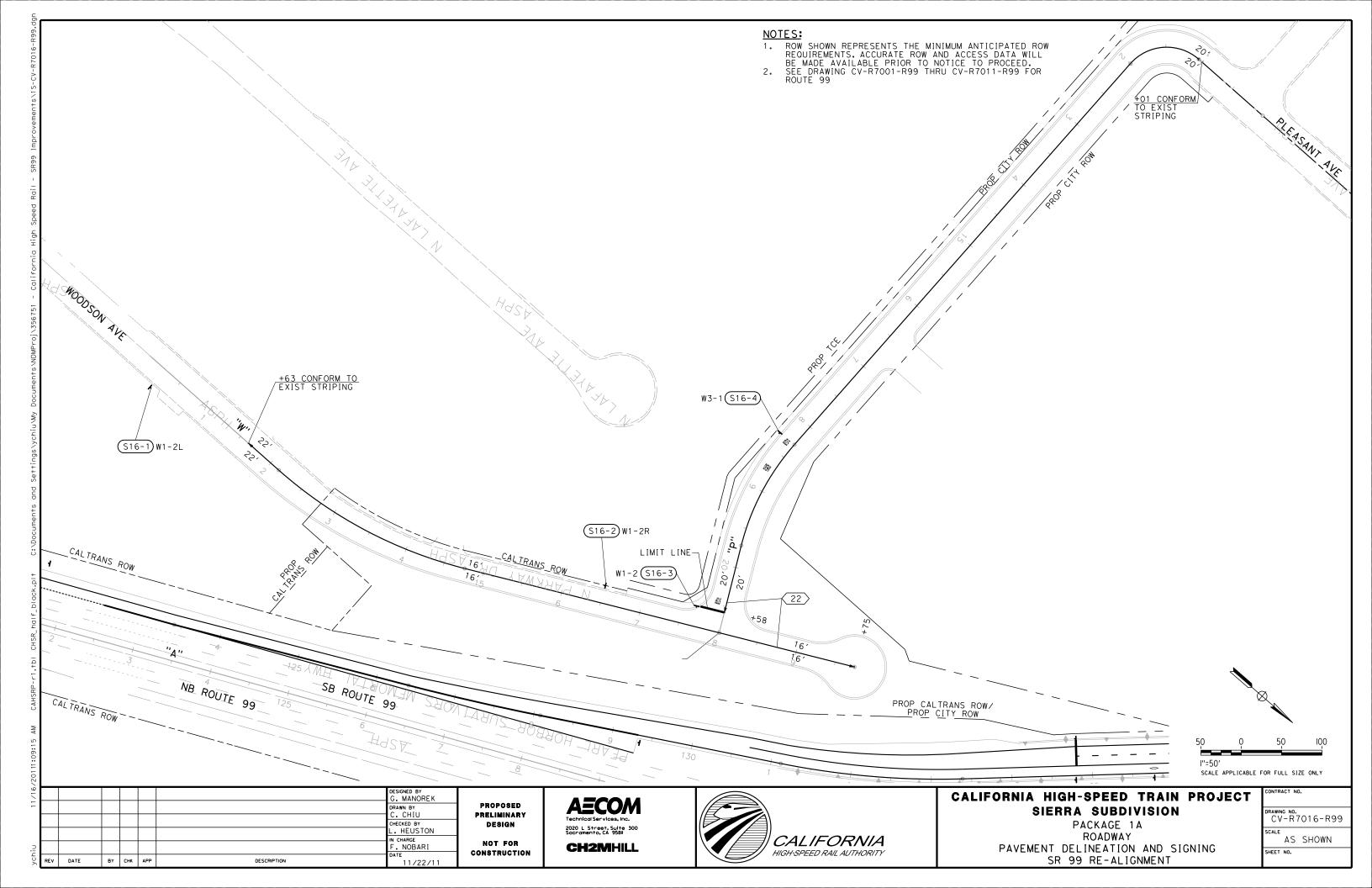


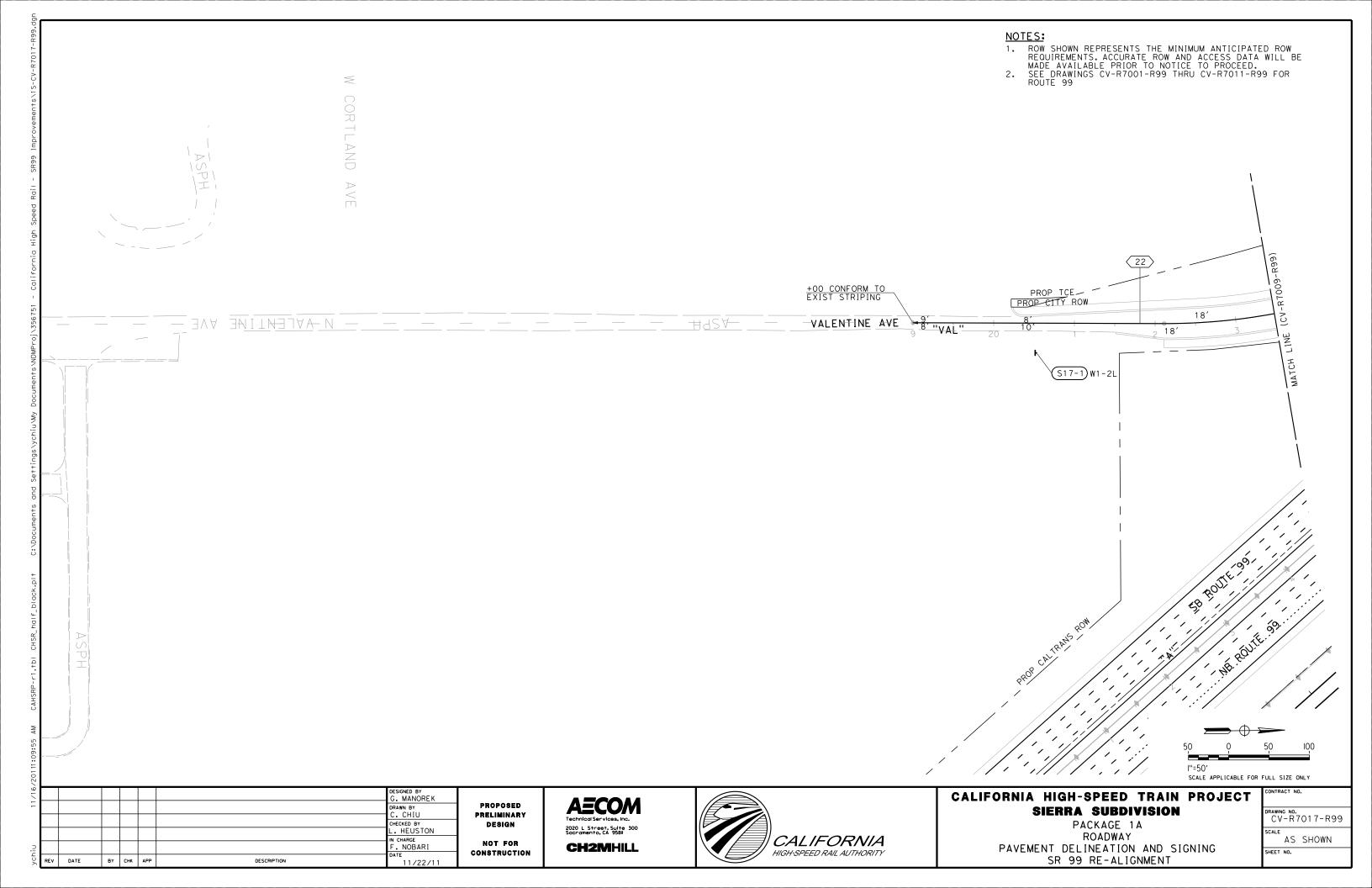


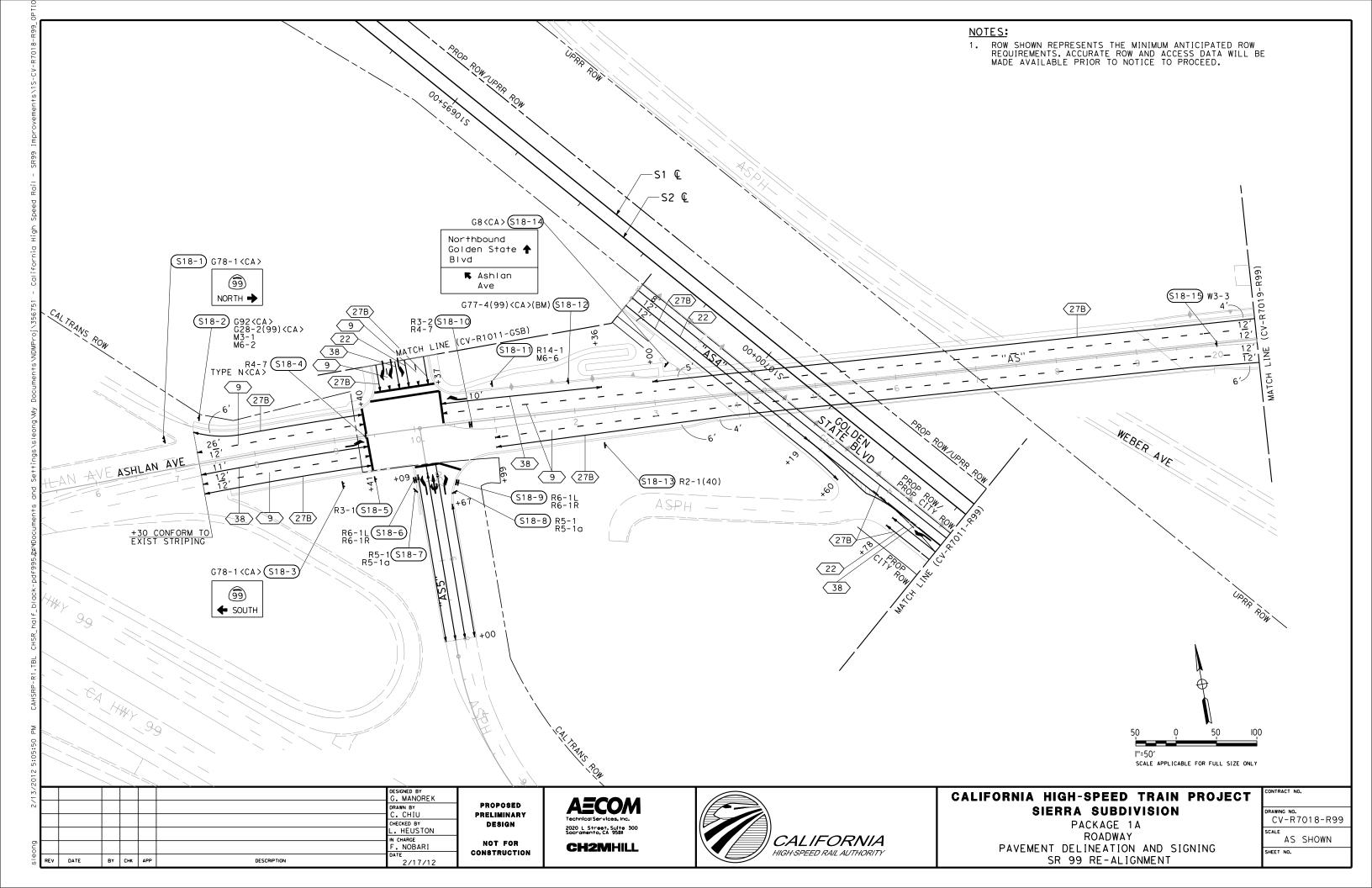


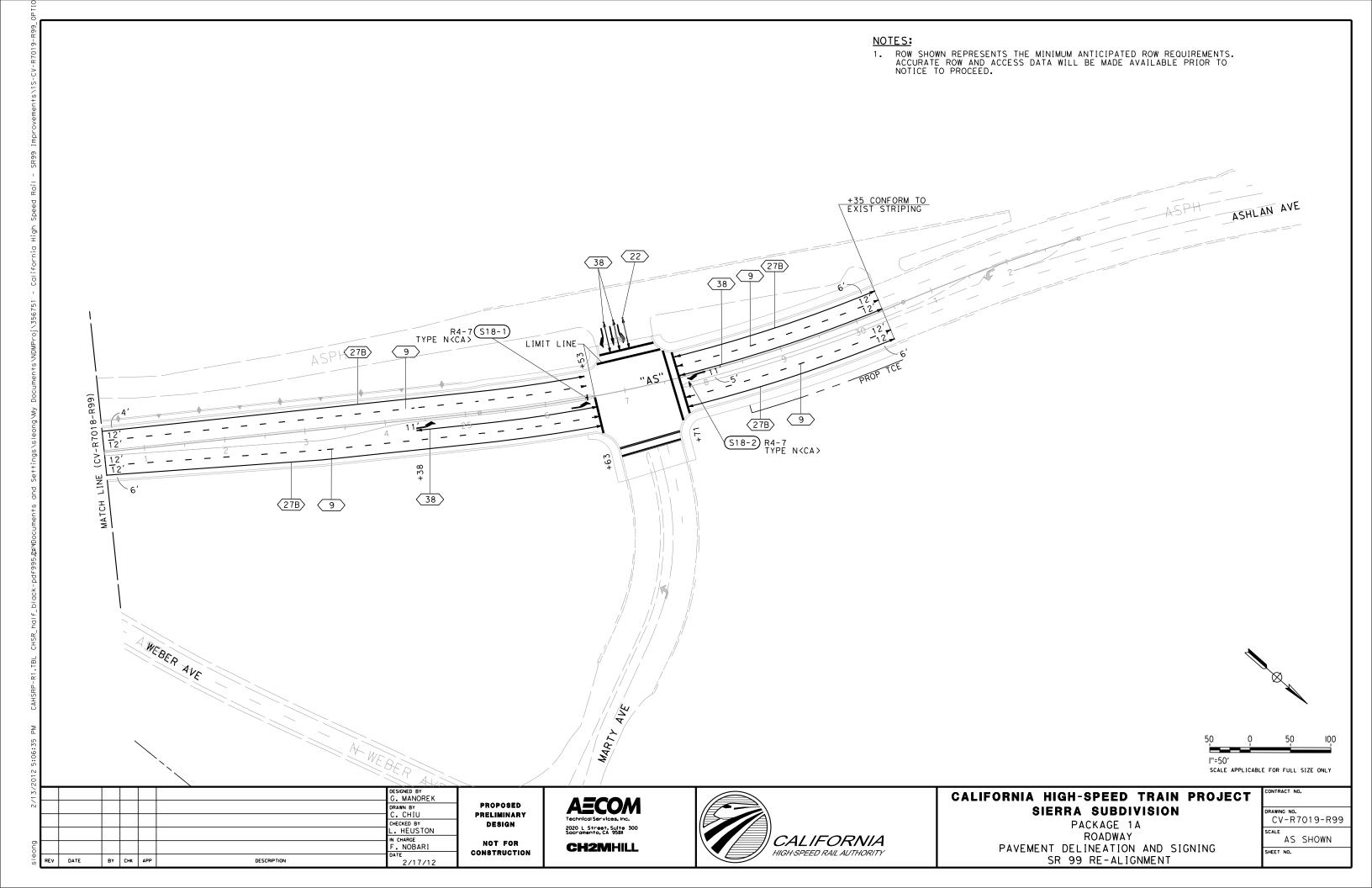


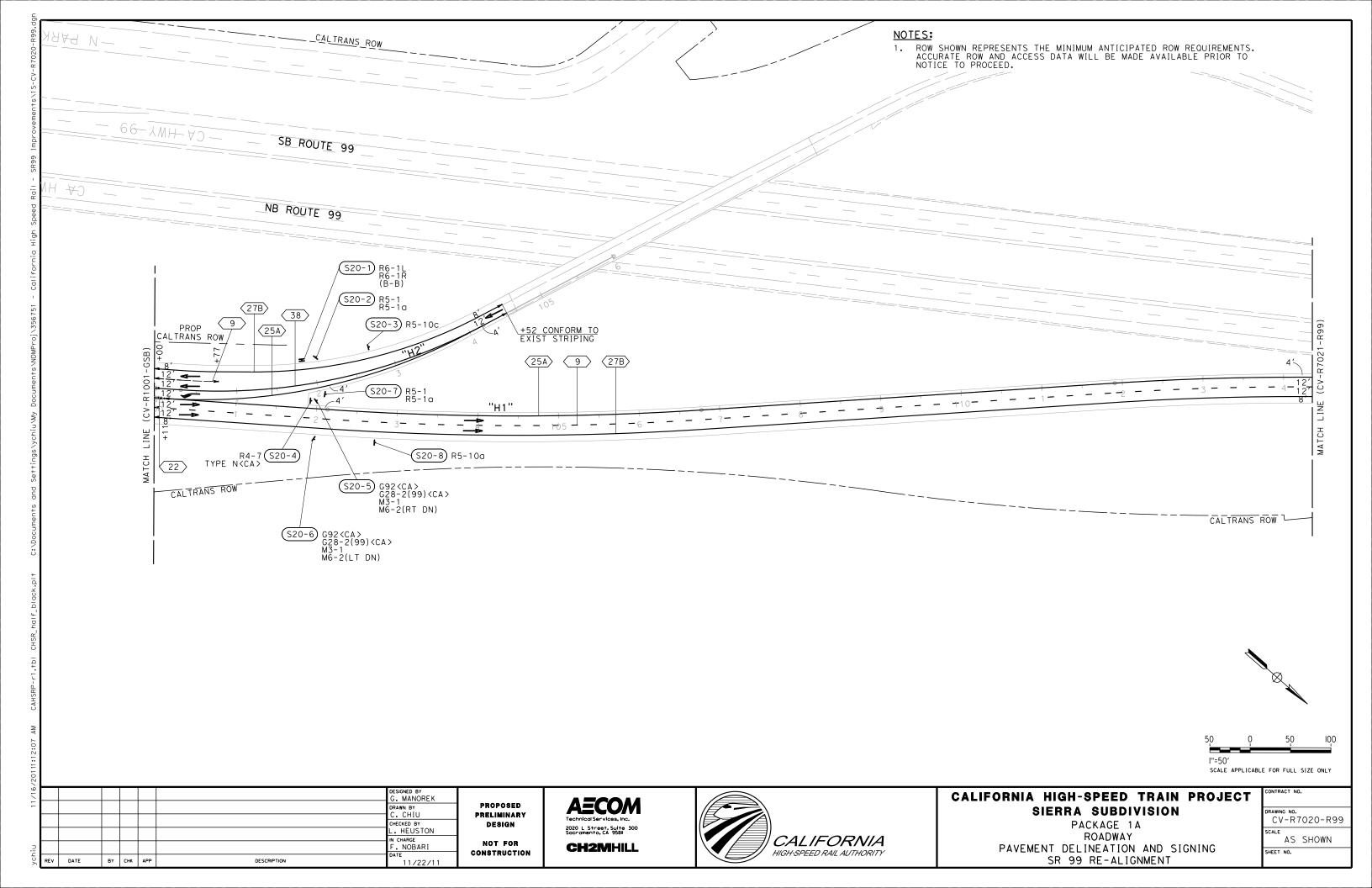


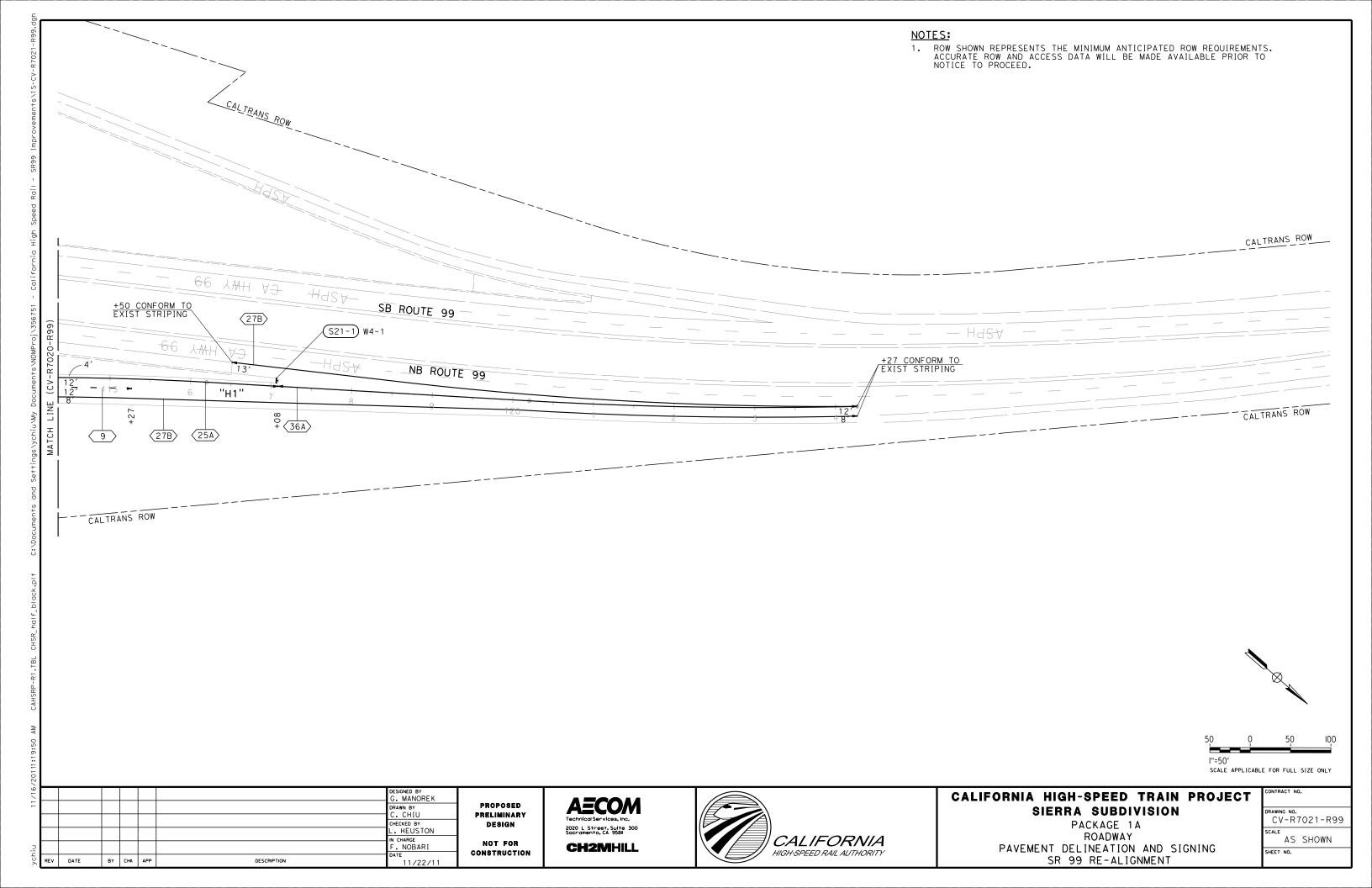


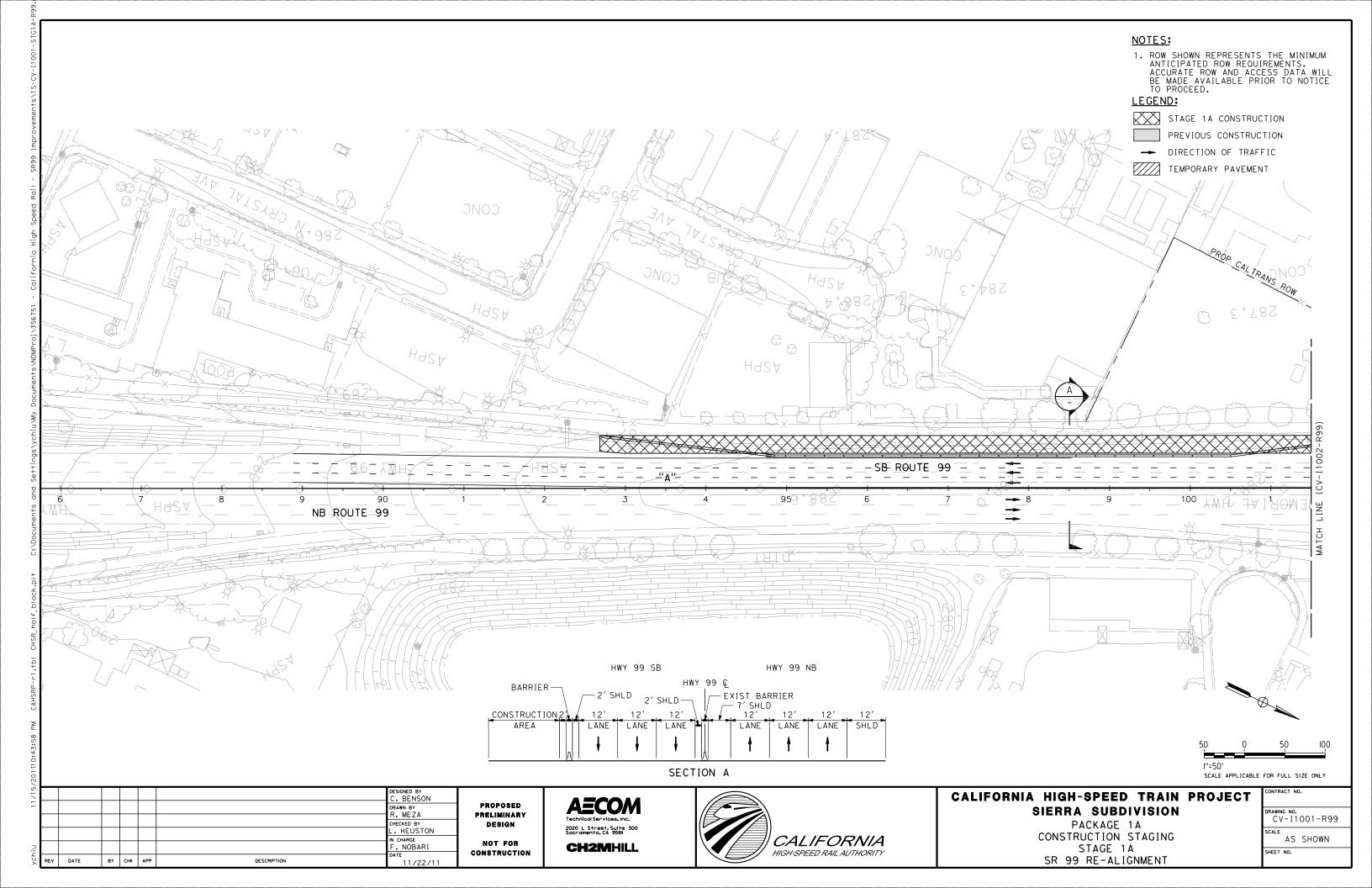


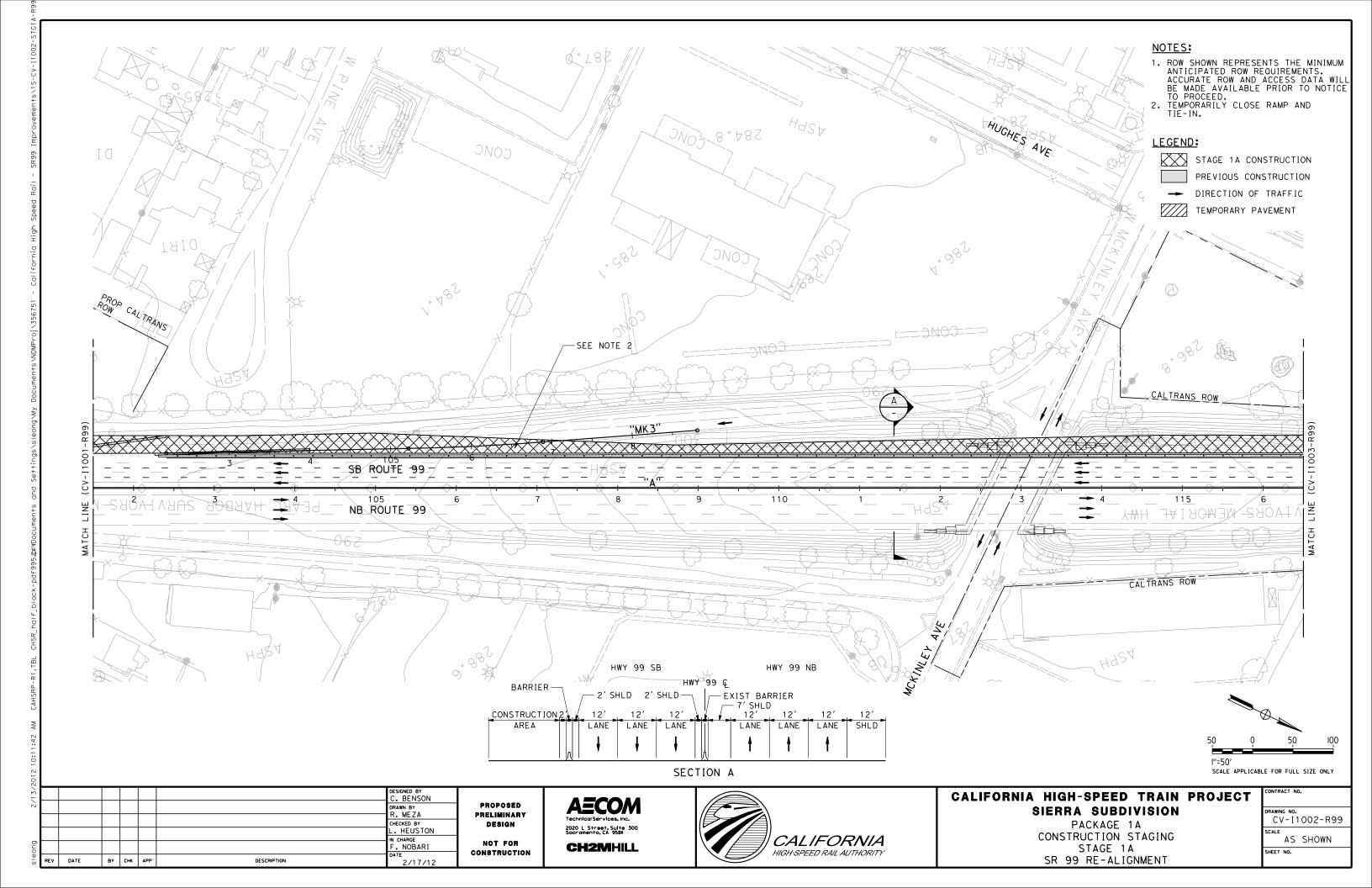


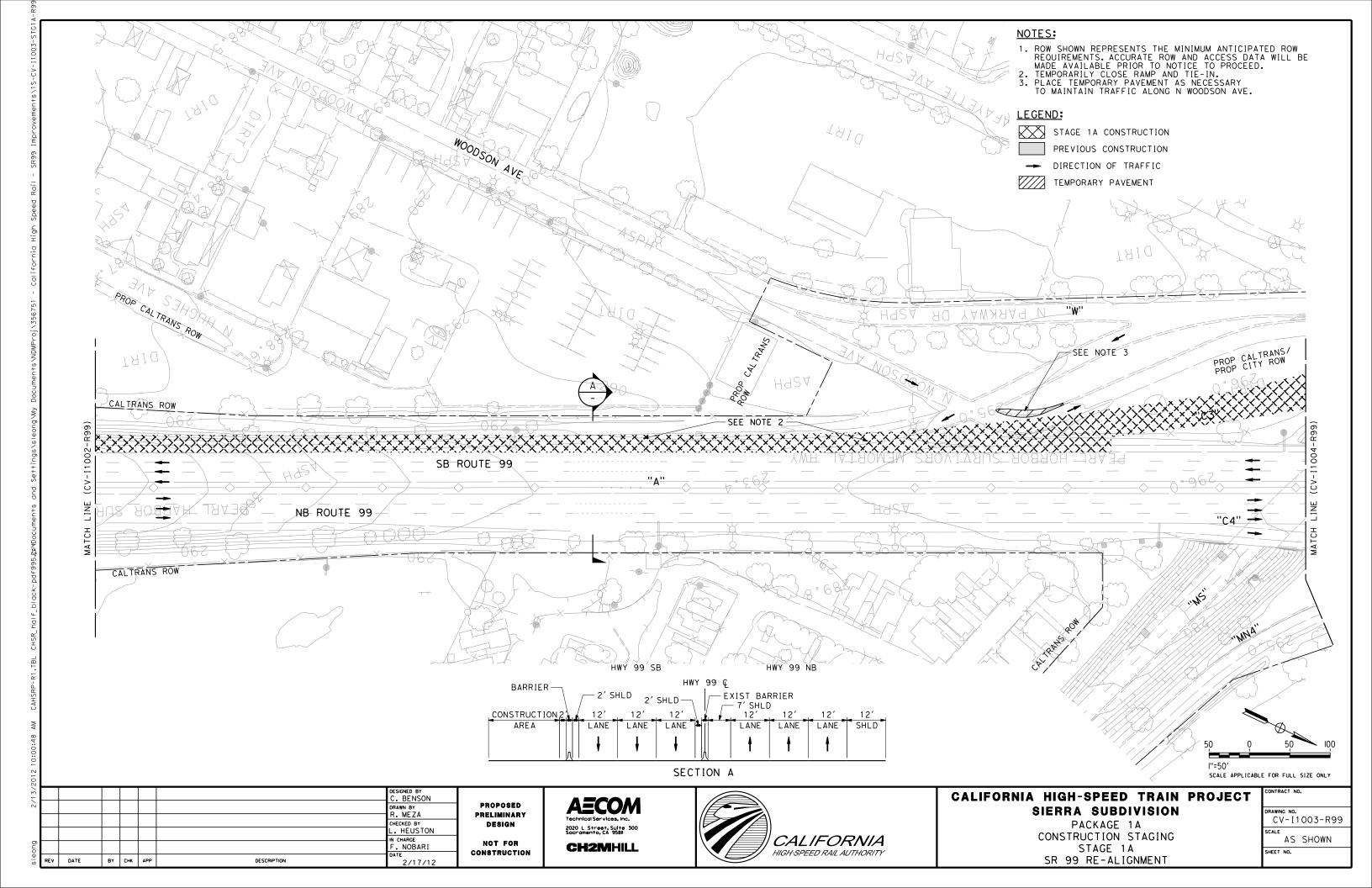


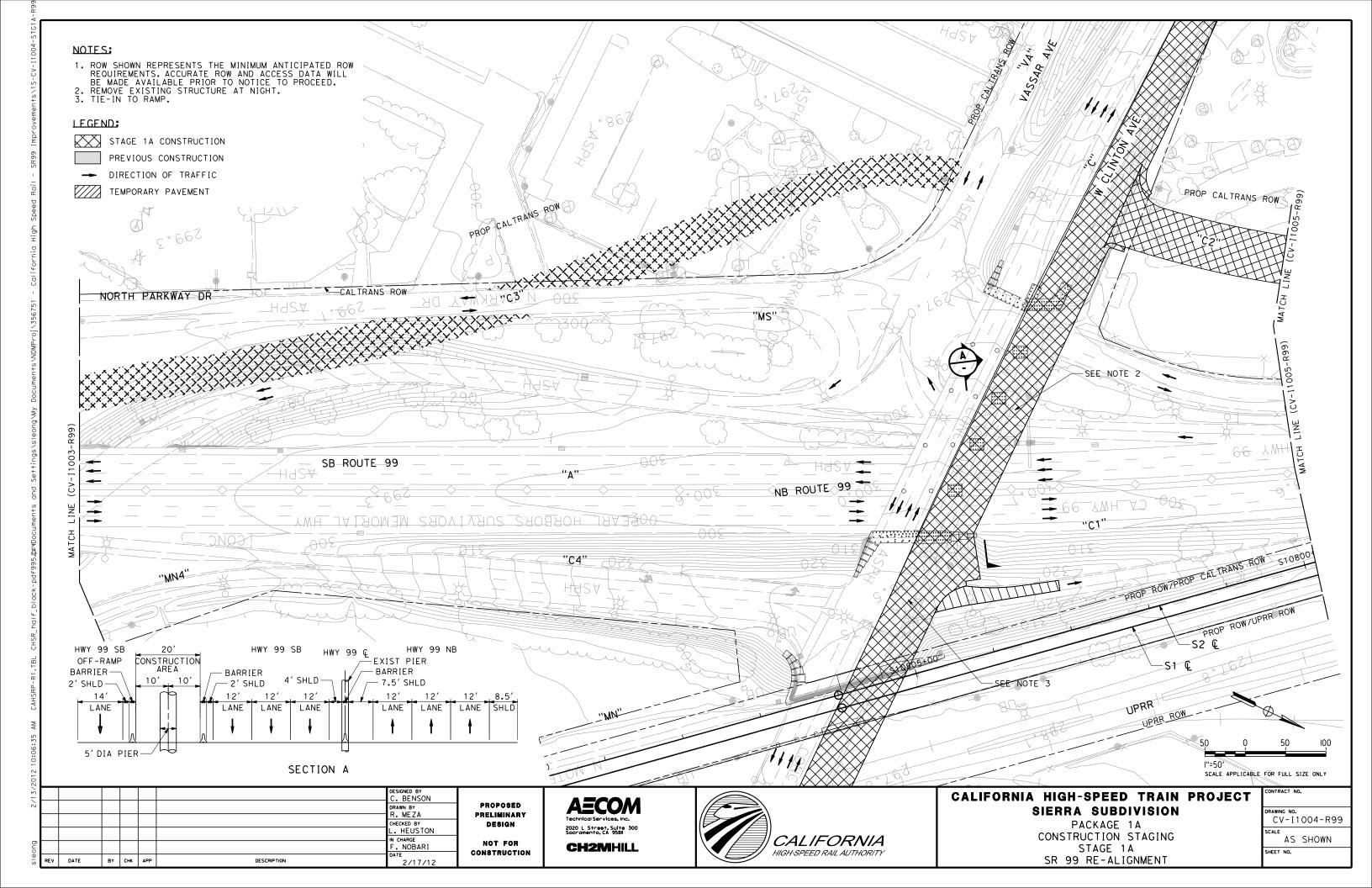


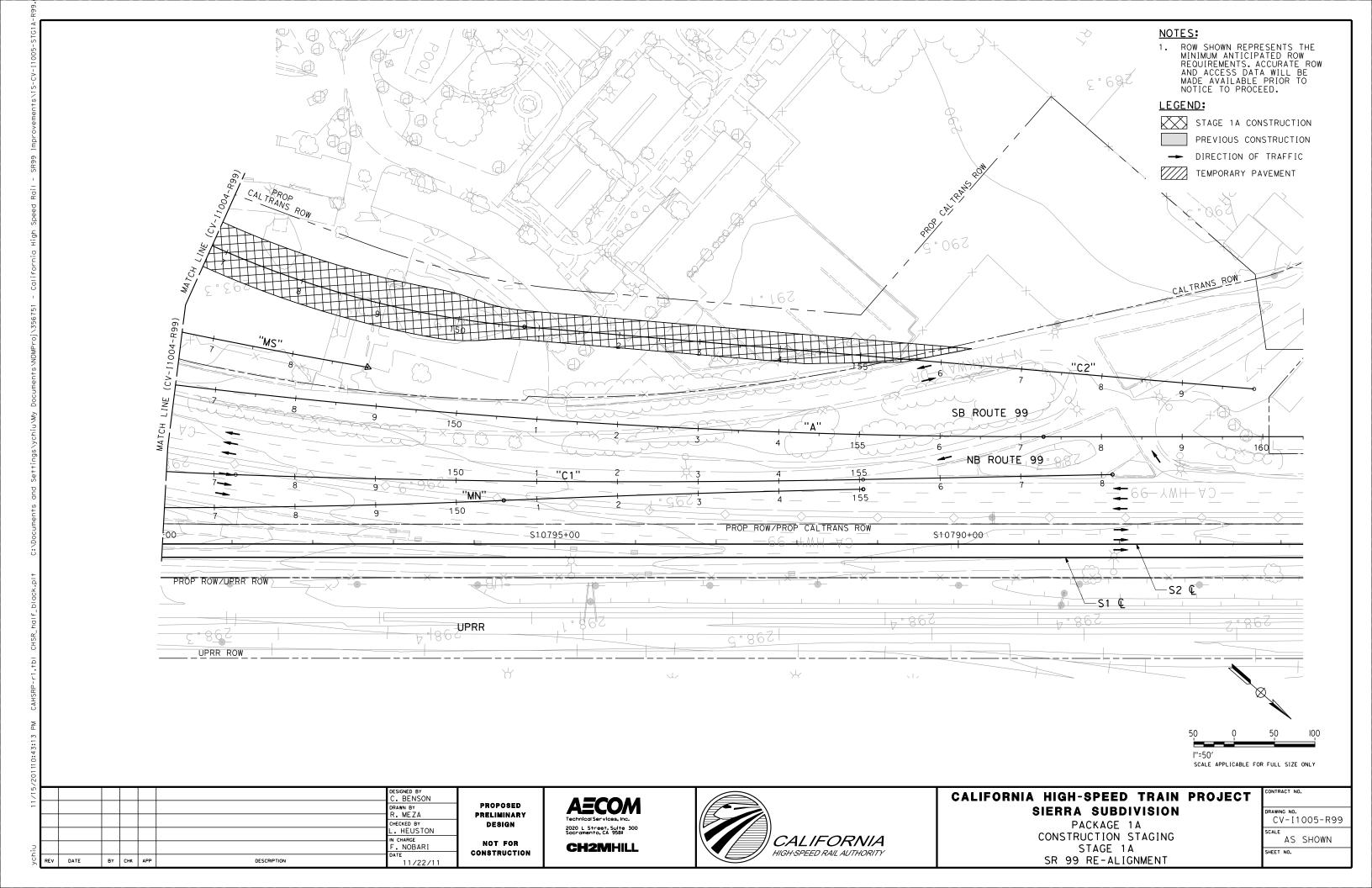


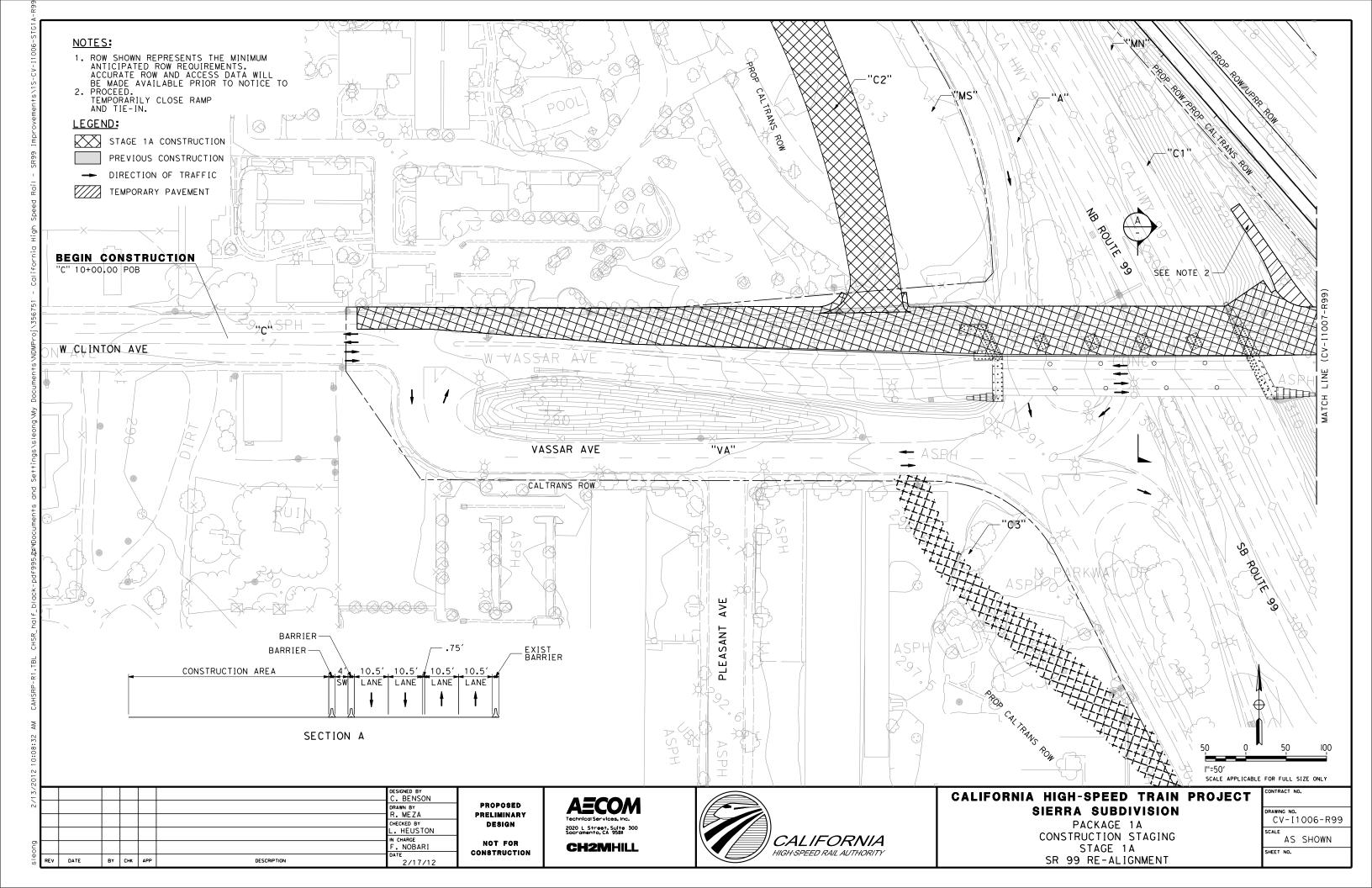


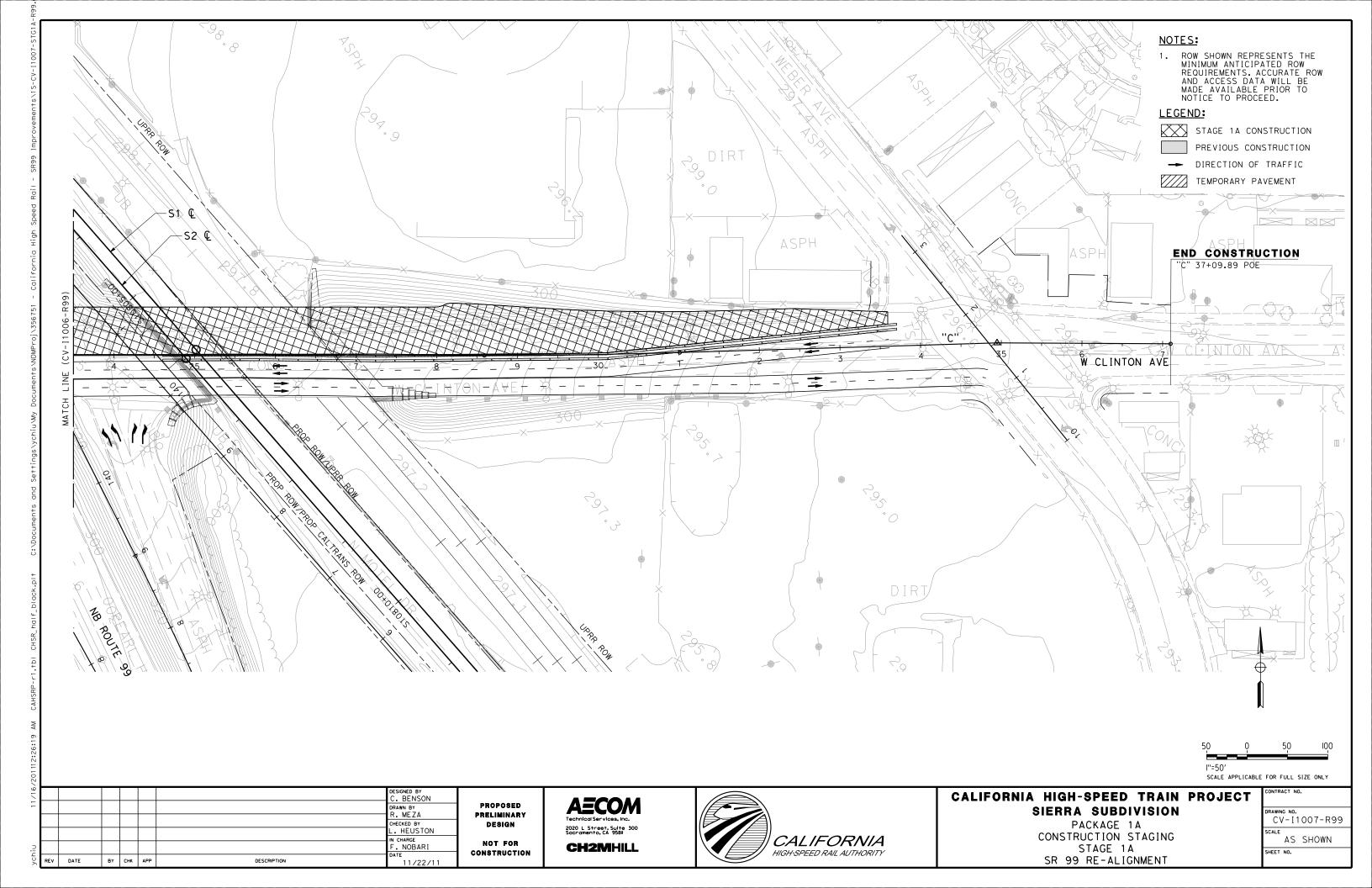


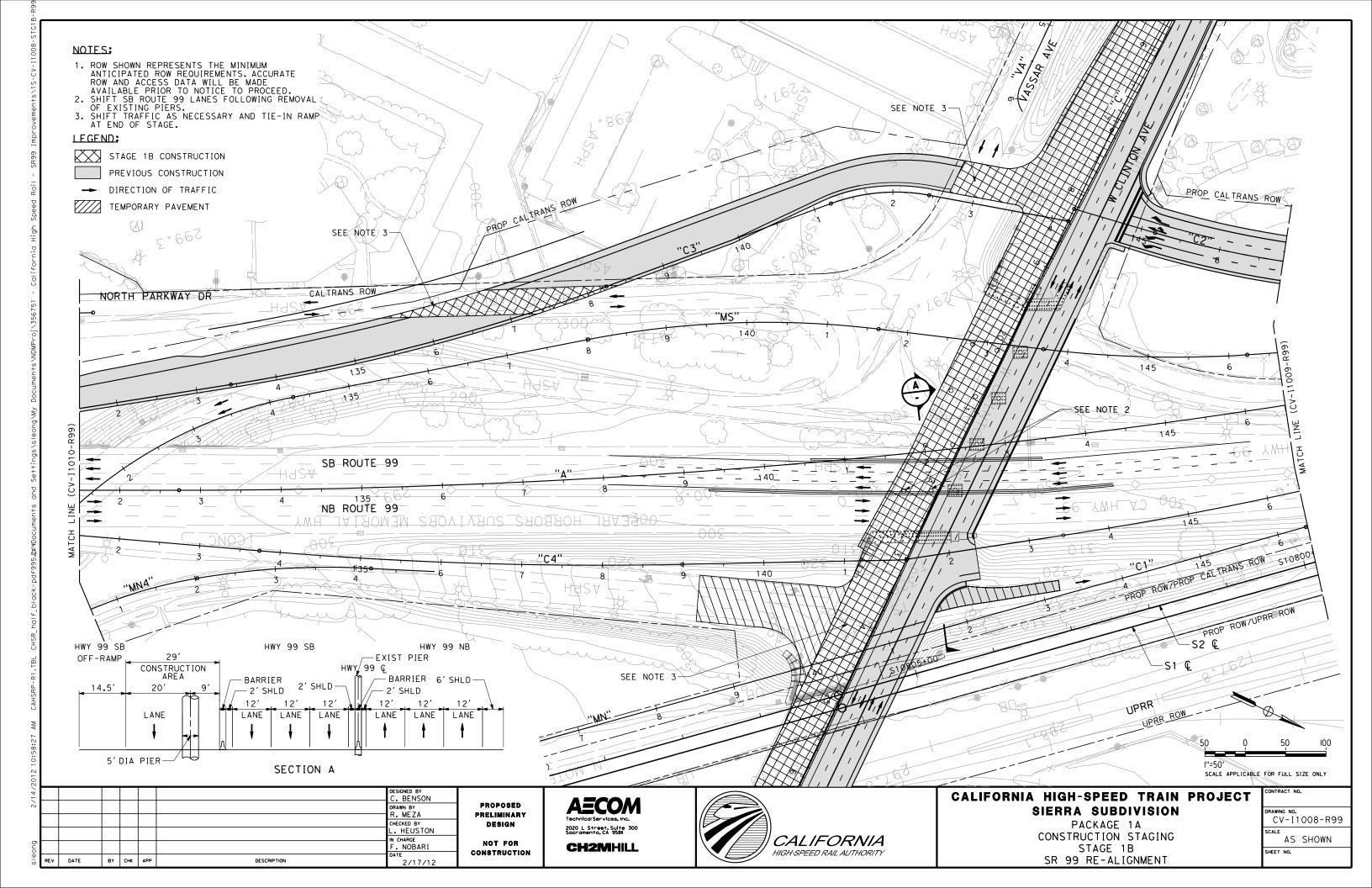


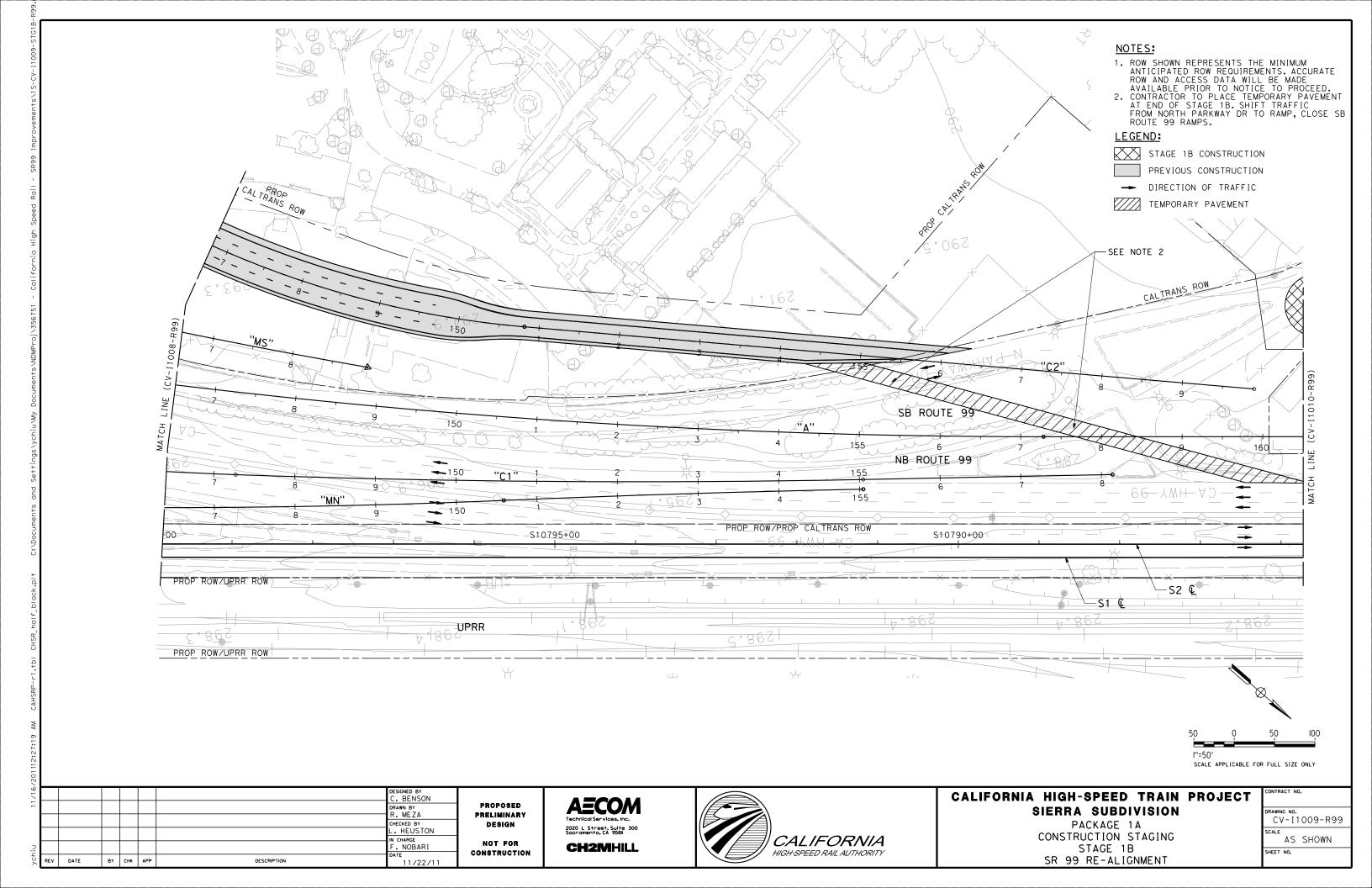


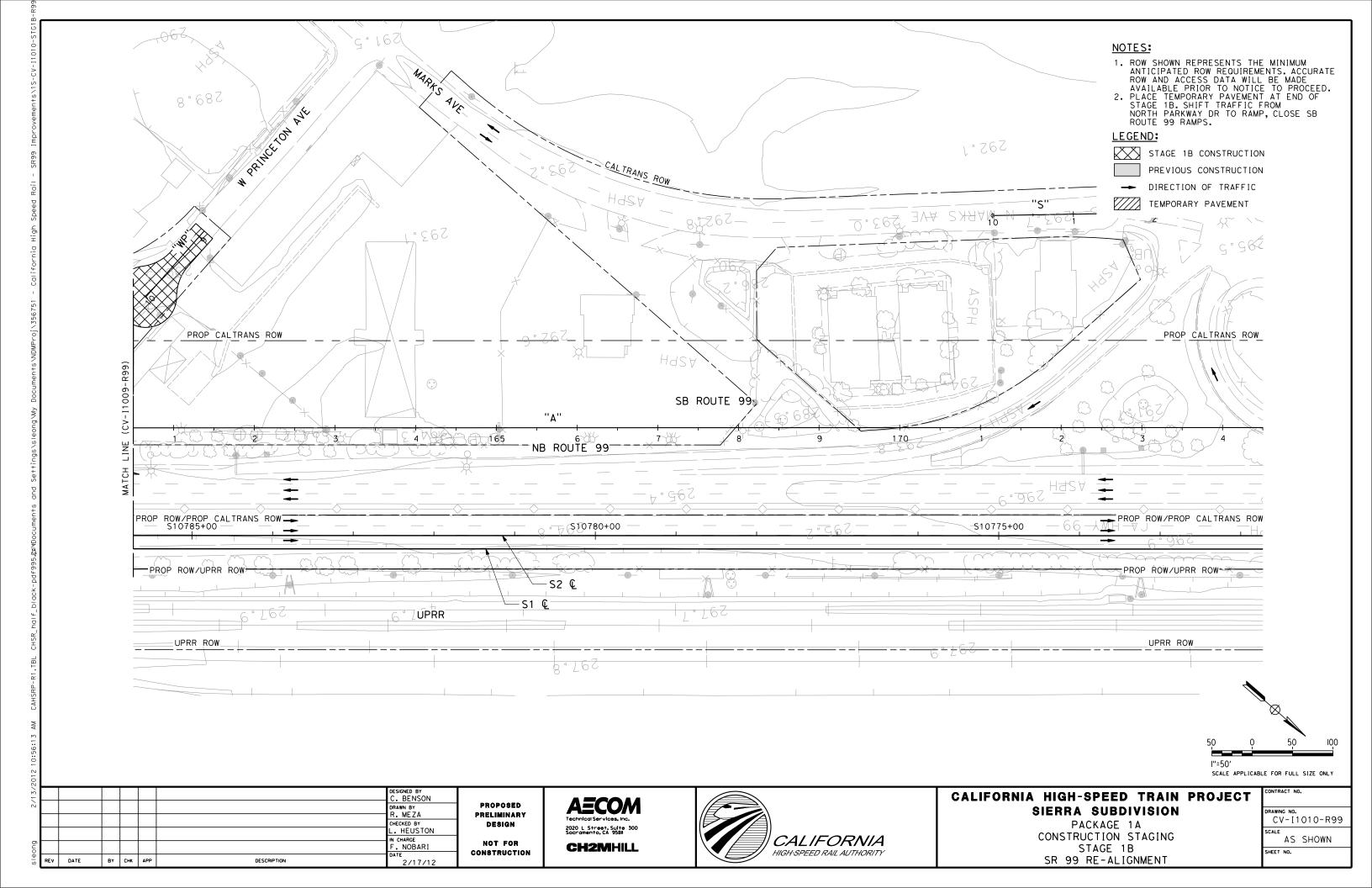


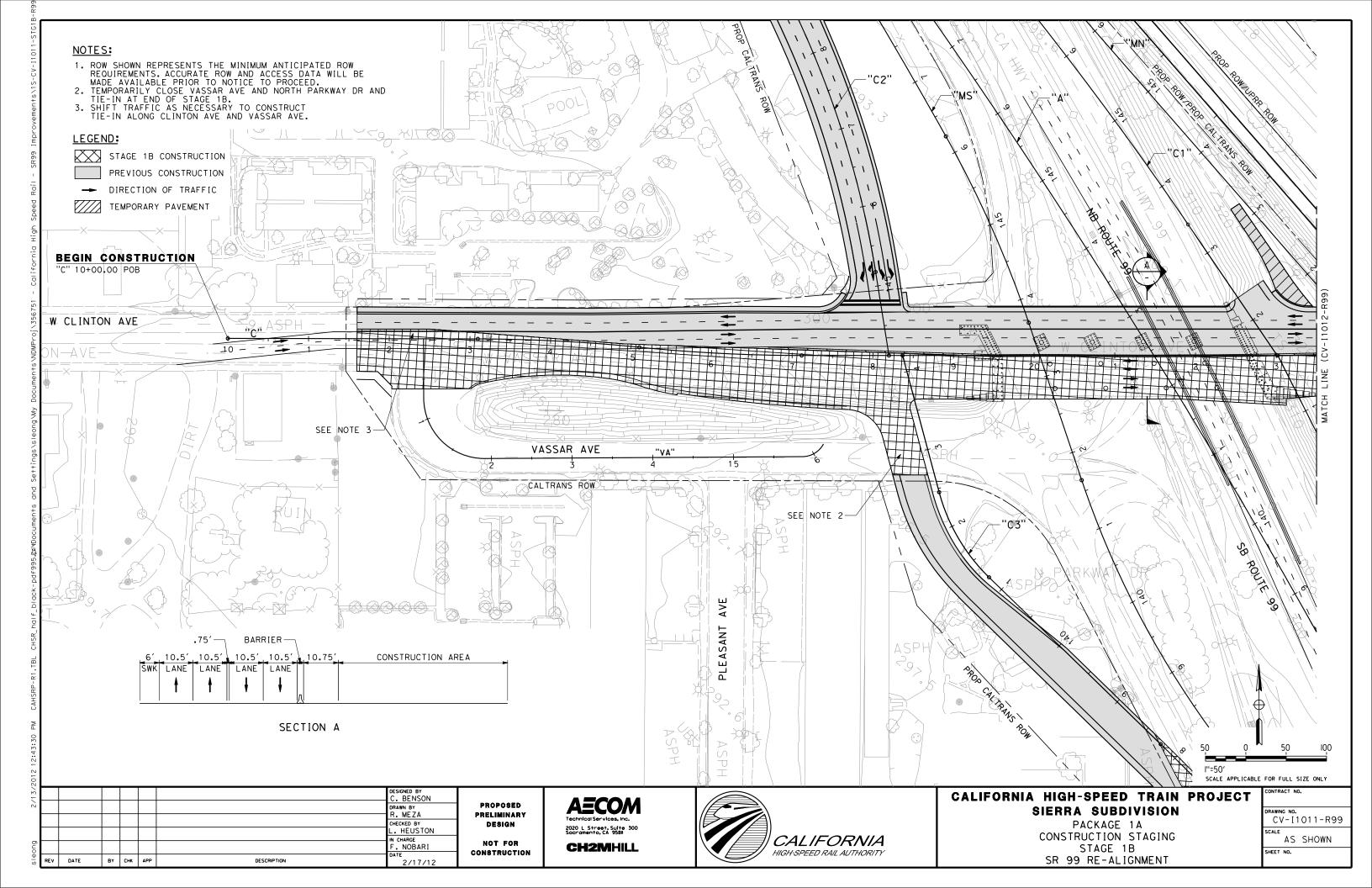


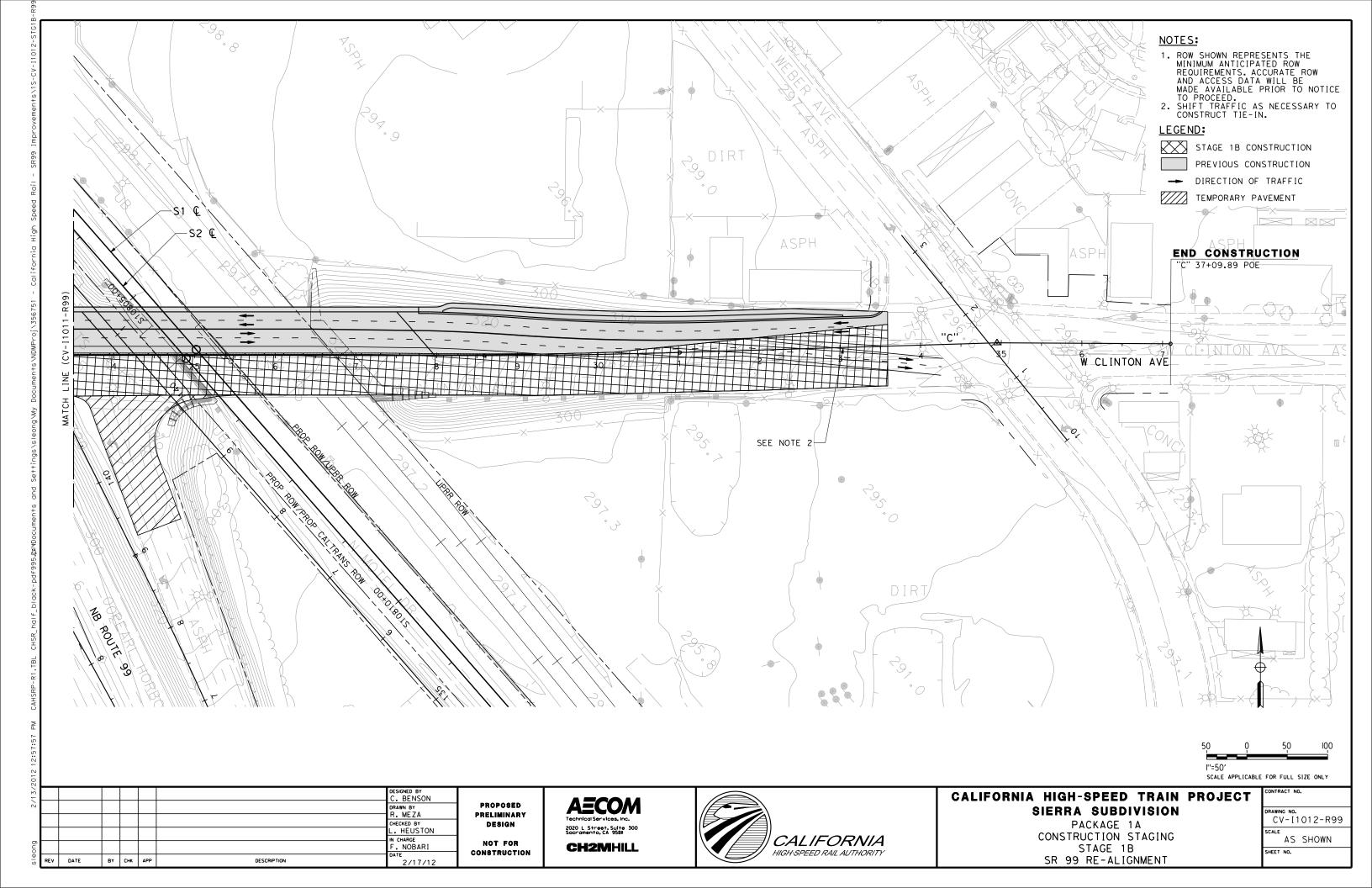


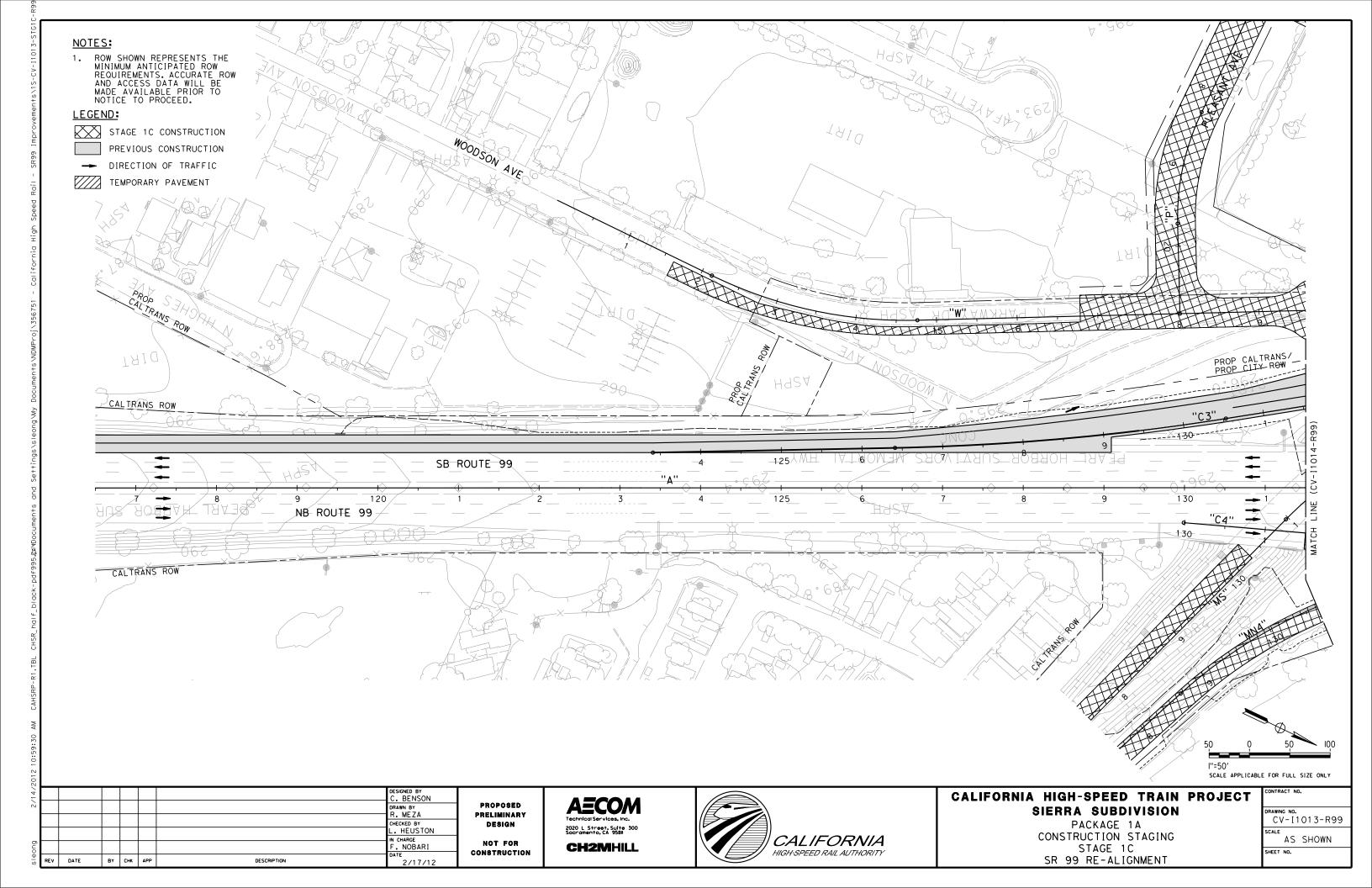


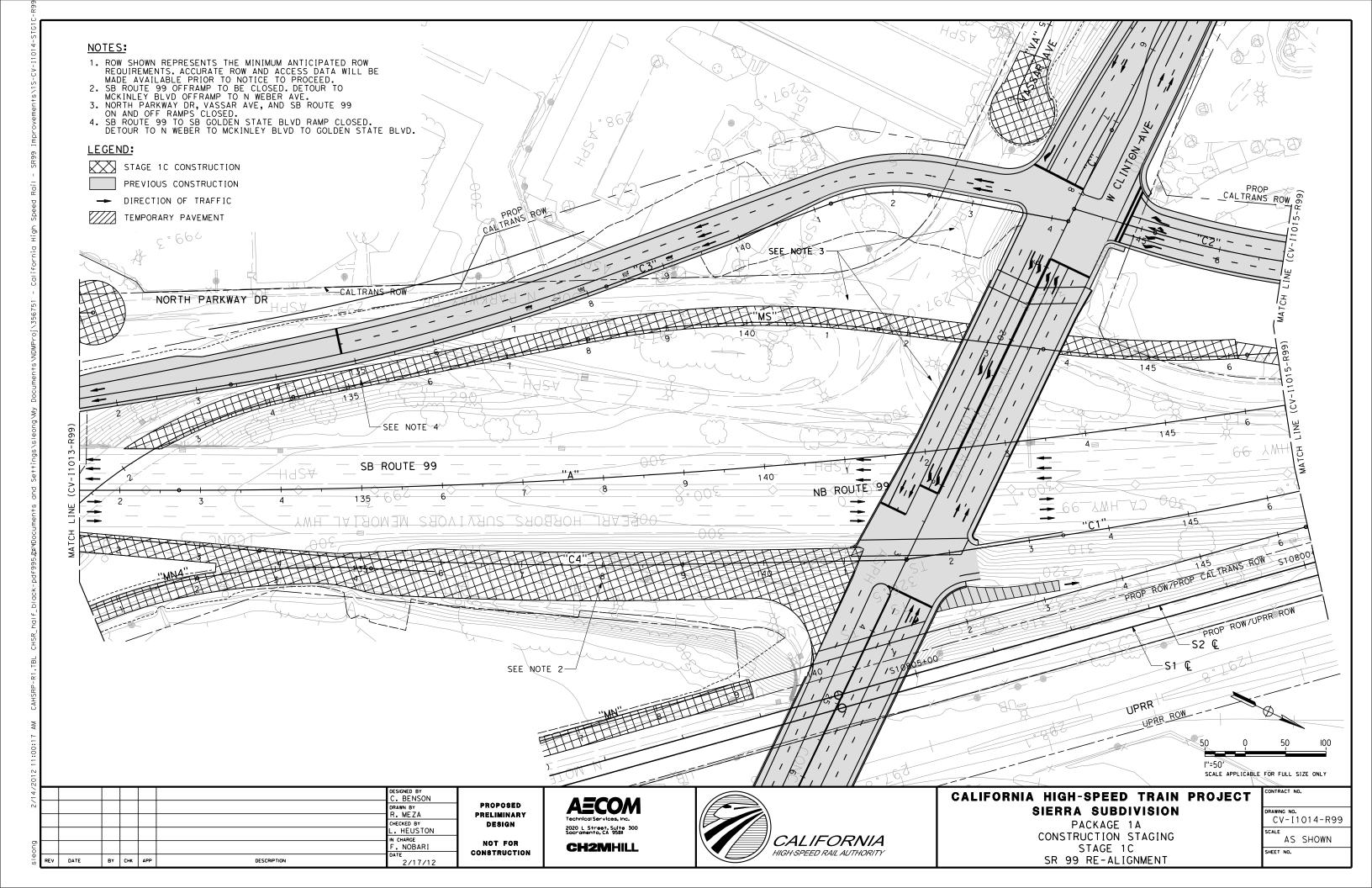


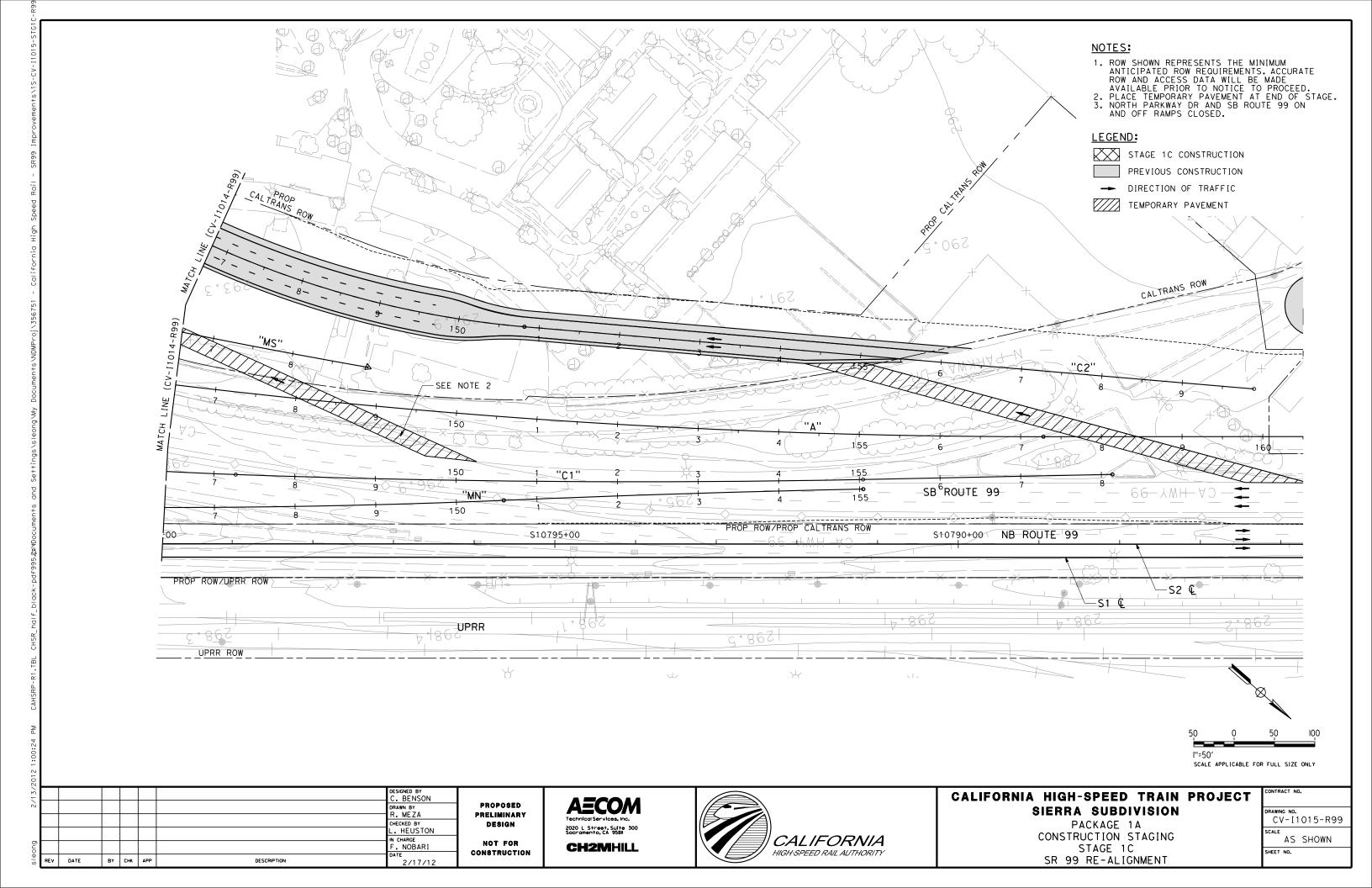


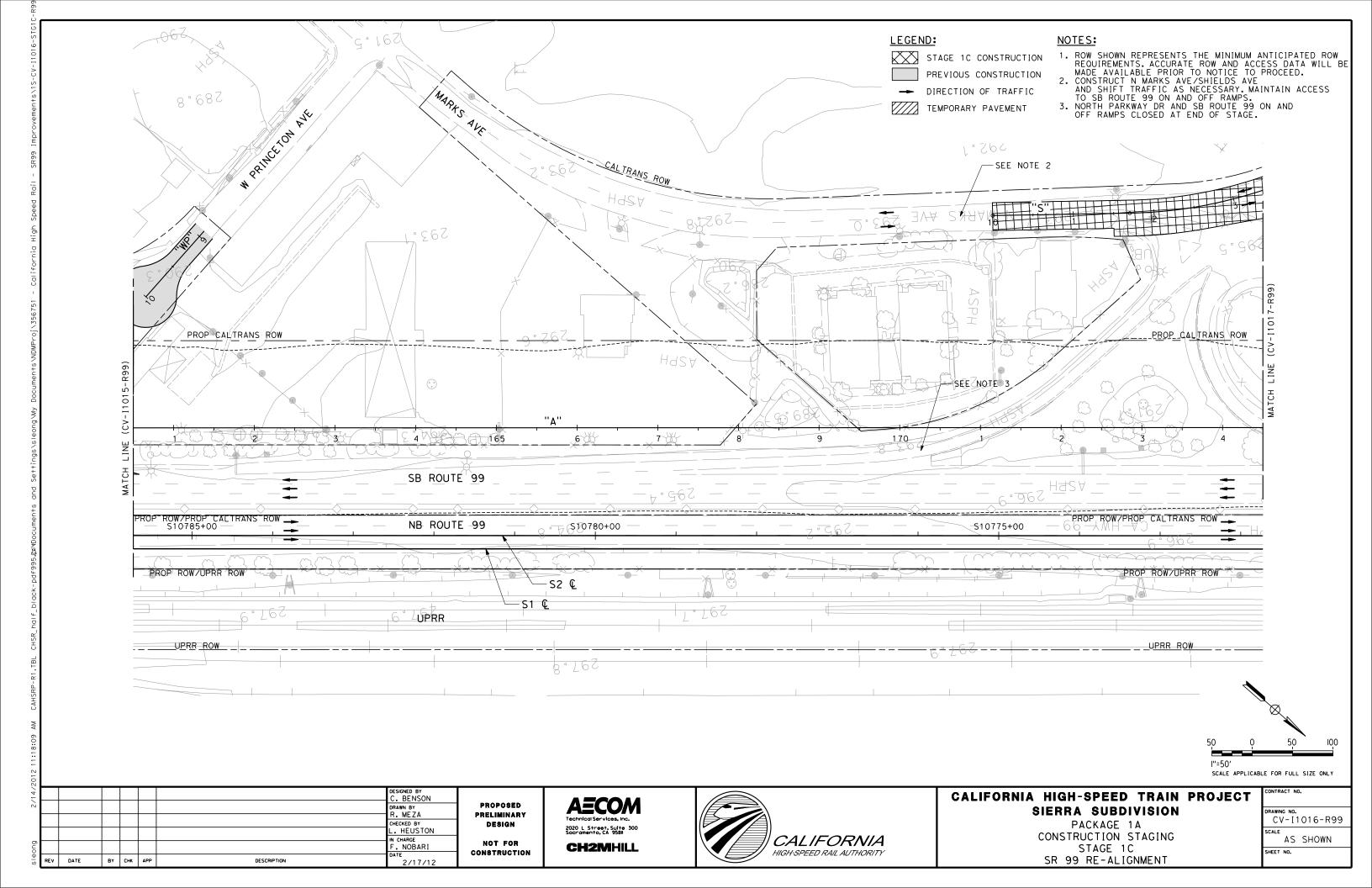


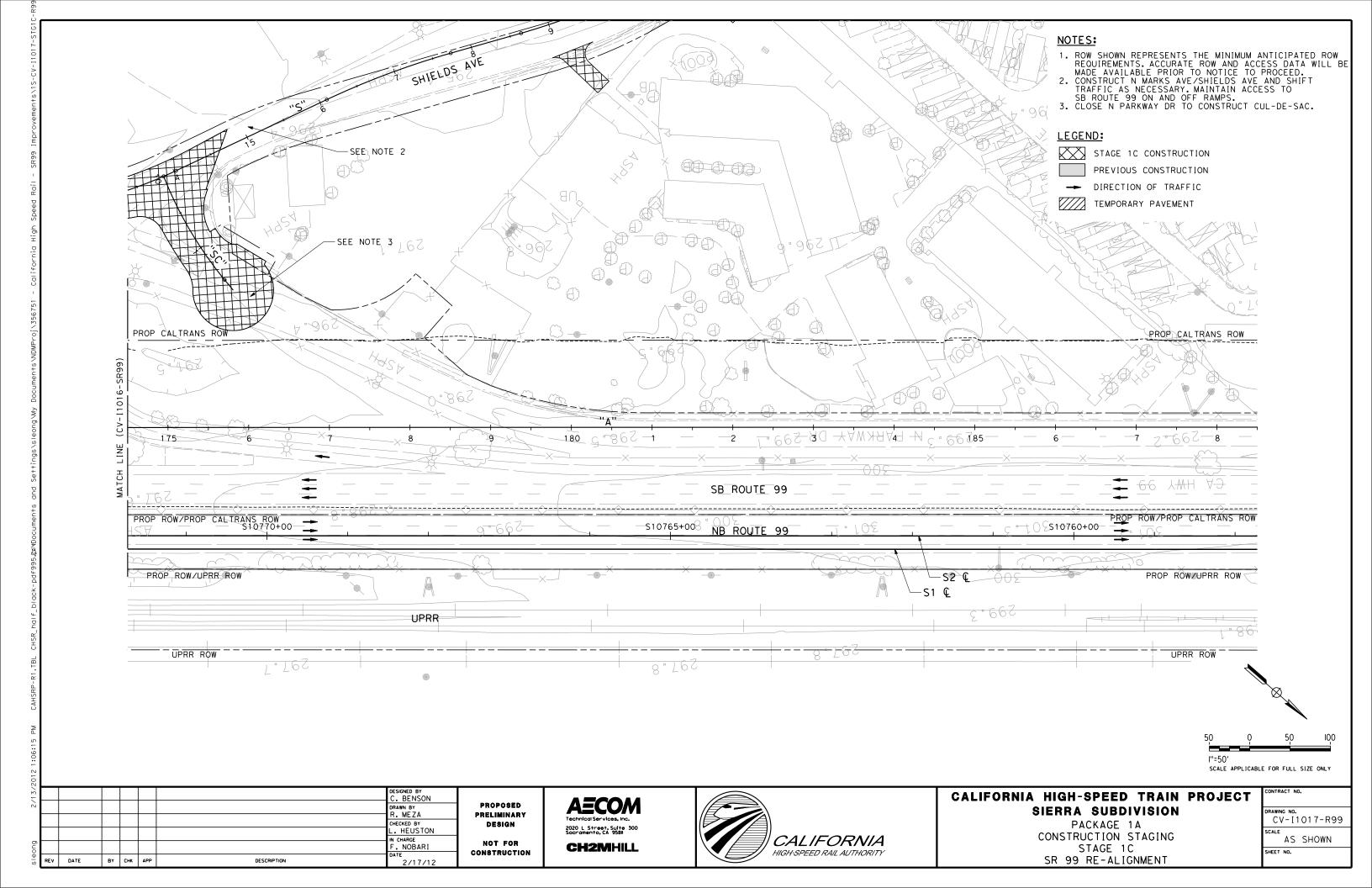


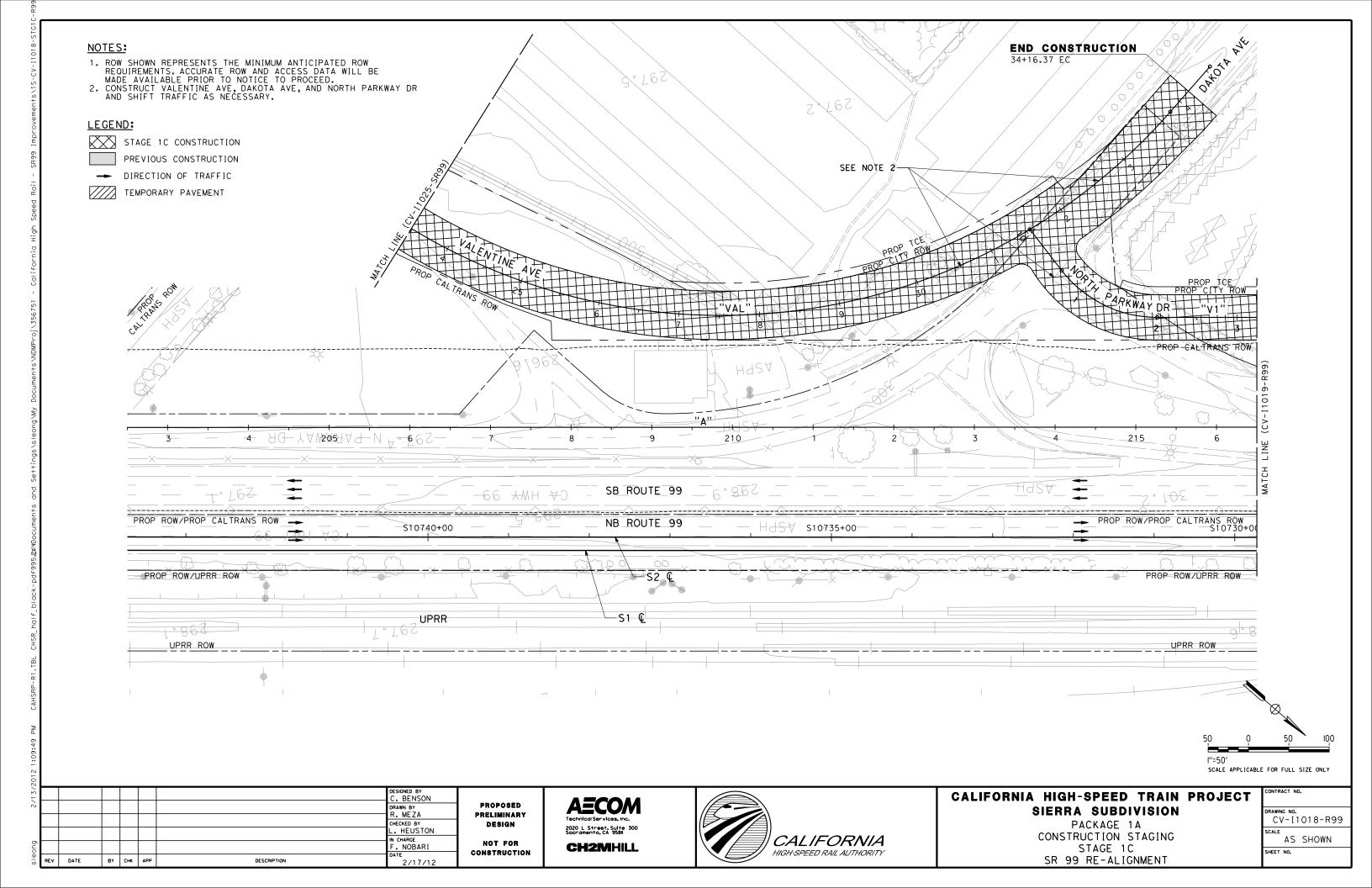


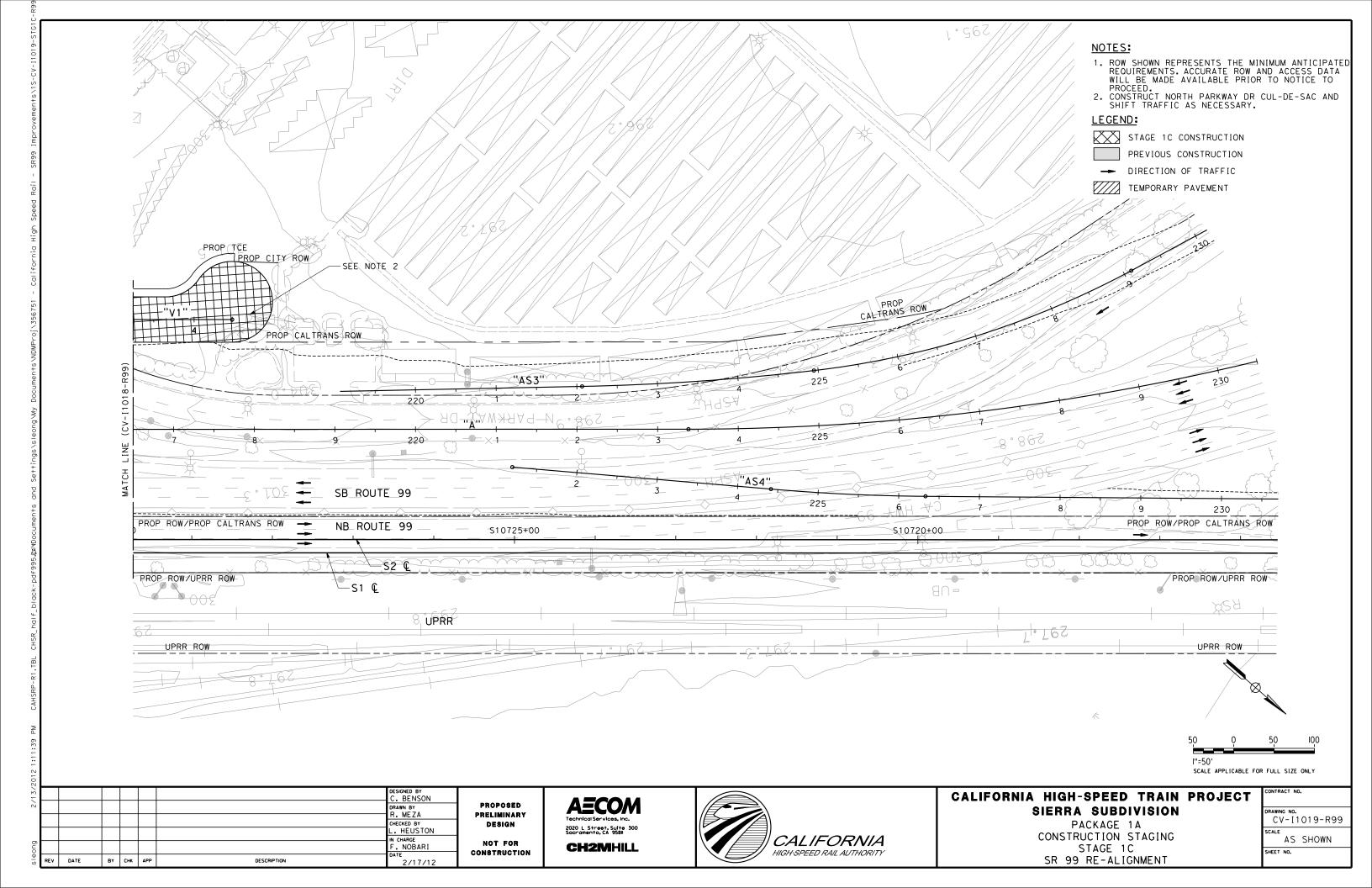


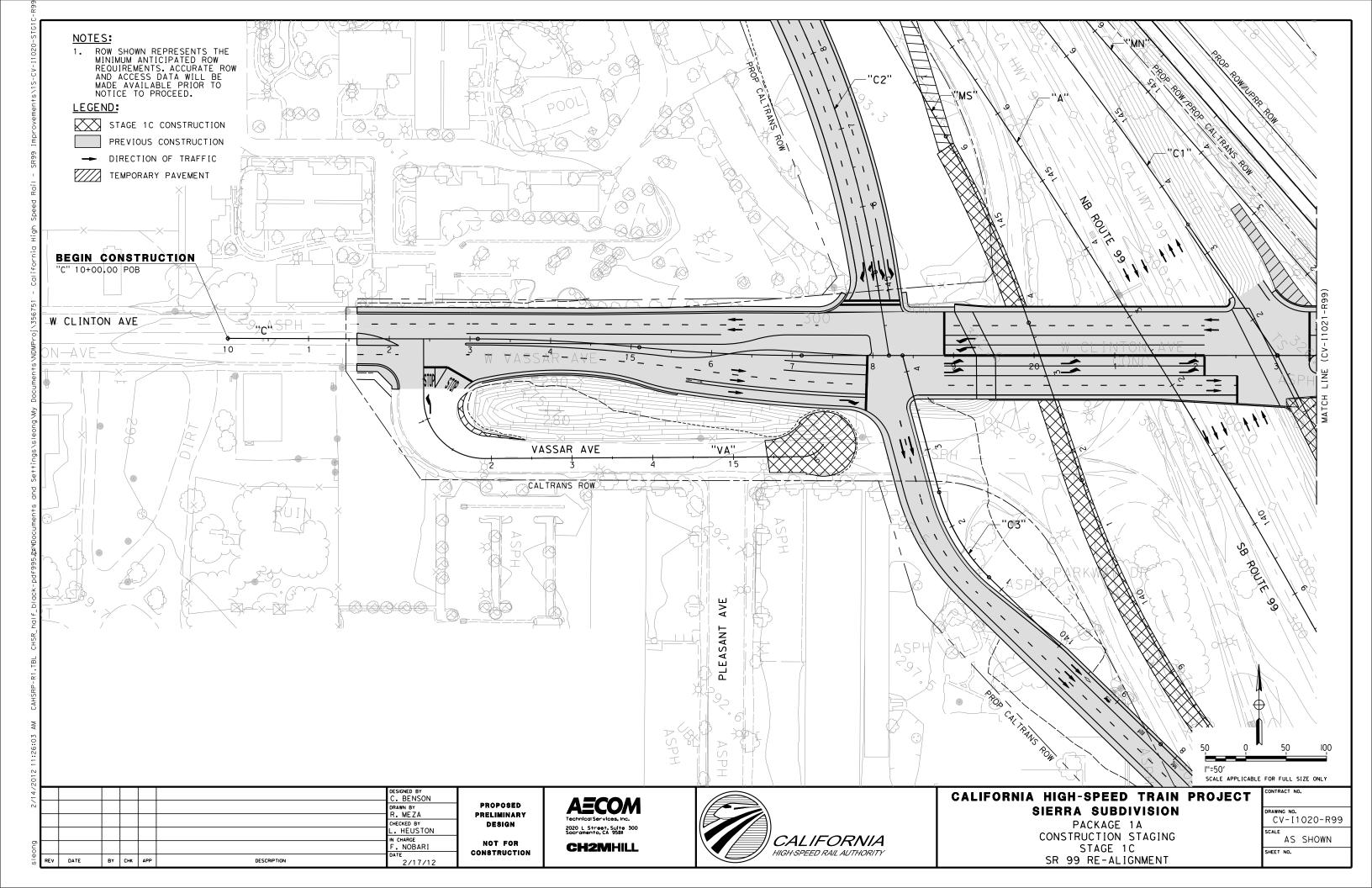


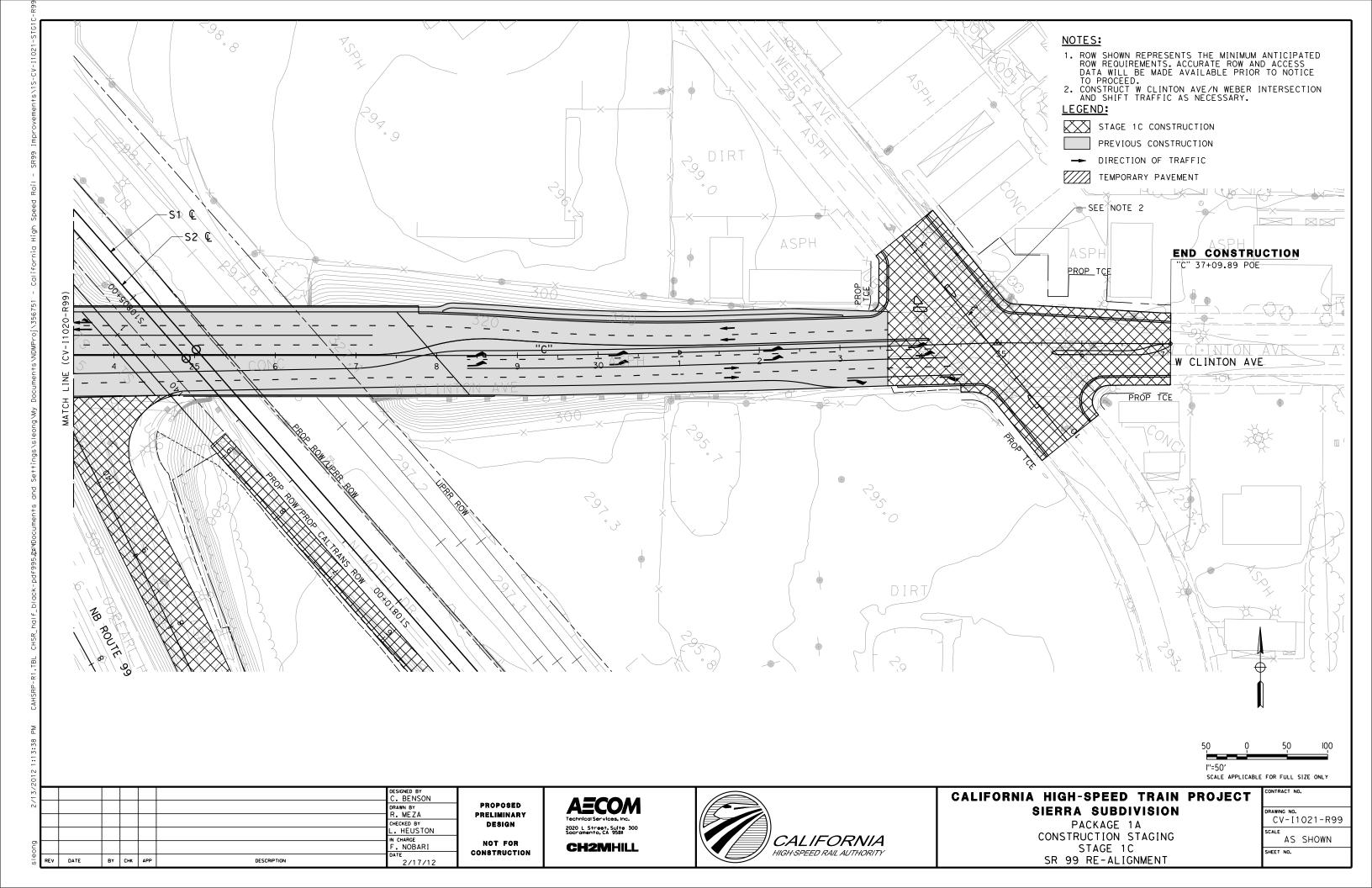


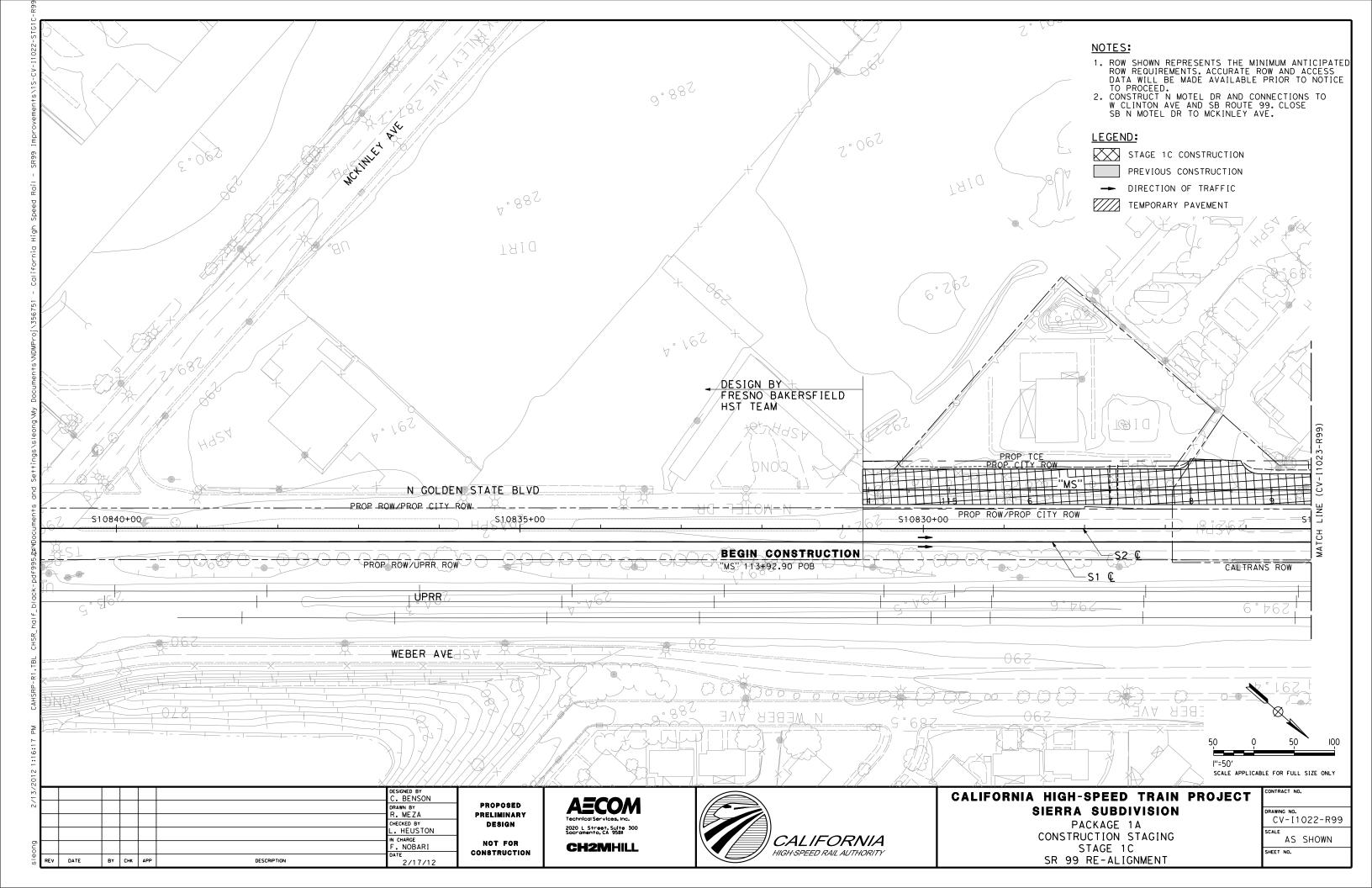


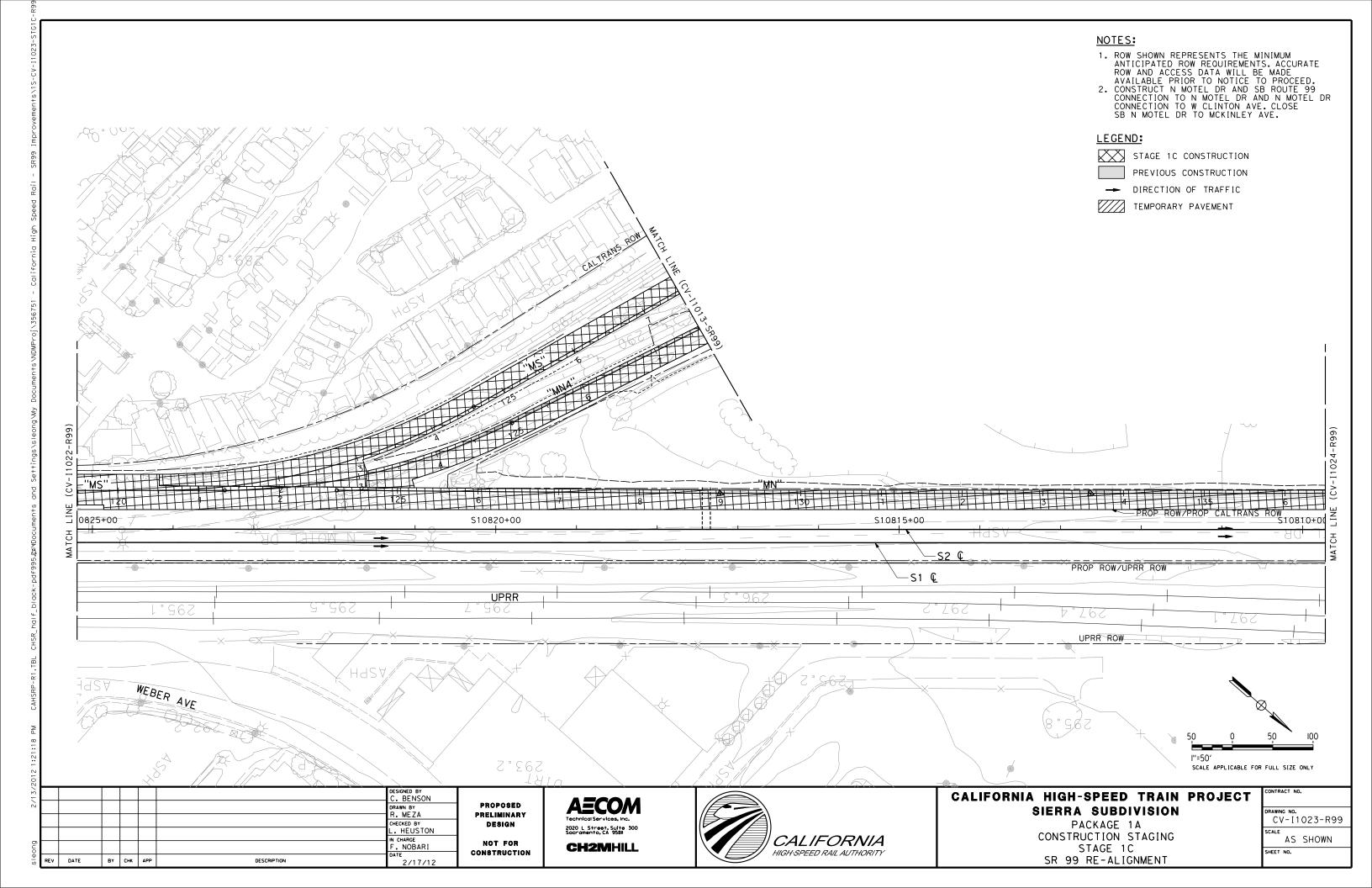


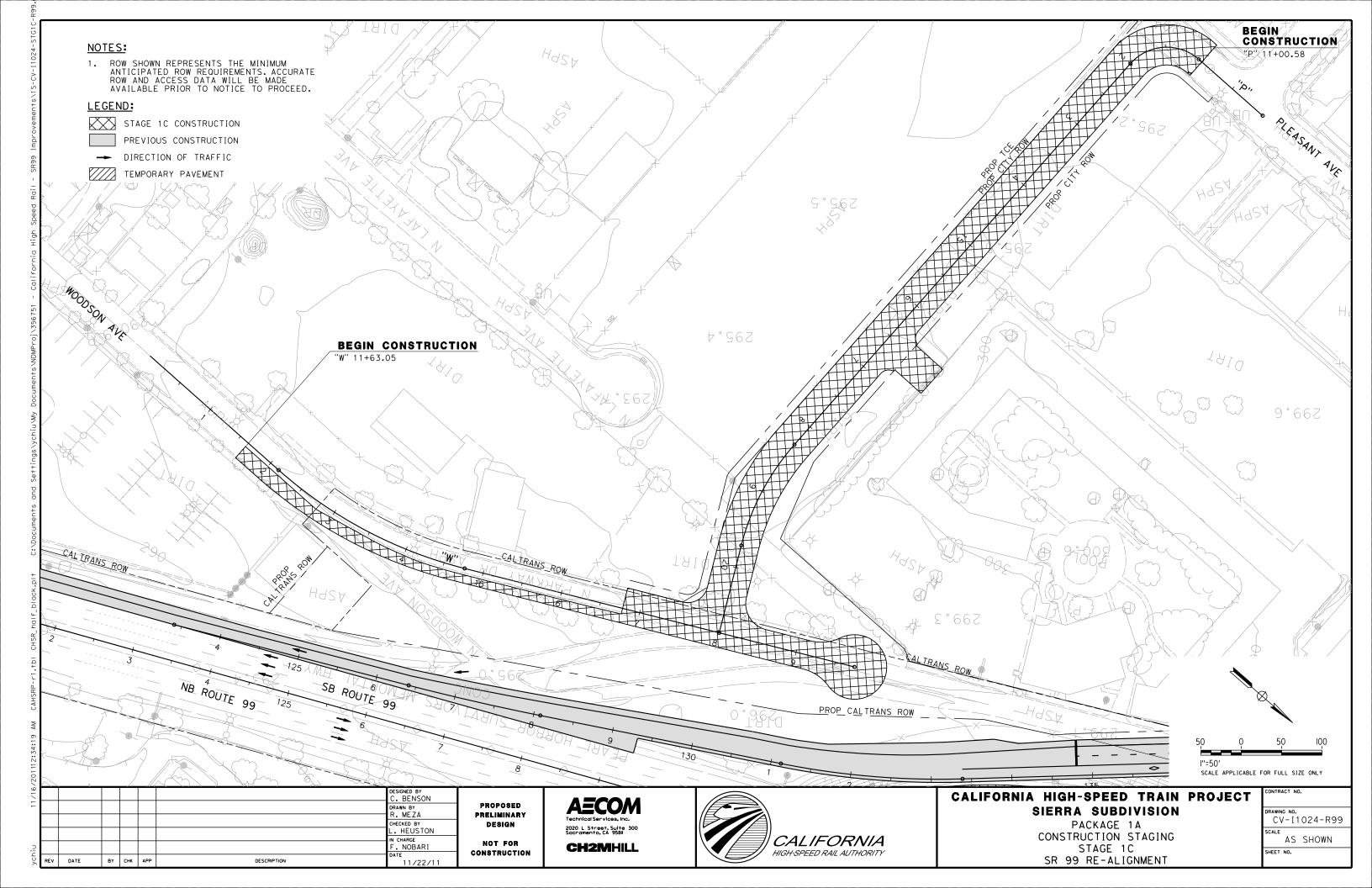


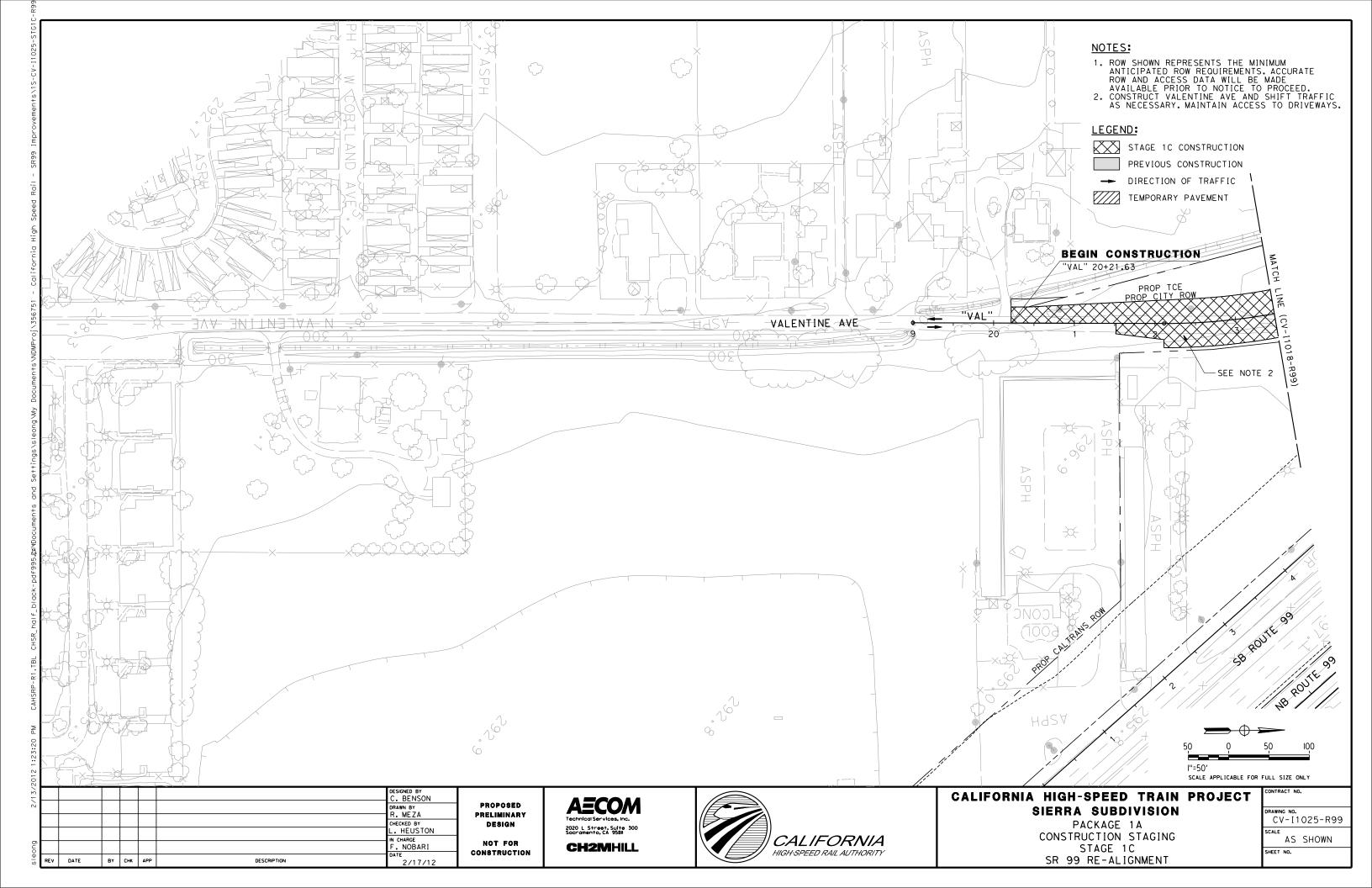


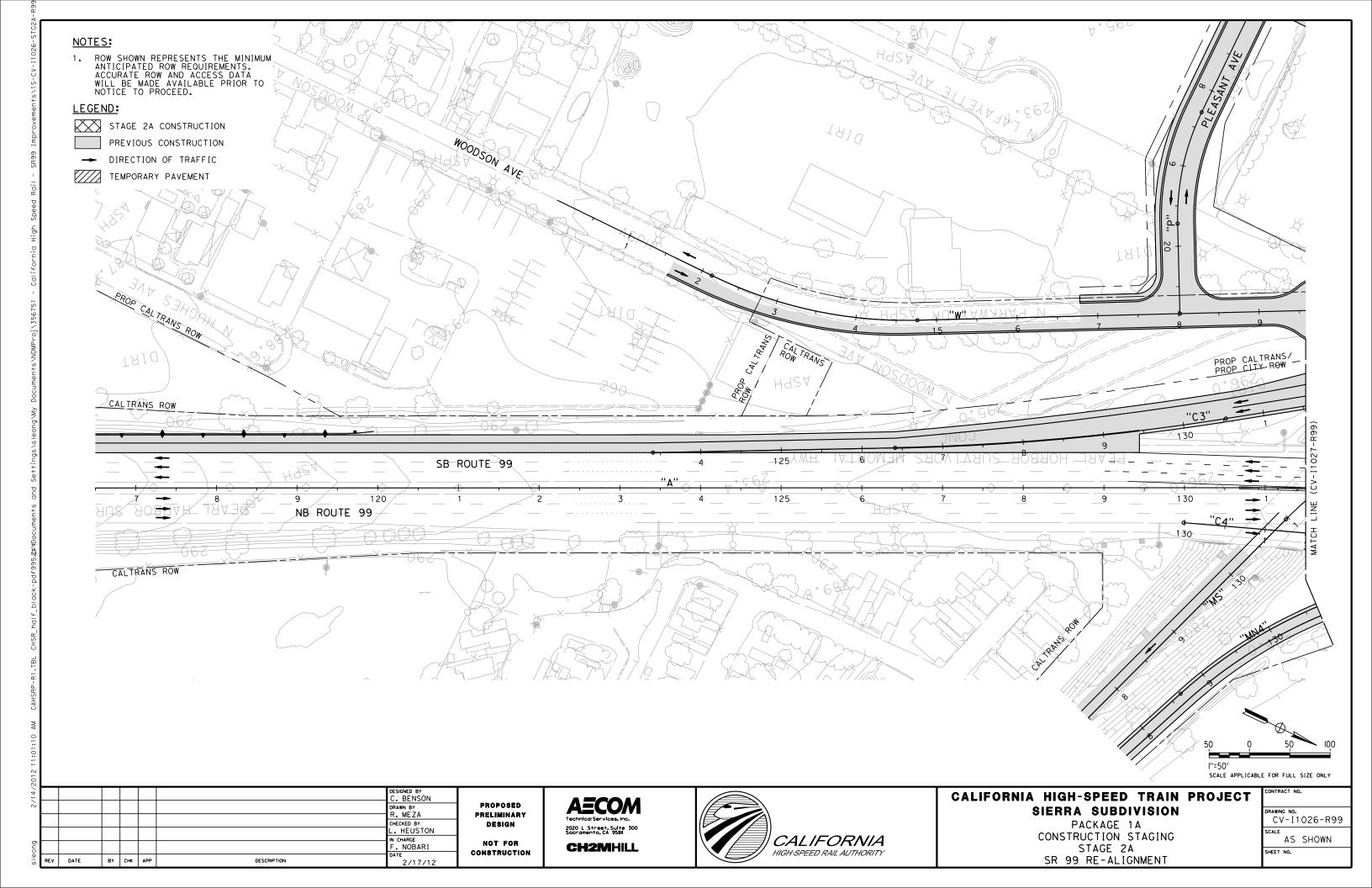


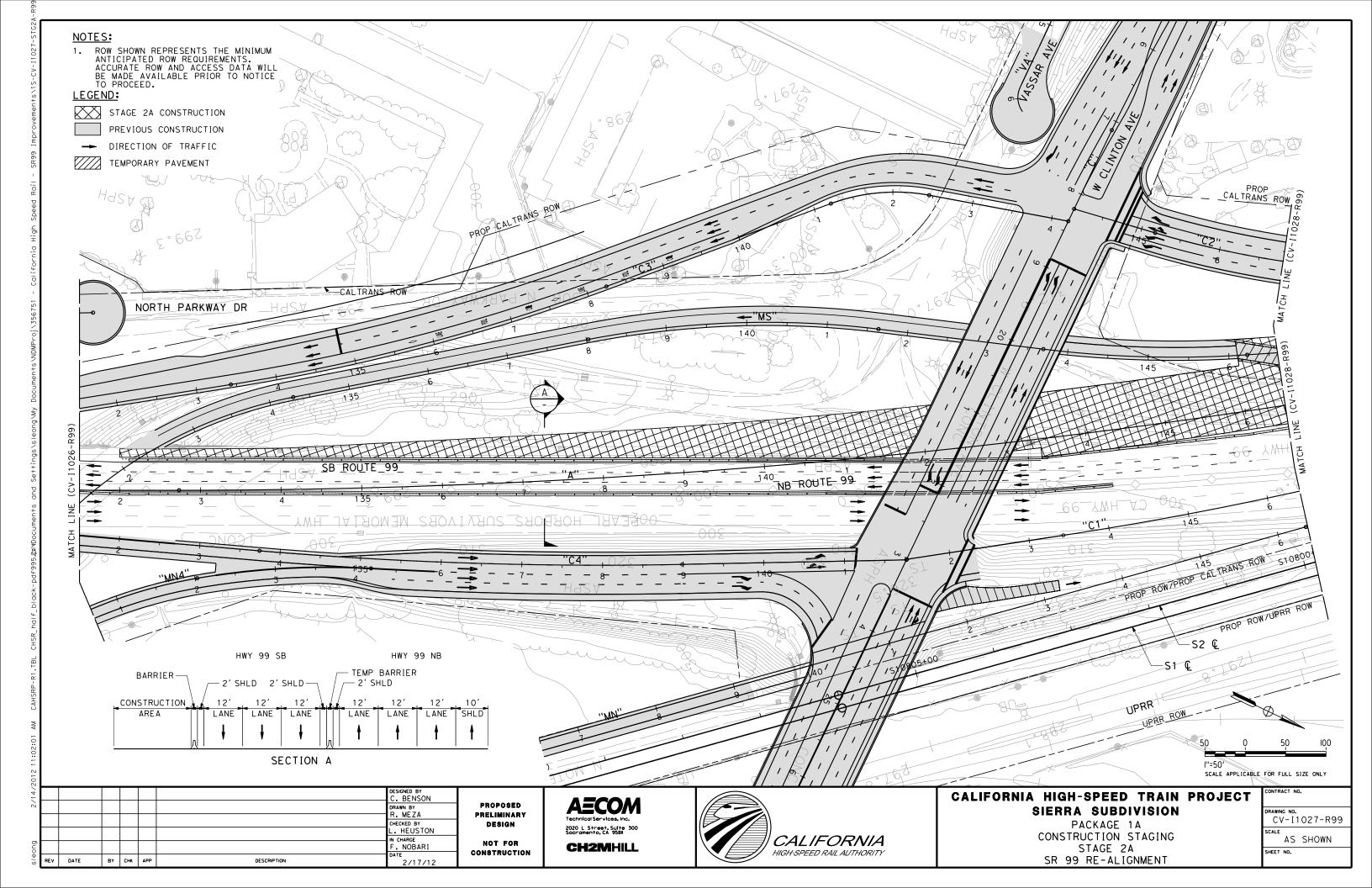


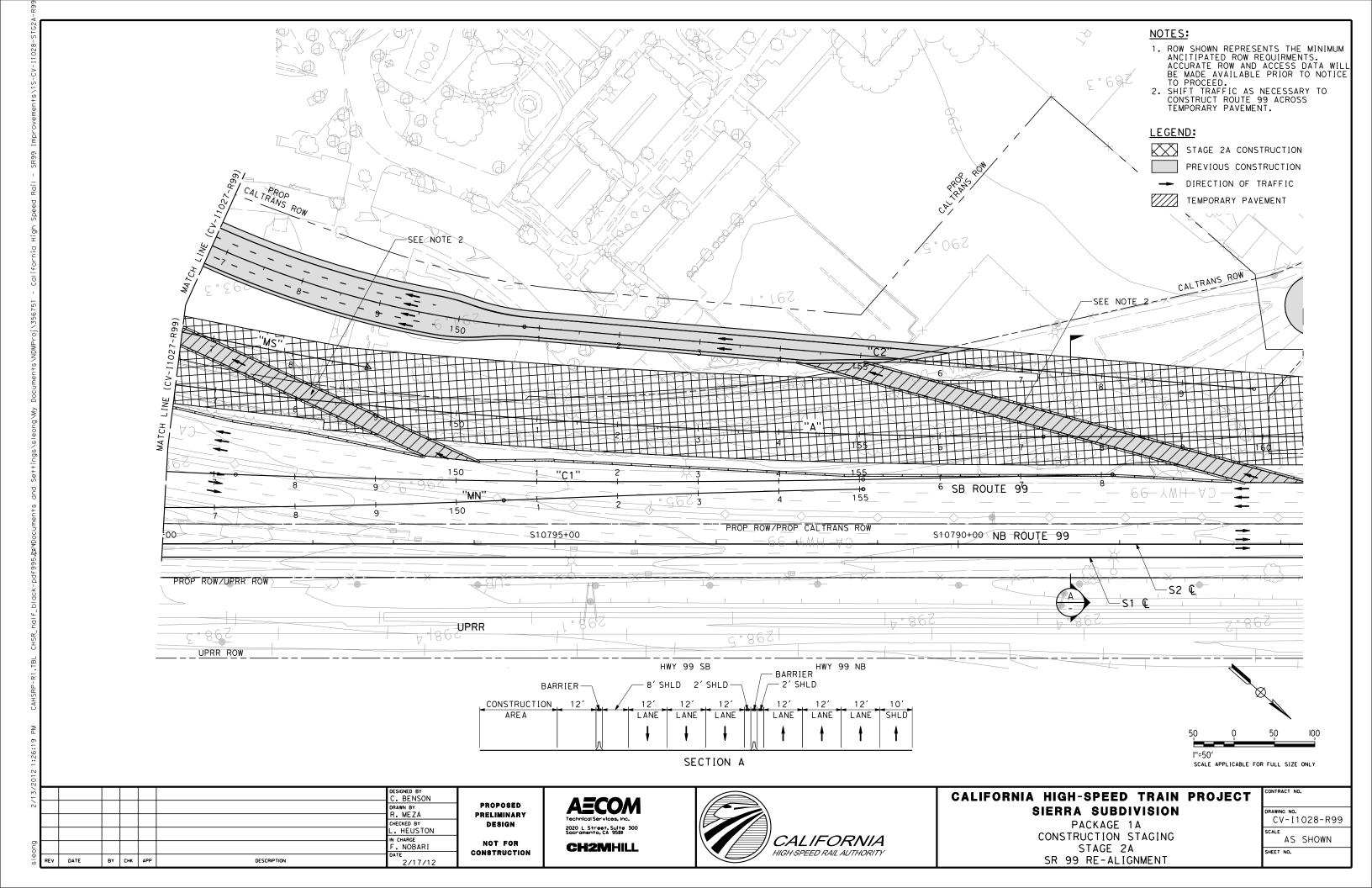


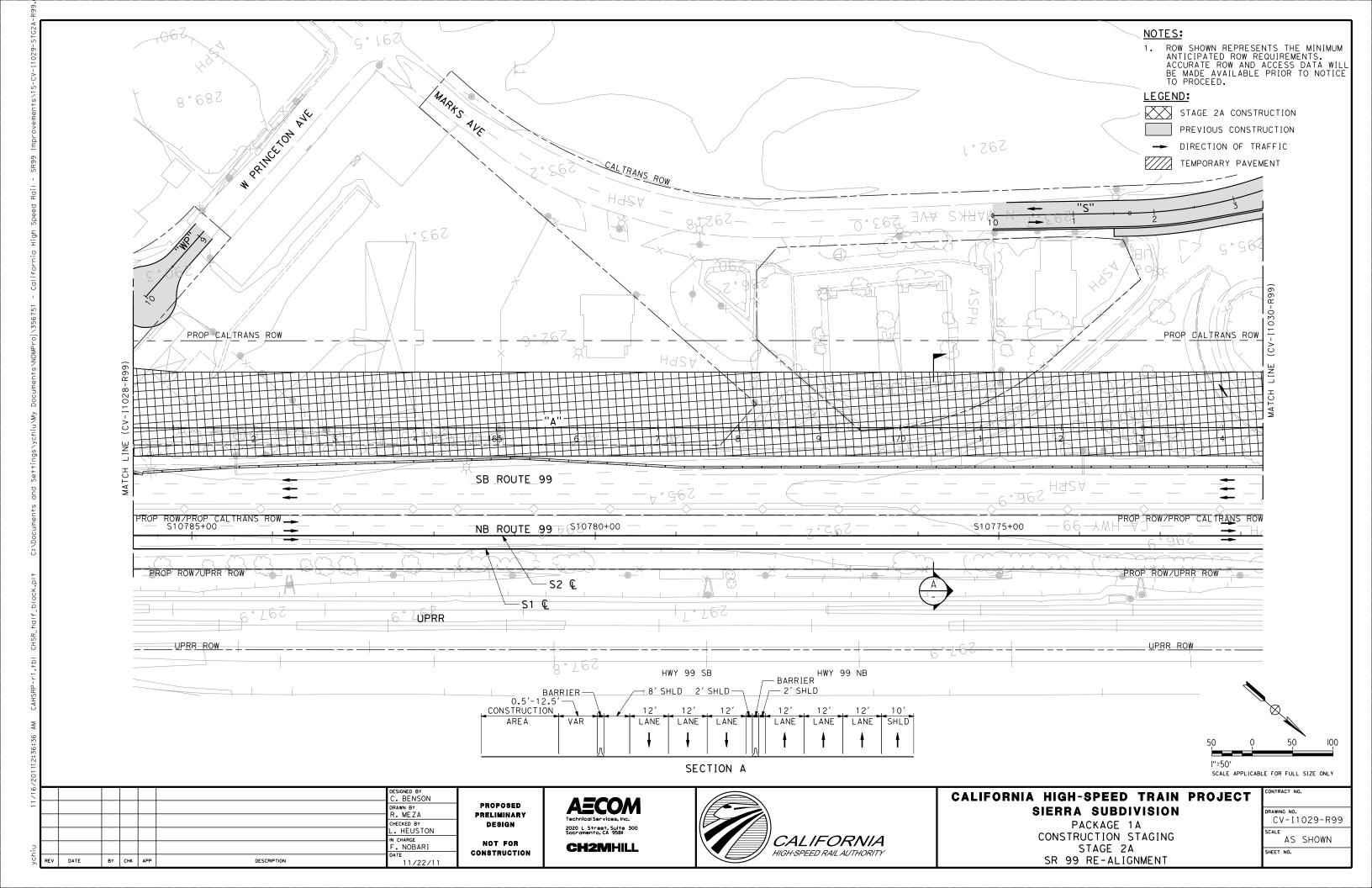


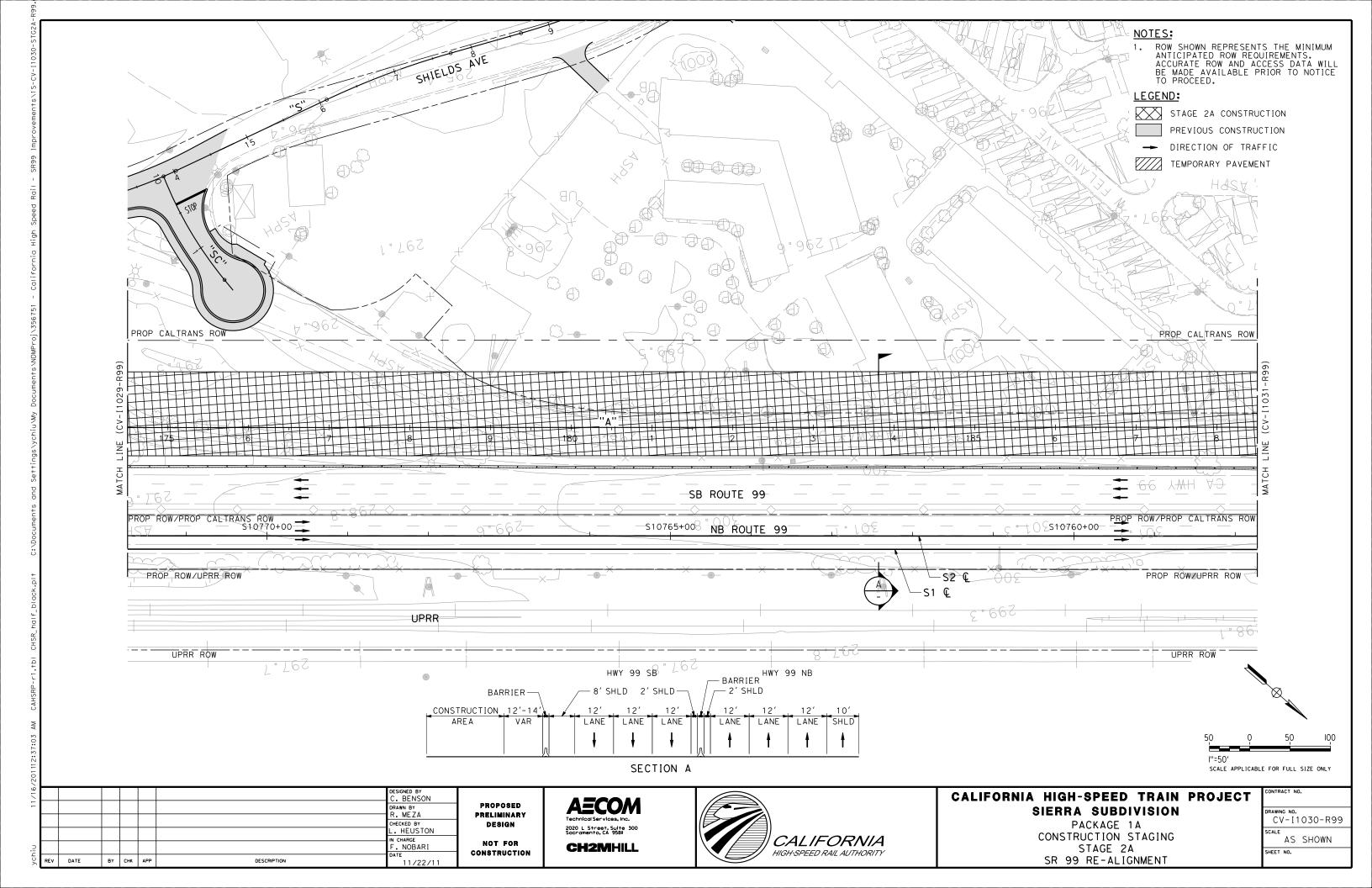


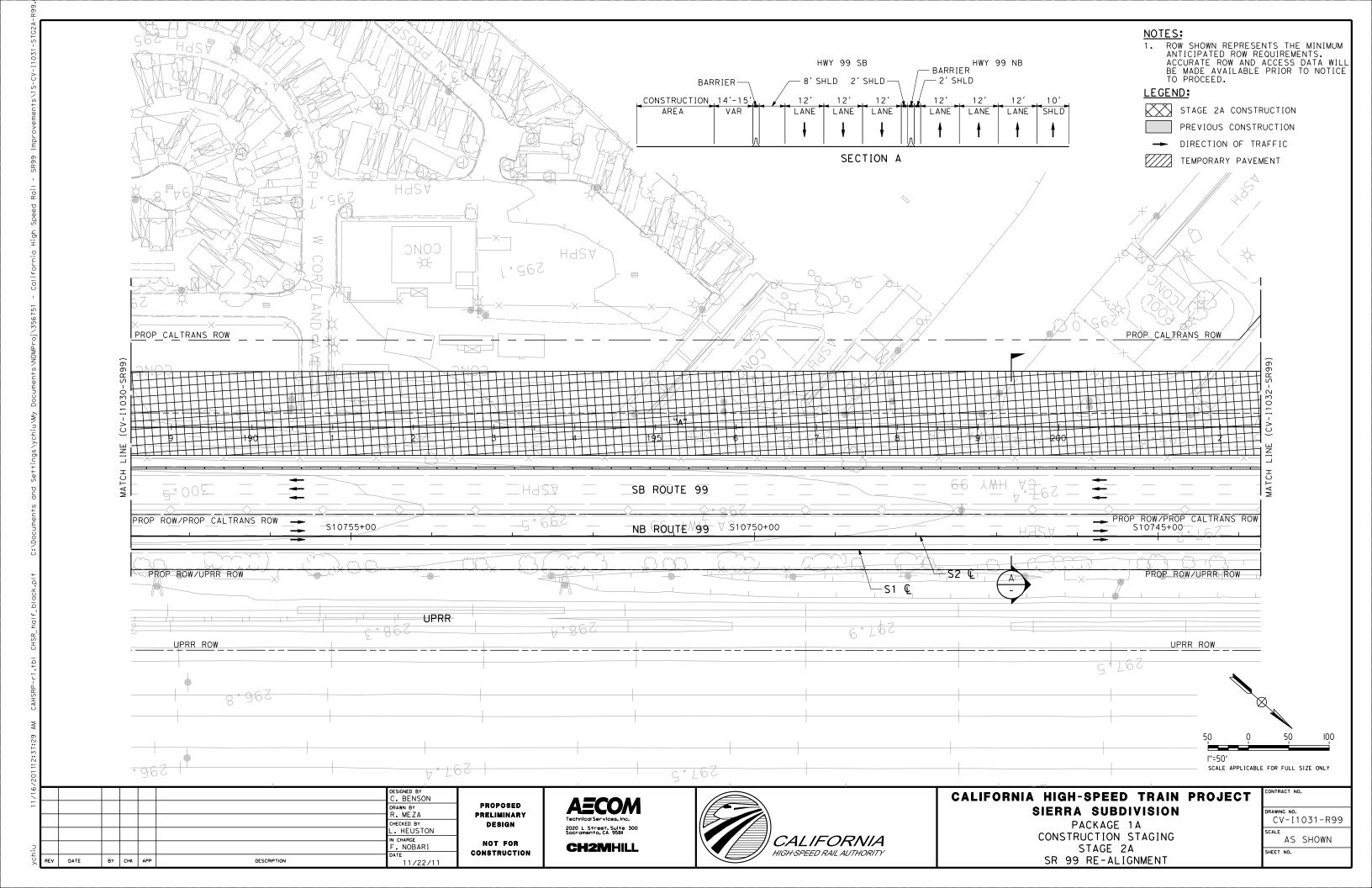


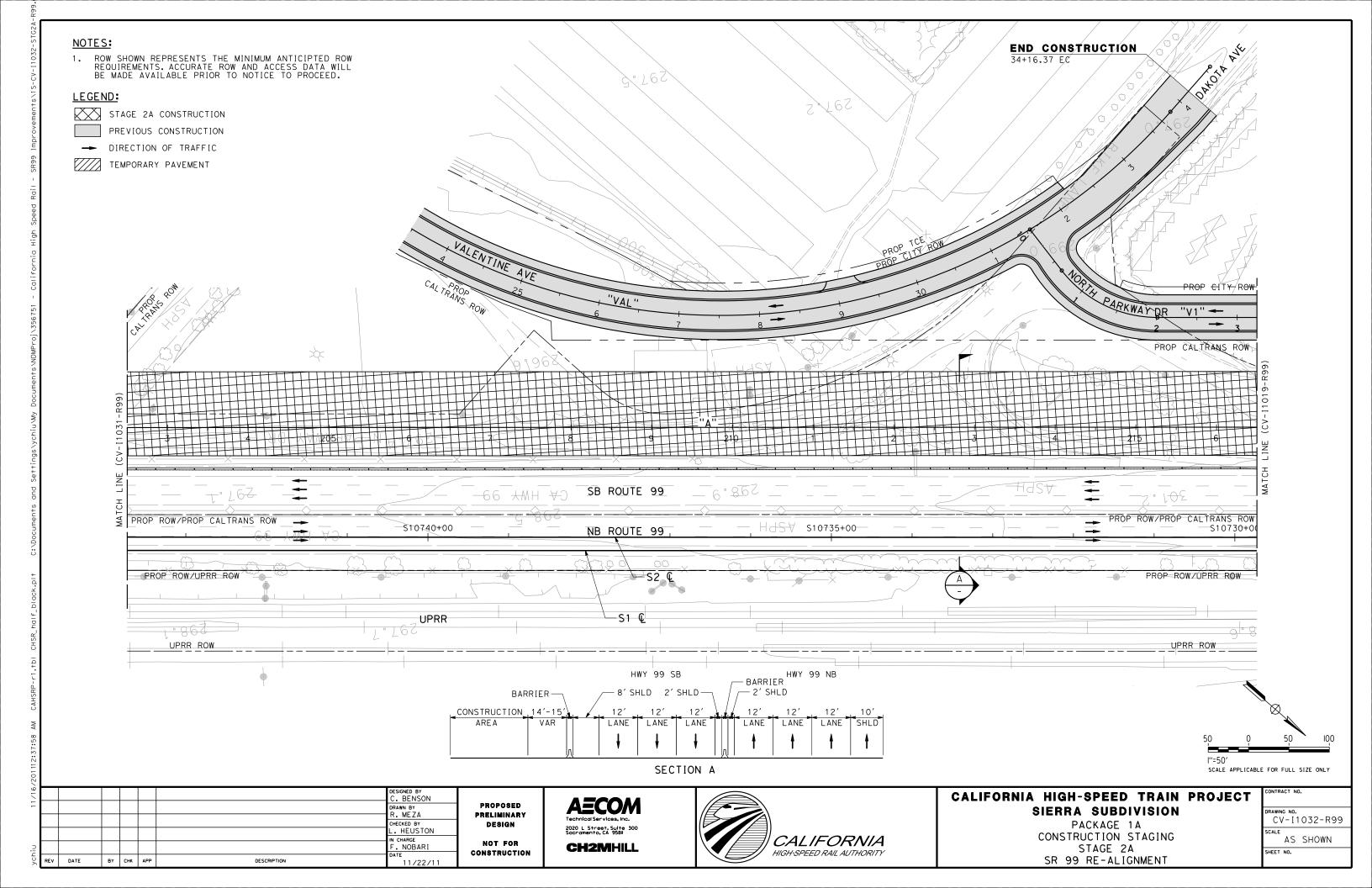


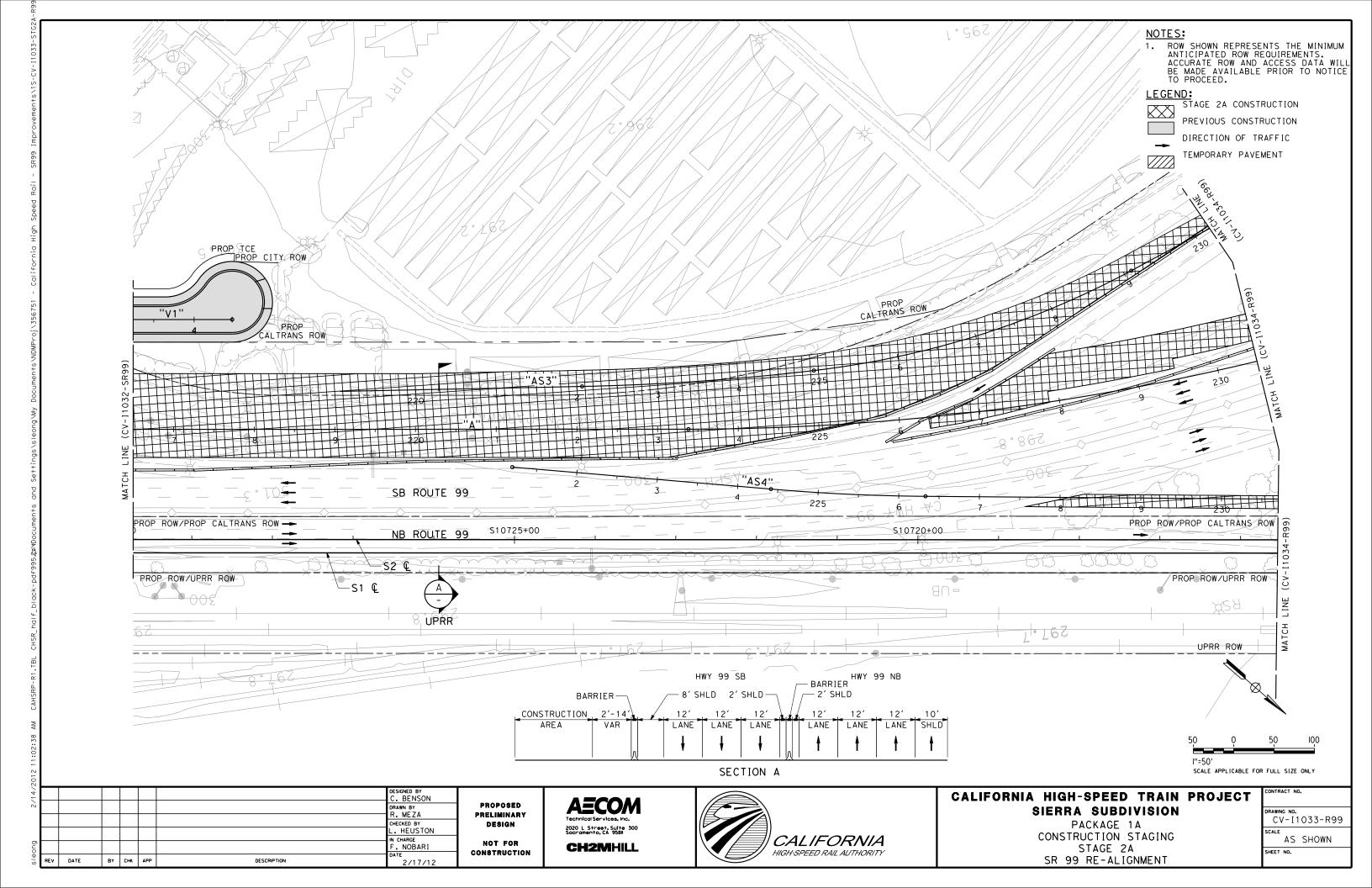


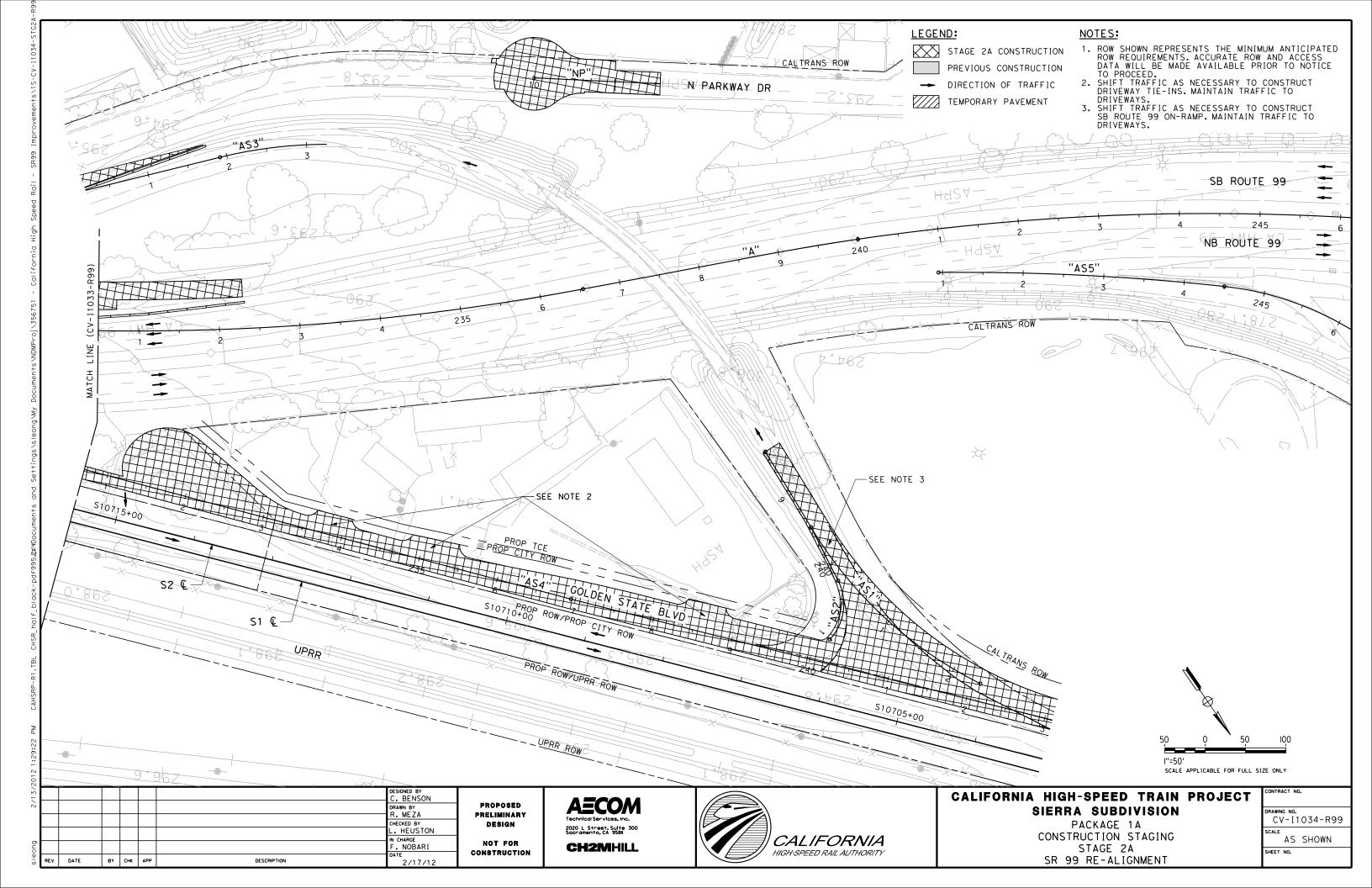


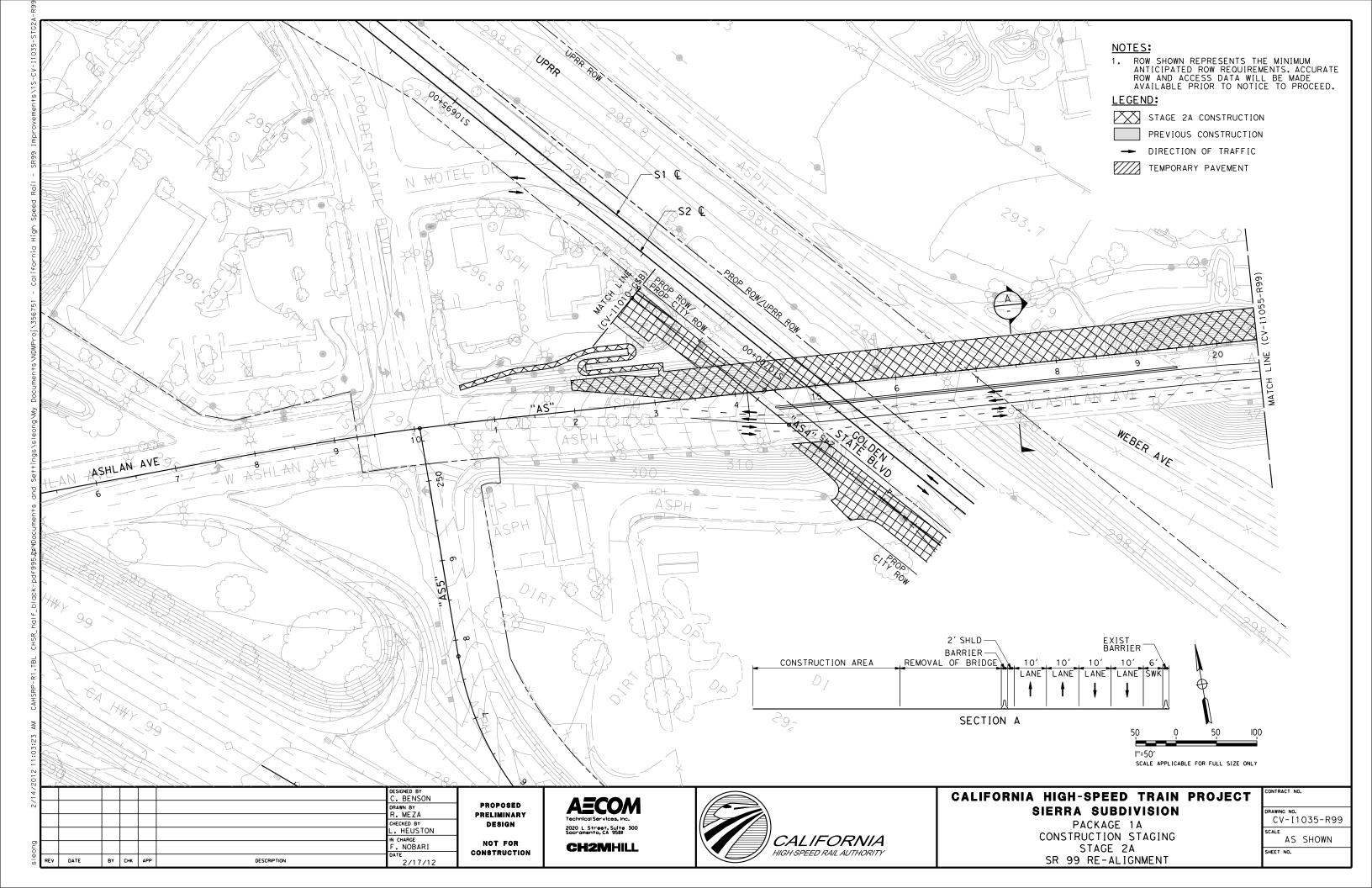


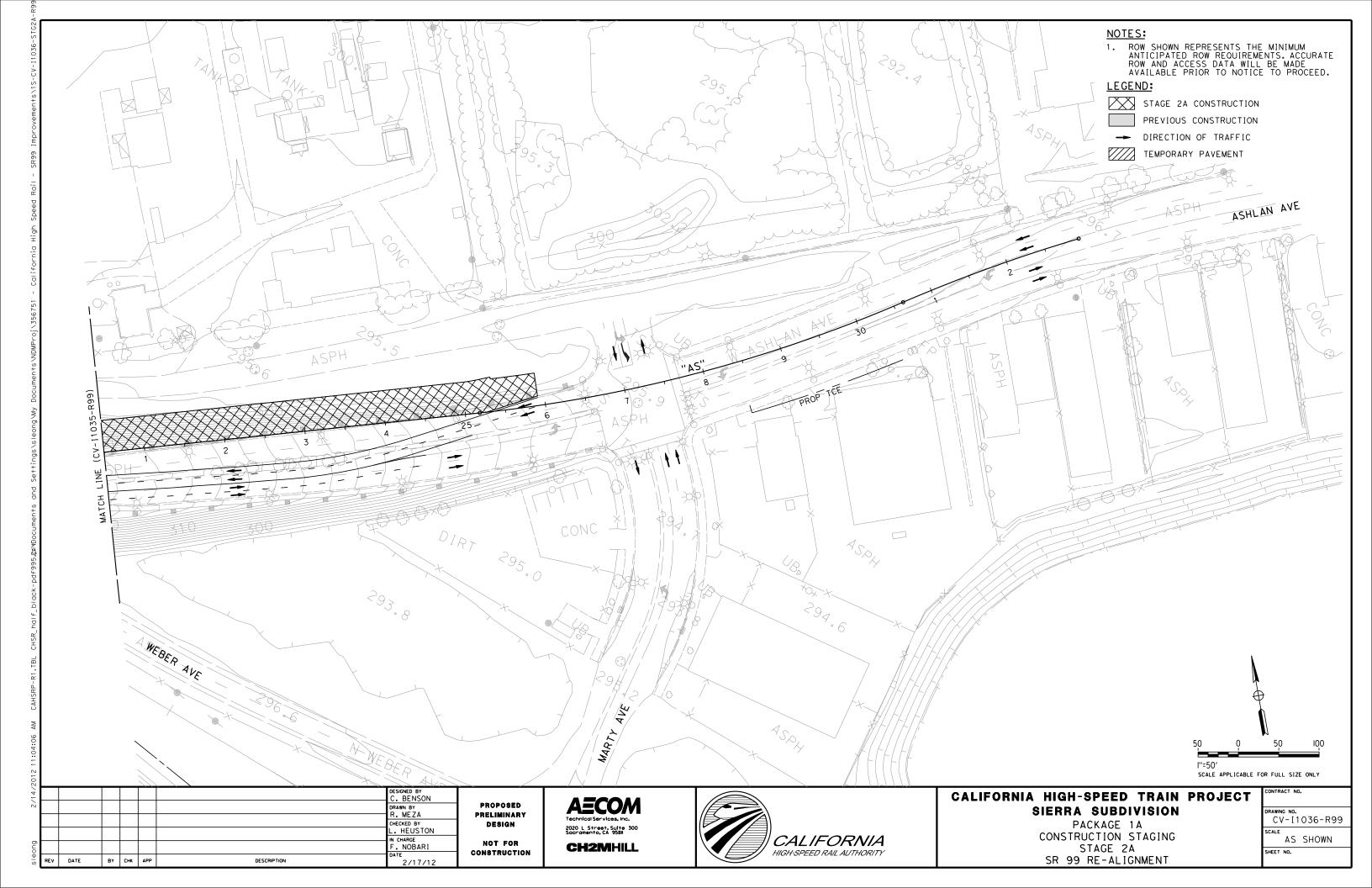


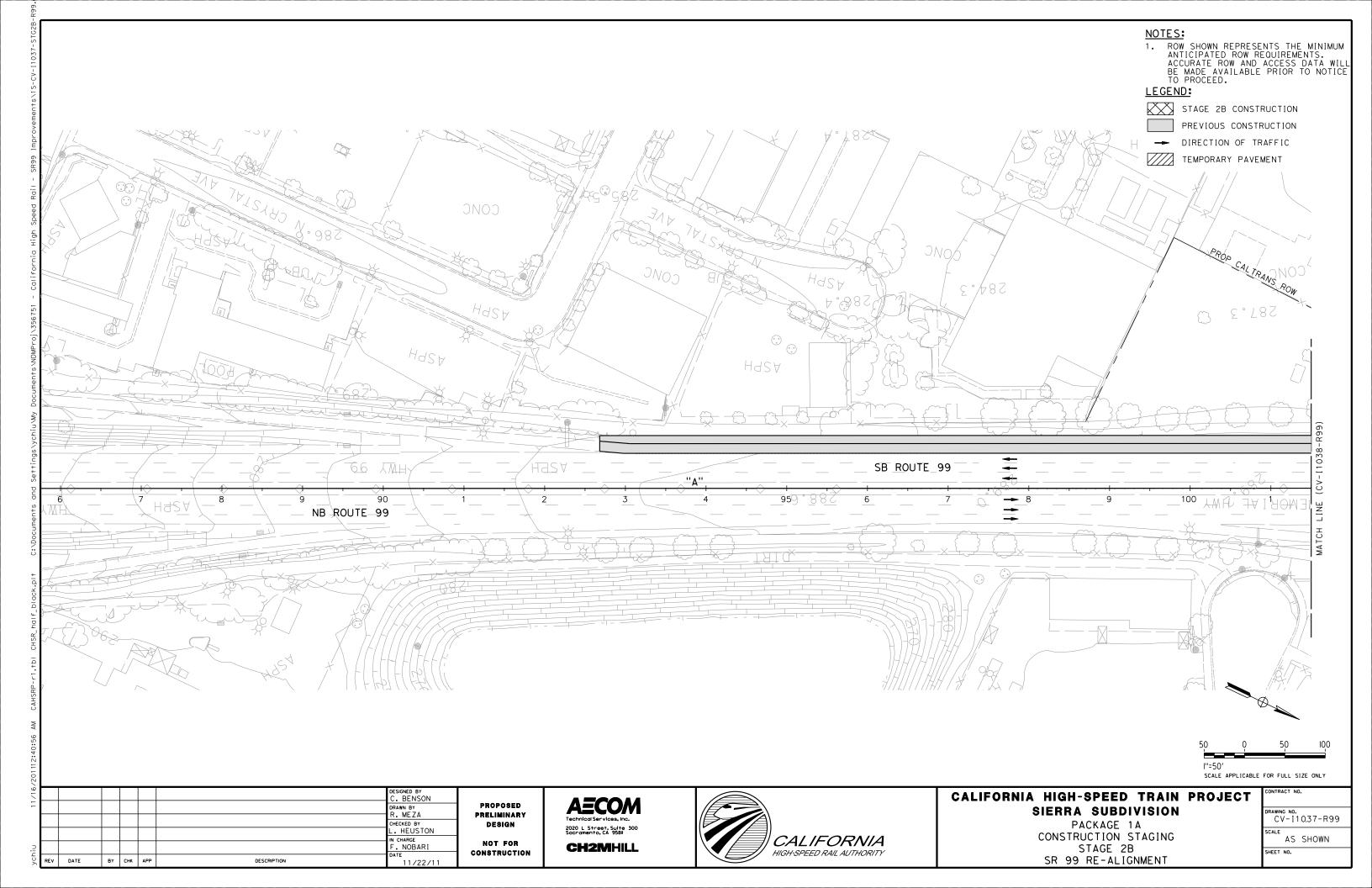


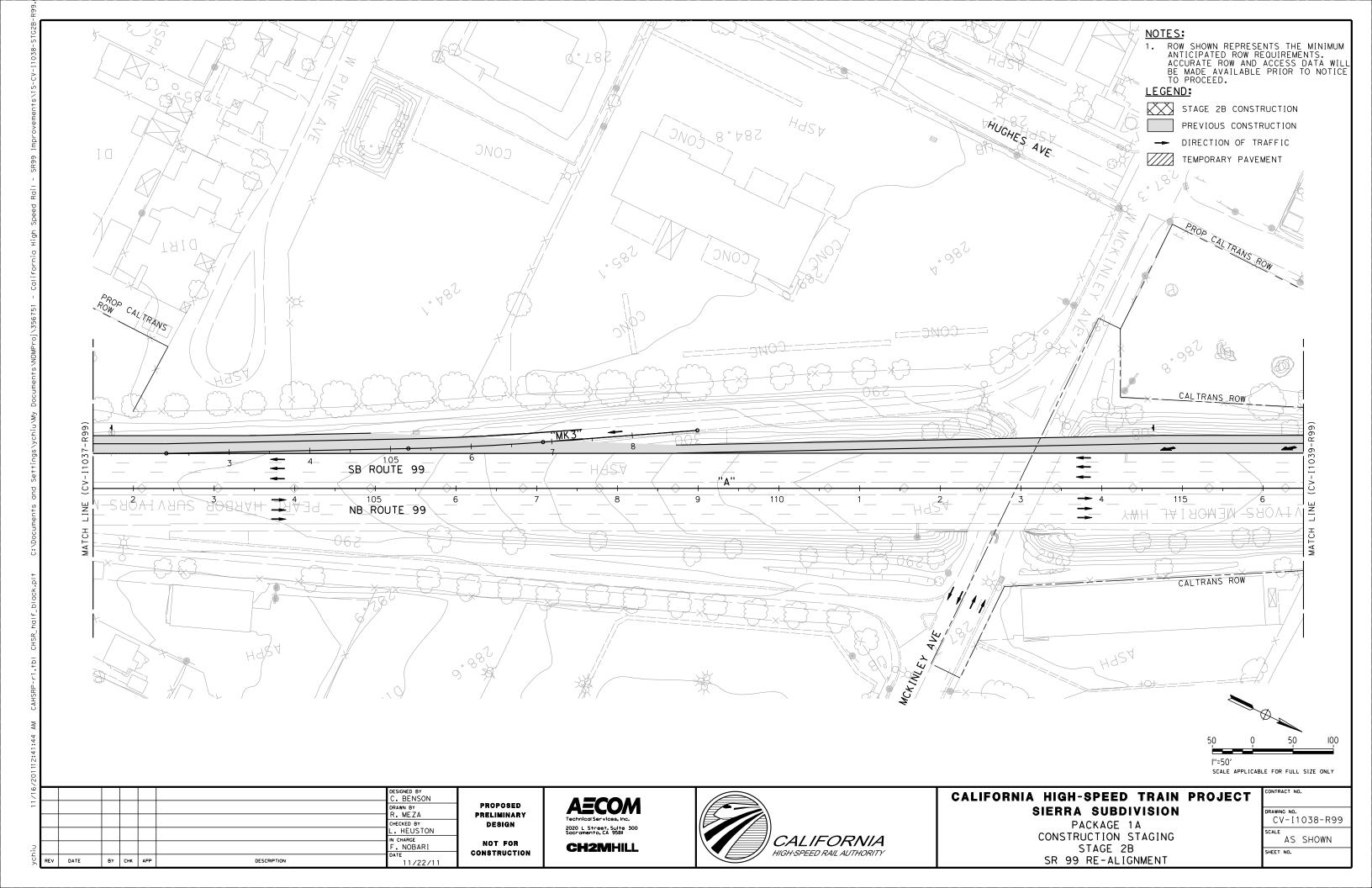


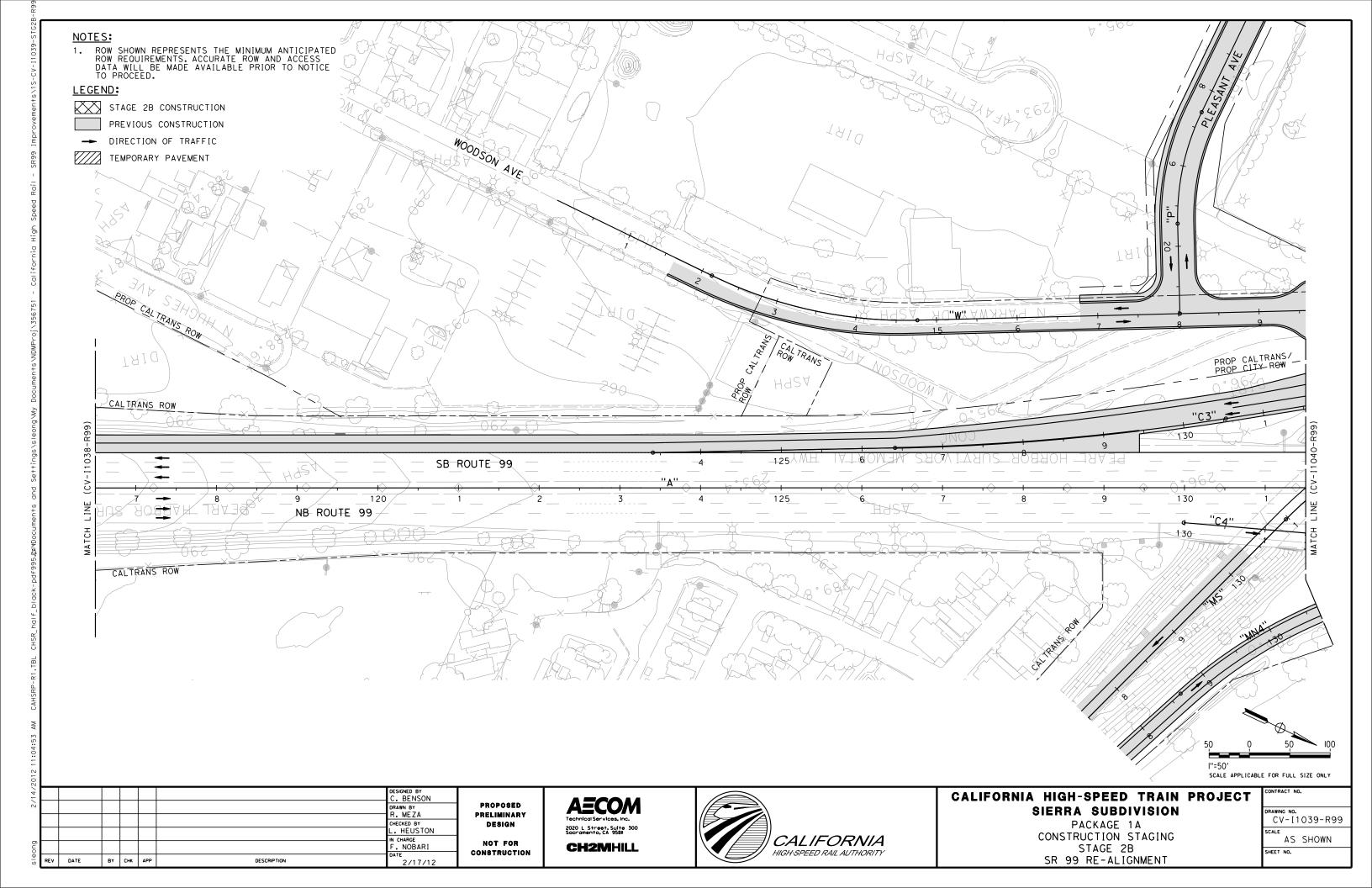


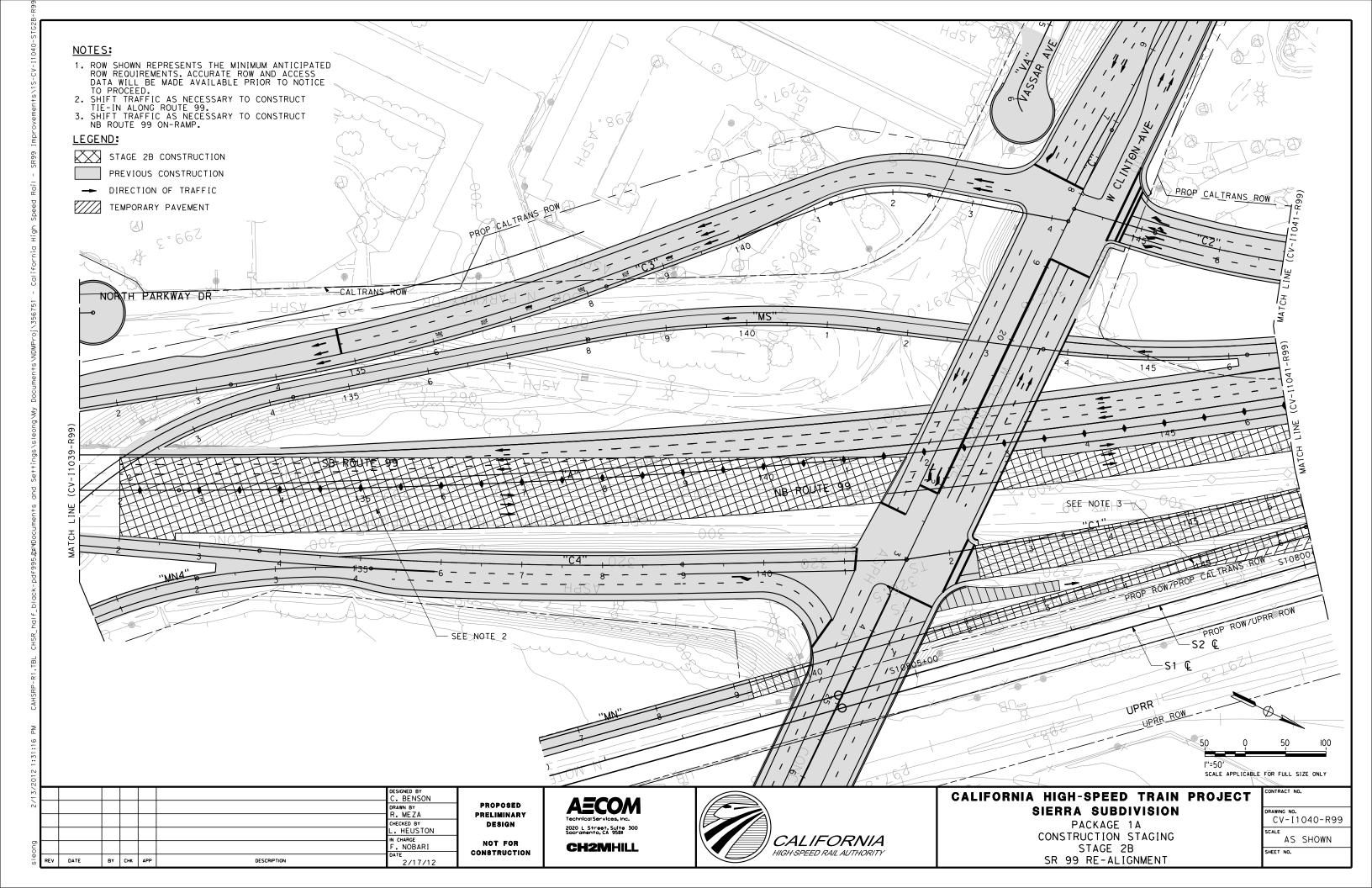


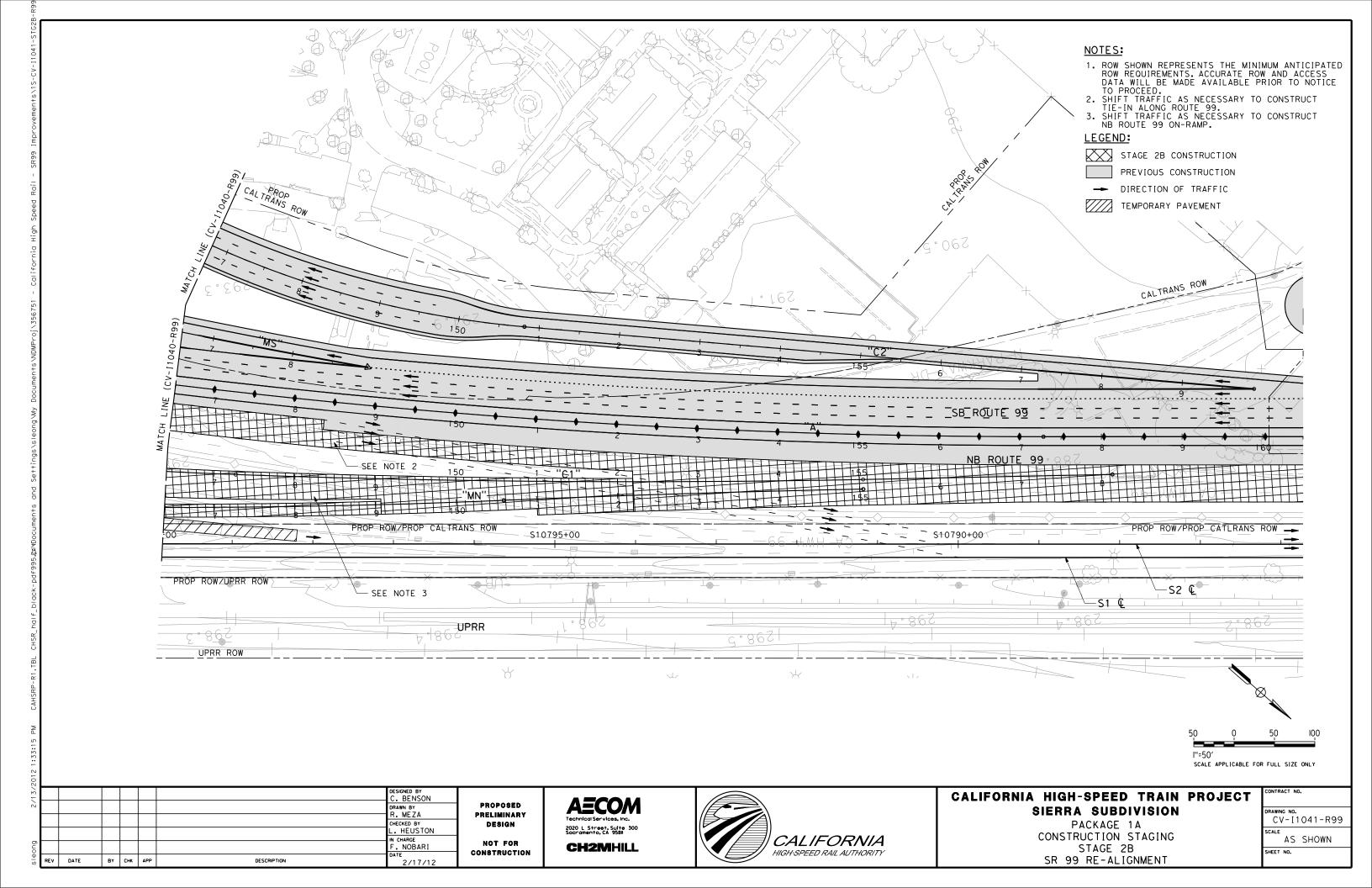


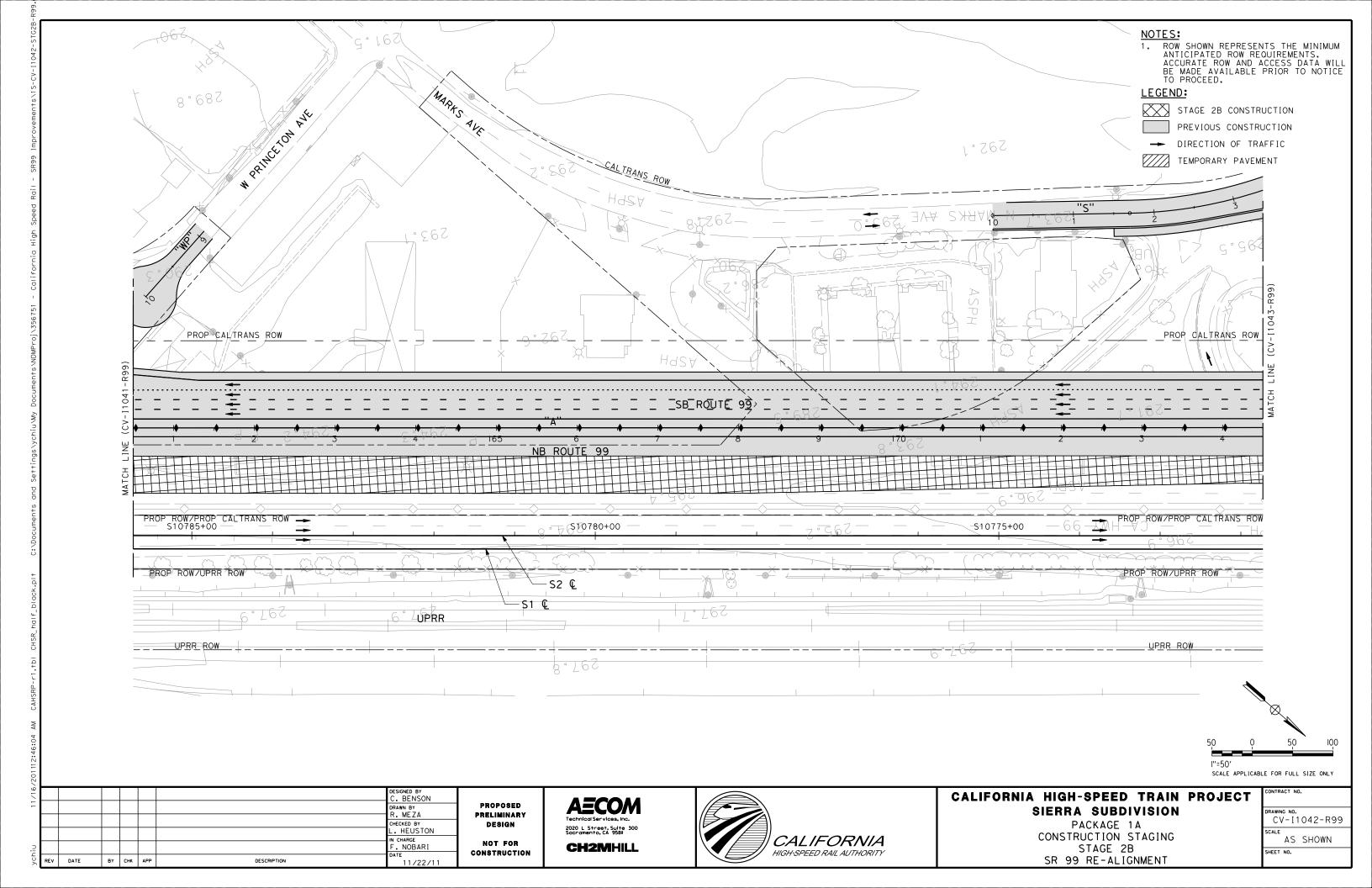


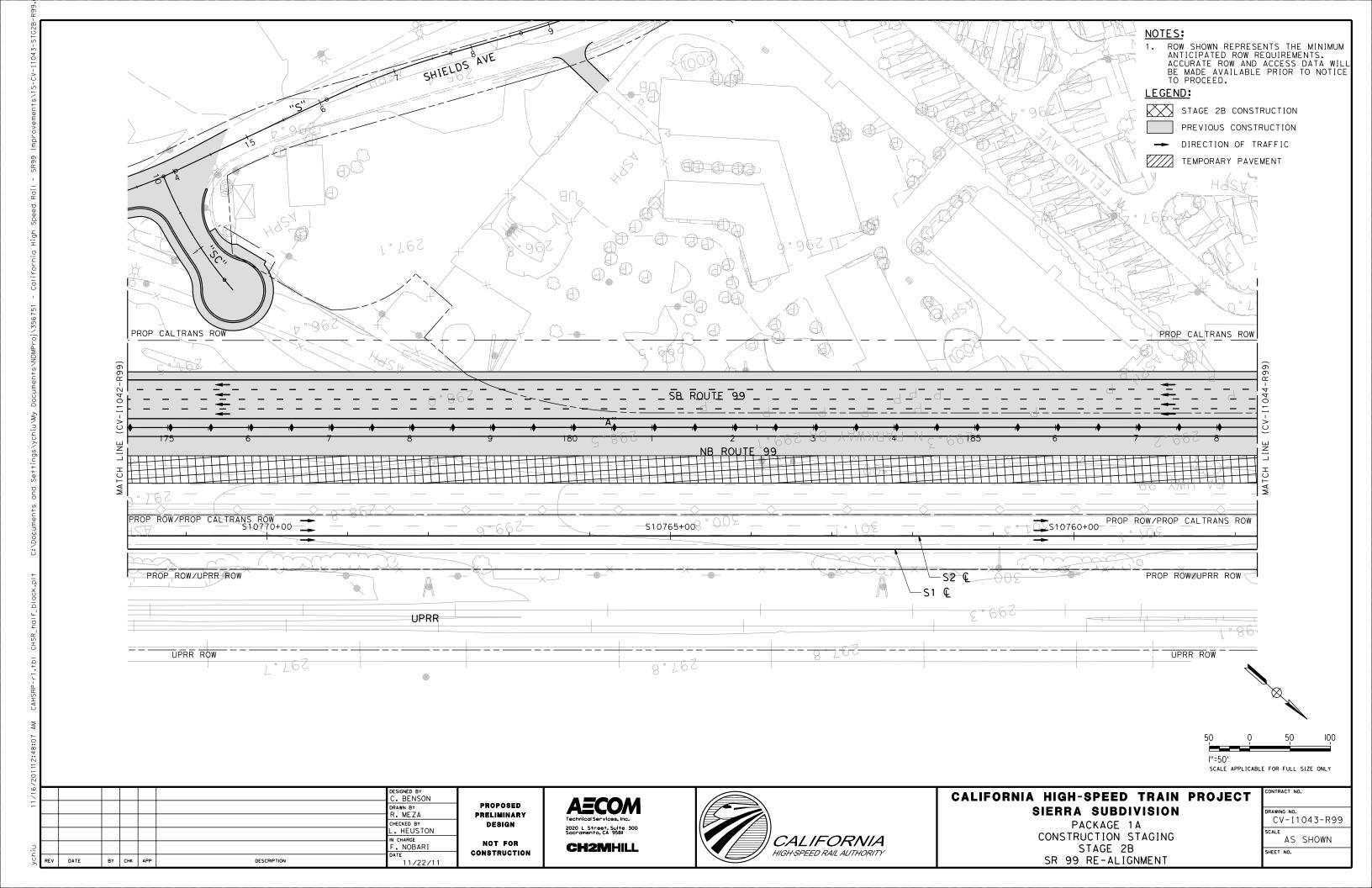


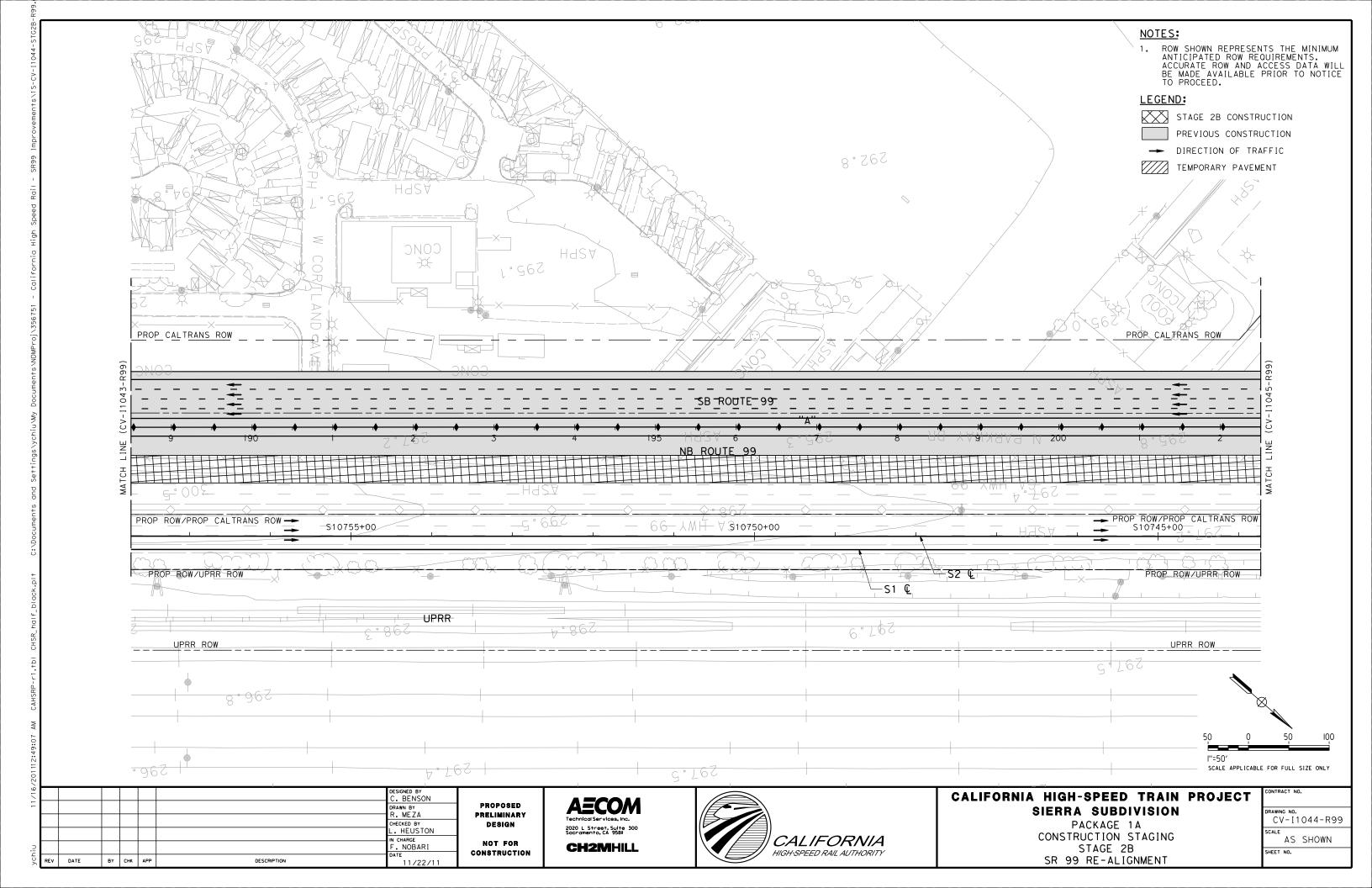


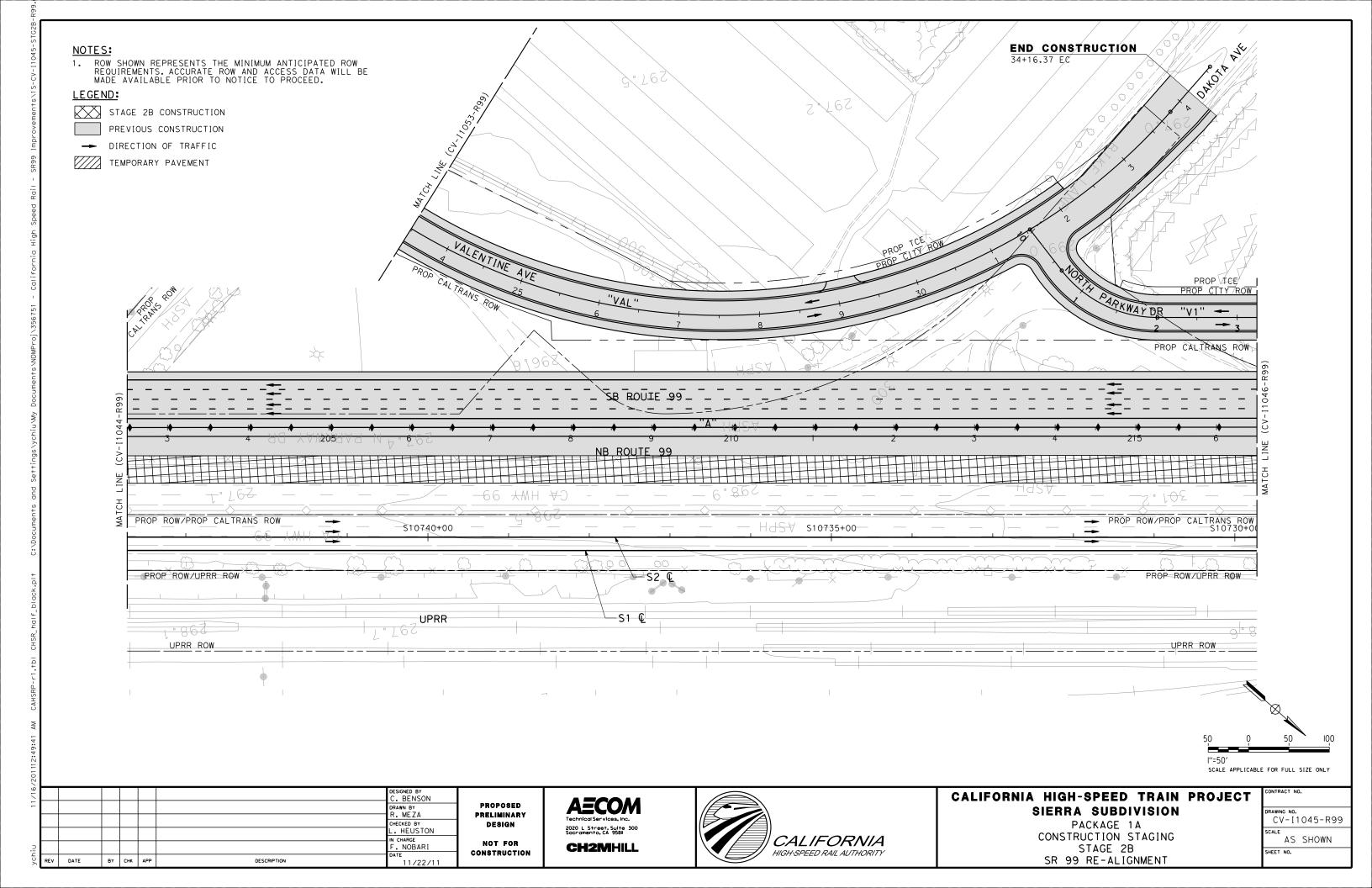


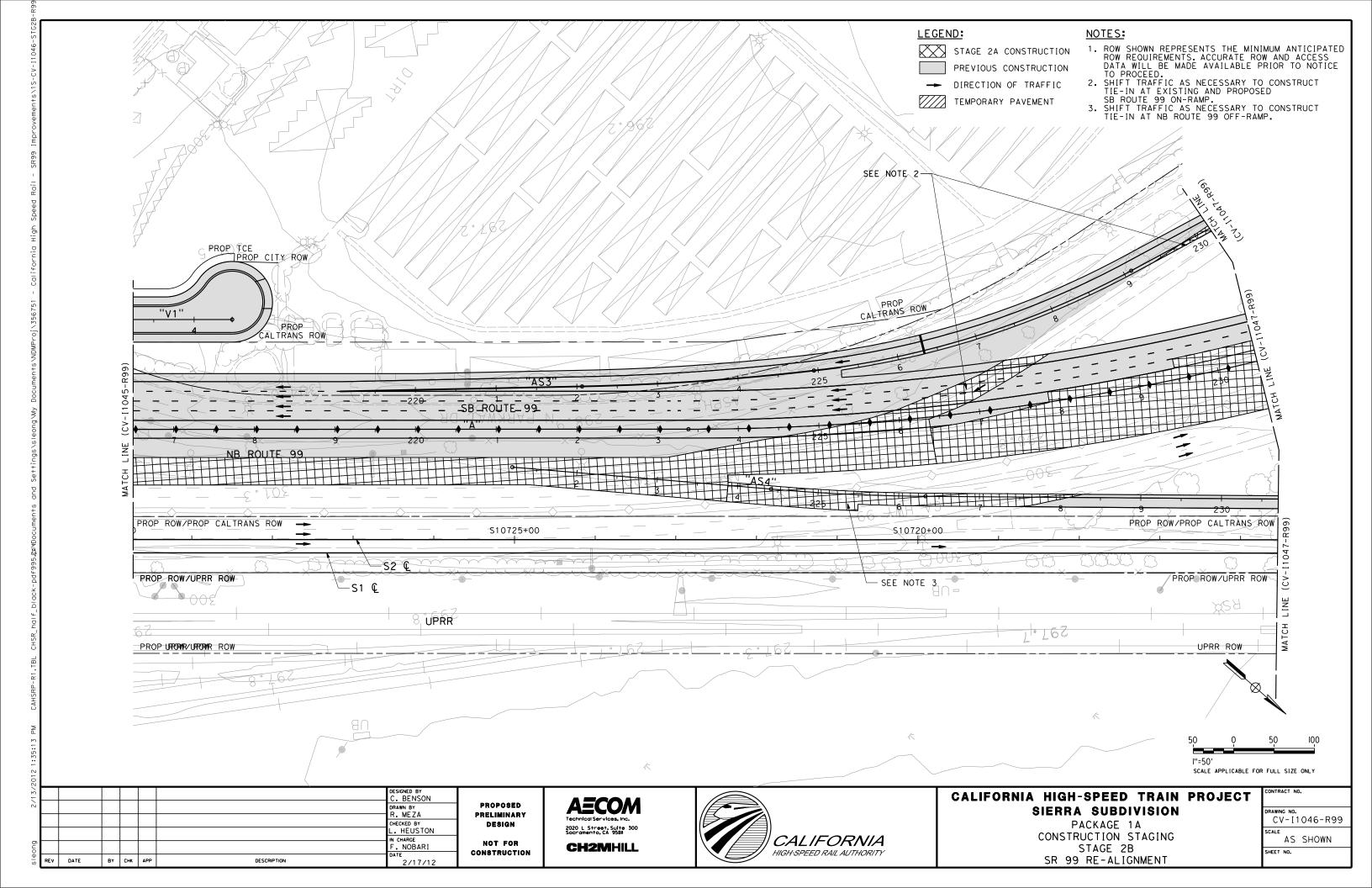


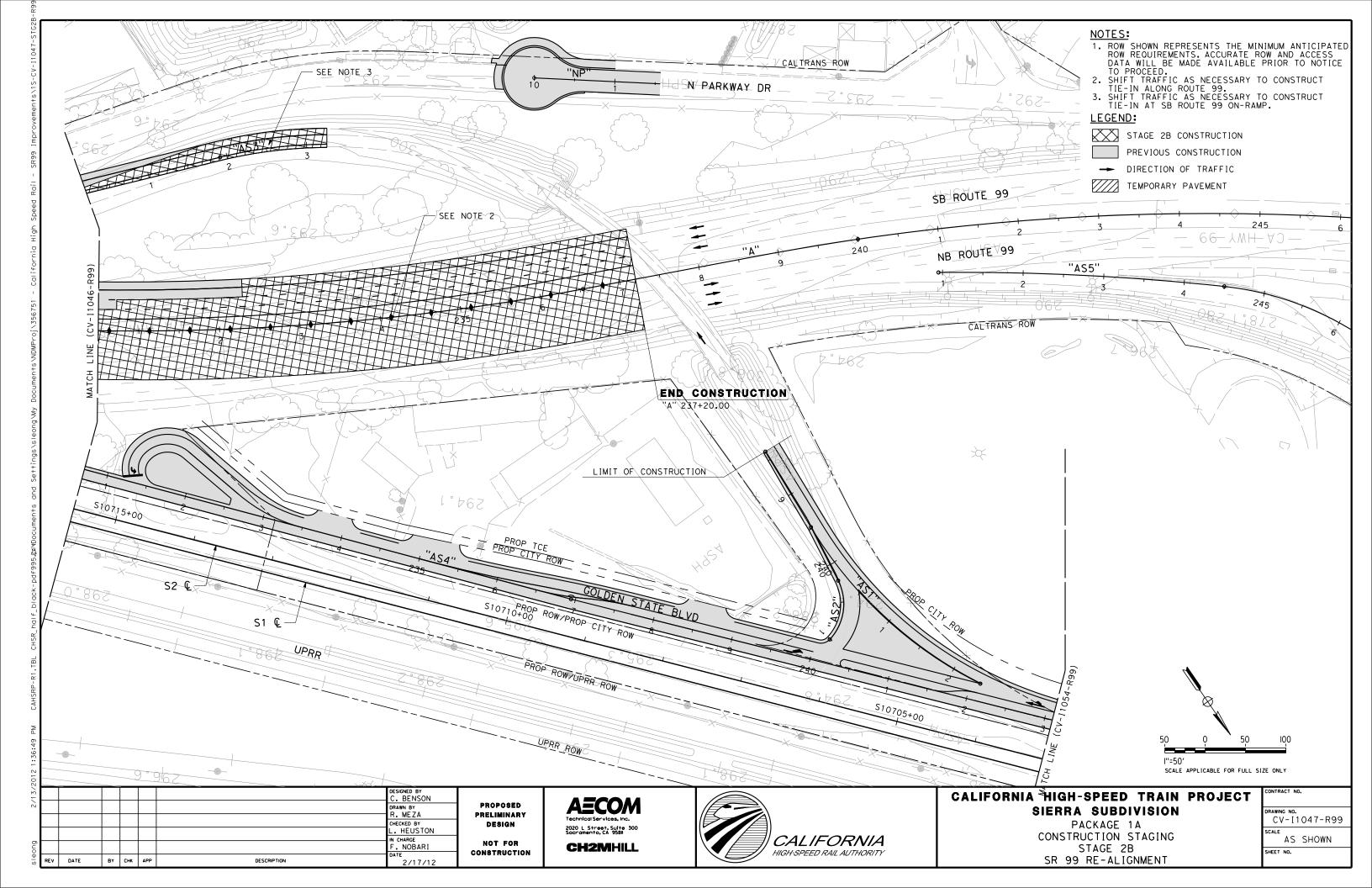


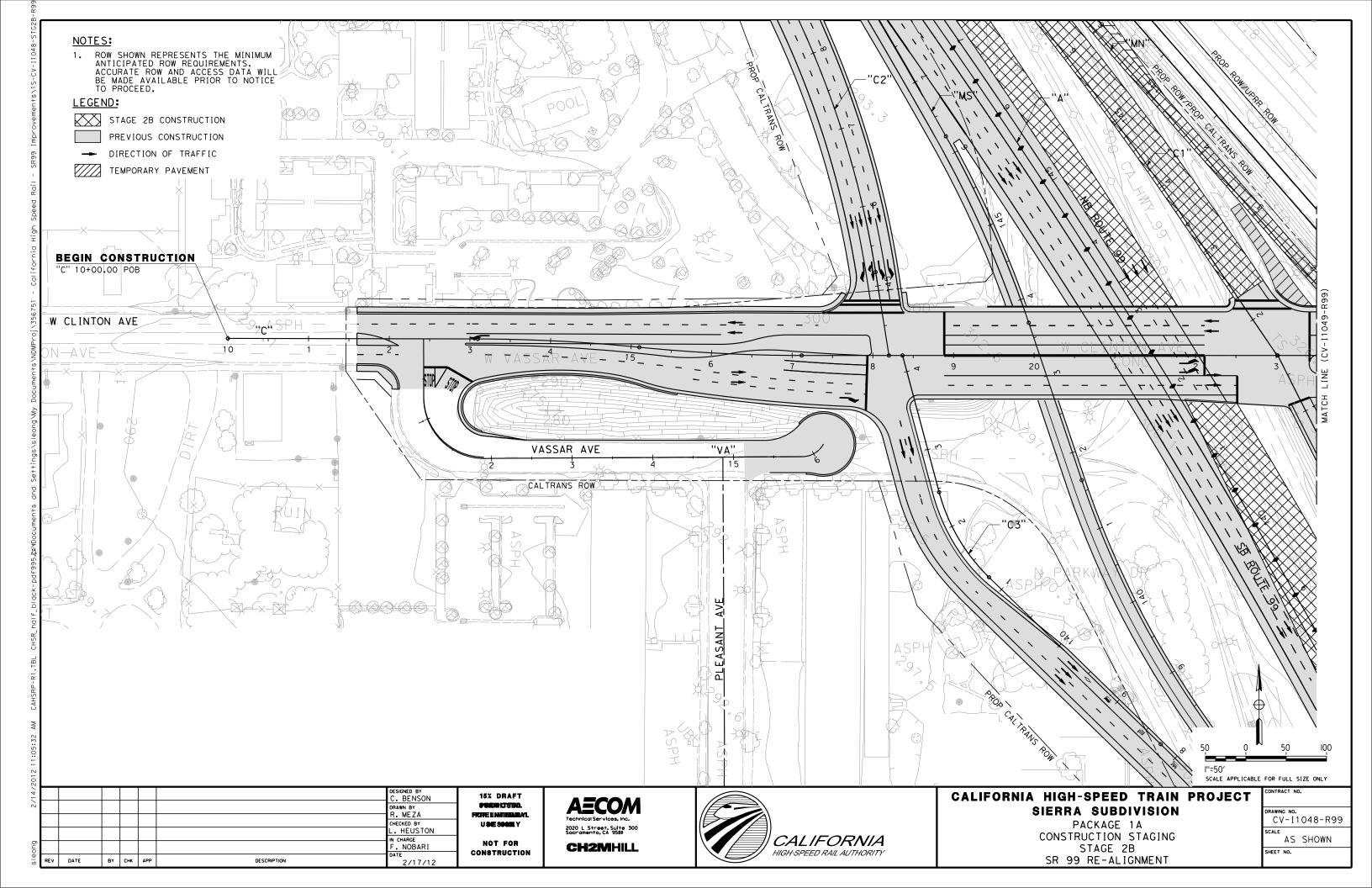


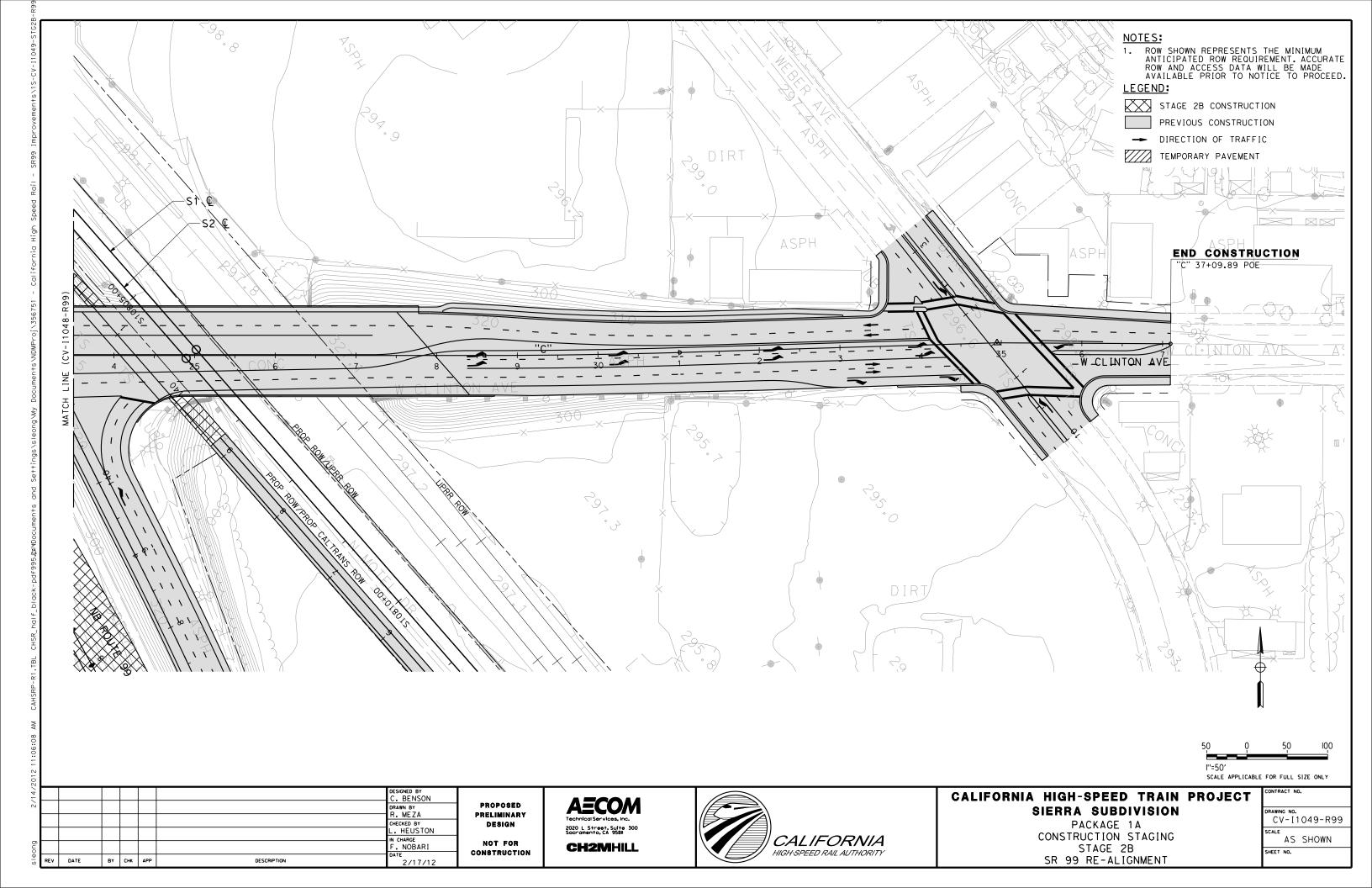


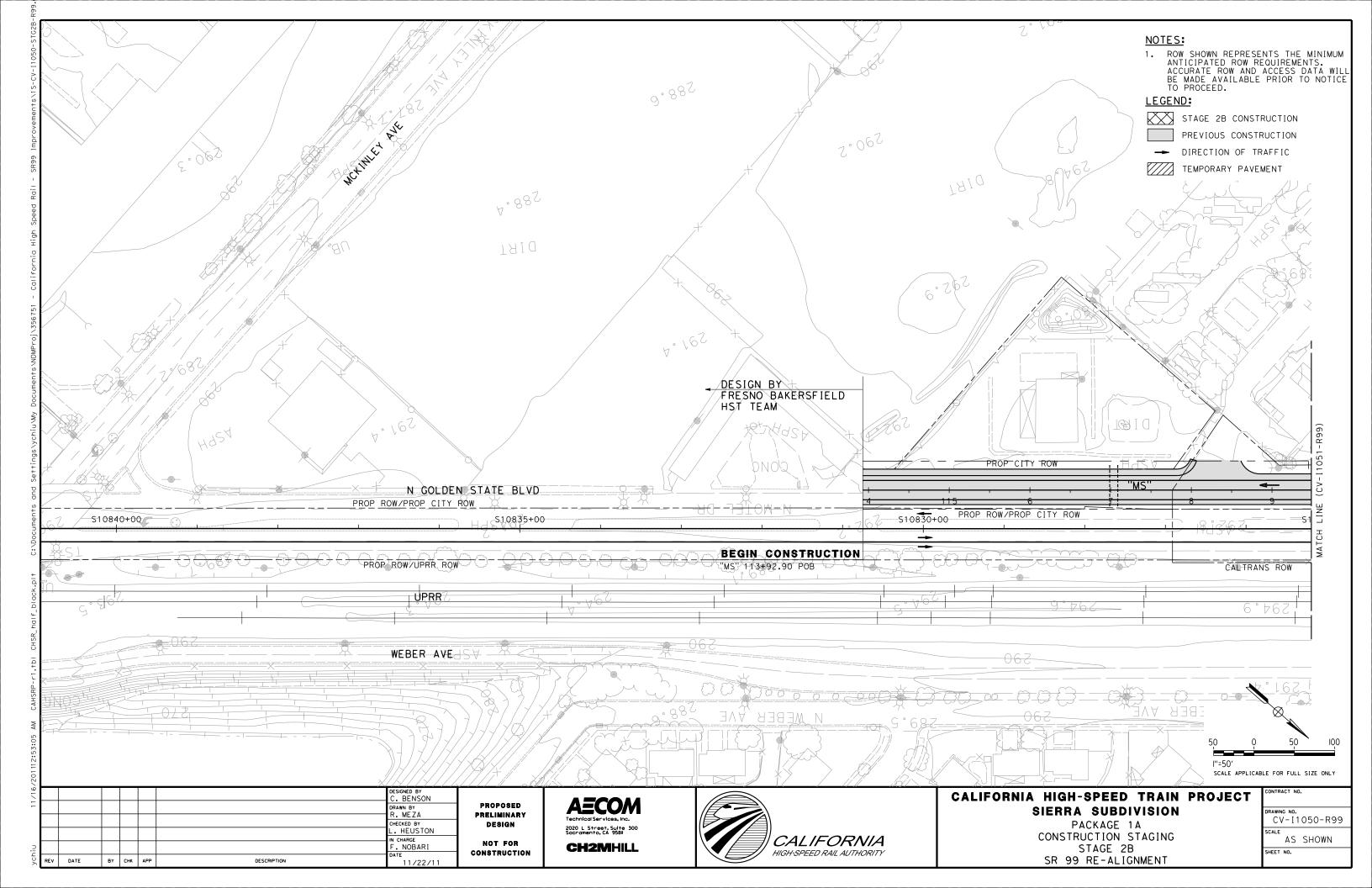


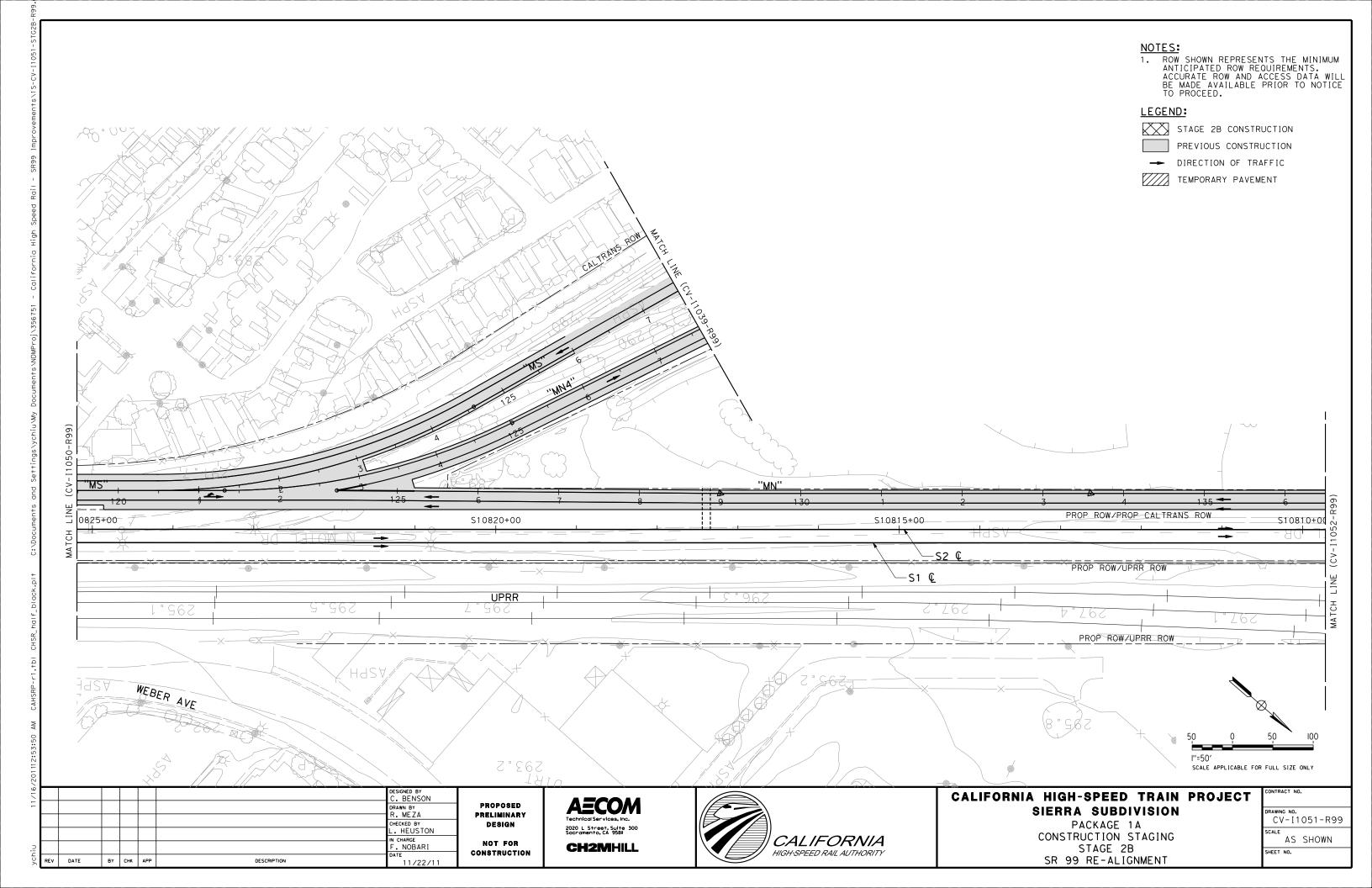


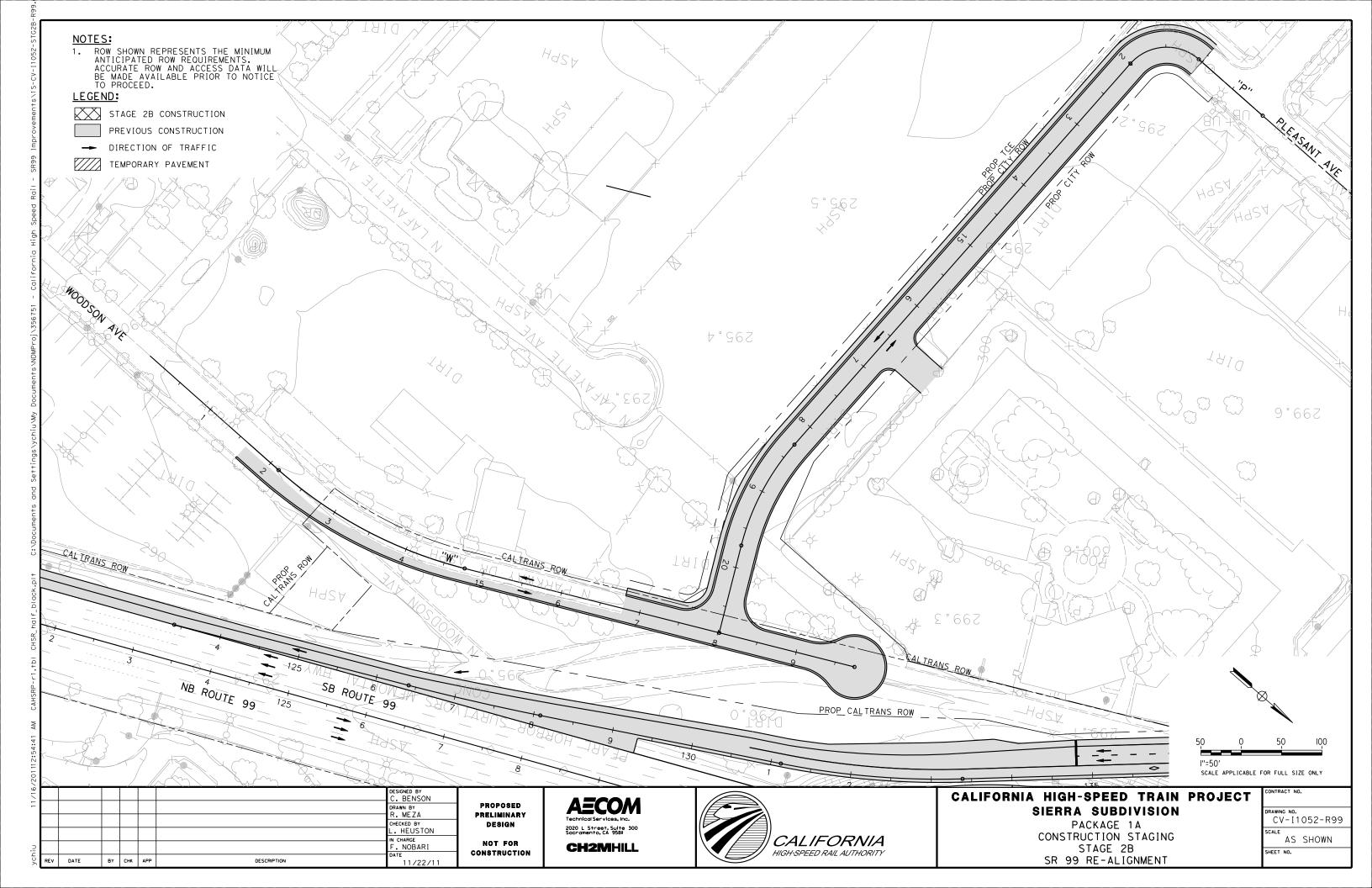


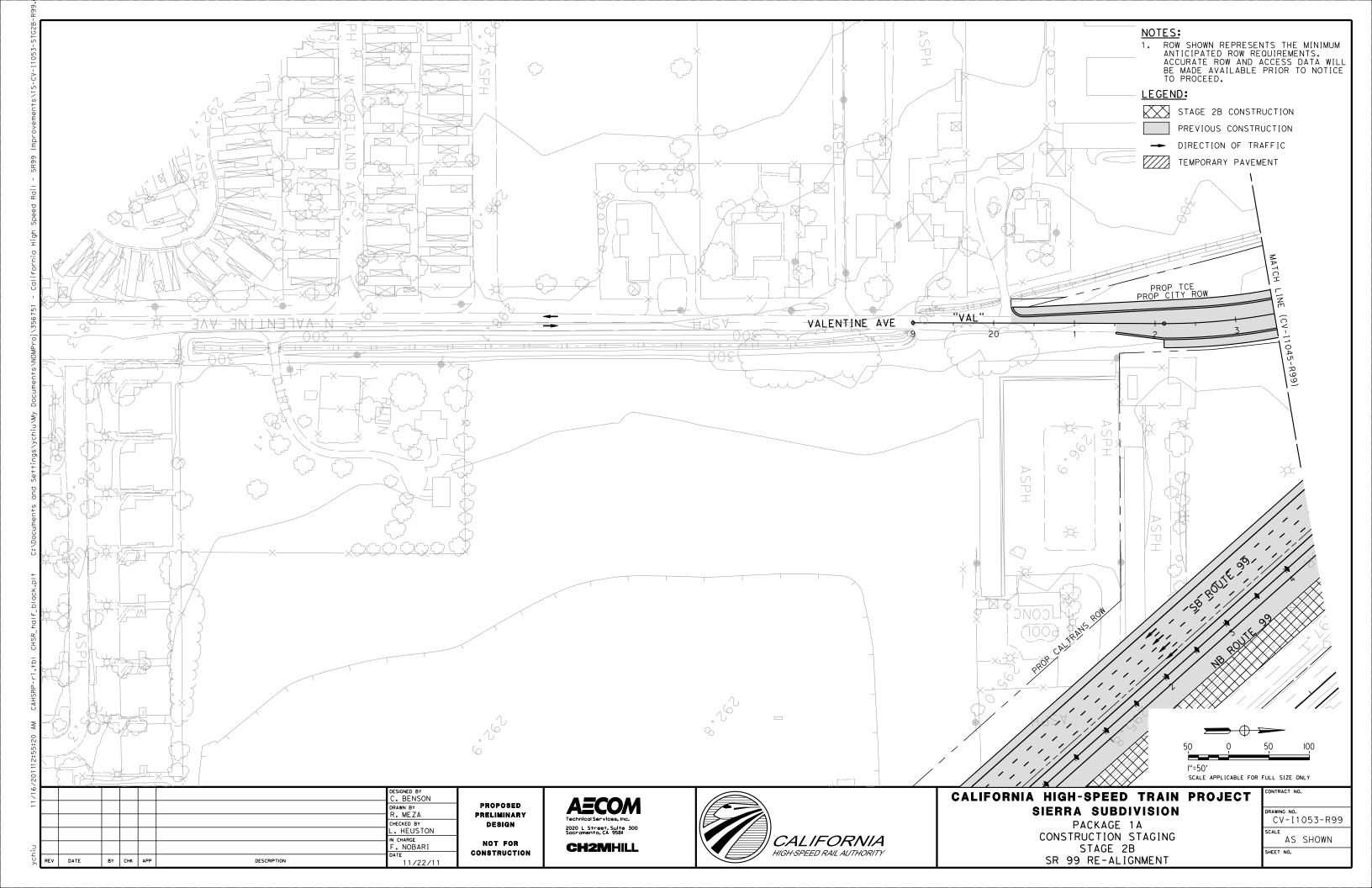


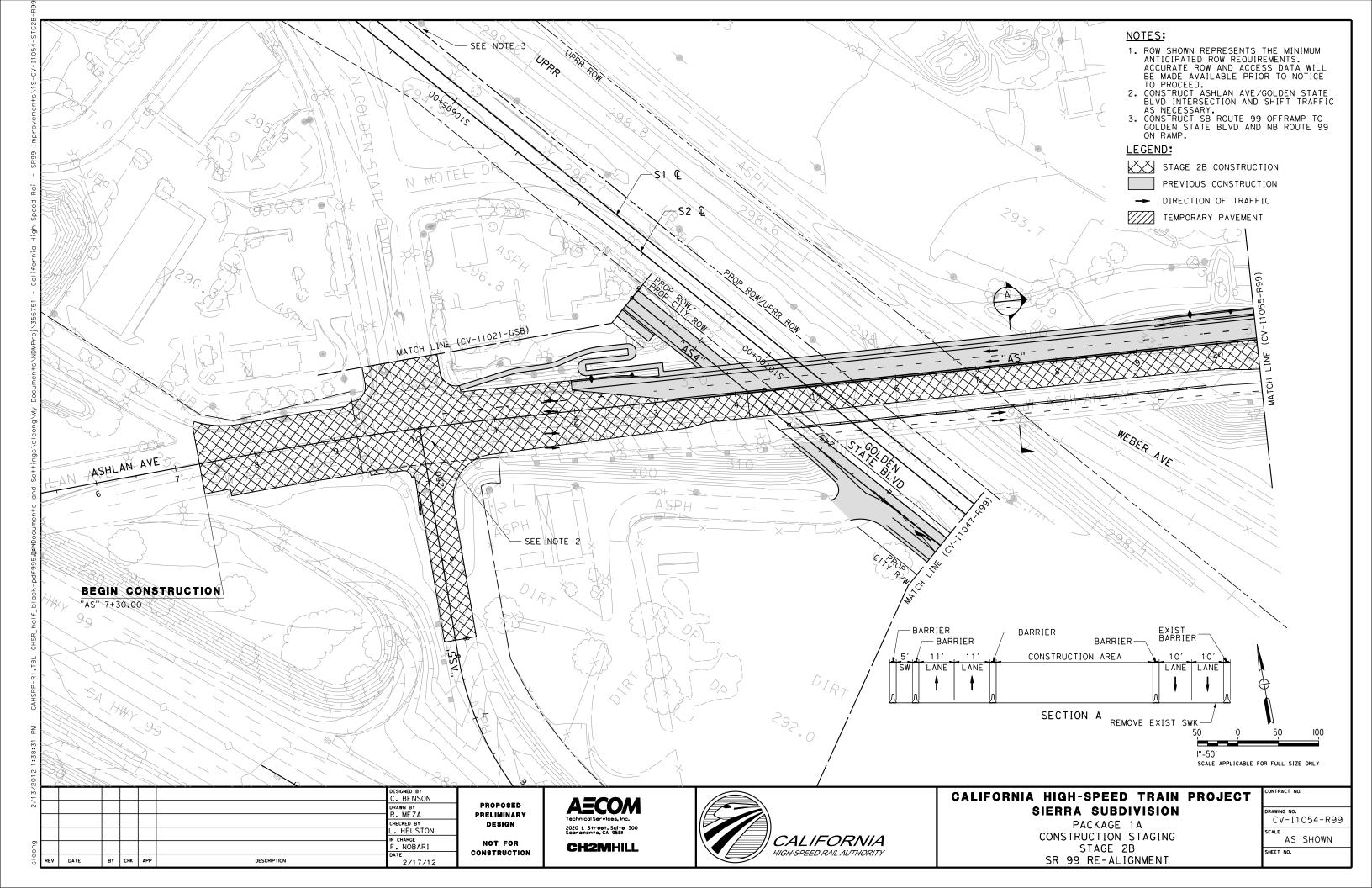


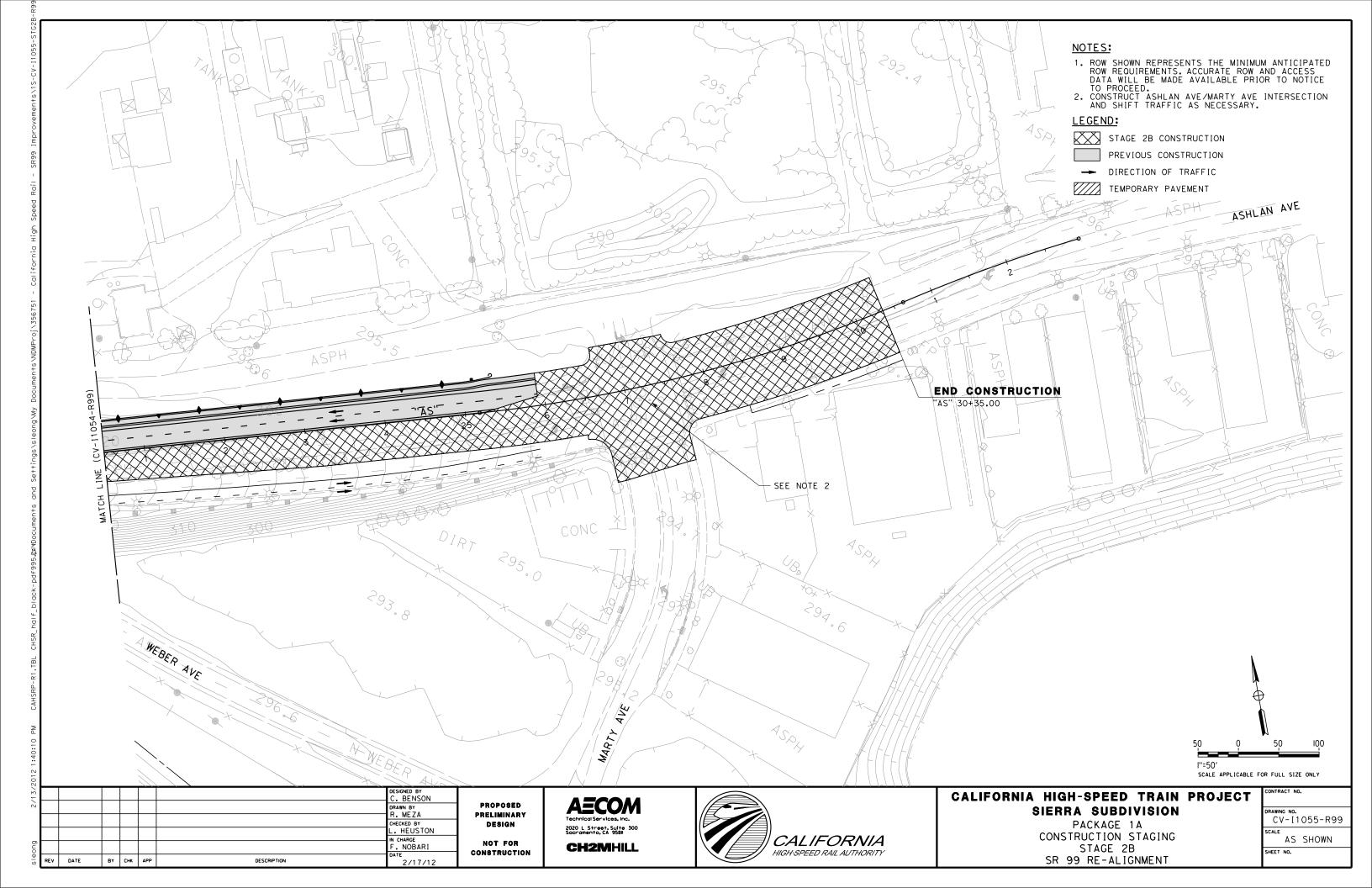


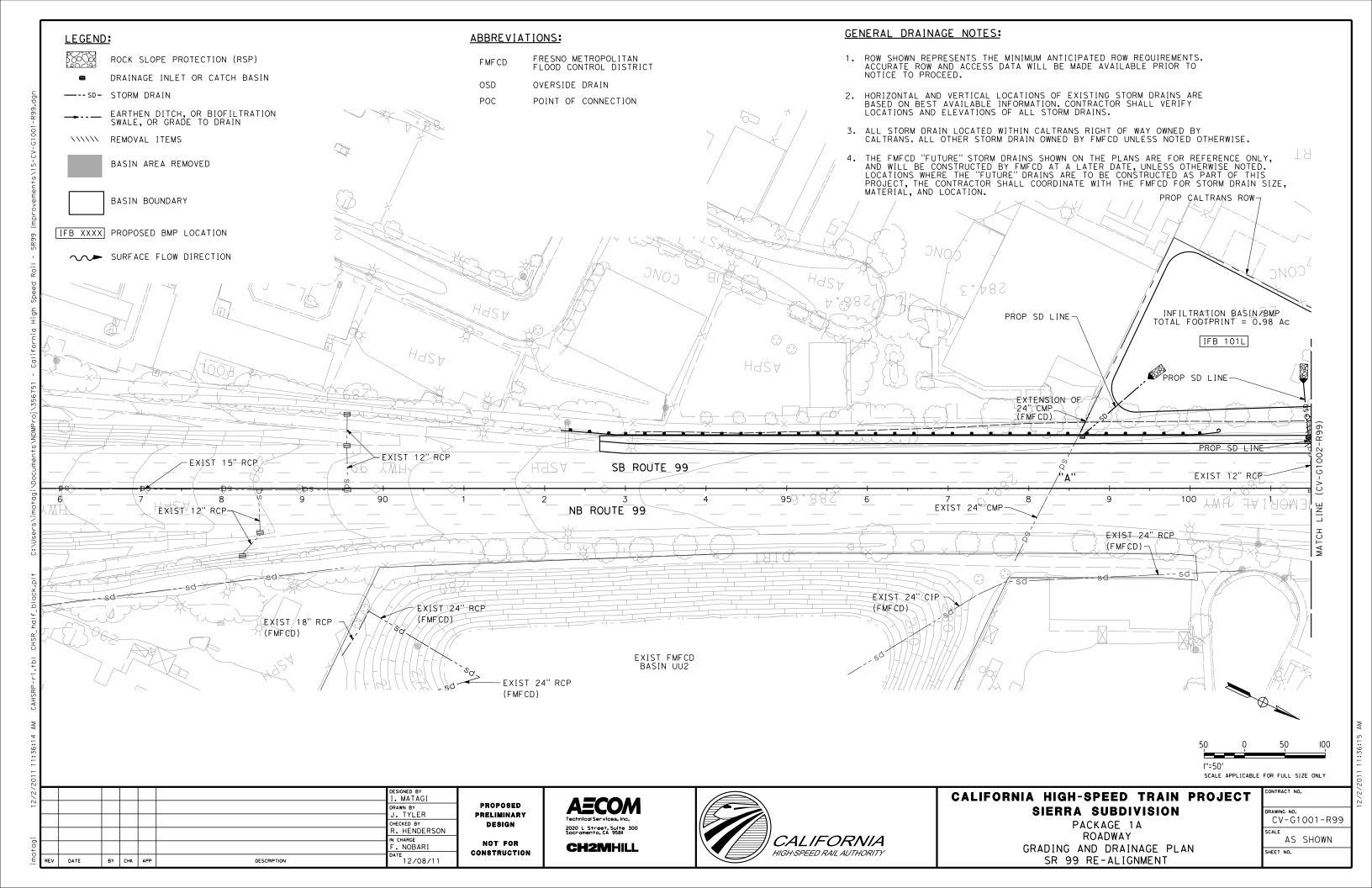


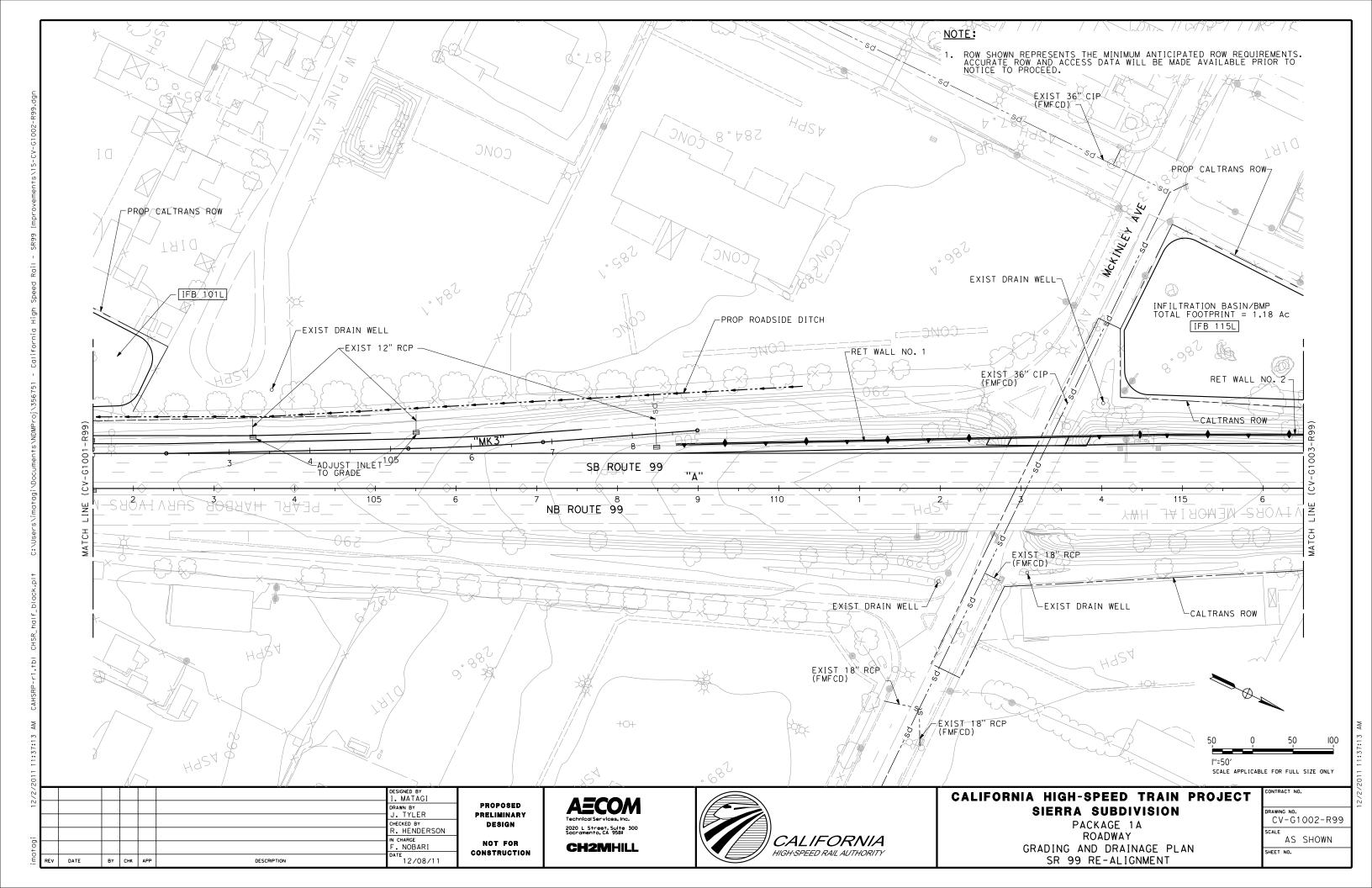


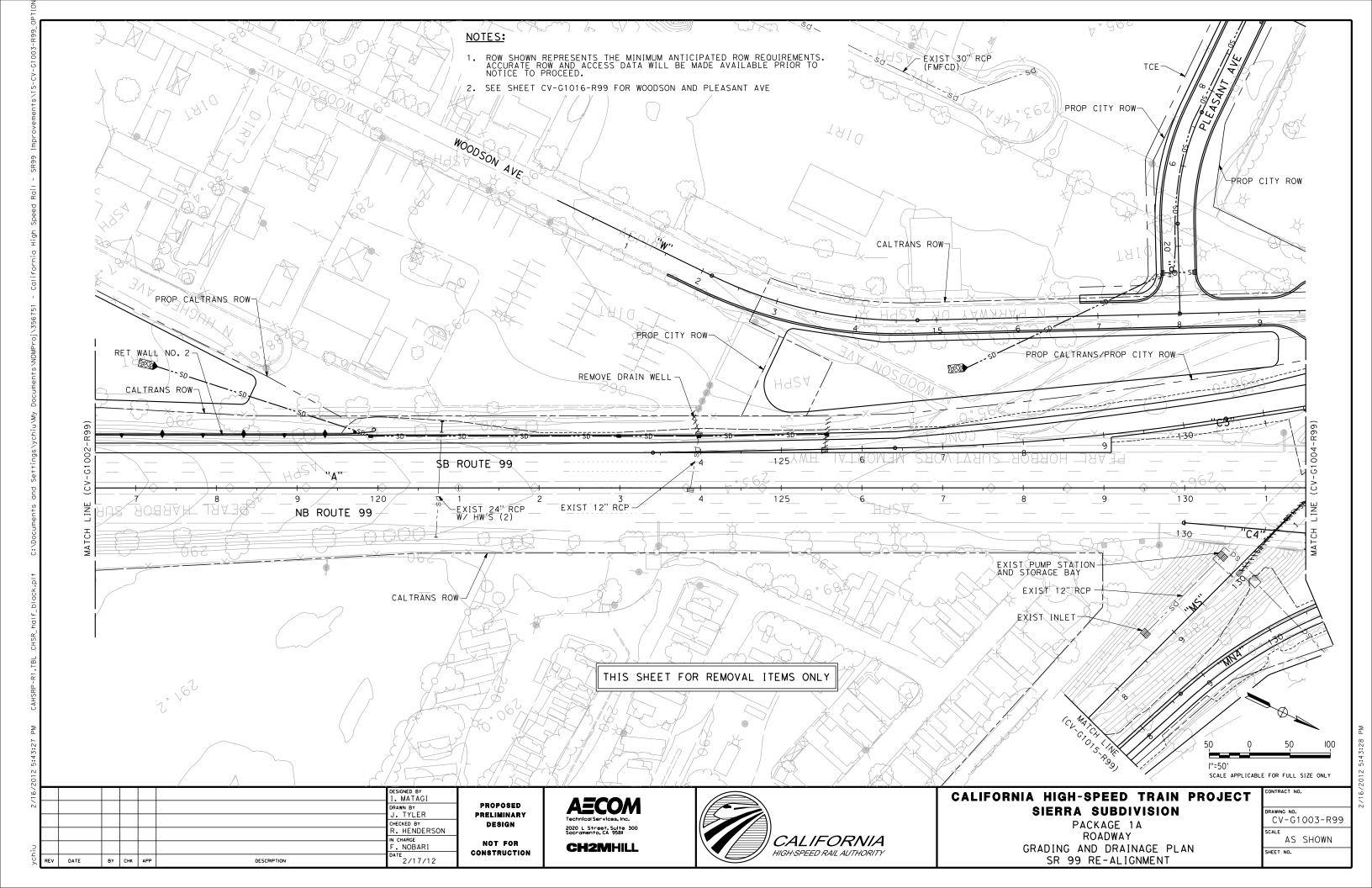


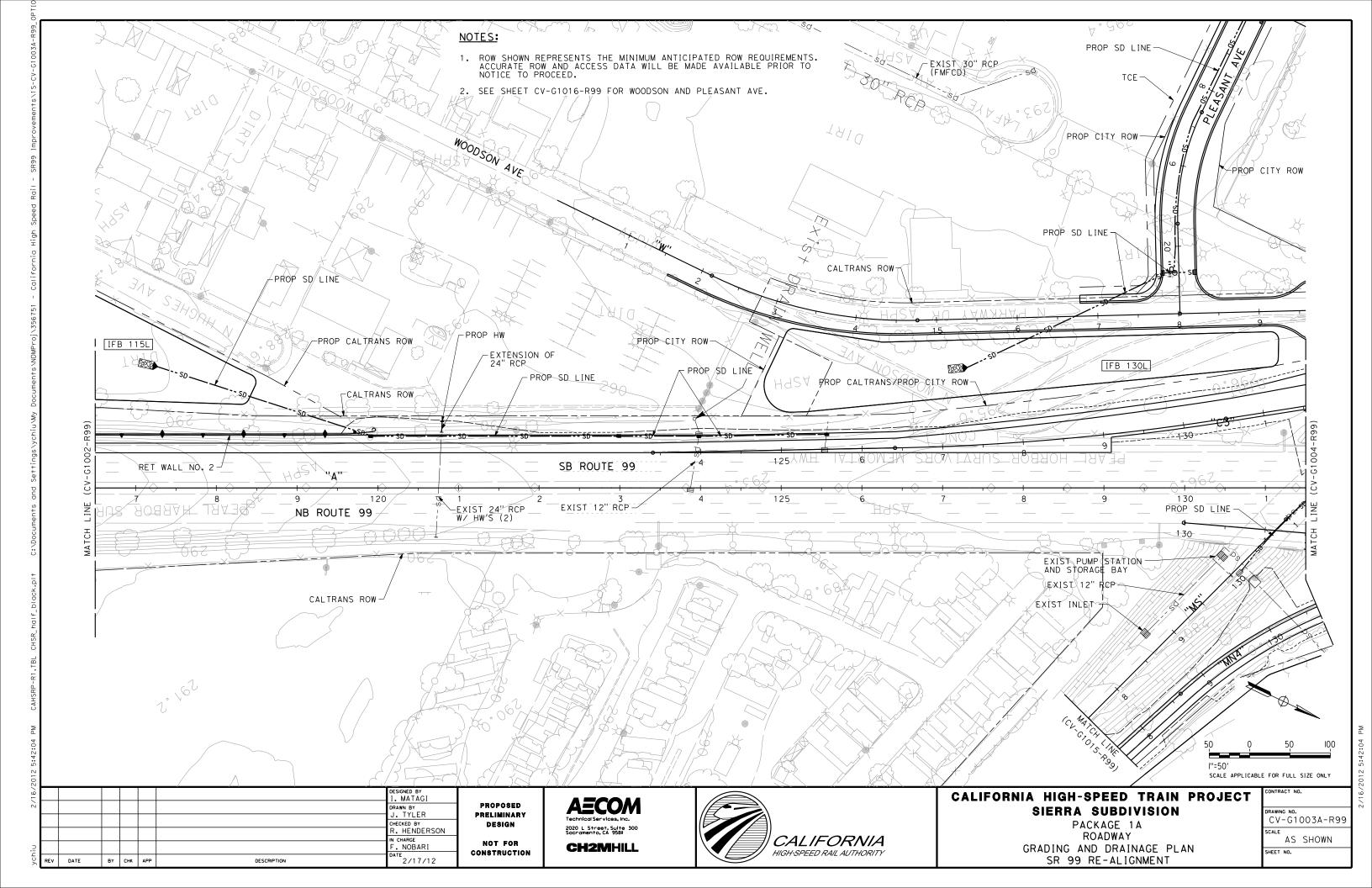


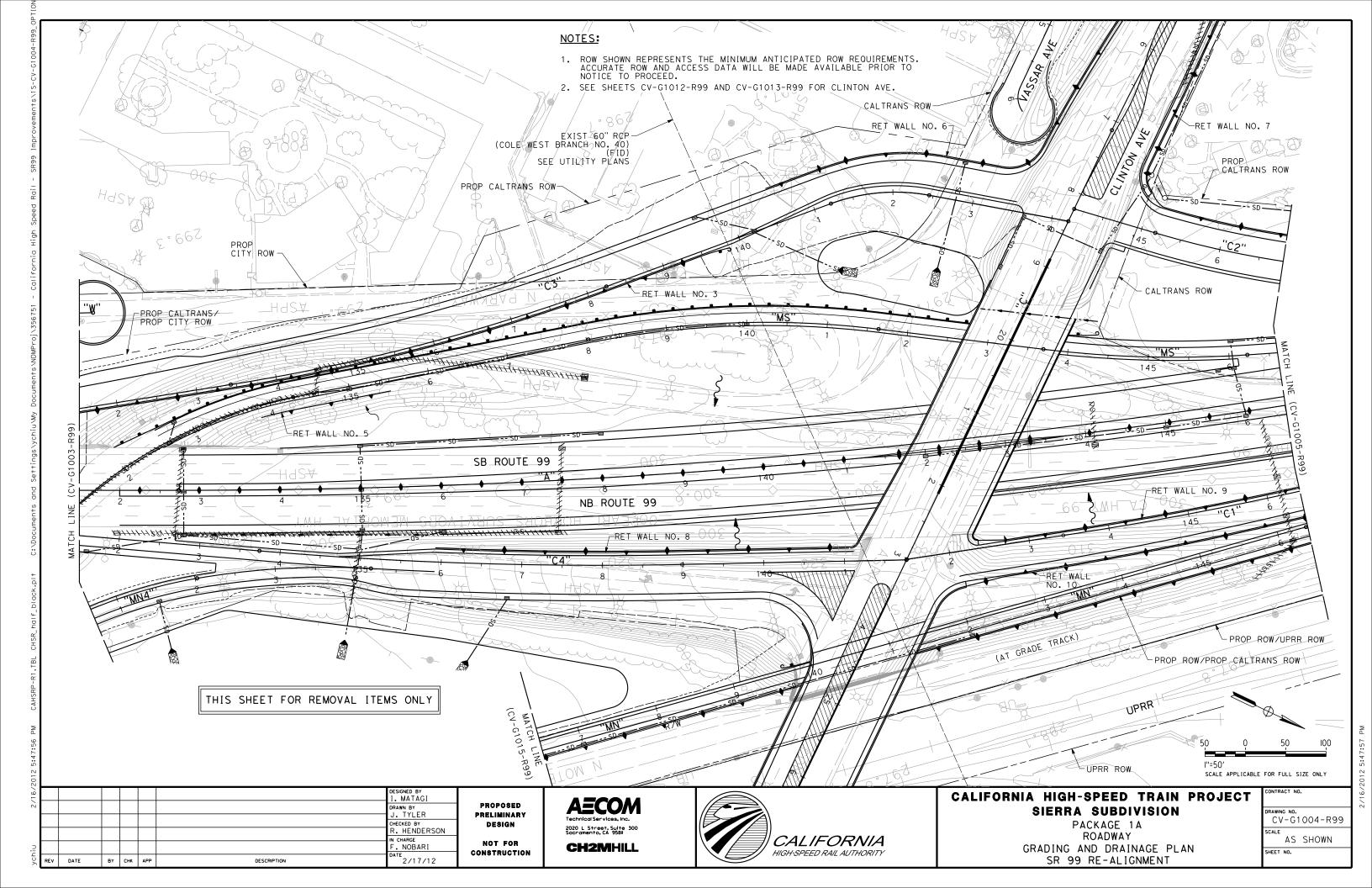


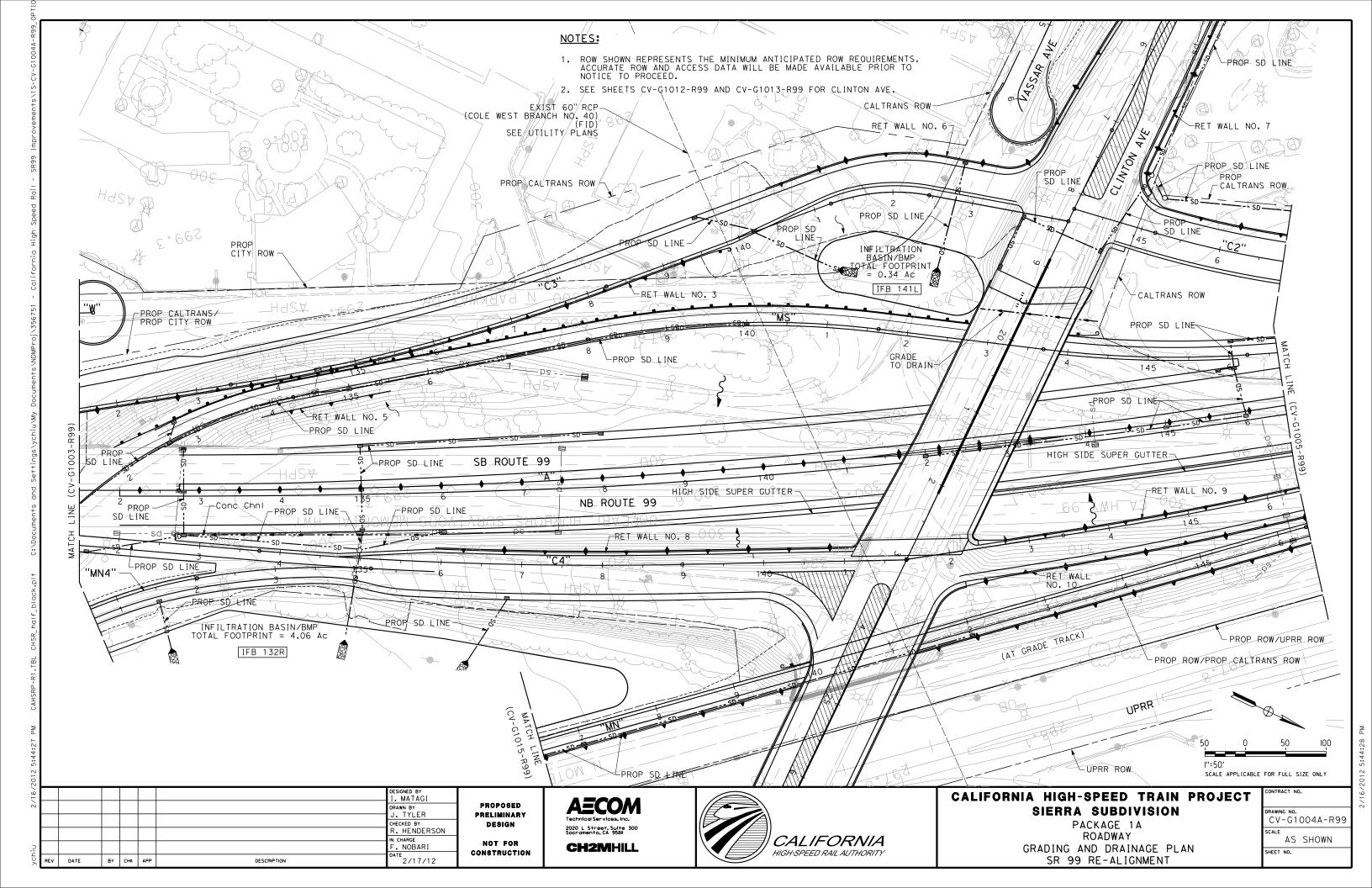


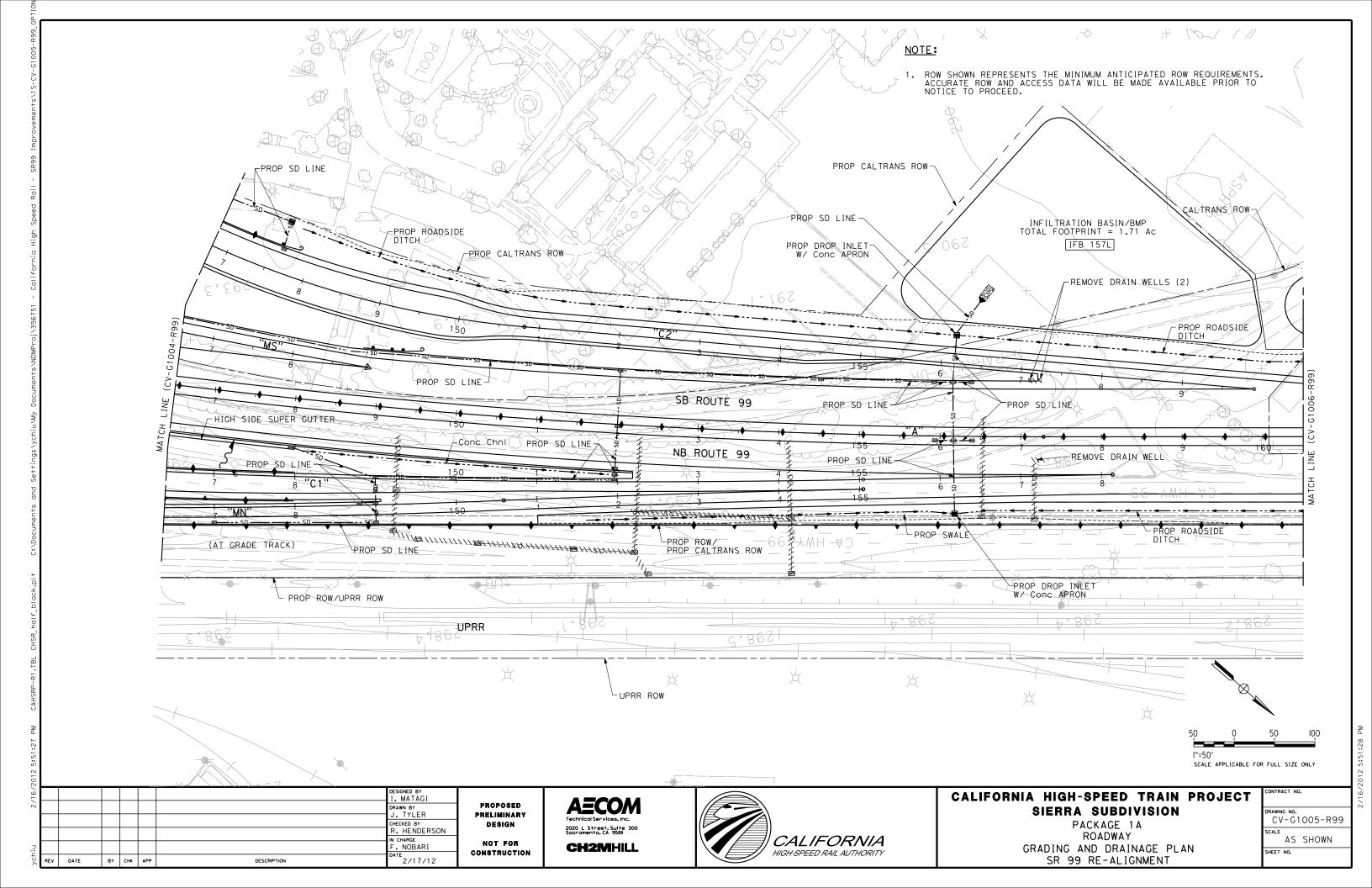


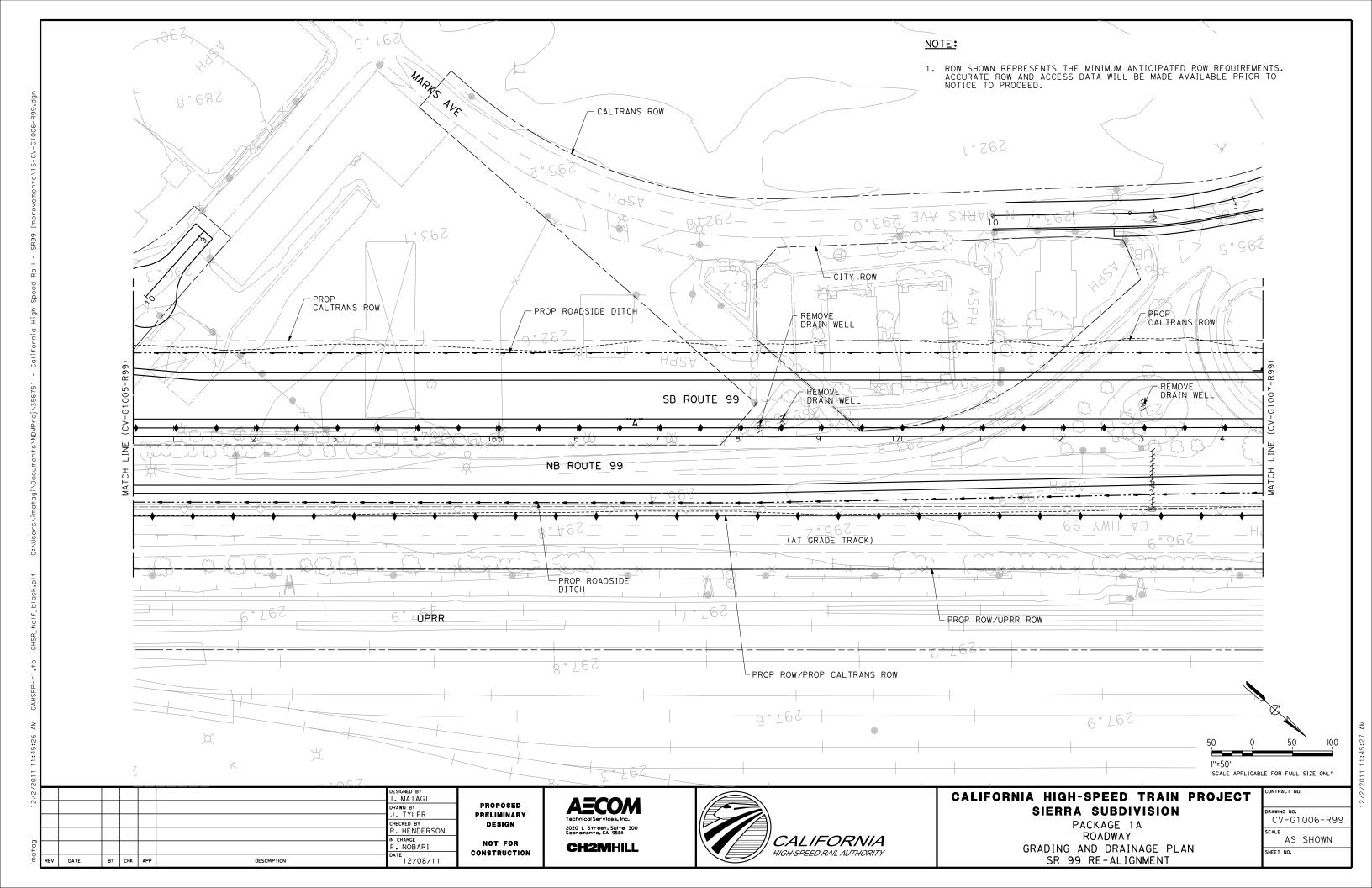


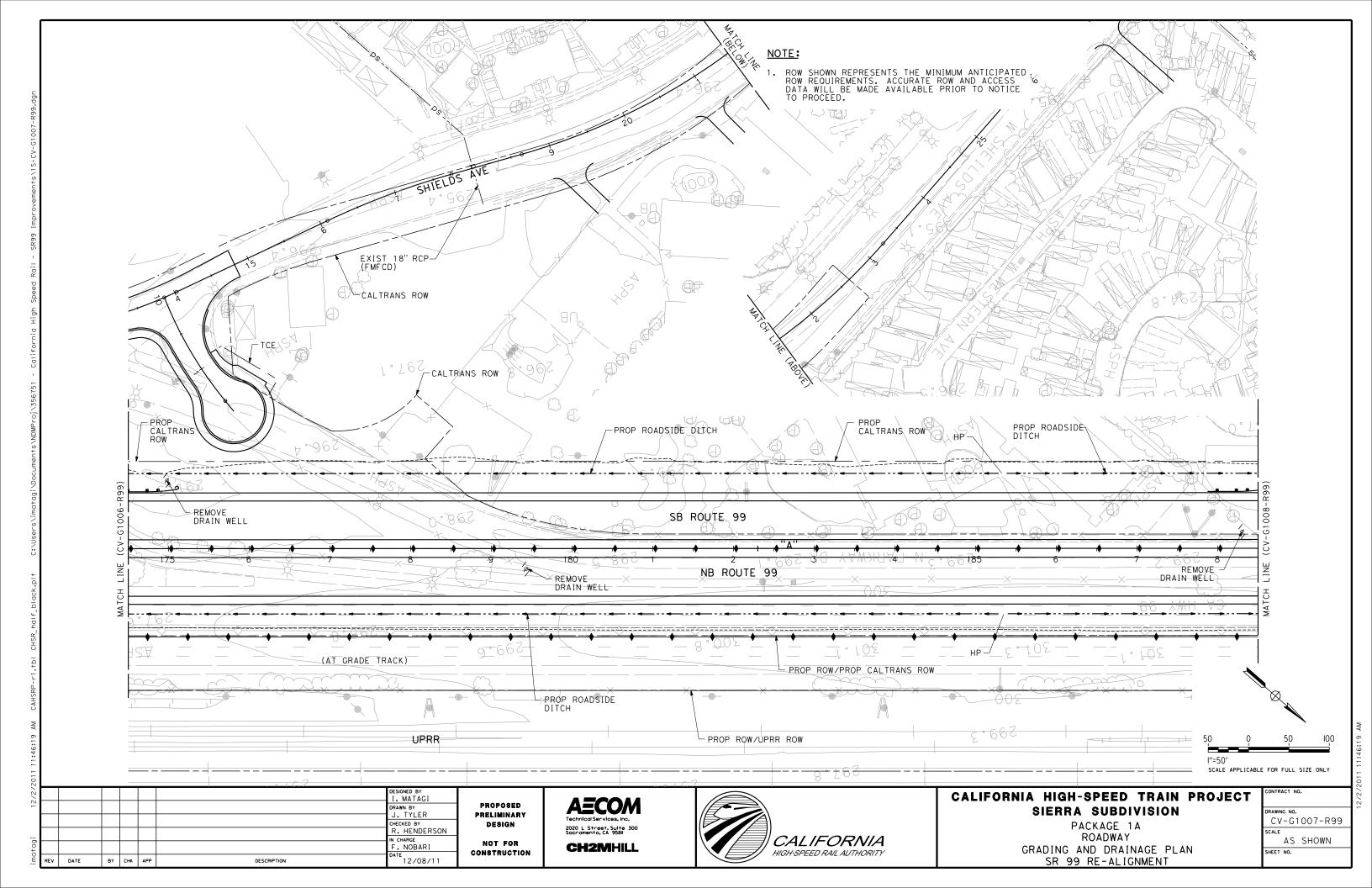


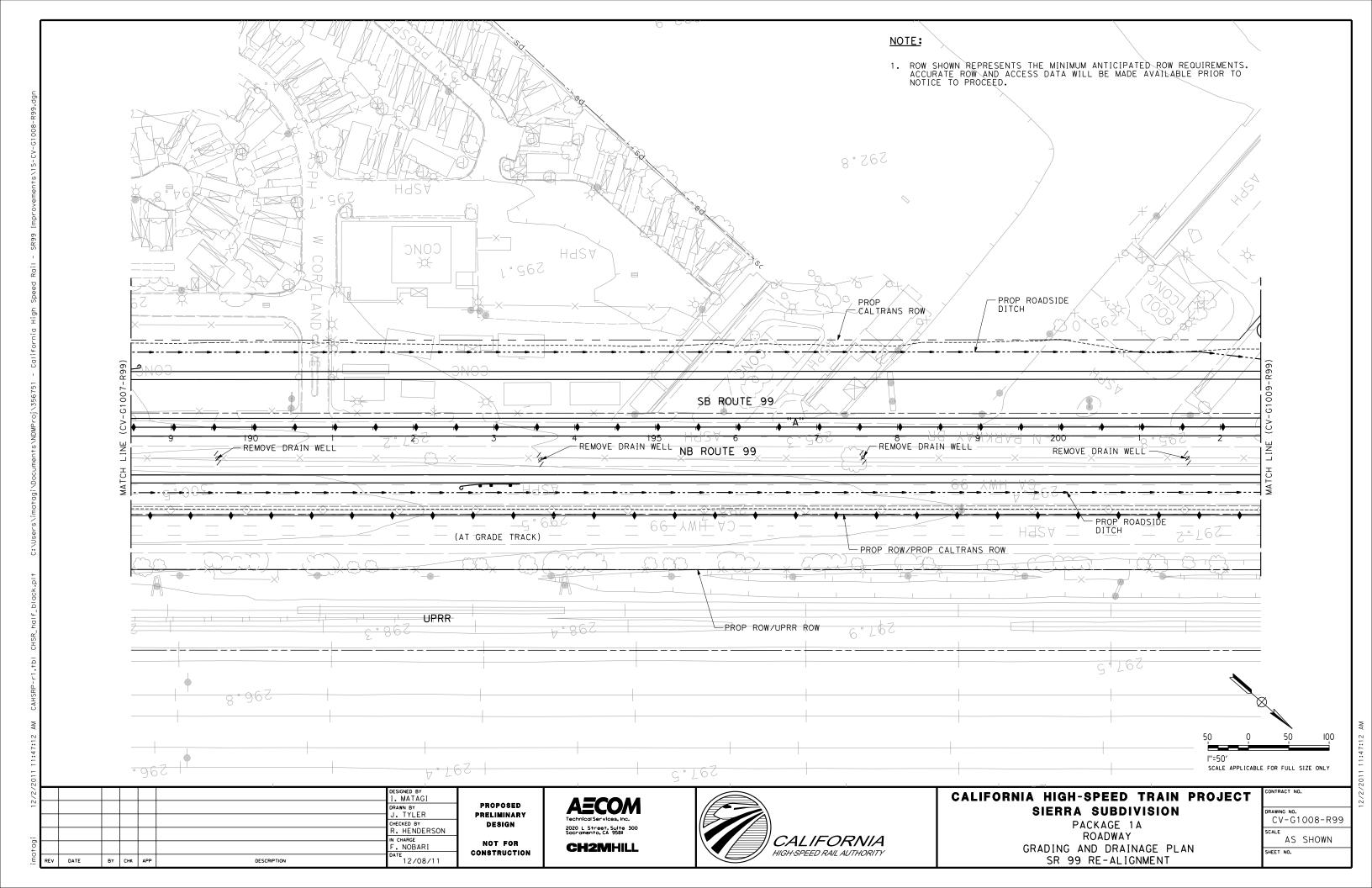


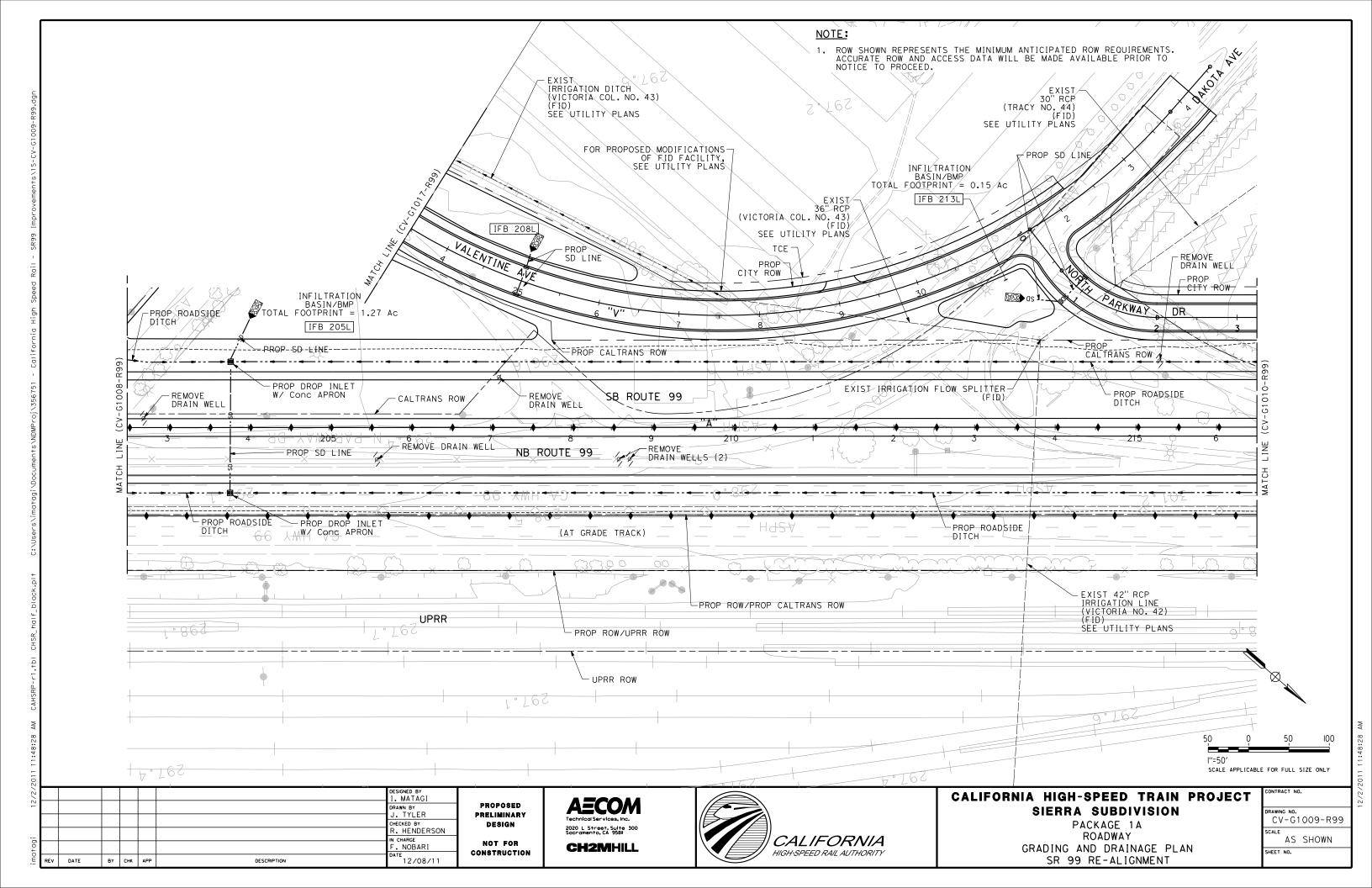


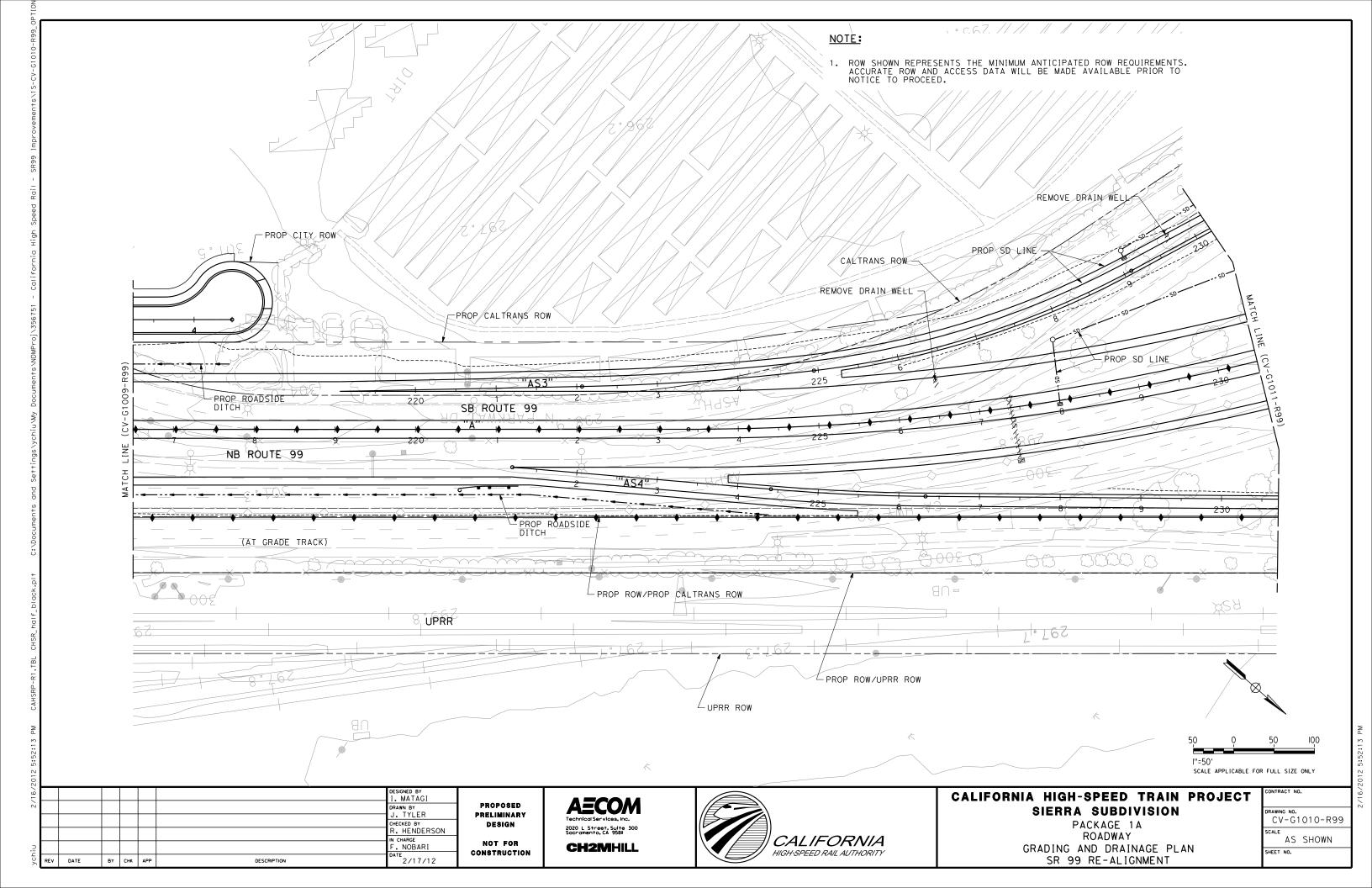


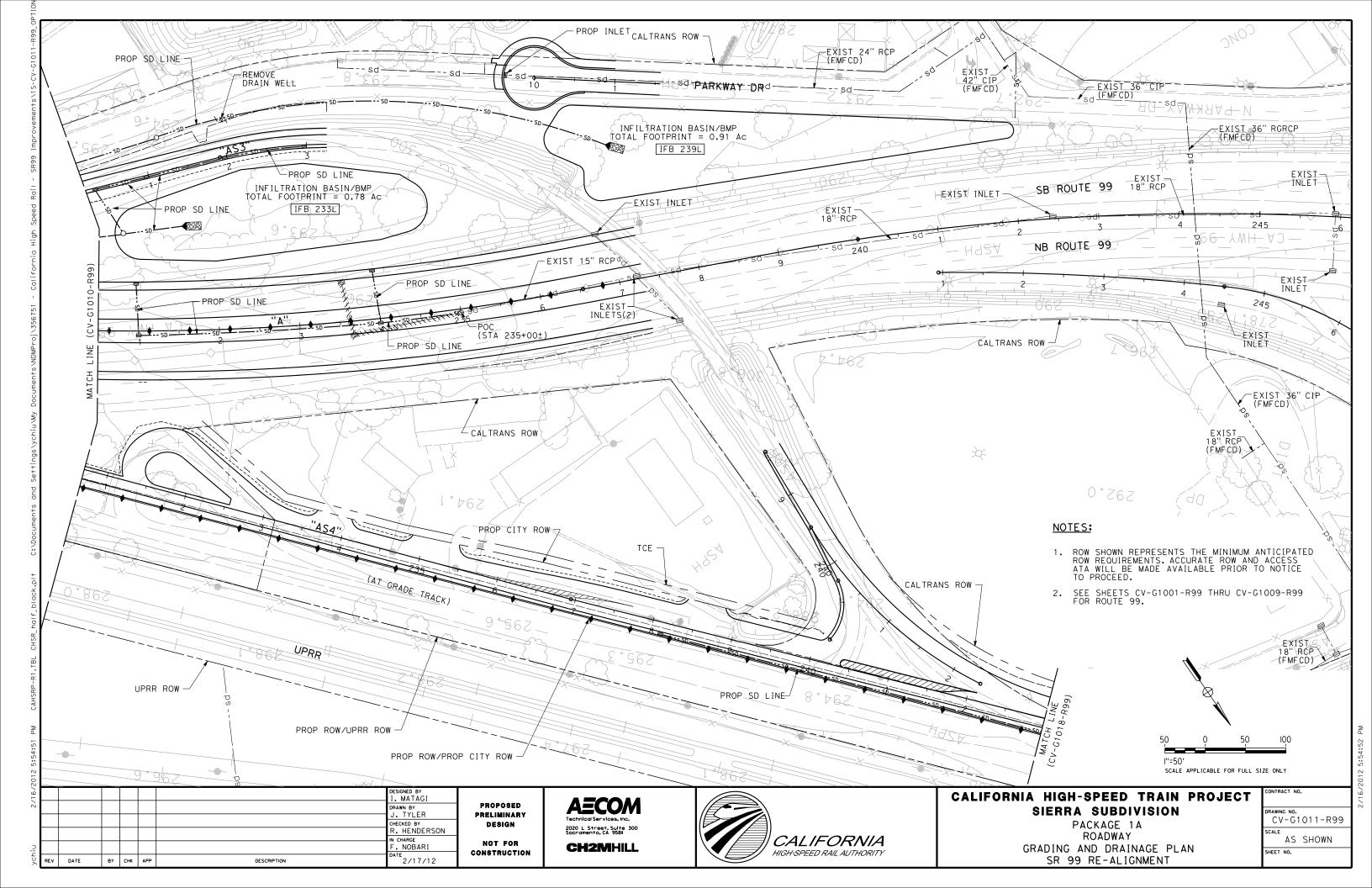


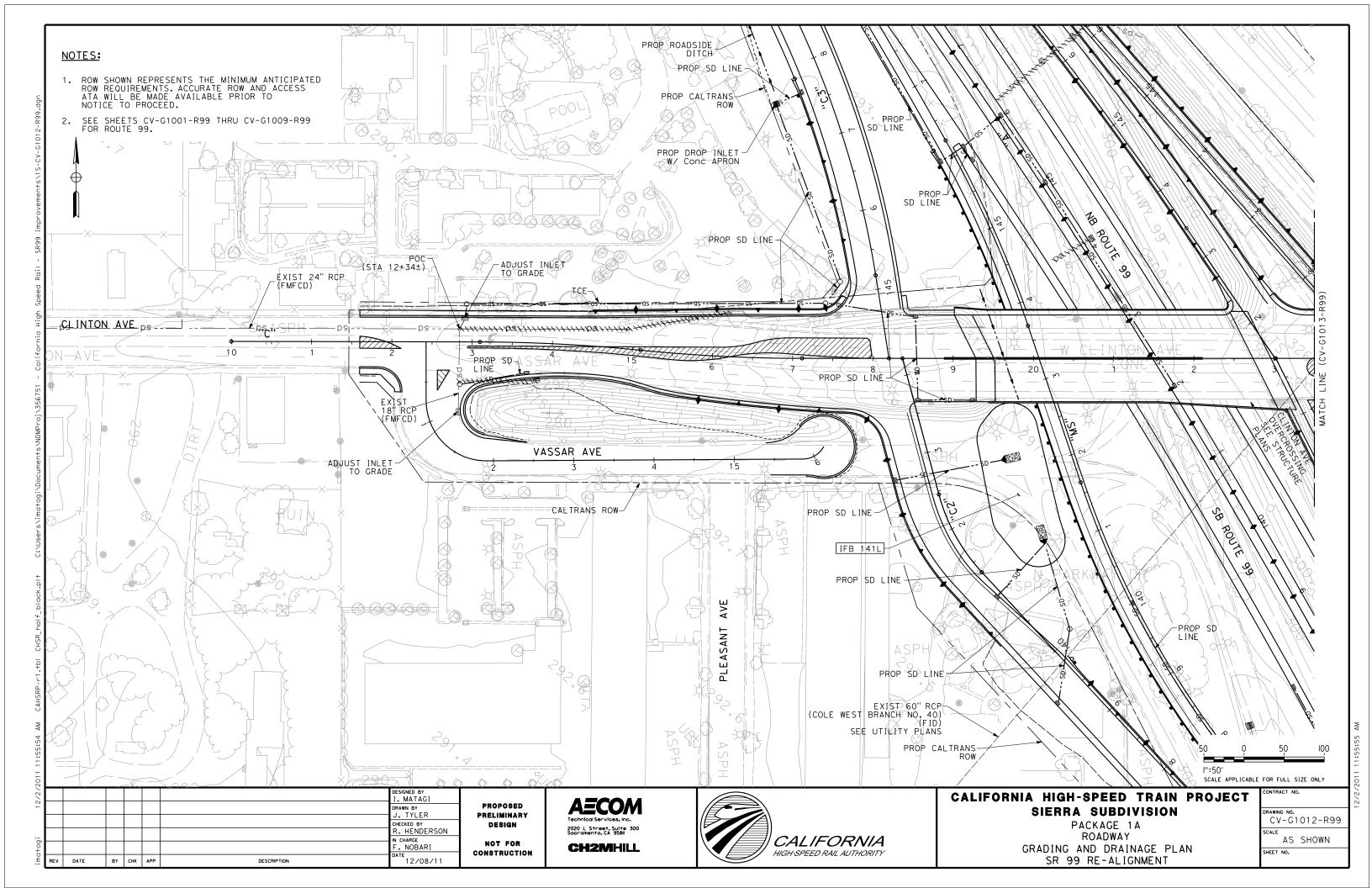


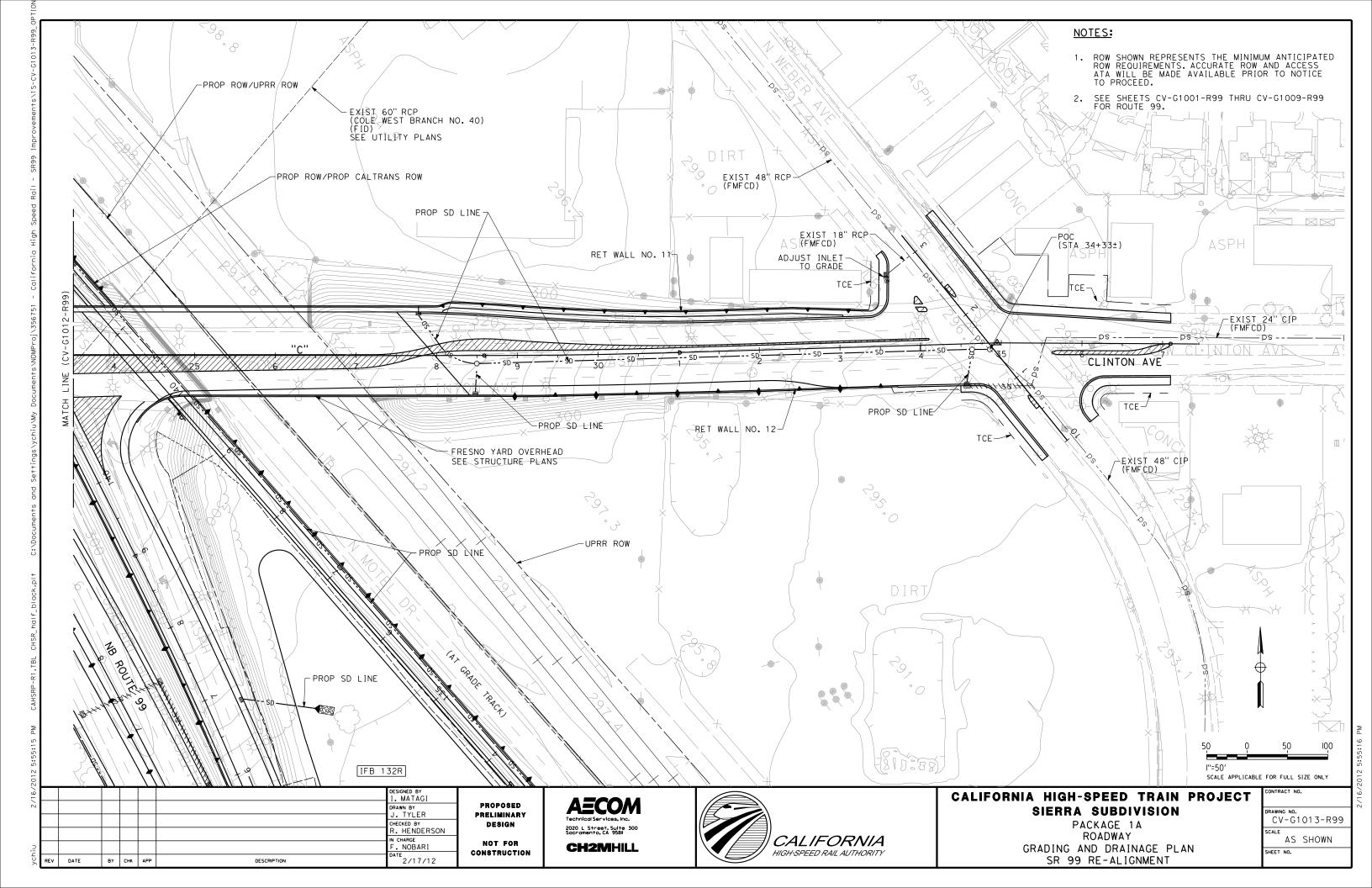


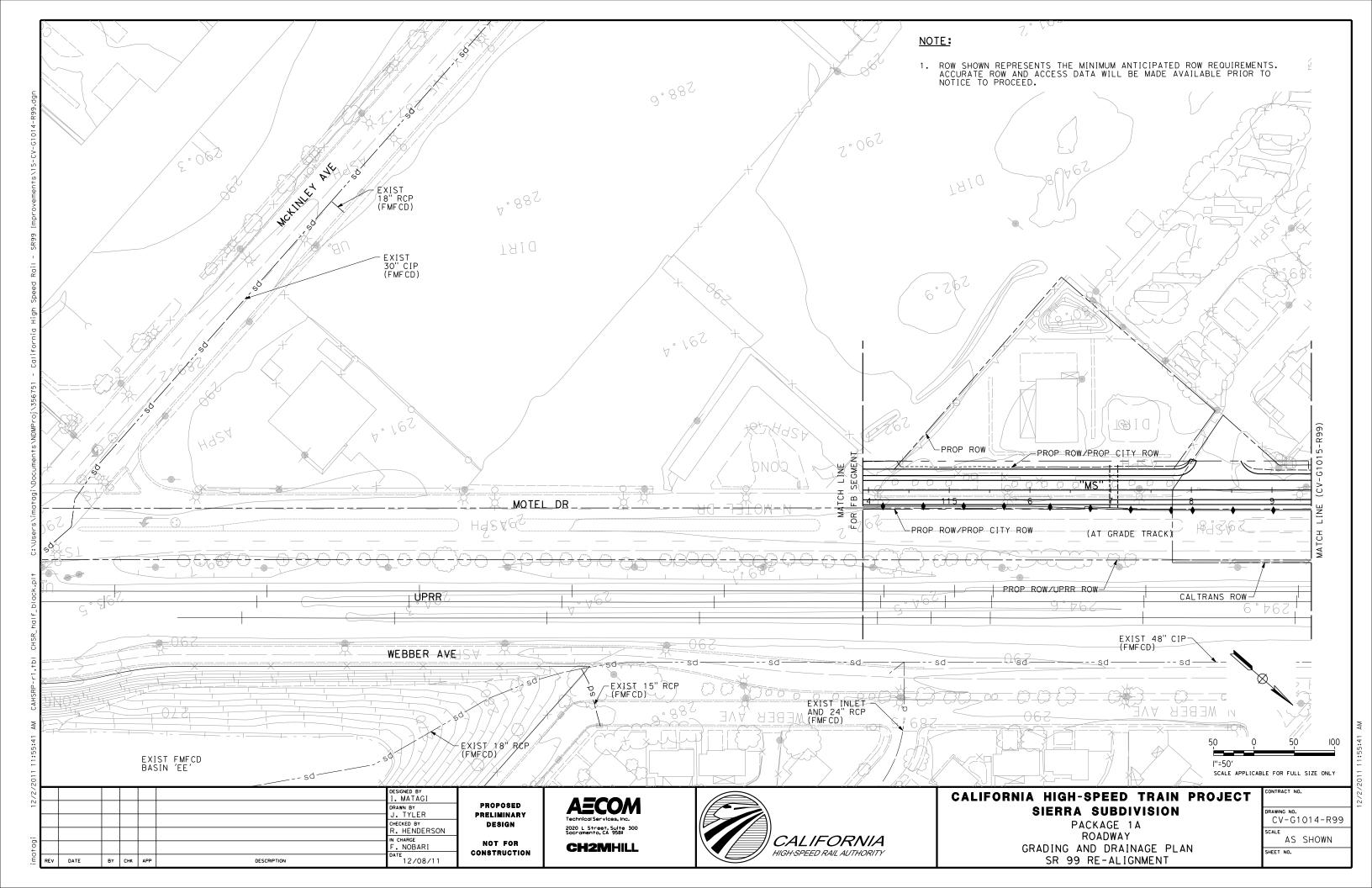


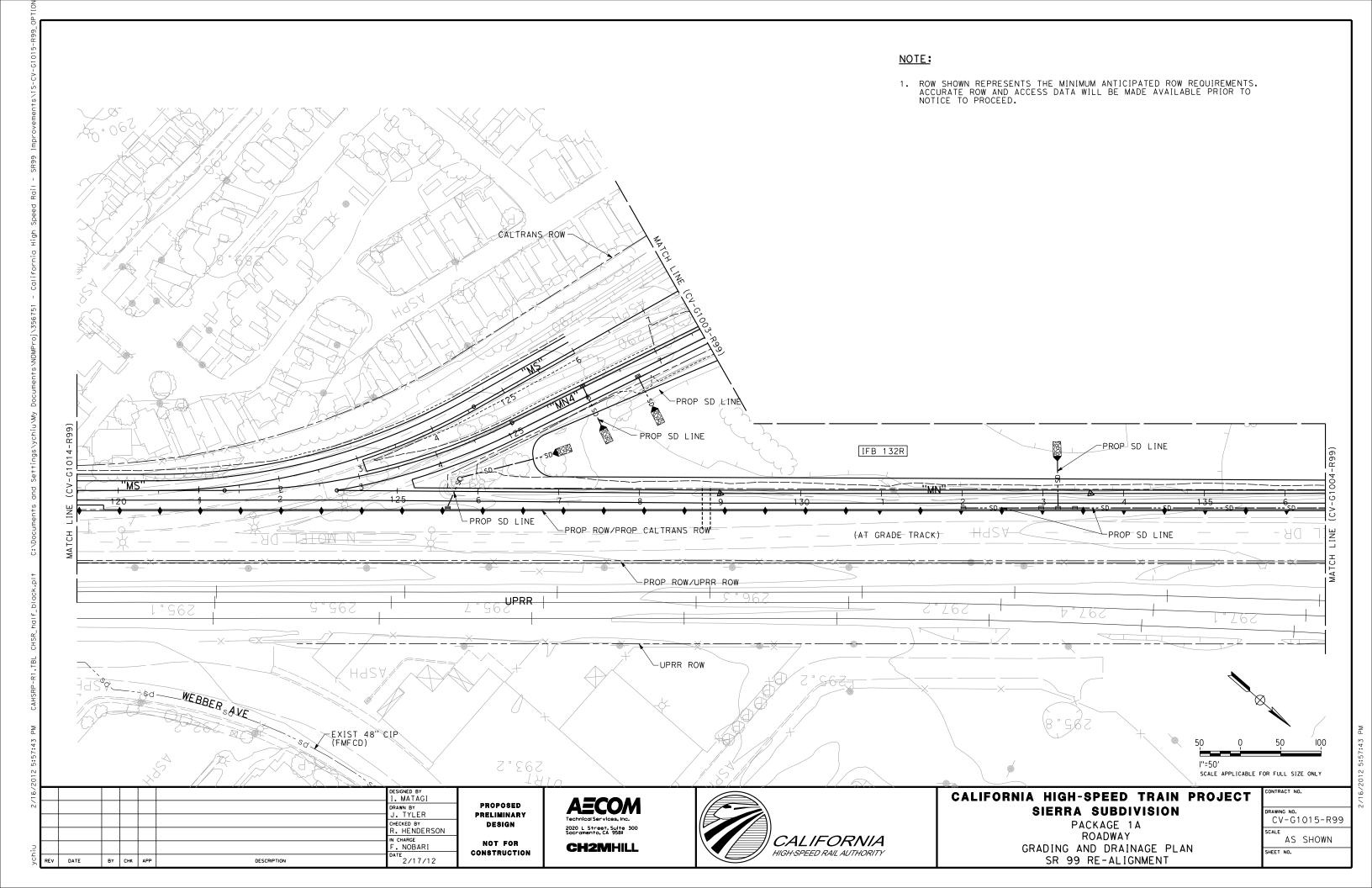


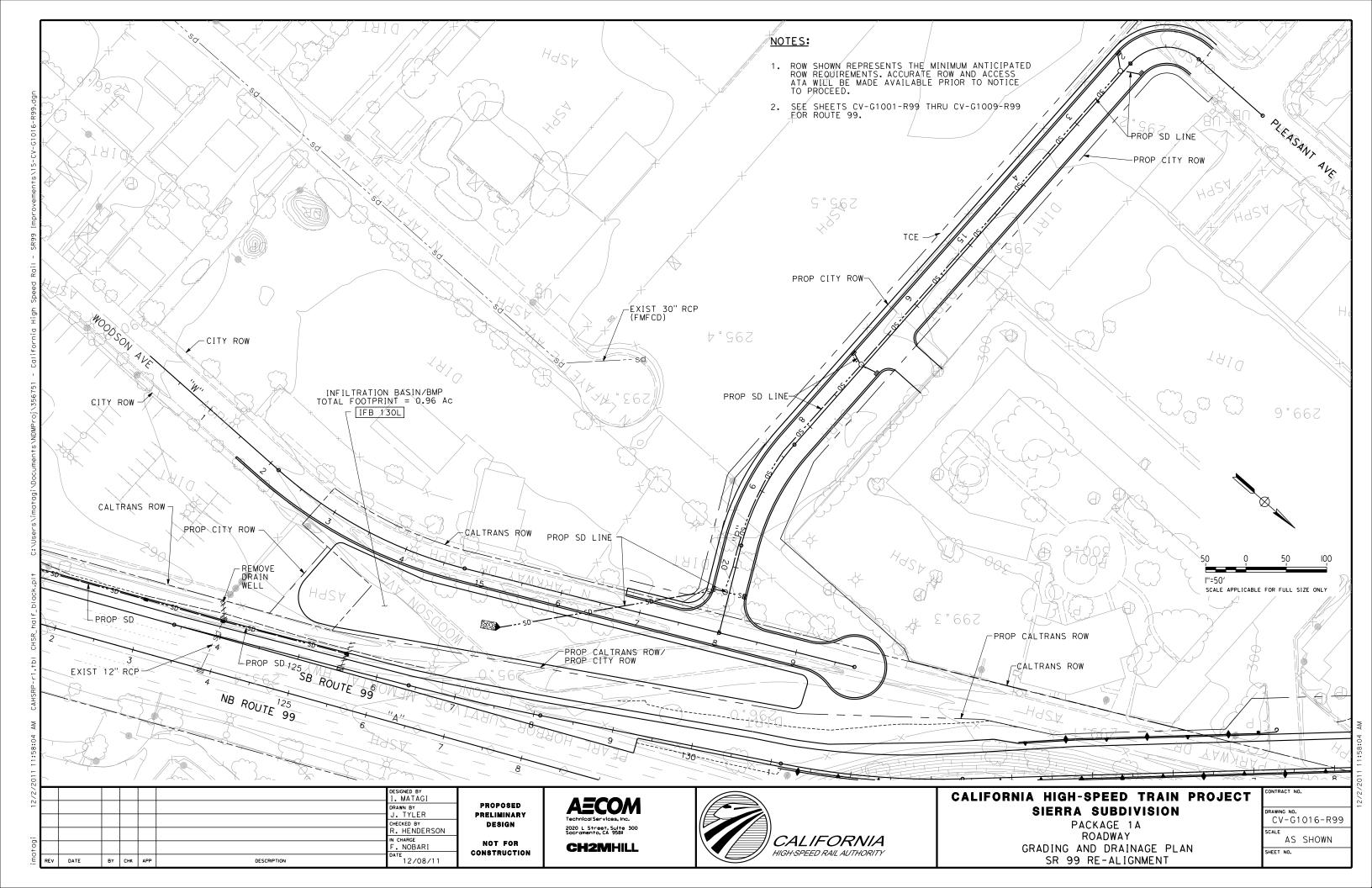


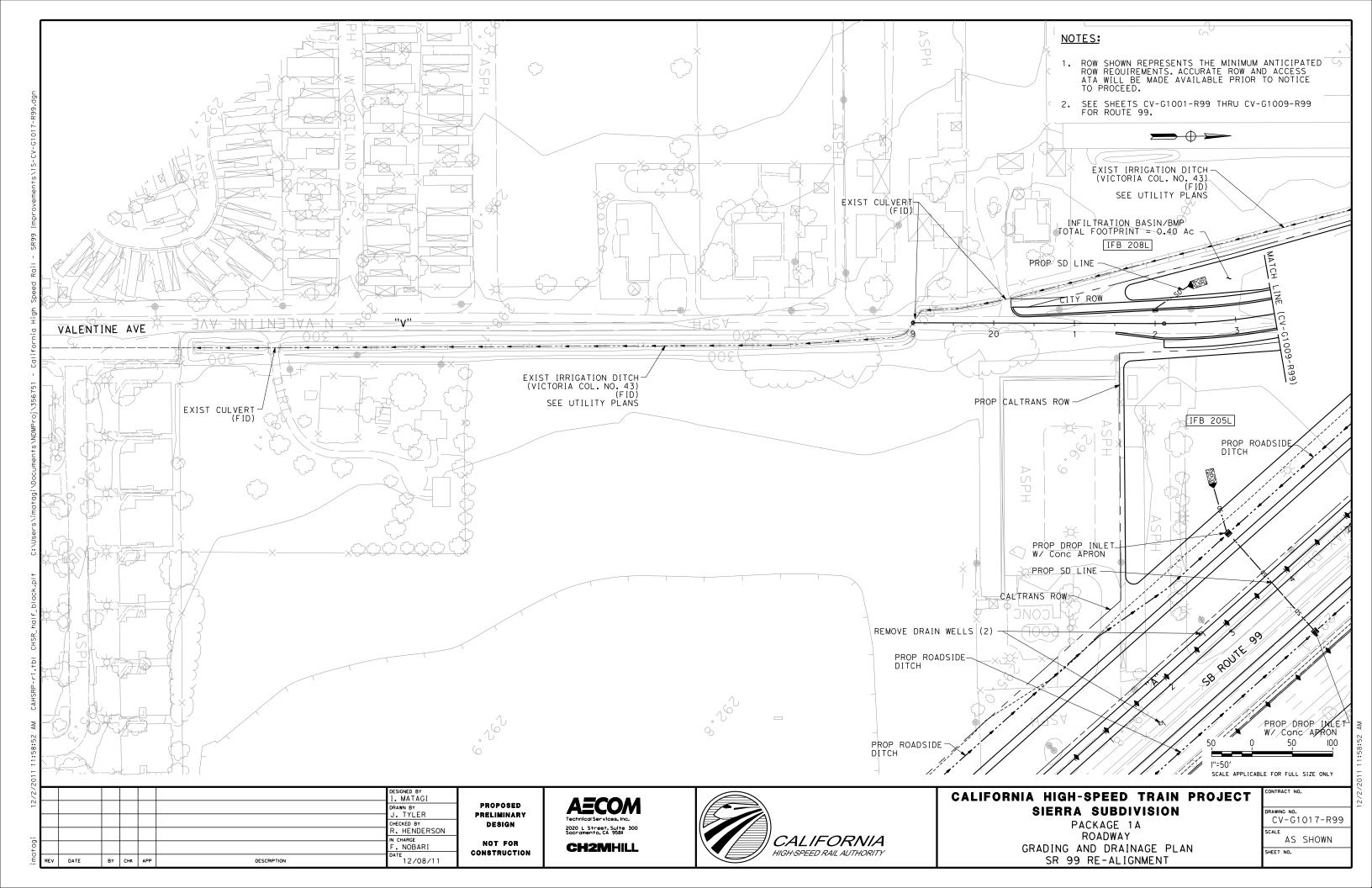


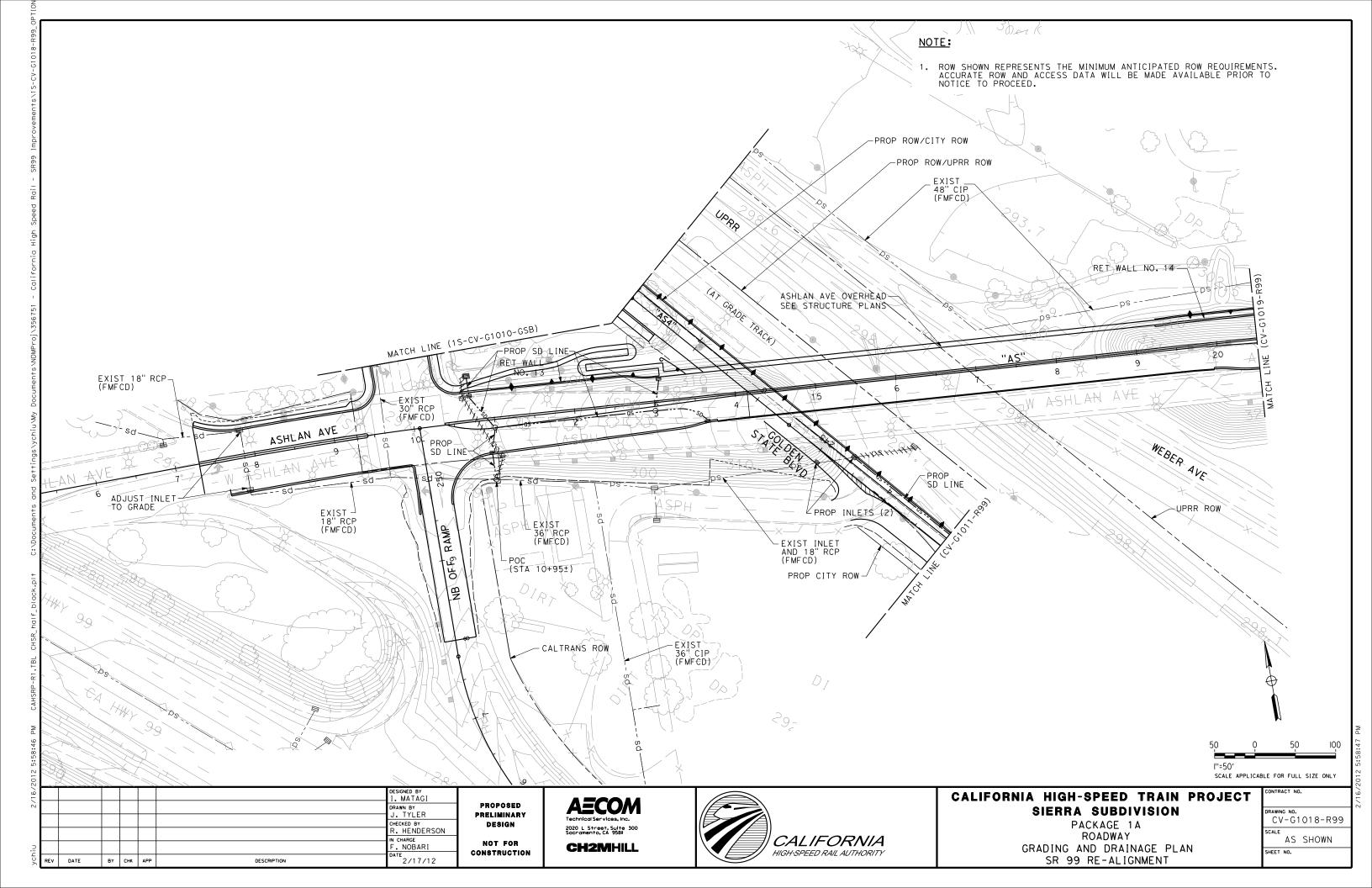


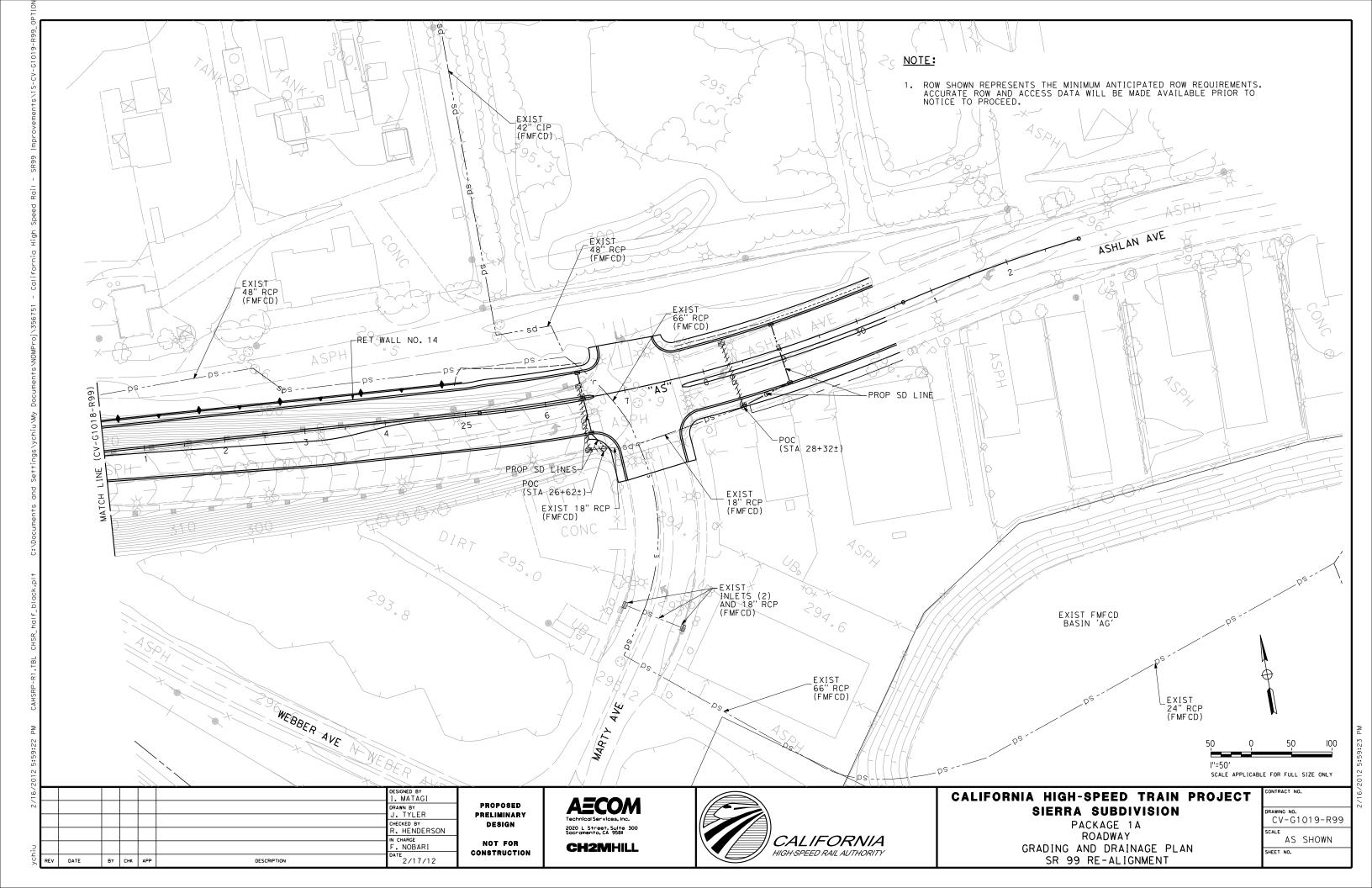


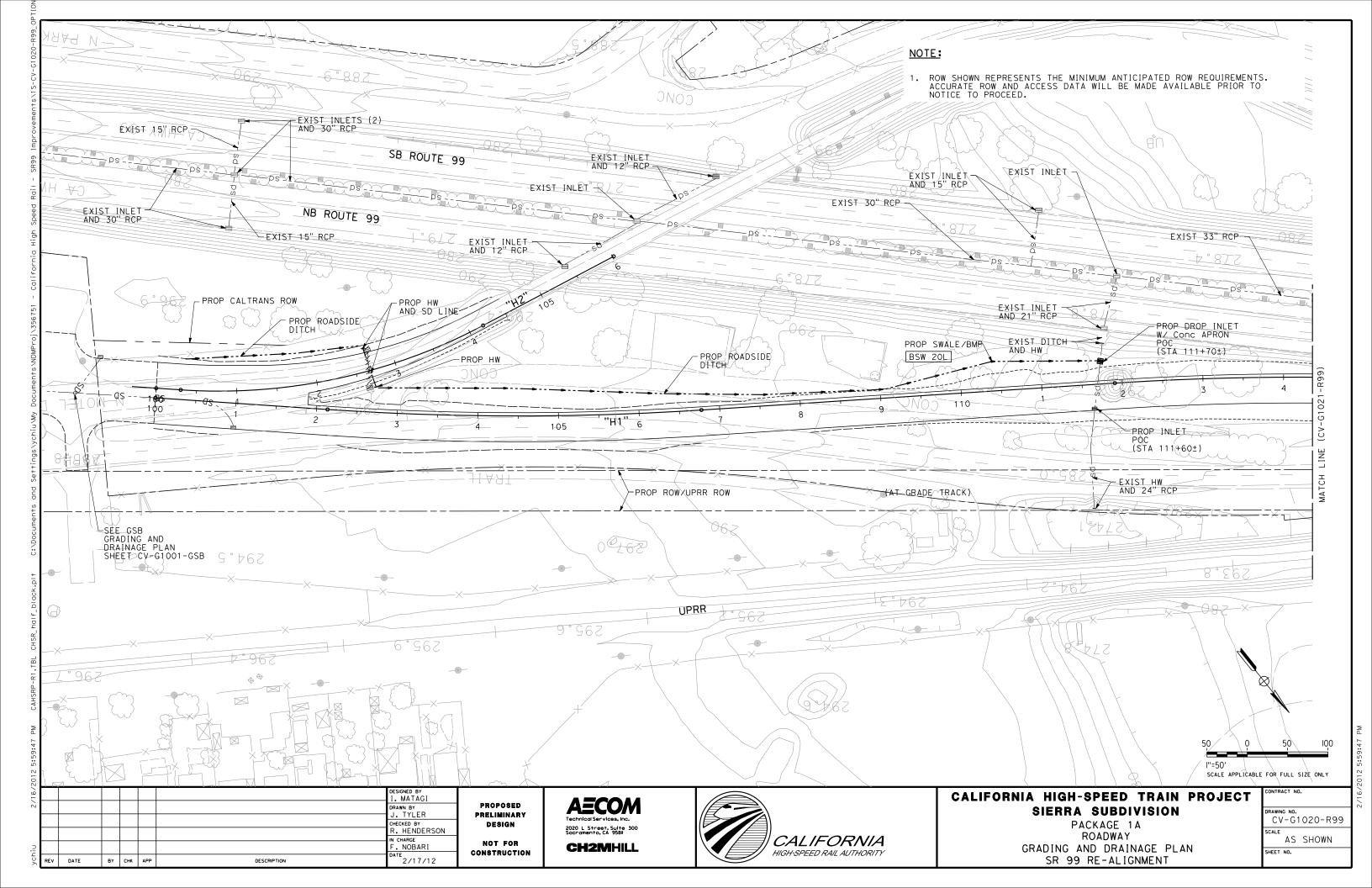


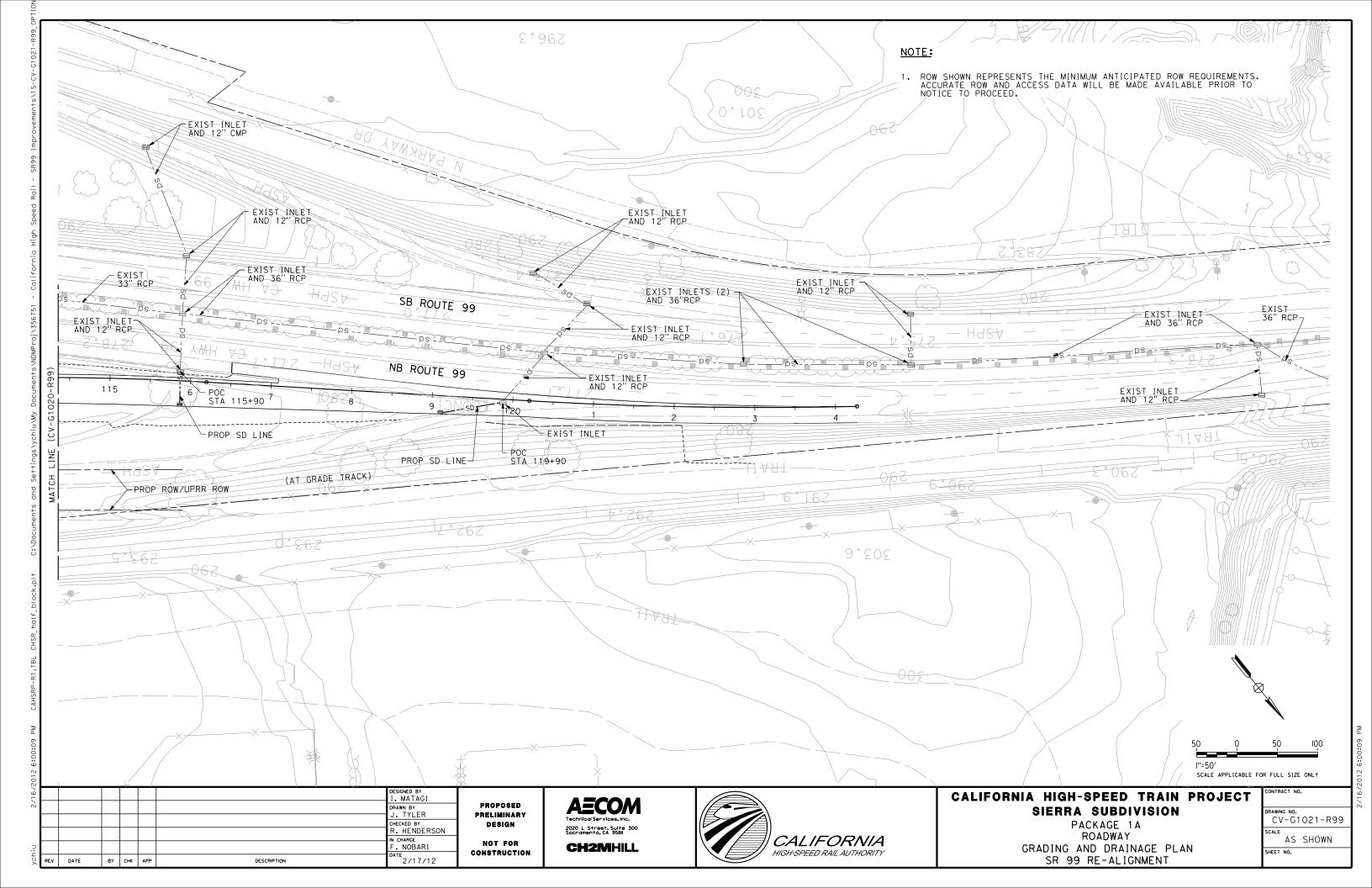












VOLUME 4 - UTILITIES

DRAWING No.	REV No.	DRAWING DESCRIPTION
		GENERAL
GE-A0007		PACKAGE 1A - UTILITIES - INDEX OF DRAWINGS - SHEET 1 OF 3
GE-A0008		PACKAGE 1A - UTILITIES - INDEX OF DRAWINGS - SHEET 2 OF 3
GE-A0009		PACKAGE 1A - UTILITIES - INDEX OF DRAWINGS - SHEET 3 OF 3
UT-B0001		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - GENERAL NOTES
UT-B0002		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - LEGENDS, SYMBOLS AND ABBREVIATIONS
		COMPOSITE UTILITY PLANS
UT-B0003		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY - KEY MAP
UT-C4000		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - GSB STA. 100+00 to 110+00
UT-C4001		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - GSB STA. 110+00 TO 125+00
UT-C4002		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - GSB STA. 125+00 TO 140+00
UT-C4003		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - GSB STA. 140+00 TO 155+00
UT-C4004		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - GSB STA. 155+00 TO 170+00
UT-C4005		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - GSB STA. 170+00 TO 185+00
UT-C4006		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10526+00 TO 10540+00
UT-C4007		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10540+00 TO 10555+00
UT-C4008		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10555+00 TO 10570+00
UT-C4009		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10570+00 TO 10585+00
UT-C4010		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10585+00 TO 10600+00
UT-C4011		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10600+00 TO 10615+00
UT-C4012		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10615+00 TO 10630+00
UT-C4013		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10630+00 TO 10645+00
UT-C4014		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10645+00 TO 10660+00
UT-C4015		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10660+00 TO 10675+00
UT-C4016		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10675+00 TO 10690+00
UT-C4017		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10690+00 TO 10705+00
UT-C4018		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10705+00 TO 10720+00
UT-C4019		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10720+00 TO 10735+00
UT-C4020		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10735+00 TO 10750+00
UT-C4021		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10750+00 TO 10765+00
UT-C4022		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10765+00 TO 10780+00
UT-C4023		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10780+00 TO 10795+00
UT-C4024		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10795+00 TO 10806+00
UT-C4025		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - SR 99 STA. 136+25 TO 121+25
UT-C4026		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - SR 99 STA. 121+25 TO 106+25
UT-C4027		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - SR 99 STA. 106+25 TO 100+00
UT-C4028		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - VETERANS BLVD / BULLARD AVE
UT-C4029		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - BULLARD AVE
UT-C4030		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - SHAW AVE
UT-C4031		PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - SHAW AVE
[1

					DESIGNED BY A. SHIELDS	
					DRAWN BY A. SHIELDS	1
					CHECKED BY A. BOONE	
					IN CHARGE A. BOONE	
REV	DATE	BY	СНК	APP	DESCRIPTION DATE 02/01/2012	

PROPOSED PRELIMINARY DESIGN NOT FOR

CONSTRUCTION

AECOM
Technical Services, inc.
2020 L Street, Suite 300
Sacramento, CA 958II **CH2MHILL**



CALIFORNIA HIGH-SPEED TRAIN PROJECT

PACKAGE 1A UTILITIES INDEX OF DRAWINGS SHEET 1 OF 3

ſ	CONTRACT NO.
	DRAWING NO.
	GE-A0007
	SCALE
	NONE
	SHEET NO.

SIERRA SUBDIVISION

VOLUME 4 - UTILITIE	S
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UT-C4032	PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - CORNELIA AVE / SANTA ANA AVE
UT-C4033	PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - N VALENTINE AVE
UT-C4034	PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - W SHIELDS AVE
UT-C4035	PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10806+00 TO STA. 10820+00
UT-C4036	PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10820+00 TO STA. 10835+00
UT-C4037	PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10835+00 TO STA. 10850+00
UT-C4038	PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10850+00 TO STA. 10865+00
UT-C4039	PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10865+00 TO STA. 10880+00
UT-C4040	PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10880+00 TO STA. 10895+00
UT-C4041	PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10895+00 TO STA. 10910+00
UT-C4042	PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10910+00 TO STA. 10925+00
UT-C4043	PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10925+00 TO STA. 10940+00
UT-C4044	PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10940+00 TO STA. 10955+00
UT-C4045	PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - STA. 10955+00 TO STA. 10970+00
UT-C4046	PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - W MCKINLEY AVE
UT-C4047	PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - W MCKINLEY AVE
UT-C4048	PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - W OLIVE AVE
UT-C4049	PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - W OLIVE AVE
UT-C4050	PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - W BELMONT AVE
UT-C4051	PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - W BELMONT AVE
UT-C4052	PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - E BELMONT AVE
UT-C4053	PACKAGE 1A - UTILITIES - COMPOSITE UTILITY PLAN - DIVISADERO ST/G ST
	UT-C4052 TO UT-C5010 NOT USED
UT-C5011	PACKAGE 1A- UTILTIES - COMPOSITE UTILITY PLAN - SECTION AND DETAILS
	UTILITY PROTECTION AND RELOCATION PLANS
UT-B0004	PACKAGE 1A - UTILITIES - UTILITY PROTECTION AND RELOCATION - KEY MAP
UT-D1040	PACKAGE 1A - UTILITIES - UTILITY PROTECTION AND RELOCATION - 96" STORM DRAIN PLAN AND PROFILE
UT-D1041	PACKAGE 1A - UTILITIES - UTILITY PROTECTION AND RELOCATION - 96" STORM DRAIN PLAN AND PROFILE
UT-D1042	The man and the state of the st
01-01042	PACKAGE 1A - UTILITIES - UTILITY PROTECTION AND RELOCATION - 96" STORM DRAIN PLAN AND PROFILE
01-01042	
CV-B0004	PACKAGE 1A - UTILITIES - UTILITY PROTECTION AND RELOCATION - 96" STORM DRAIN PLAN AND PROFILE
	PACKAGE 1A - UTILITIES - UTILITY PROTECTION AND RELOCATION - 96" STORM DRAIN PLAN AND PROFILE GRADING AND DRAINAGE PLANS
CV-B0004	PACKAGE 1A - UTILITIES - UTILITY PROTECTION AND RELOCATION - 96" STORM DRAIN PLAN AND PROFILE GRADING AND DRAINAGE PLANS PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE - GENERAL NOTES
CV-B0004 CV-B0005	PACKAGE 1A - UTILITIES - UTILITY PROTECTION AND RELOCATION - 96" STORM DRAIN PLAN AND PROFILE GRADING AND DRAINAGE PLANS PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE - GENERAL NOTES PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE - KEY MAP
CV-B0004 CV-B0005 CV-G1000	PACKAGE 1A - UTILITIES - UTILITY PROTECTION AND RELOCATION - 96" STORM DRAIN PLAN AND PROFILE GRADING AND DRAINAGE PLANS PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE - GENERAL NOTES PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE - KEY MAP PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE PLAN - STA. 10535+00 TO 10554+00
CV-B0004 CV-B0005 CV-G1000 CV-G1001	PACKAGE 1A - UTILITIES - UTILITY PROTECTION AND RELOCATION - 96" STORM DRAIN PLAN AND PROFILE GRADING AND DRAINAGE PLANS PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE - GENERAL NOTES PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE - KEY MAP PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE PLAN - STA. 10535+00 TO 10554+00 PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE PLAN - STA. 10554+00 TO 10582+00
CV-B0004 CV-B0005 CV-G1000 CV-G1001 CV-G1002	PACKAGE 1A - UTILITIES - UTILITY PROTECTION AND RELOCATION - 96" STORM DRAIN PLAN AND PROFILE GRADING AND DRAINAGE PLANS PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE - GENERAL NOTES PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE - KEY MAP PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE PLAN - STA. 10535+00 TO 10554+00 PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE PLAN - STA. 10582+00 TO 10610+00

						DESIGNED BY A. SHIELDS
						DRAWN BY A. SHIELDS
						CHECKED BY A. BOONE
						IN CHARGE A. BOONE
REV	DATE	BY	СНК	APP	DESCRIPTION	DATE 02/01/2012

PROPOSED PRELIMINARY DESIGN NOT FOR

CONSTRUCTION





CALIFORNIA HIGH-SPEED TRAIN PROJECT CONTRACT NO. SIERRA SUBDIVISION

PACKAGE 1A UTILITIES INDEX OF DRAWINGS SHEET 2 OF 3

•	
	DRAWING NO.
	GE-A0008
	SCALE

SHEET NO.

VOLUME 4 - UTILITIES

CV-G1006	PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE PLAN - STA. 10694+00 TO 10722+00
CV-G1007	PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE PLAN - STA. 10722+00 TO 10750+00
CV-G1008	PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE PLAN - STA. 10750+00 TO 10778+00
CV-G1009	PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE PLAN - STA. 10778+00 TO 10806+00
CV-G1010	PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE PLAN - STA. 10806+00 TO 10834+00
CV-G1011	PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE PLAN - STA. 10834+00 TO 10862+00
CV-G1012	PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE PLAN - STA. 10862+00 TO 10890+00
CV-G1013	PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE PLAN - STA. 10890+00 TO 10918+00
CV-G1014	PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE PLAN - STA. 10918+00 TO 10946+00
CV-G1015	PACKAGE 1A - UTILITIES - GRADING AND DRAINAGE PLAN - STA. 10946+00 TO 10974+00

						DESIGNED BY A. SHIELDS	
						DRAWN BY A. SHIELDS	
						CHECKED BY A. BOONE	
						IN CHARGE A. BOONE	
REV	DATE	BY	СНК	APP	DESCRIPTION	02/01/2012	

PROPOSED
PRELIMINARY
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AECOM
Technical Services, Inc.
2020 L Street, Suite 300
Sacramento, CA 958II
CH2MHILL



CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A UTILITIES INDEX OF DRAWINGS SHEET 3 OF 3

	CONTRACT NO.					
	DRAWING NO.					
	GE-A0009					
- 1	SCALE.					

NONE SHEET NO.

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- OBTAIN ALL PERMITS NECESSARY FROM APPLICABLE AGENCIES TO PERFORM THE WORK SHOWN ON THESE PLANS PRIOR TO COMMENCING WORK.
- NOTIFY THE CITY OF FRESNO, FMFCD, FID, PG&E, AT&T AND OTHER UTILITY ENTITIES AT LEAST FORTY-EIGHT (48) HOURS PRIOR TO COMMENCEMENT OF PARTICULAR WORK RELATIVE TO THE UTILITY AGENCY HAVING JURISDICTION.
- NOTIFY THE UTILITY AGENCY HAVING JURISDICTION AT LEAST THREE (3) WORKING DAYS PRIOR TO WHEN WORK REQUIRING SPECIAL INSPECTION IS AVAILABLE FOR INSPECTION. ALL WORK PERFORMED WITHOUT REQUIRED INSPECTION OR NOTIFICATION IS SUBJECT TO REMOVAL AND REPLACEMENT AT THE DIRECTION OF THE AUTHORITY AT THE CONTRACTOR'S EXPENSE WITH NO IMPACT TO THE PROJECT SCHEDULE.
- COORDINATE ALL NECESSARY UTILITY RELOCATIONS, OR ANY WORK INVOLVING UTILITIES. WITH THE APPROPRIATE UTILITY AGENCIES.
- NOTIFY UNDERGROUND SERVICE ALERT (USA) AT 811 AT LEAST TEN (10) DAYS PRIOR TO START OF CONSTRUCTION AND AT LEAST FORTY-EIGHT (48) HOURS PRIOR TO THE START OF ANY EXCAVATION.
- EXISTING UNDERGROUND UTILITIES AND IMPROVEMENTS ARE SHOWN IN THEIR APPROXIMATE LOCATION BASED UPON RECORD INFORMATION AVAILABLE AT THE TIME OF PREPARATION OF THESE PLANS. LOCATIONS HAVE NOT BEEN VERIFIED IN THE FIELD AND NO GUARANTEE IS MADE AS TO THE ACCURACY OR COMPLETENESS OF THE INFORMATION SHOWN. VERIFY THE EXISTENCE AND LOCATIONS AND DEPTHS OF ANY UNDERGROUND UTILITIES OR STRUCTURES WITHIN THE LIMITS OF THE PROJECT. LOCATE, PROTECT AND MAINTAIN ALL EXISTING UTILITIES WITHIN THE LIMITS OF THE PROJECT. ANY ADDITIONAL COSTS INCURRED AS A RESULTS OF CONTRACTOR'S FAILURE TO VERIFY LOCATIONS OF EXISTING UTILITIES PRIOR TO BEGINNING OF CONSTRUCTION SHALL BE BORNE BY THE CONTRACTOR.
- PROVIDE 24 HOUR EMERGENCY TELEPHONE NUMBERS TO THE CITY OF FRESNO POLICE DEPARTMENT, FIRE DEPARTMENT AND THE CITY ENGINEER AND NOTIFY THEM OF ANY LANE CLOSURES OR DETOURS. DETOURS SHALL NOT BE PERMITTED WITHOUT PRIOR WRITTEN APPROVAL BY THE CITY ENGINEER.
- 10. PROVIDE AND MAINTAIN SUFFICIENT SAFETY DEVICES INCLUDING BARRICADES, FENCING, WARNING SIGNS, LIGHTS OR FLAGGERS TO PROVIDE FOR THE SAFETY OF THE GENERAL PUBLIC WITHIN THE PROJECT AREA, TO THE SATISFACTION OF THE CITY ENGINEER. THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS.
- 11. PROVIDE ONE (1) COMPLETE SET OF AS-BUILT DRAWINGS WITH CHANGES INDICATED IN RED INK ON FULL-SIZE DRAWINGS. AS-BUILTS SHALL BE DELIVEREDTO THE AUTHORITY'S REPRESENTATIVE AT THE COMPLETION OF WORK PRIOR TO FINAL PROGRESS PAYMENT.
- 12. ALL CONSTRUCTION ACTIVITIES SHALL BE KEPT WITHIN THE PUBLIC RIGHT-OF-WAY AND EASEMENT OBTAINED FOR THE PROJECT UNLESS OTHERWISE SHOWN. THIS SHALL INCLUDE, BUT NOT LIMITED TO: VEHICLES AND EQUIPMENT, LIMITS OF TRENCH EXCAVATIONS, STOCKPILED NEW MATERIAL, EXCAVATED MATERIAL AND BACKFILL MATERIAL.

- 13. RESTORE ALL EXISTING PRIVATE AND PUBLIC IMPROVEMENTS TO THEIR EXISTING CONDITION OR BETTER TO THE SATISFACTION OF THE CITY ENGINEER INCLUDING, BUT NOT LIMITED TO: LANDSCAPING, IRRIGATION, DRIVEWAYS, AC PAVING, CONCRETE AND UTILITIES.
- 14. PROTECT ALL SURVEY MONUMENTS AND BENCHMARKS WITHIN THE WORK AREA. ANY PERMANENT SURVEY MONUMENTS OR MARKERS DESTROYED DURING OPERATIONS SHALL BE REPLACED BY A LICENSED LAND SURVEYOR AT THE CONTRACTOR'S EXPENSE.
- 15. ALL TRAFFIC CONTROLS REQUIRED FOR CONSTRUCTION ACTIVITIES SHALL CONFORM TO THE REQUIREMENTS OF THE LATEST EDITION OF THE CALIFORNIA MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (CA MUTCD). ALL PROPOSED LANE CLOSURES AND DETOURS REQUIRE A TRAFFIC CONTROL PLAN TO BE SUBMITTED FOR REVIEW AND APPROVAL BY THE AUTHORITY REPRESENTATIVE AND THE CITY ENGINEER OF THE CITY OF FRESNO AT LEAST FIVE (5) WORKING DAYS PRIOR TO THE SCHEDULED CLOSURE.
- 16. MAINTAIN ACCESS TO PRIVATE PROPERTY AND DRIVEWAYS. OR PROVIDE ALTERNATE/TEMPORARY ACCESS, AT ALL TIMES.
- 17. ALL TREES WITHIN THE PUBLIC RIGHT-OF-WAY SHALL BE PROTECTED FROM CONSTRUCTION DAMAGE, UNLESS OTHERWISE IDENTIFIED FOR REMOVAL ON PLANS.
- 18. CONSTRUCTION ACTIVITIES AFFECTING SERVICE TO ANY FIRE HYDRANT SHALL BE COORDINATED WITH THE CITY OF FRESNO FIRE DEPARTMENT A MINIMUM OF SEVENTY-TWO (72) HOURS PRIOR TO STARTING WORK.
- 19. CONFORM TO EXISTING STREETS, LANDSCAPE AND OTHER IMPROVEMENTS WITH A SMOOTH TRANSITION AND CONFORM IN PAVING, CURBS, GUTTERS AND SIDEWALK TO AVOID ANY ABRUPT CHANGES IN GRADES OR CROSS SLOPES, LOW SPOTS, OR HAZARDOUS CONDITIONS.
- 20. VERIFY ALL FIELD DIMENSIONS AND ELEVATIONS SHOWN ON THE PLANS AND SHALL NOTIFY THE ENGINEER OF ANY DISCREPANCIES OR POTENTIAL FIELD CONFLICTS PRIOR TO PROCEEDING WITH THE WORK, ANY COSTS RESULTING FROM THE CONTRACTOR'S FAILURE TO REPORT SUCH CONFLICTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- 21. CONDUCT GRADING AND CONSTRUCTION ACTIVITIES IN SUCH A MANNER TO CONTROL AIRBORNE DUST AND RELATED DAMAGE TO ADJACENT PROPERTIES. SUFFICIENT WATERING TO CONTROL DUST AND WIND BLOWN MATERIAL IS REQUIRED AT ALL TIMES AND CONTRACTOR ASSUMES FULL LIABILITY FOR DAMAGE CLAIMS RELATING TO DUST.
- 22. ANY MATERIAL CONTAINING ASBESTOS OR OTHER HAZARDOUS MATERIAL THAT ARE REMOVED FROM THE PROJECT SITE MUST BE PERFORMED ONLY BY A PRE-APPROVED STATE OF CALIFORNIA LICENSED ASBESTOS ABATEMENT CONTRACTOR REGISTERED WITH CAL-OSHA. ASBESTOS AND OTHER HAZARDOUS MATERIAL HANDLING MUST BE IN ACCORDANCE WITH CAL-OSHA, FED-OSHA, U.S. EPA, AND CALIFORNIA DEPARTMENT OF TOXIC SUBSTANCE CONTROL.

SPECIAL NOTE FOR UTILITY DATA:

THE LOCATION OF EXISTING UTILITIES ARE BASED ON TWO SOURCES: 1.) TOPOGRAPHIC SURVEY

2.) G.I.S. CONDUCT CONTRACTOR'S SURVEY AND VERIFY THE LOCATION OF EXISTING UTILITIES AND RECONCILE THE SURVEY DATA AND GIS DATA.

SPECIAL NOTE FOR FUTURE STORM DRAIN LINES:

ALL FUTURE SD LINES ANNOTATED AS '(FUT) XX" SD' ARE NOT IN CONTRACT AND ARE SHOWN FOR INFORMATION ONLY. COORDINATE WITH FMFCD FOR FUTURE SD LINES CROSSING THE HST ROW AND PROVIDE CASING FOR FUTURE SD LINES, CASING INVERT ELEVATIONS AND SLOPES SHALL BE DETERMINED BY FMFCD.

SPECIAL NOTES FOR WATER AND SEWER:

- 1. LOCATE, PROTECT AND RECONNECT ALL EXISTING WATER AND SEWER SERVICE LATERALS TO NEW WATER AND SEWER MAINS.
- 2. RELOCATE EXISTING WATER METERS TO EDGE OF NEW ROADWAYS IN ACCORDANCE WITH CITY OF FRESNO WATER STANDARD DRAWINGS.
- 3. WATER MAIN HUNG ON OR SUPPORTED BY BRIDGE STRUCTURES SHALL BE CIP.

APPLICABLE STANDARD PLANS

CITY OF FRESNO STANDARD DRAWINGS:

PUBLIC WORKS STANDARD DRAWINGS

WATER STANDARD DRAWINGS

SEWER STANDARD DRAWINGS

ELECTRICAL STANDARD DRAWINGS

ALTERNATE PUBLIC IMPROVEMENT DRAWINGS

INTELLIGENT TRANSPORTATION SYSTEM STANDARD DRAWINGS

FRESNO METROPOLITAN FLOOD CONTROL DISTRICT STANDARD DRAWINGS:

A-1 THRU A-9

B-1 THRU B-7

C - 1

D-1 THRU D-9

E-1 THRU E-8

F-1 THRU F-4

G-1 THRU G-10

FRESNO IRRIGATION DISTRICT ENGINEERING HANDBOOK OF SPECIFICATIONS AND DRAWINGS:

PAGES 12 THRU 59

A. HU . VENTURA HECKED BY CHARGE • MINISTER REV DATE BY CHK APP DESCRIPTION 01/09/2012

PROPOSED PRELIMINARY DESIGN

NOT FOR CONSTRUCTION 2020 L Street, Suite 300 Sacramento, CA 95811 CH2MHILL



CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

PACKAGE 1A UTILITIES COMPOSITE UTILITY PLAN GENERAL NOTES

RAWING NO. UT-B0001 N.T.S

ONTRACT NO.

SHEET NO.

<u>NEW</u>

- ---- IRR---- ---- IRR--

— - - TC —— - - (OH)—

—--- TV ————---(OH)—

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+0+

DESCRIPTION

ELECTRICAL OVERHEAD CONDUIT

TELECOMMUNICATION OVERHEAD LINE

TELECOMMUNICATION LINE

TELEVISION OVERHEAD LINE

ELECTRICAL (UG) CONDUIT

NATURAL GAS LINE

IRRIGATION LINE

STORM DRAIN LINE

OIL LINE

SEWER LINE

WATER LINE

TELEPHONE LINE

TELEVISION LINE

SEWER MANHOLE

FIRE HYDRANT

NORTH ARROW

RIGHT-OF-WAY

WATER WELL

OR RELOCATED

UTILITY ENCASING

HST TRACK CENTER LINE

CITY ROAD CENTER LINE

UTILITY TO BE REMOVED

SR 99 CENTER LINE

EXISTING

 \odot

+O+

S10655+00

230

135+00

PROPOSED PRELIMINARY DESIGN NOT FOR

2020 L Street, Suite 300 Sacramento, CA 95811 **CH2MHILL**



GROUND

GUTTER

GAS VALVE

HORIZONTAL

GND

GSB

GTR

HOR

G۷

EASEMENT WATER VALVE GOLDEN STATE BLVD

ABBREVIATIONS

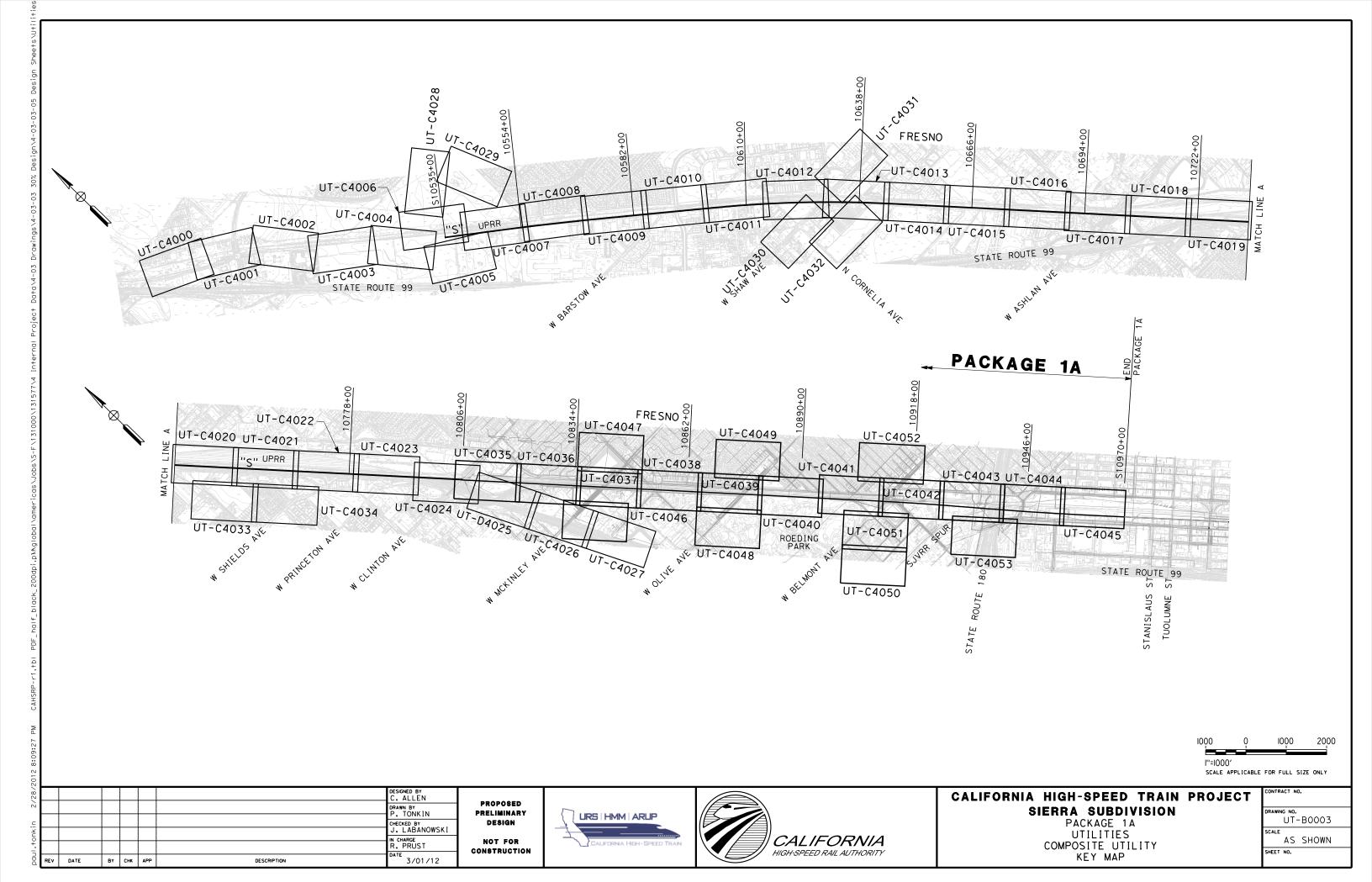
AB	AGGREGATE BASE	IN	INCH
AC	ASPHALT CONCRETE	INV	INVERT
ACB	ASPHALT CONCRETE BASE	IRR	IRRIGATION
ACP	ASBESTOS CEMENT PIPE		
A/G	AT GRADE	L	LENGTH
ABN	ABANDON	LED	LIGHT EMITTING DIODE
APPROX	APPROXIMATE	LG	LIP OF GUTTER
AVE	AVENUE	LS	LANDSCAPE
		LT	LEFT
BEG	BEGIN		
BLDG	BUILDING	MIN	MINIMUM
BLVD	BOULEVARD		
BCR	BEGIN CURVE RETURN	N	NORTH, NORTHING, NEW
B.O.	BLOW-OFF	NIC, N.I.C.	NOT IN CONTRACT
BOC	BACK OF CURB	NO	NUMBER
BSW	BACK OF SIDEWALK	N.T.S.	NOT TO SCALE
BVCE	BEGIN VERTICAL CURVE ELEVATION		
BVCS	BEGIN VERTICAL CURVE STATION	PVMT	PAVEMENT
		(P)	PROPOSED
C&G	CURB AND GUTTER	PCC	PORTLAND CEMENT CONCRETE
СВ	CATCH BASIN	PRKG	PARKING
CD	CURB DRAIN	PVC	POLYVINYL CHLORIDE
CH	CURB HEIGHT		
CIP	CAST IRON PIPE	R	RADIUS
CL	CENTERLINE	R/R	REMOVE AND REPLACE/RELOCATE
CMP	CORRUGATED METAL PIPE	R/W, ROW	RIGHT OF WAY
COF	CITY OF FRESNO	RCP	REINFORCED CONCRETE PIPE
CTV	CABLE TELEVISION	RT	RIGHT
CV	CURVE	REQ'D	REQUIRED
D	DEPTH		
DIA	DIAMETER	S	SLOPE, SOUTH, SEWER
DI	DRAINAGE INLET	SD	STORM DRAIN
DIP	DUCTILE IRON PIPE	SDI	STORM DRAIN INLET
DWG	DRAWING	SDMH	STORM DRAIN MANHOLE
		SF	SQUARE FEET
E	ELECTRICAL, ELECTRIC	SHLD	SHOULDER
Ε	EAST, EXISTING	SR	STATE ROUTE
ELEC	ELECTRICAL, ELECTRIC	SSMH	SANITARY SEWER MANHOLE
EC	END HORIZONTAL CURVE	SS	SANITARY SEWER
EDG	EDGE	STA	STATION
EL OR ELEV	ELEVATION	ST	STREET
ELEC OH	ELECTRICAL OVERHEAD	SW	SIDEWALK
ELL	ELBOW		
EP, EOP	EDGE OF PAVEMENT	T	TELEPHONE, TOTAL
ETW	EDGE OF TRAVEL WAY	TC	TELECOMMUNICATION
EVCE	END OF VERTICAL CURVE ELEVATION	TCE	TEMPORARY CONSTRUCTION EASEME
EVCS	END OF VERTICAL CURVE STATION	TEL	TELEPHONE
EXIST, EX, (E)	EXISTING	TOC	TOP OF CURB
		TS	TRAFFIC SIGNAL
FDAC	FULL DEPTH ASPHALT CONCRETE	TYP	TYPICAL
FH	FIRE HYDRANT		
FID	FRESNO IRRIGATION DISTRICT	UG, U/G	UNDERGROUND
FMFCD	FRESNO METROPOLITAN FLOOD		
	CONTROL DISTRICT	VAR	VARIES
F.O.	FIBER OPTIC	VC	VERTICAL CURVE
FOC	FACE OF CURB	VCP	VITRIFIED CLAY PIPE
FUT	FUTURE		
		W	WATER, WEST
G	GAS	W/	WITH
GALV	GALVANIZED	WM	WATER MAIN
GB	GRADE BREAK	WV	WATER VALVE
CND	CROUND		

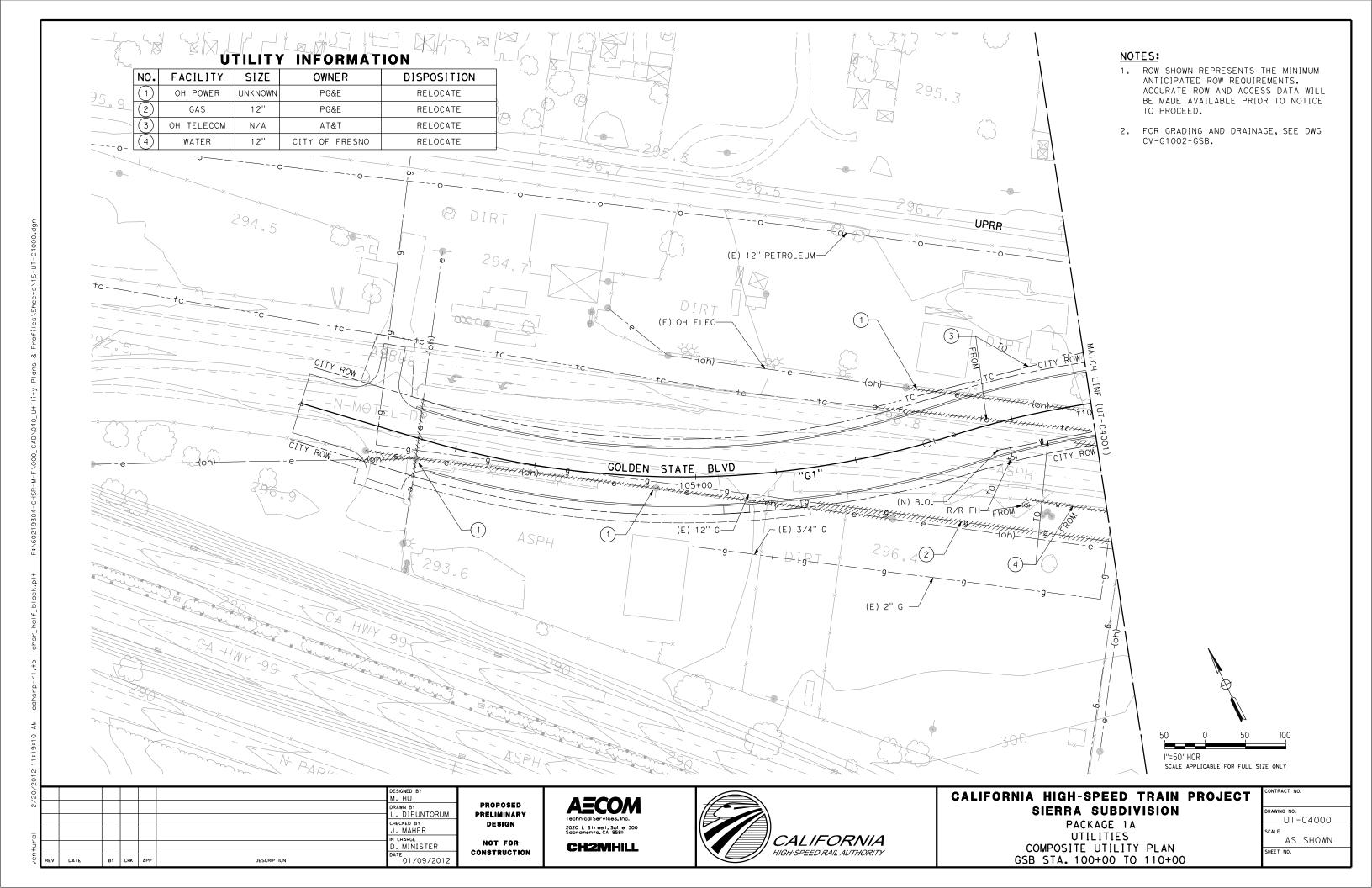
CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

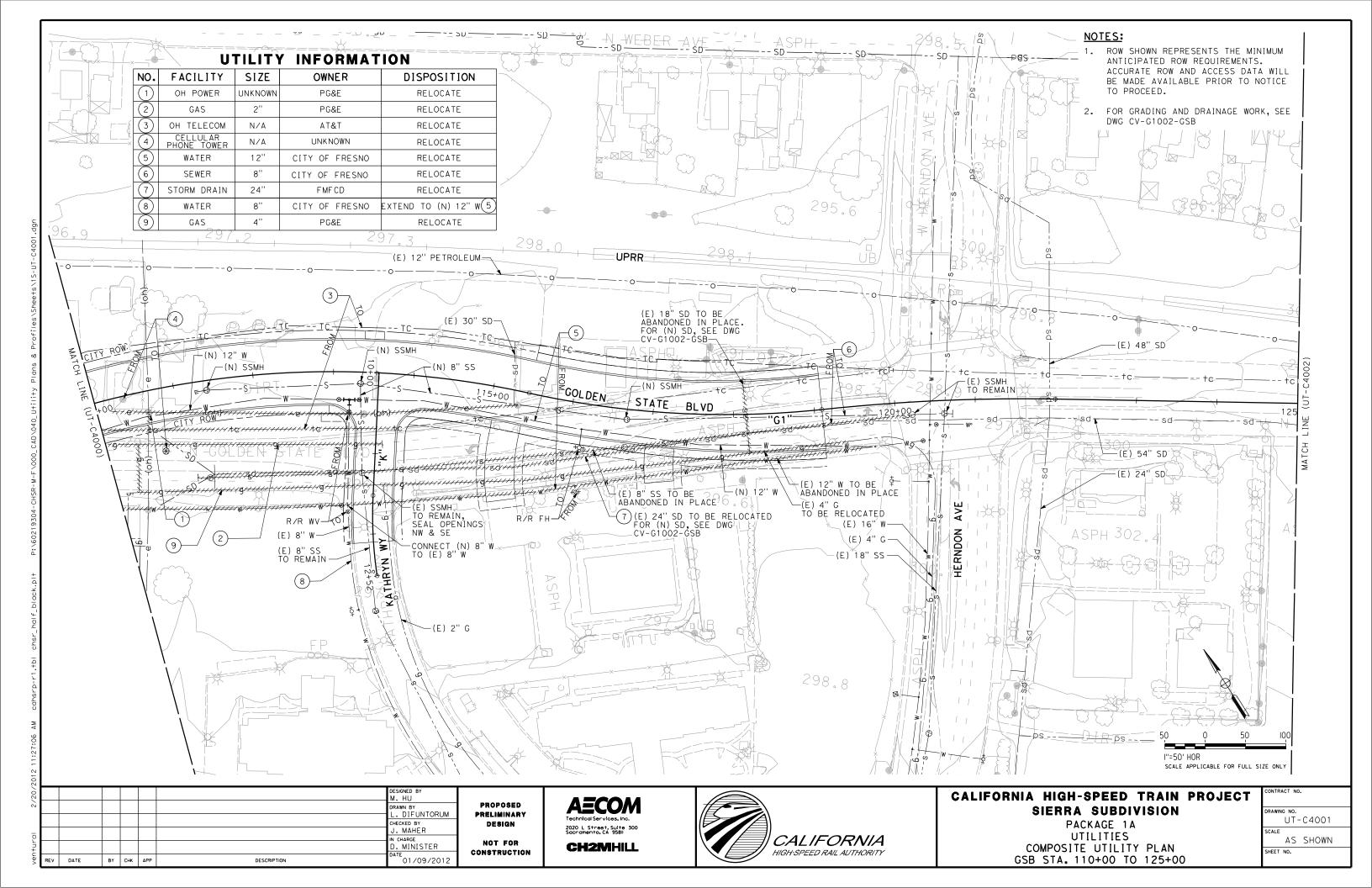
PACKAGE 1A UTILITIES COMPOSITE UTILITY PLAN LEGENDS, SYMBOLS AND ABBREVIATIONS

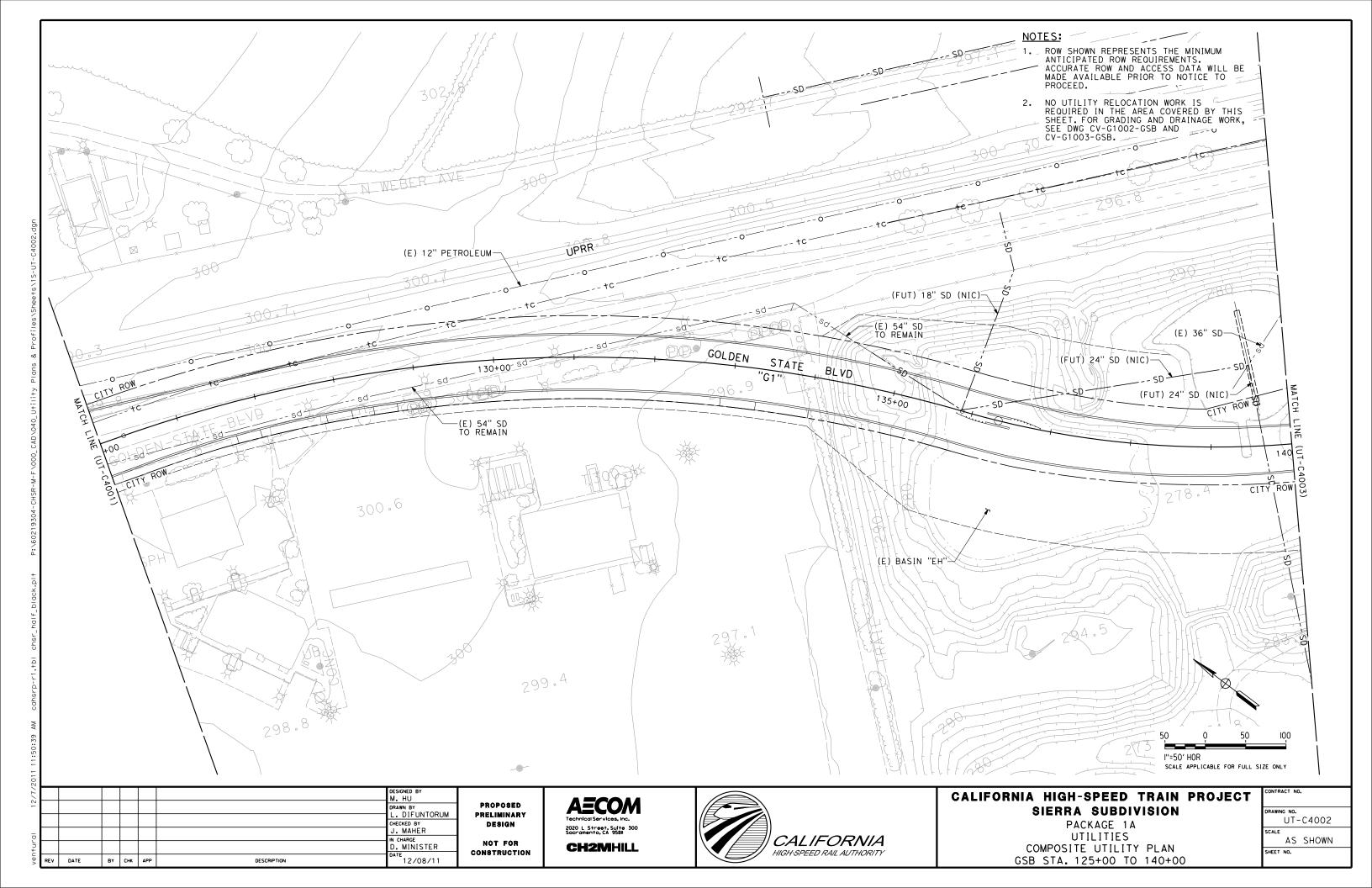
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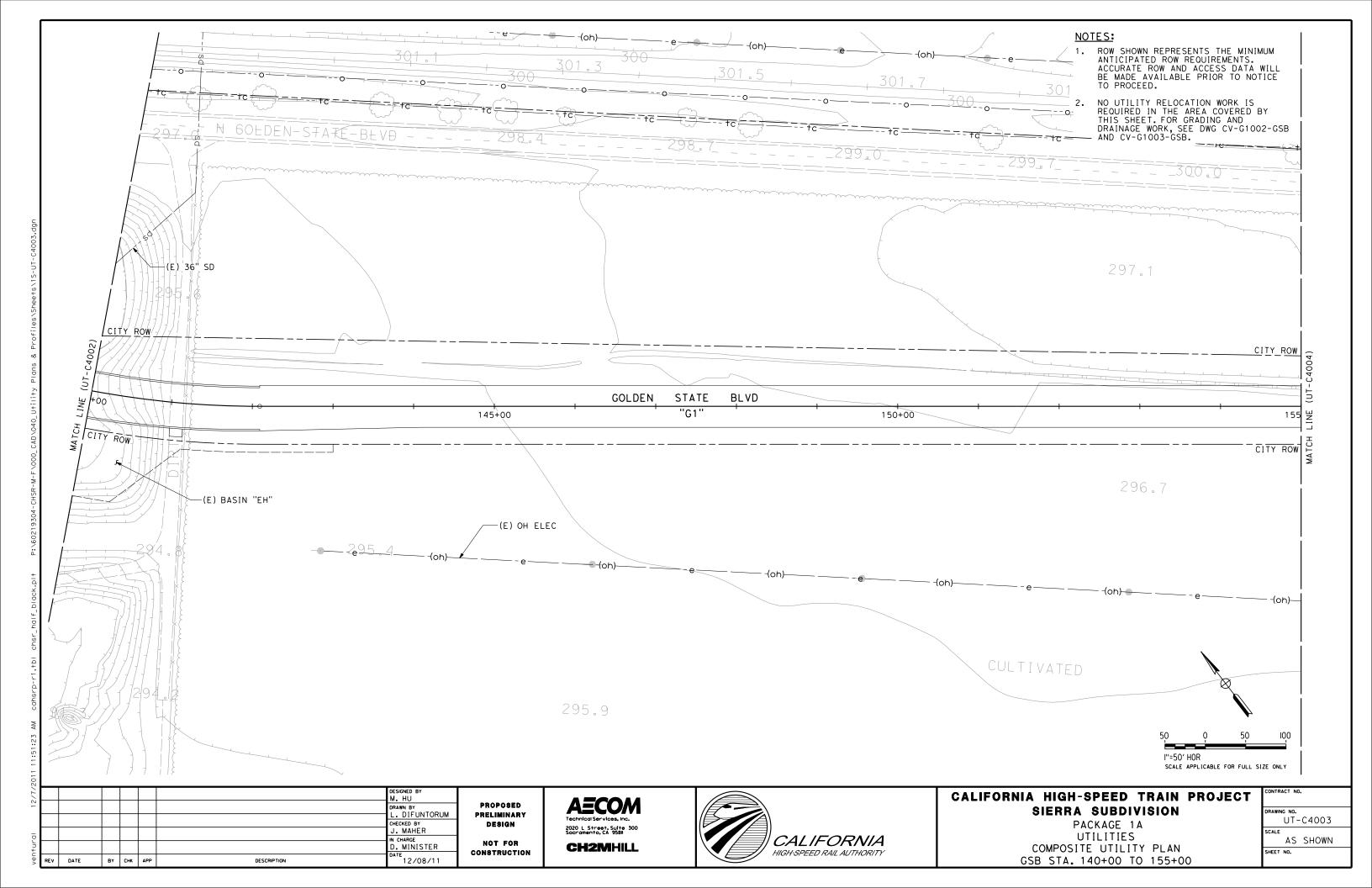
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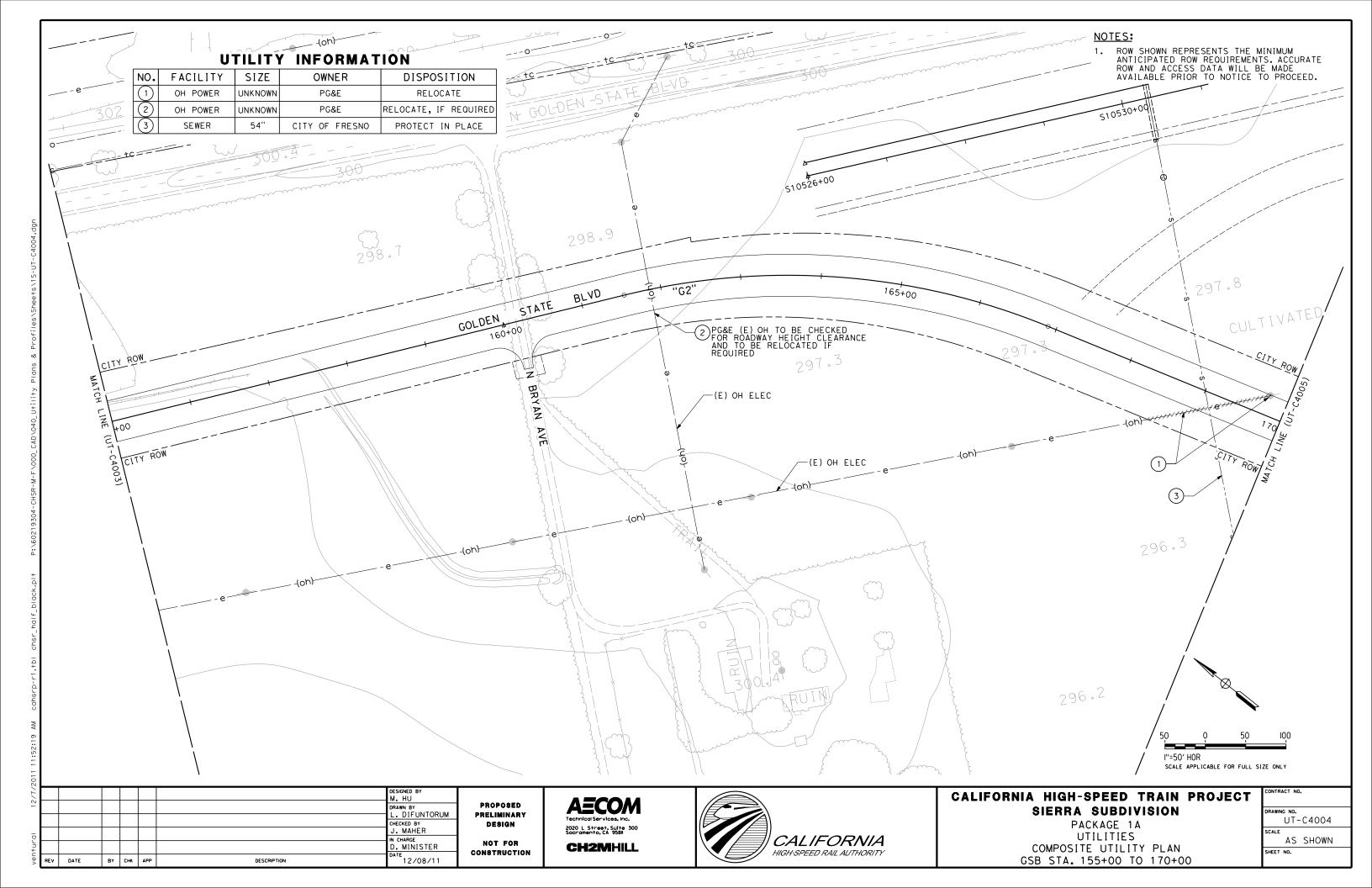


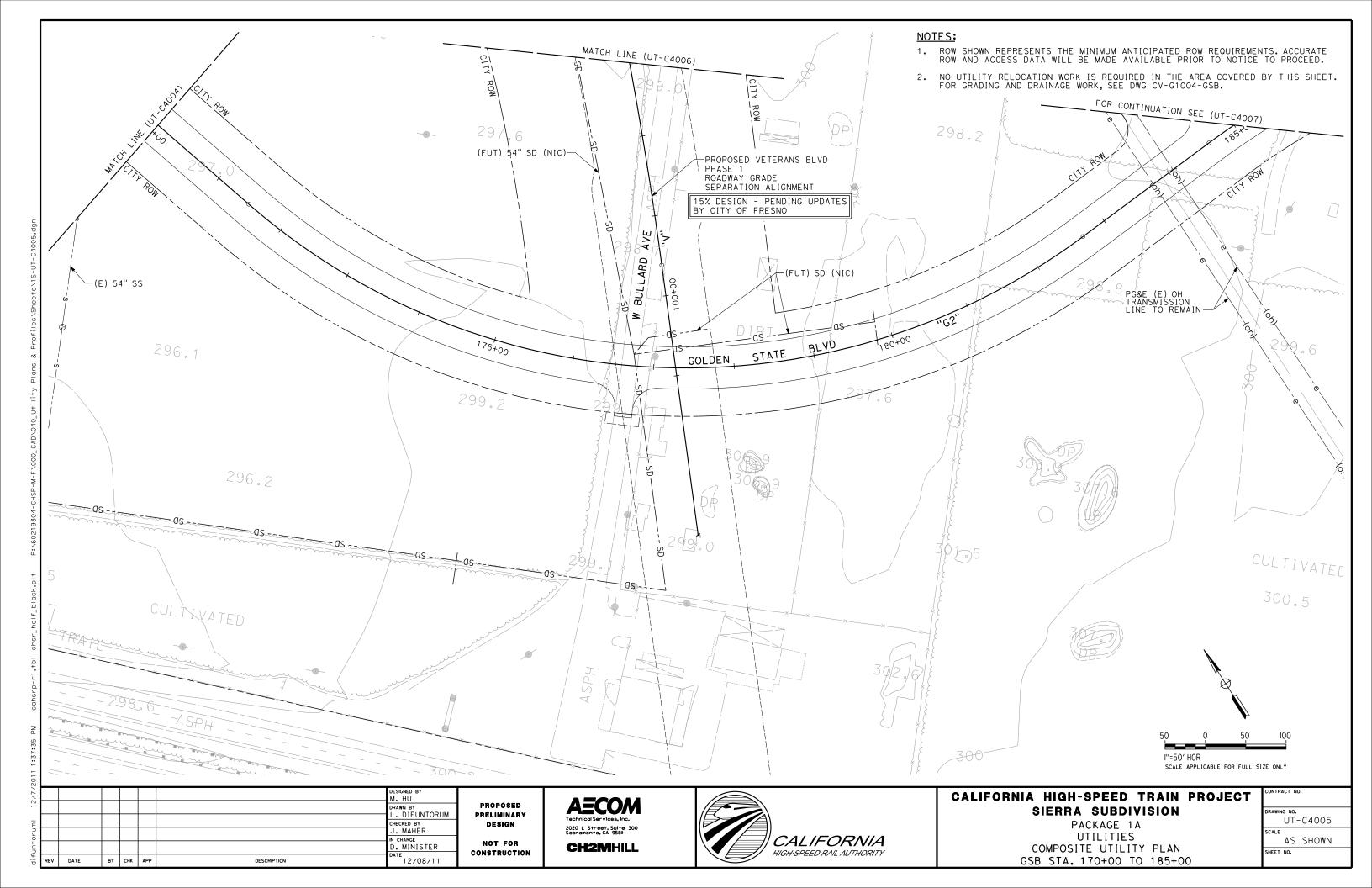


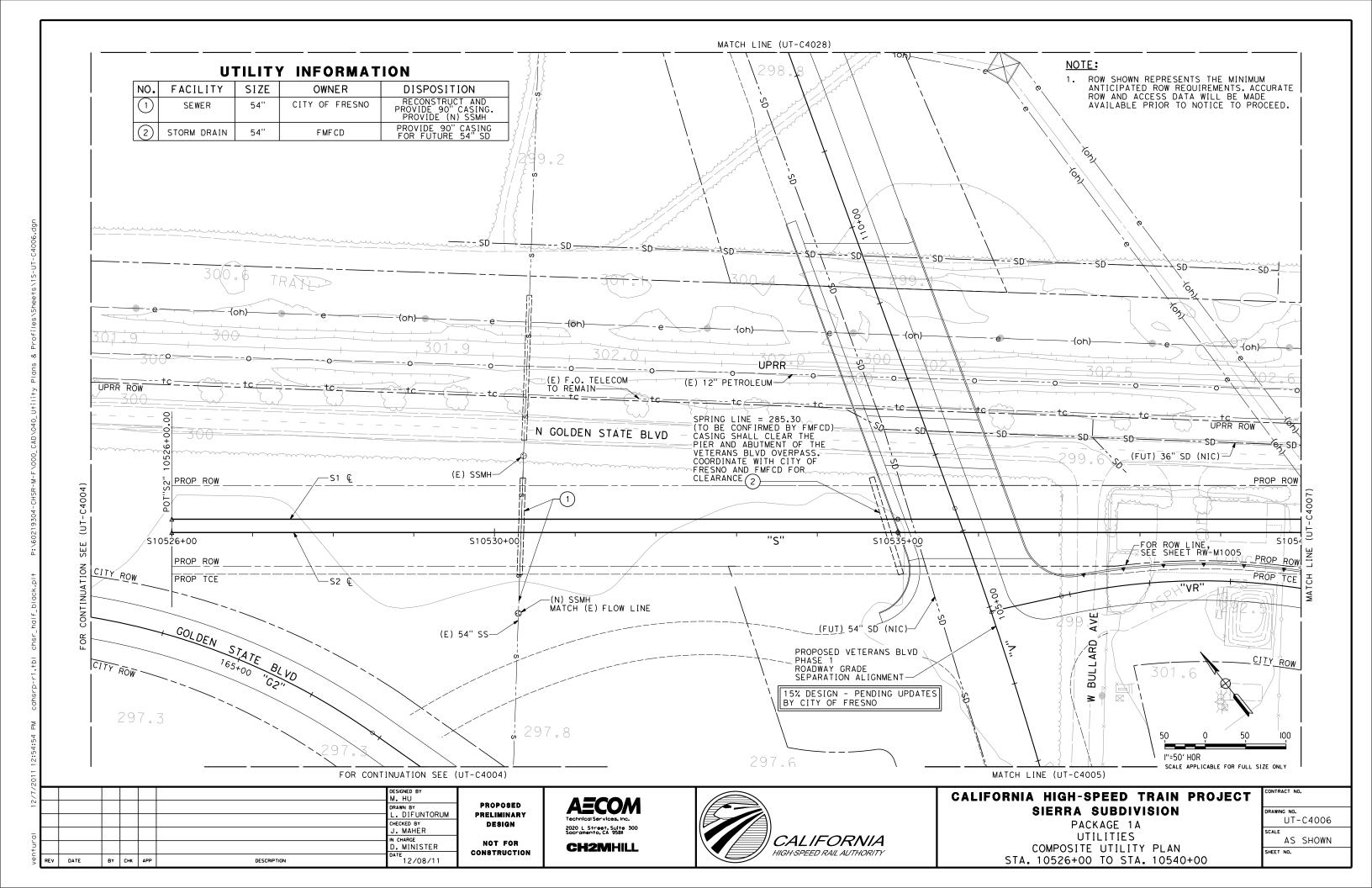


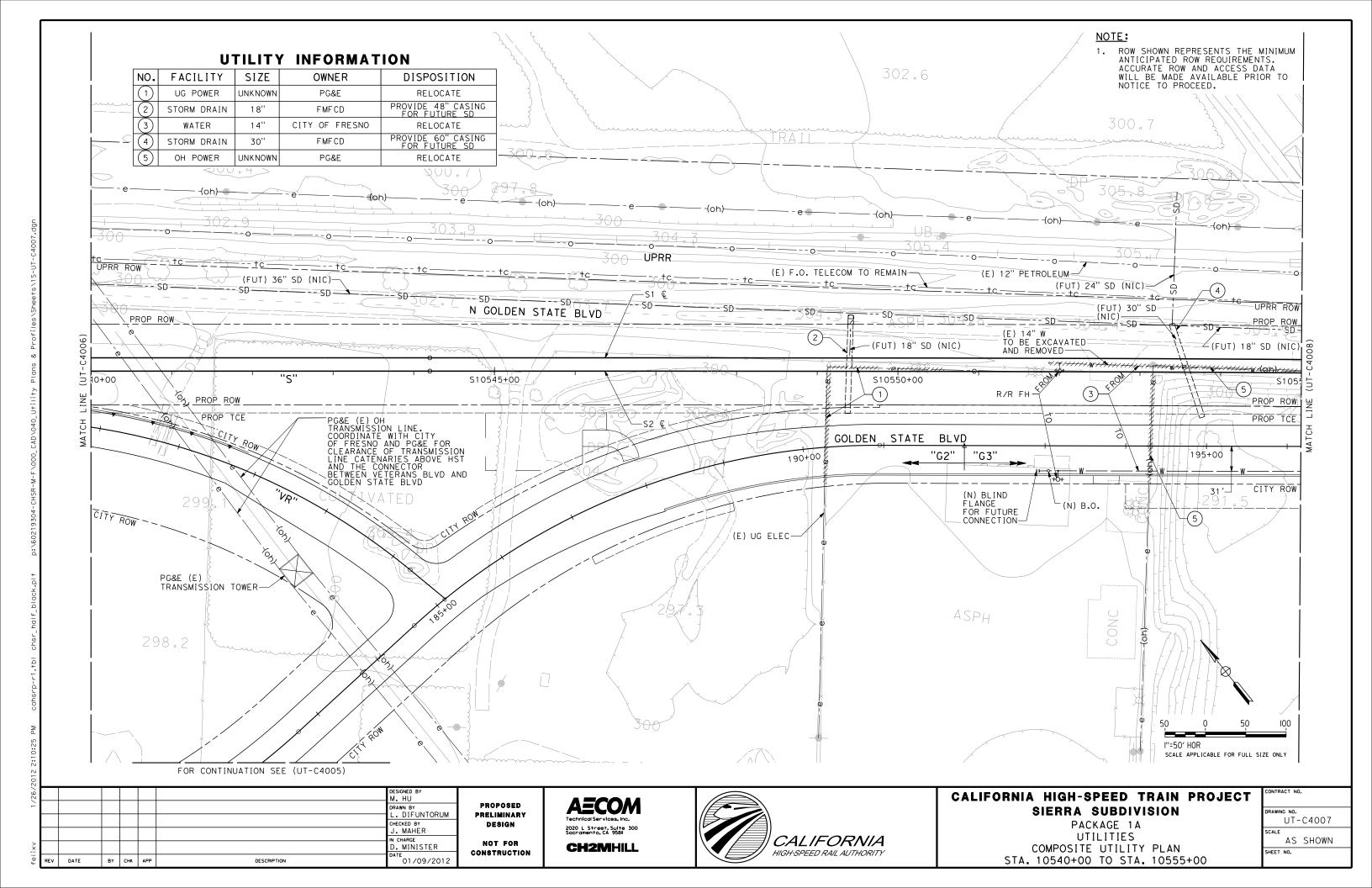


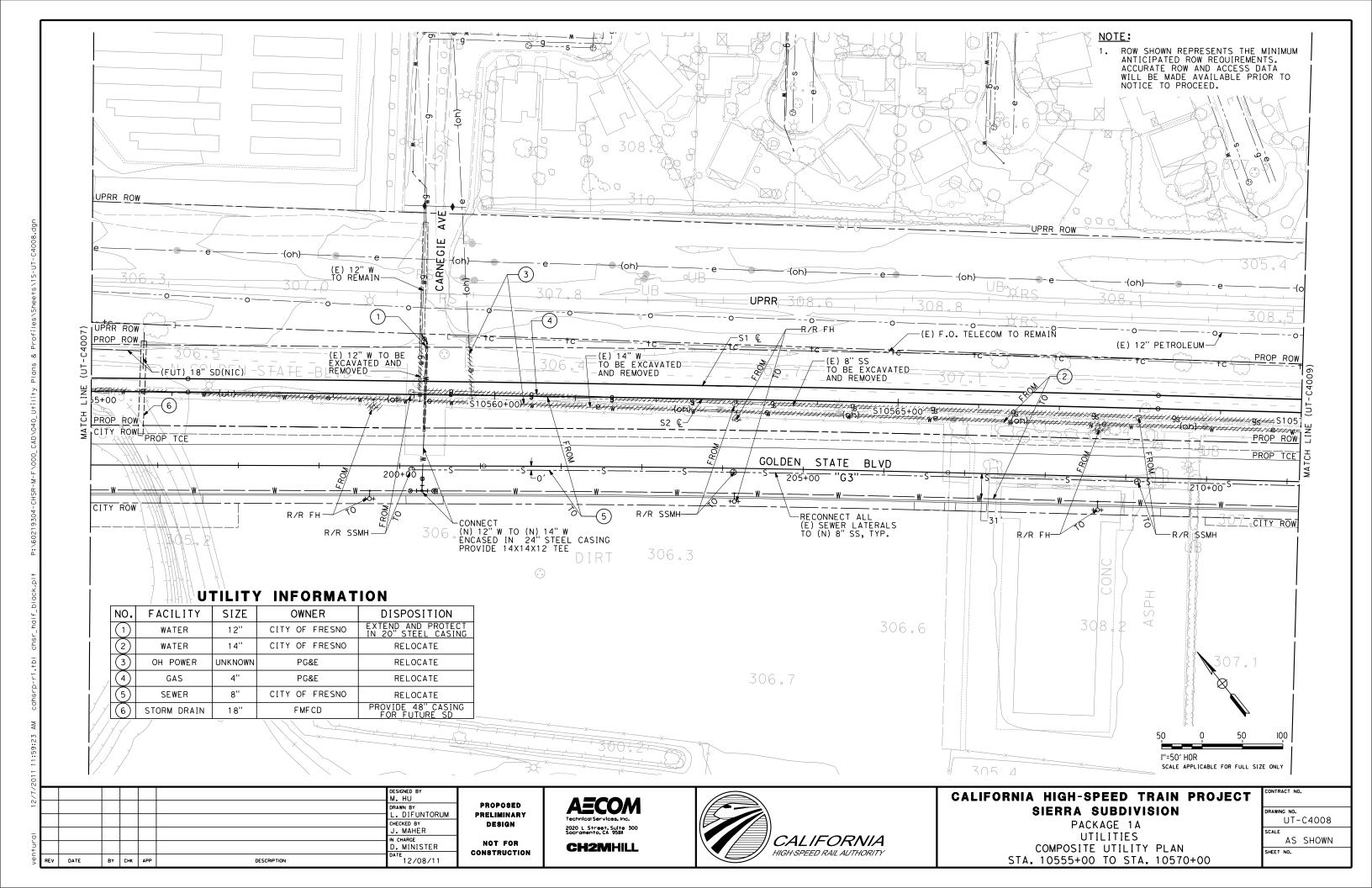


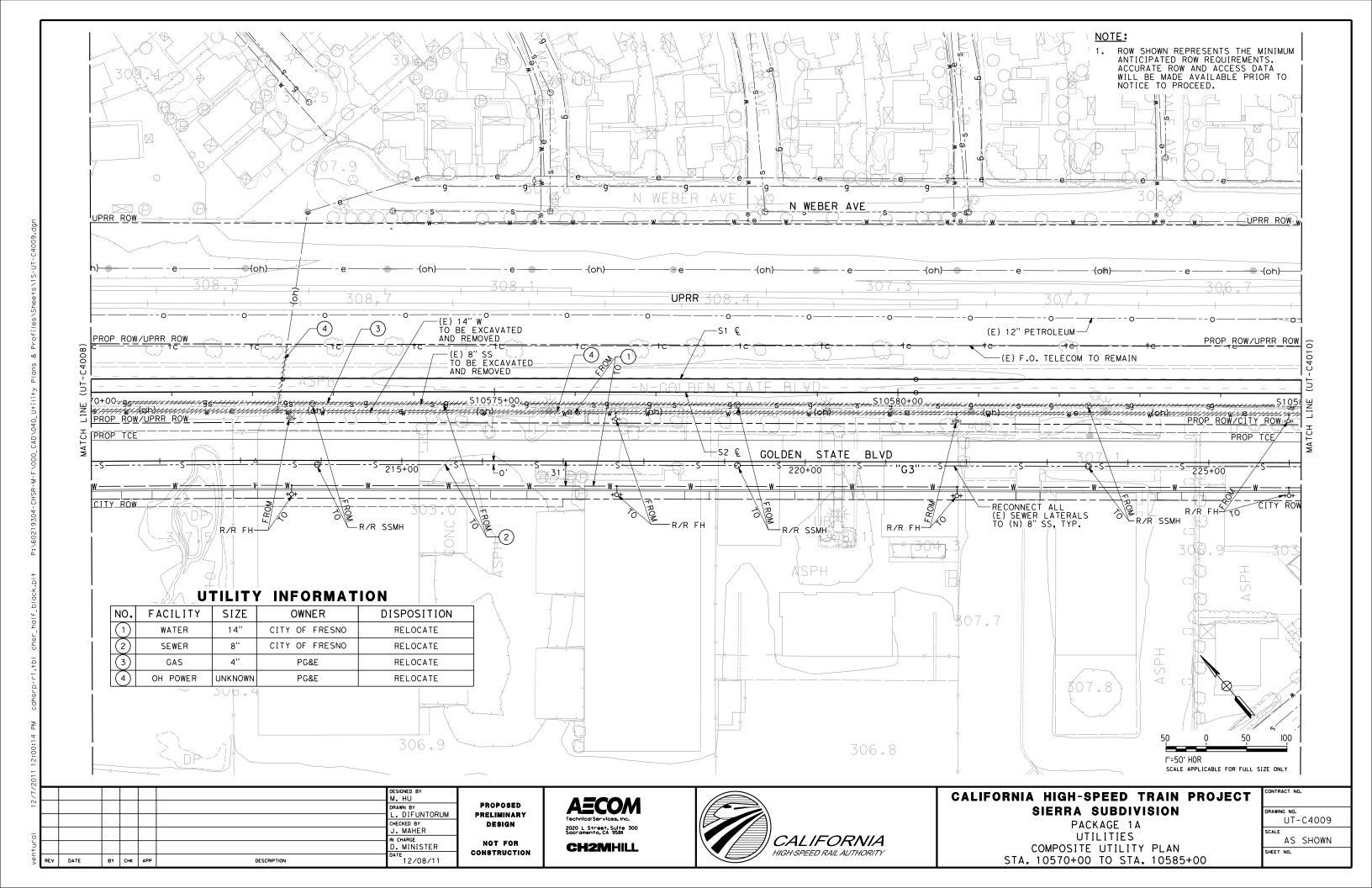


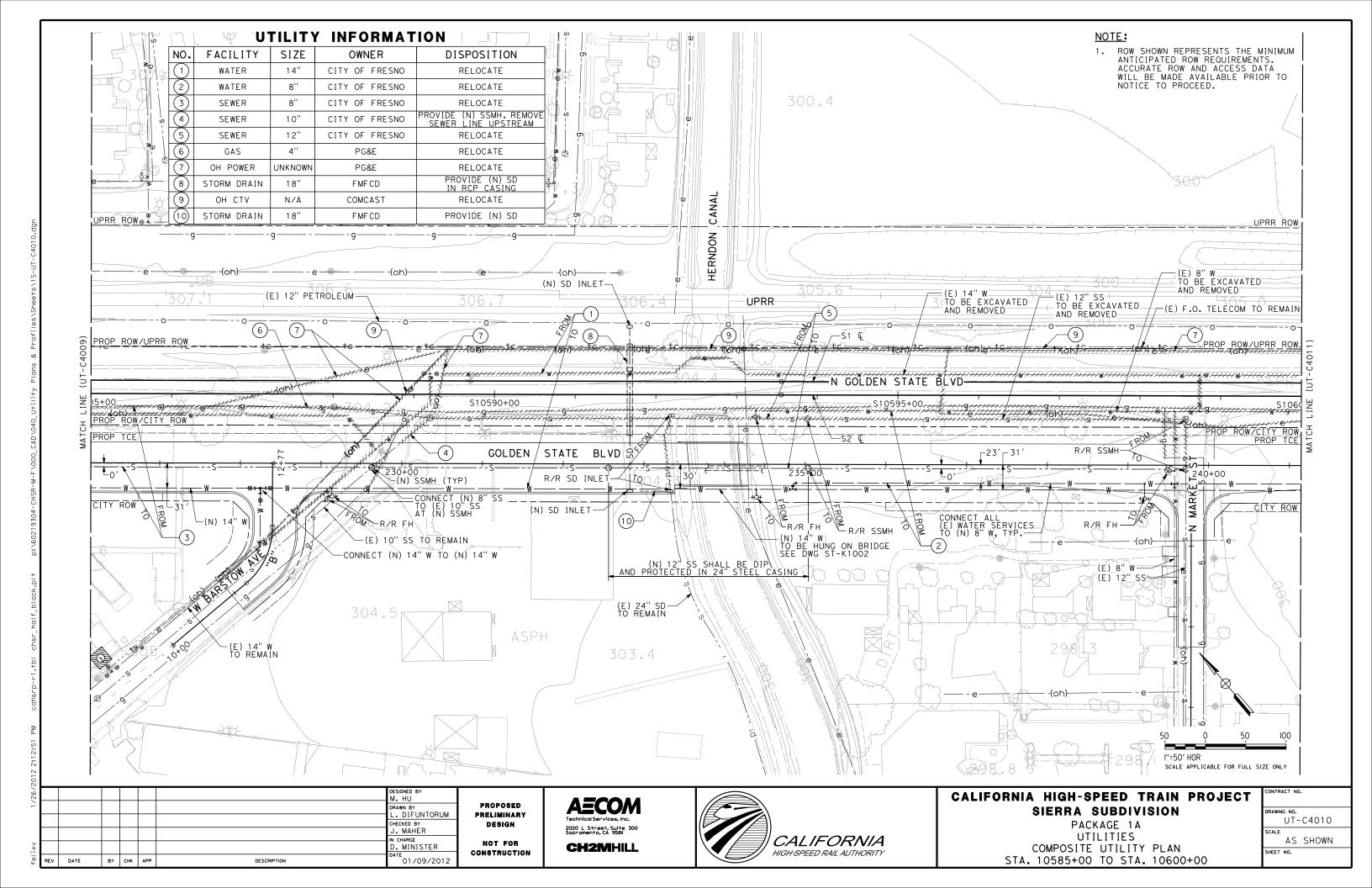


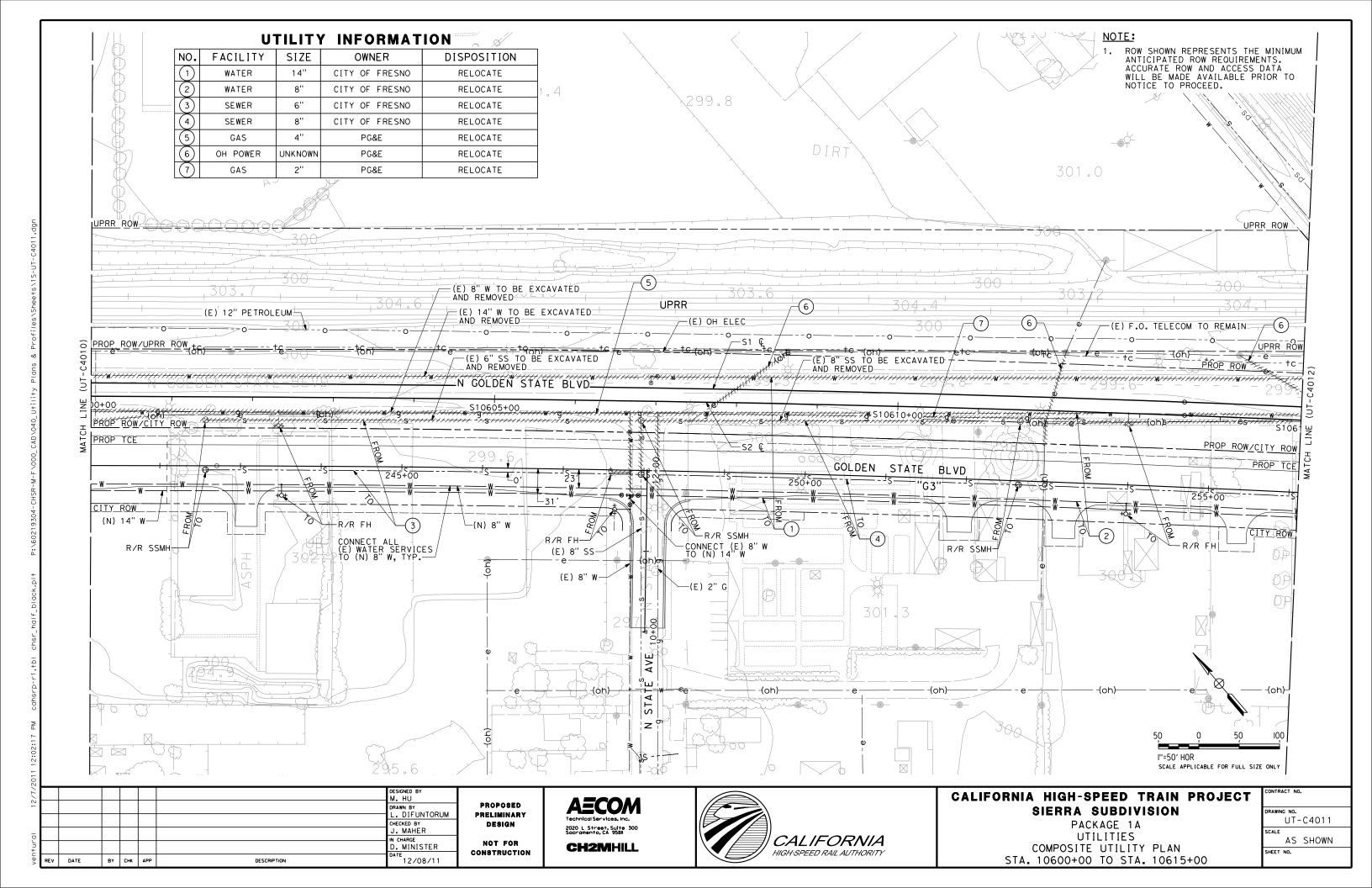


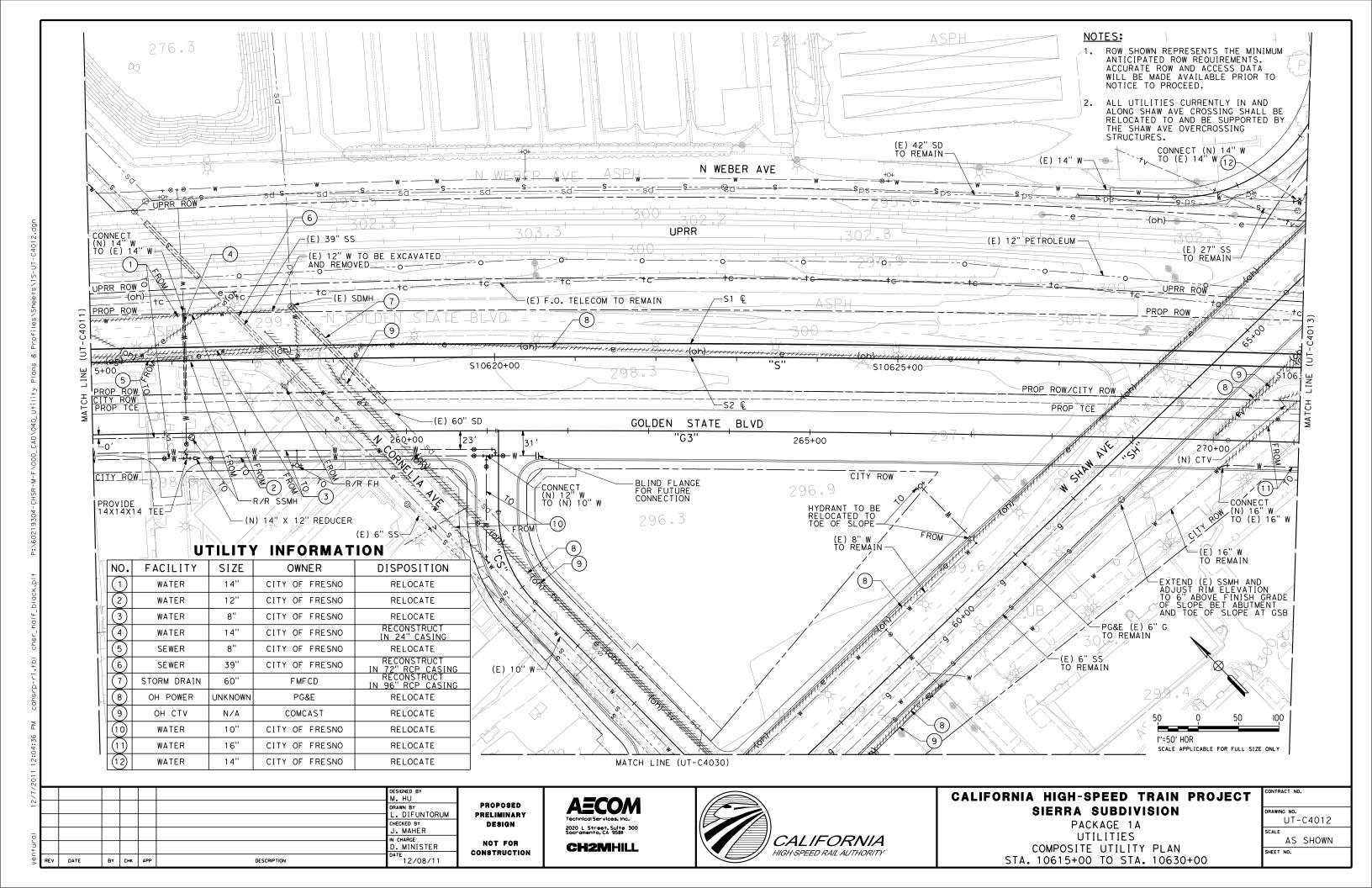


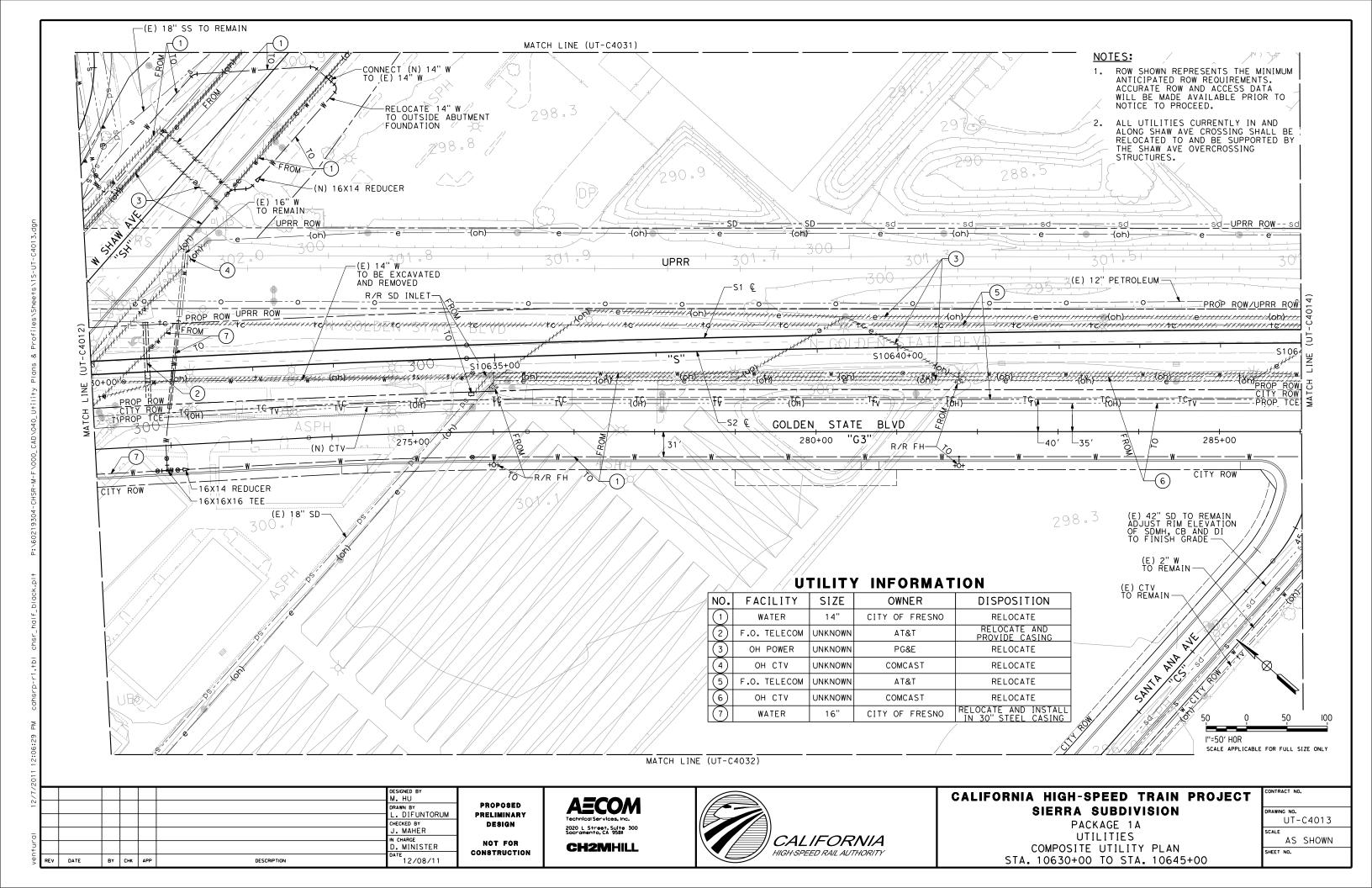


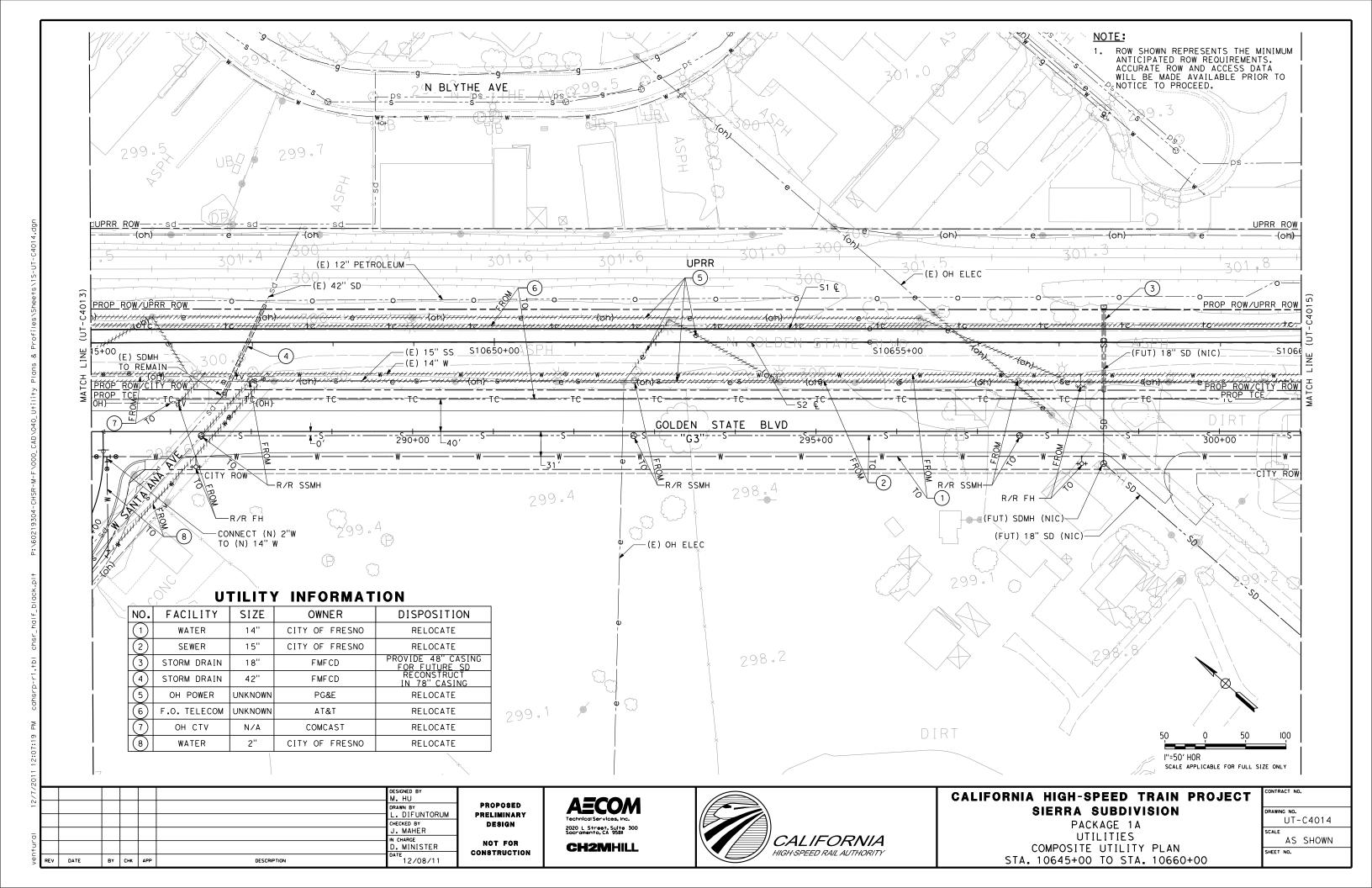


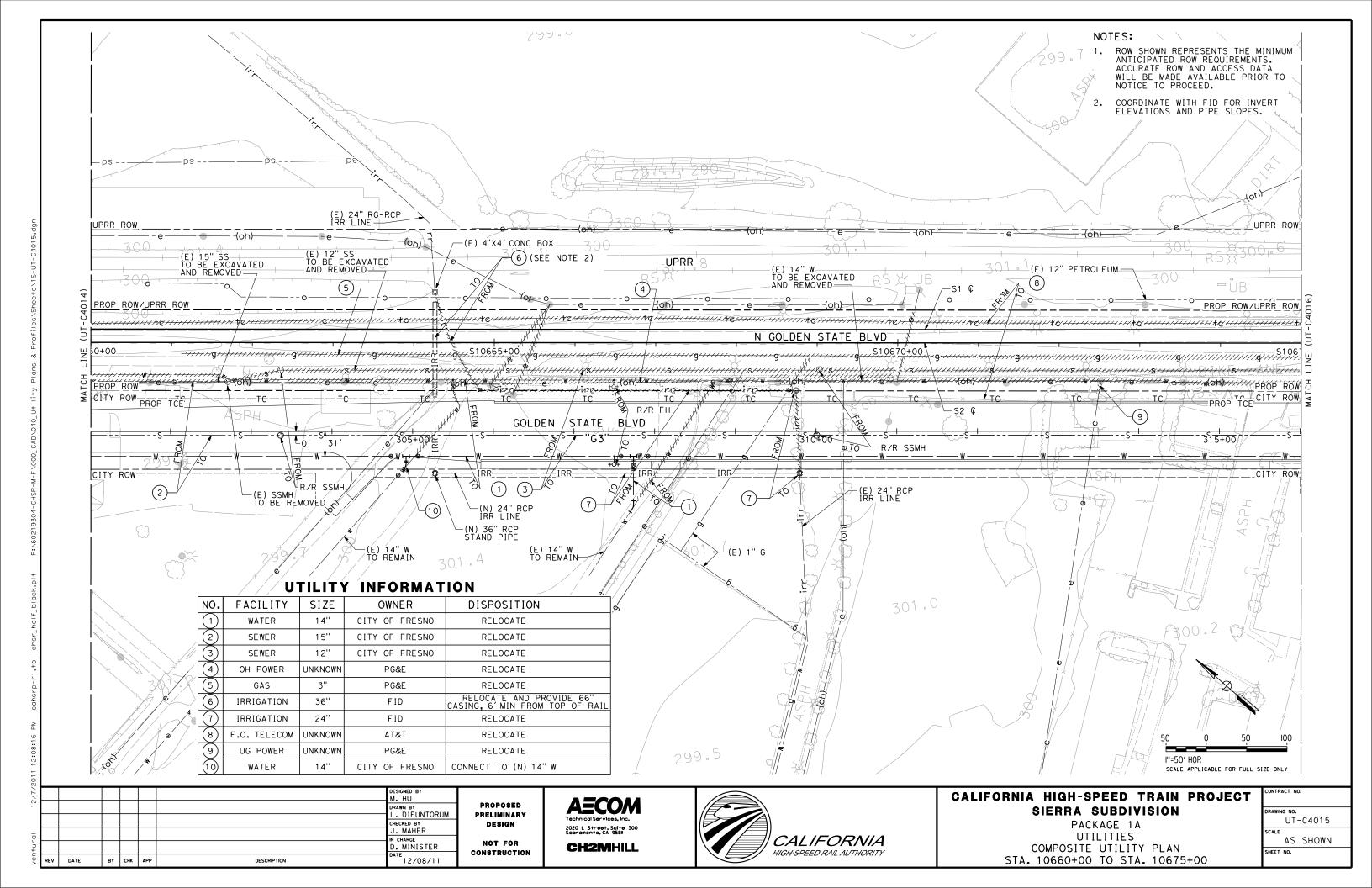


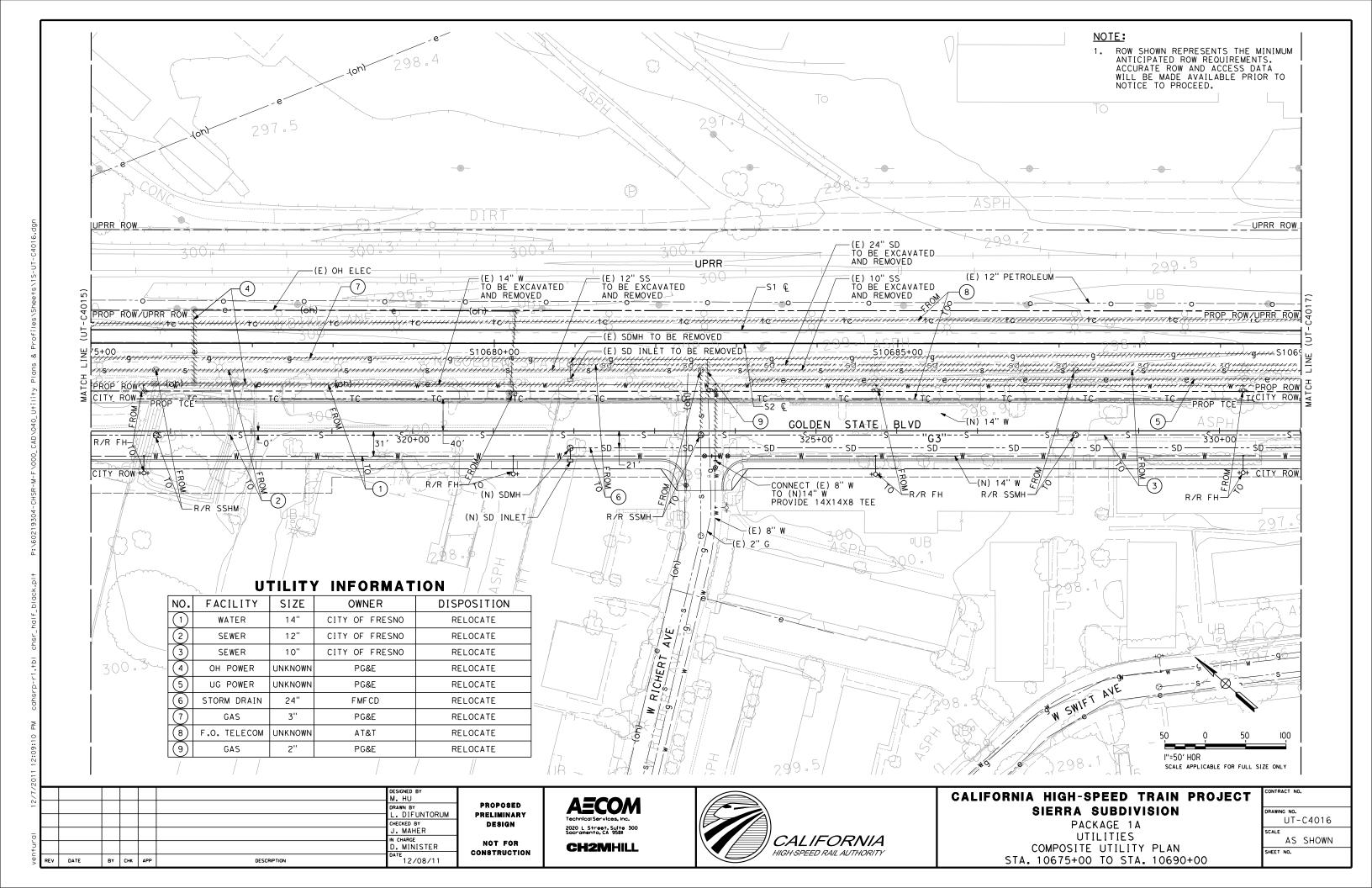


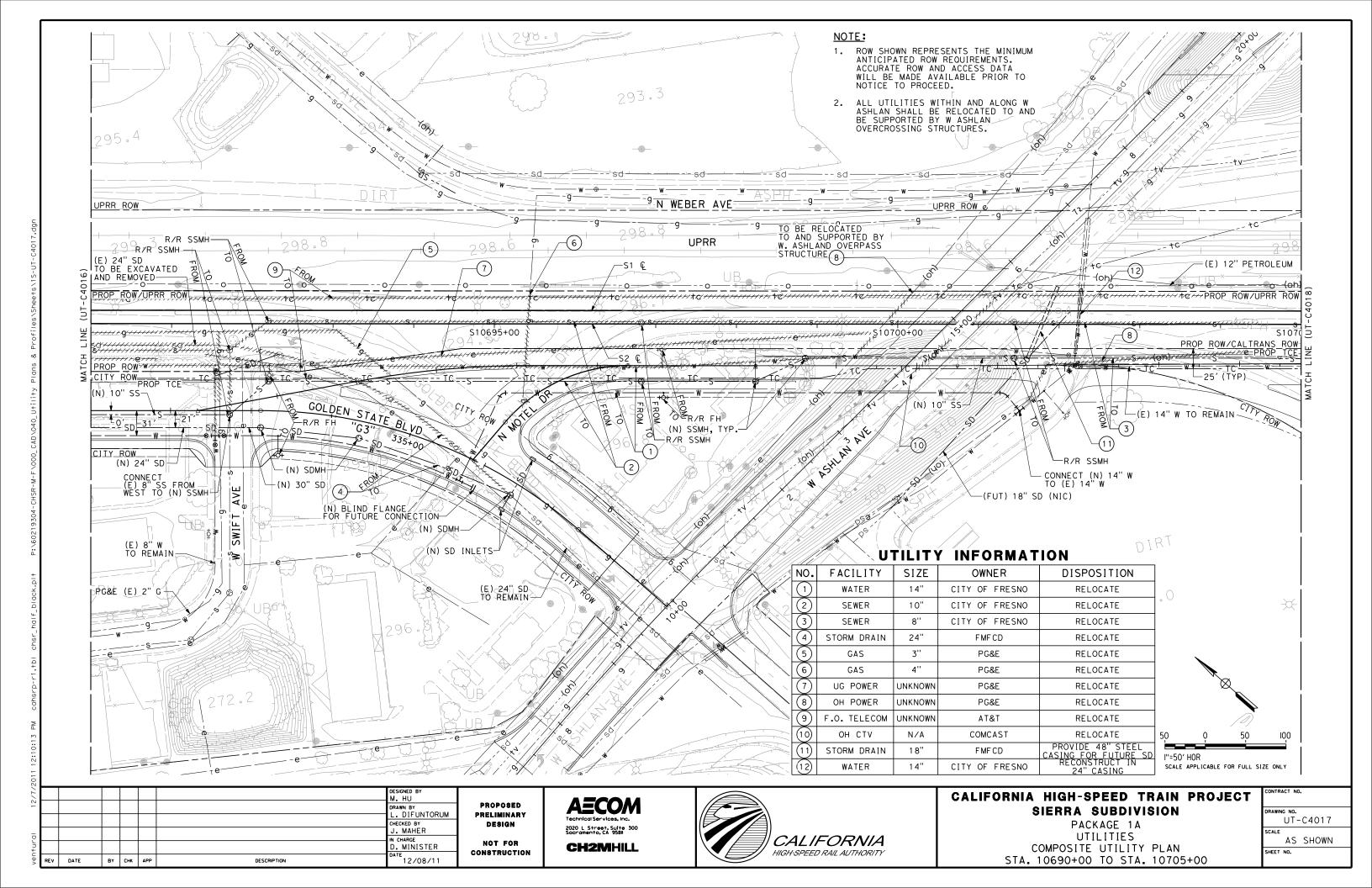


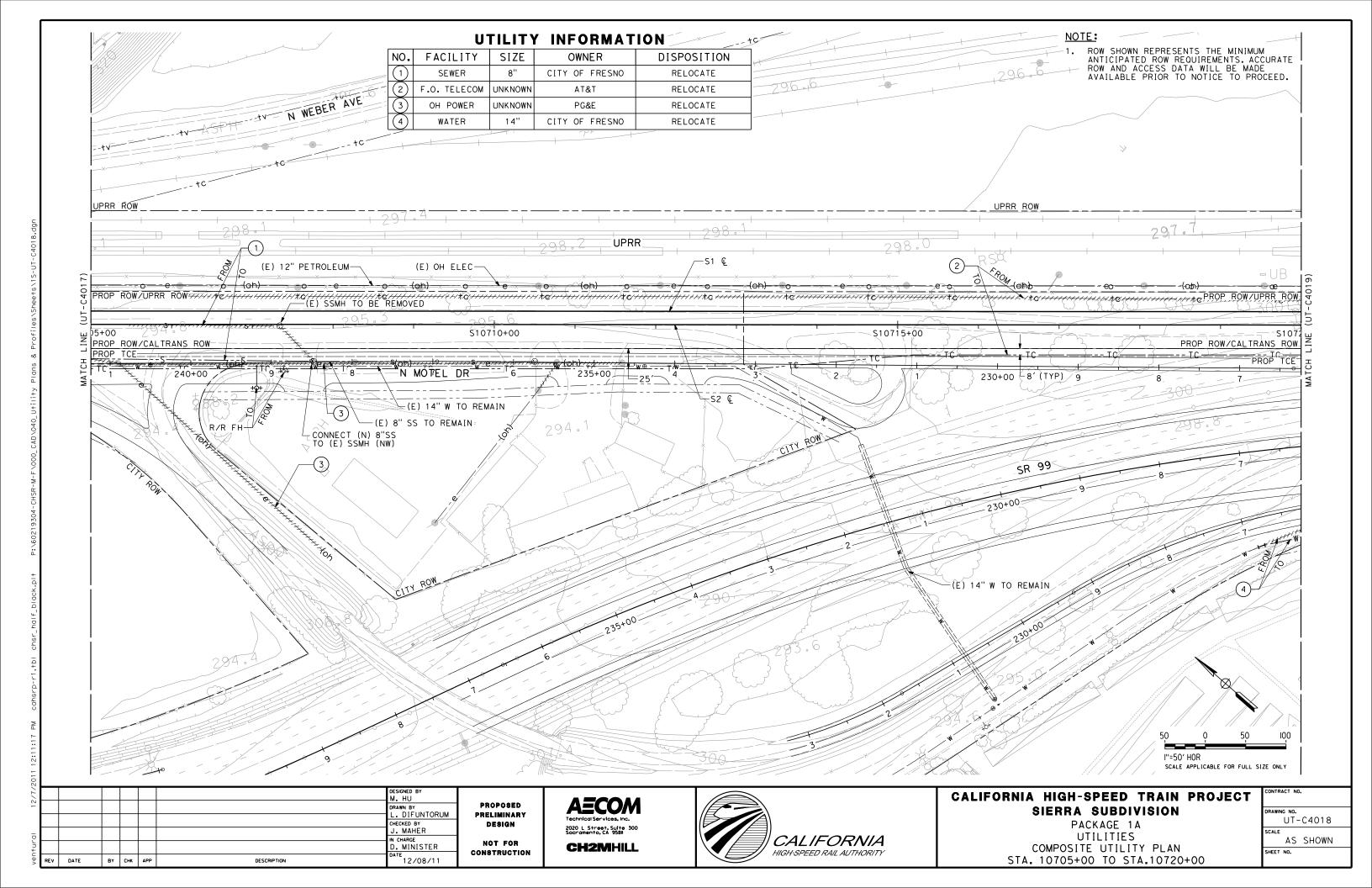


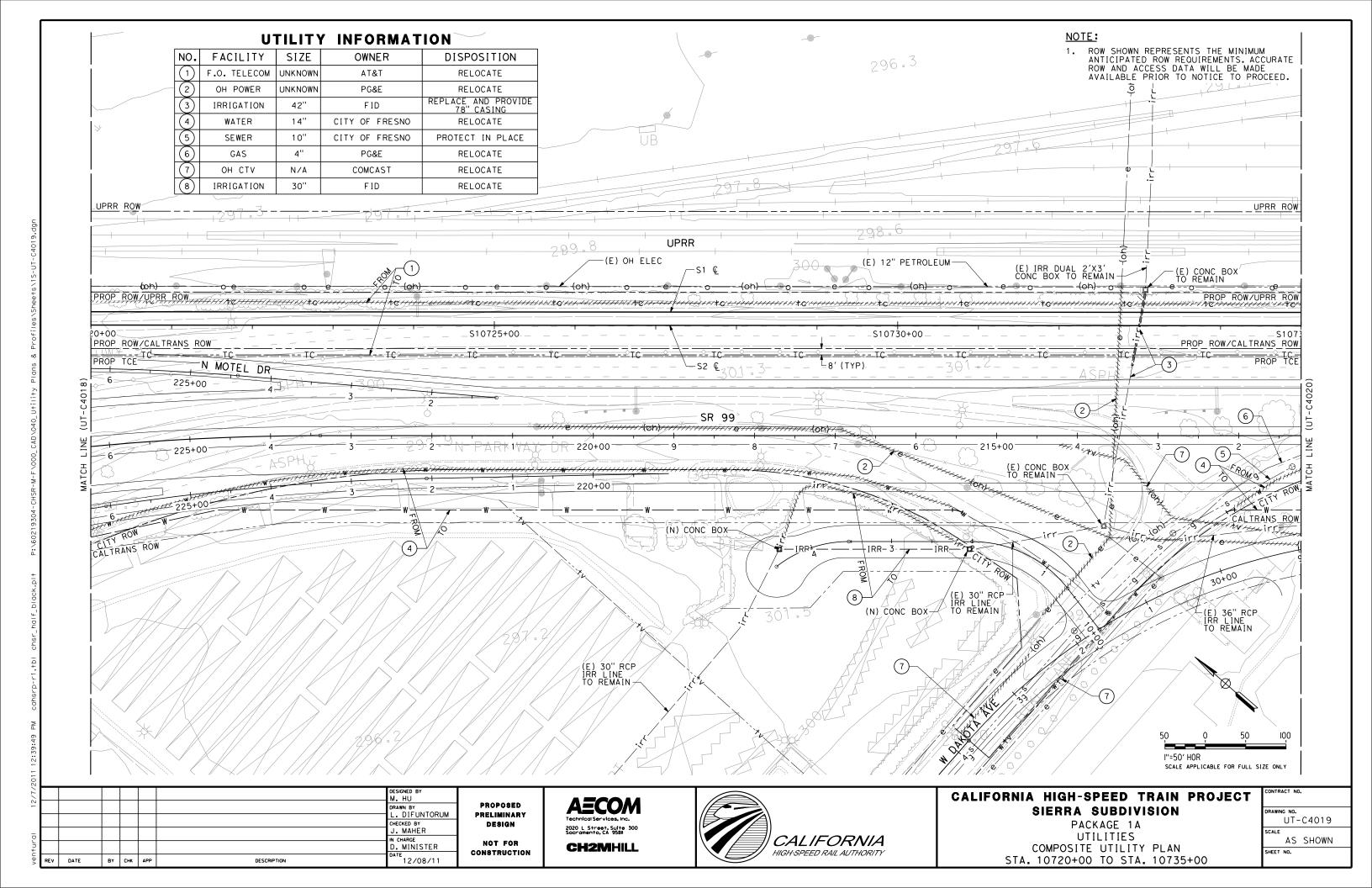


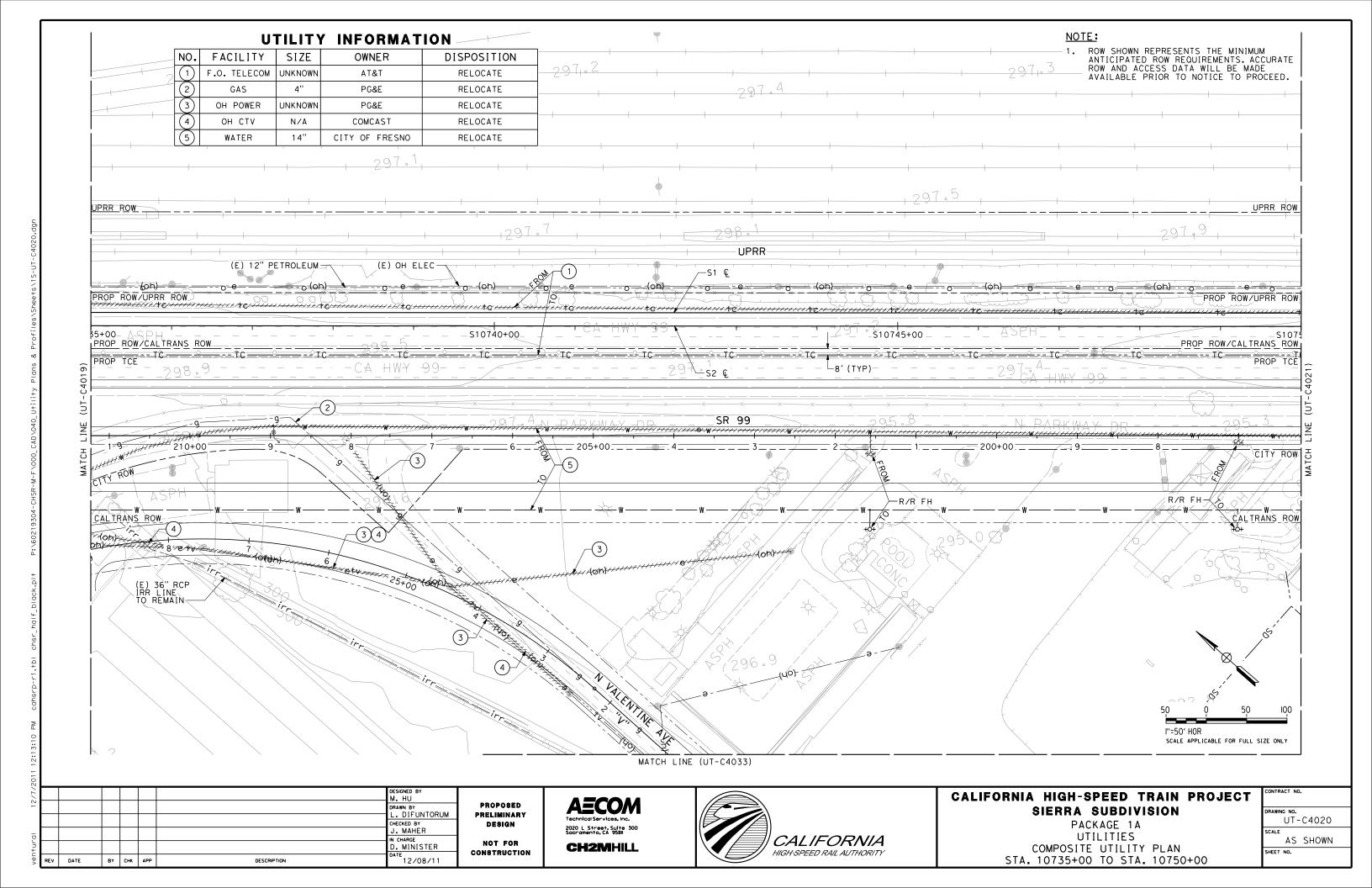


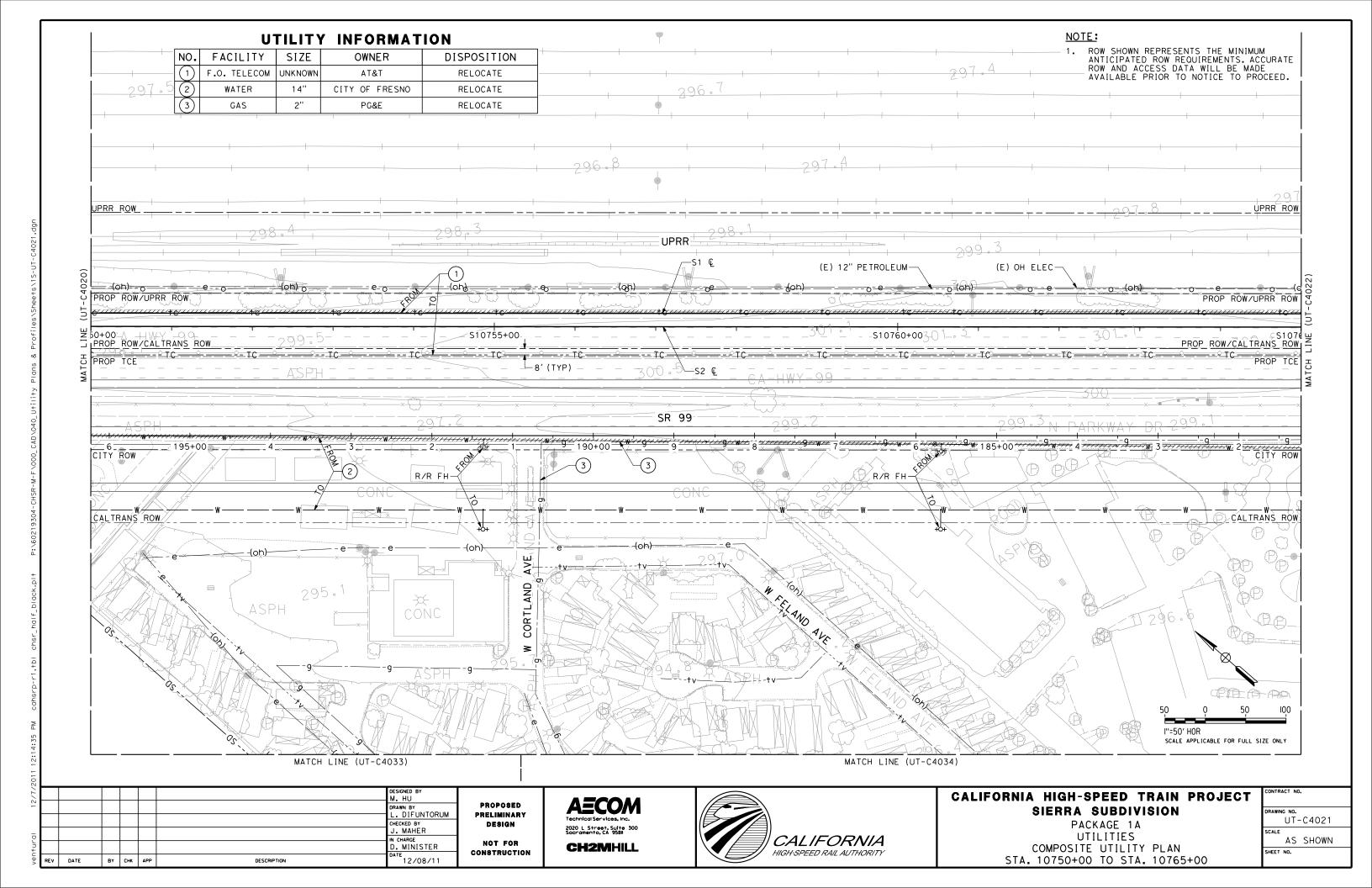


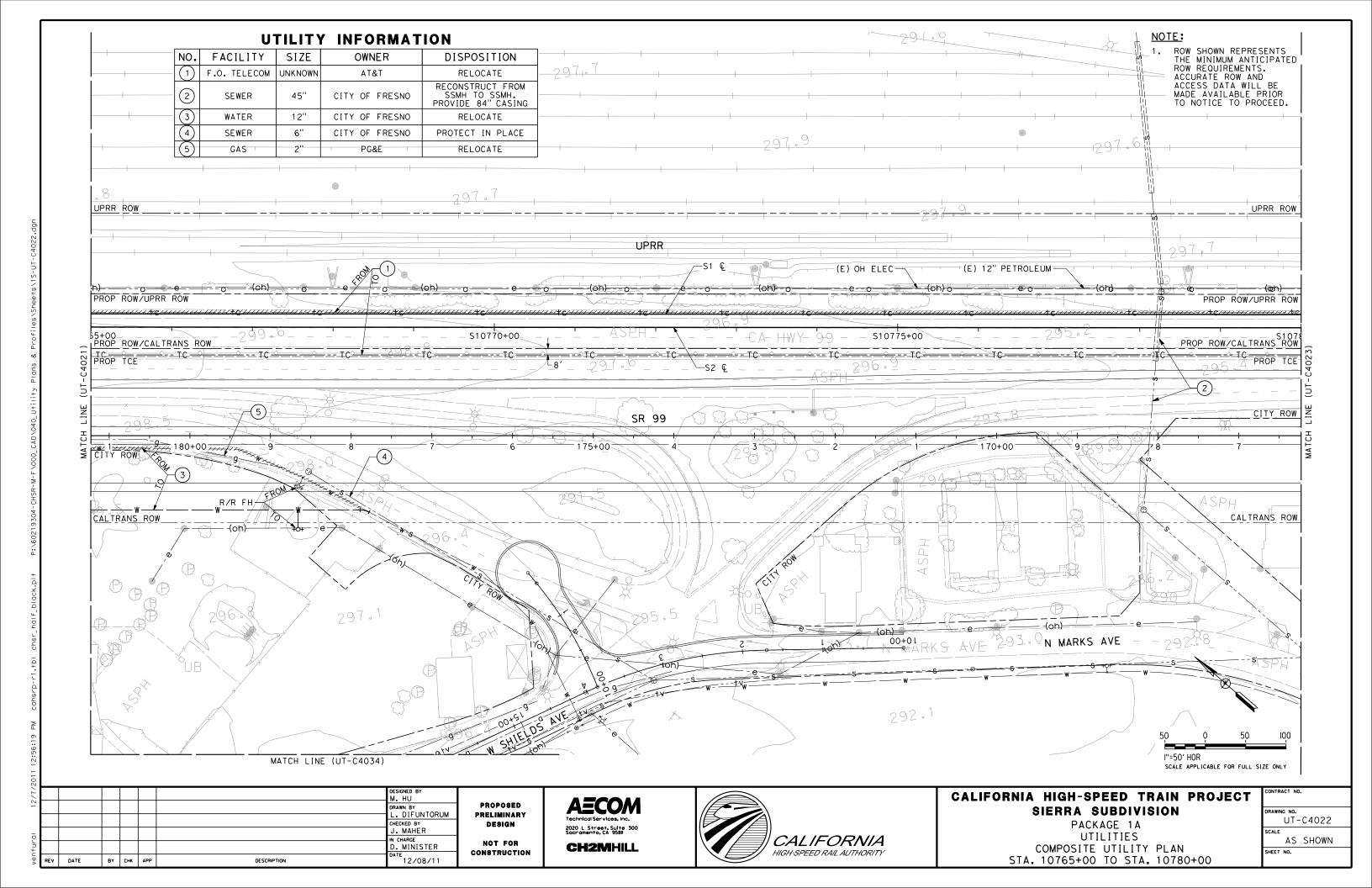


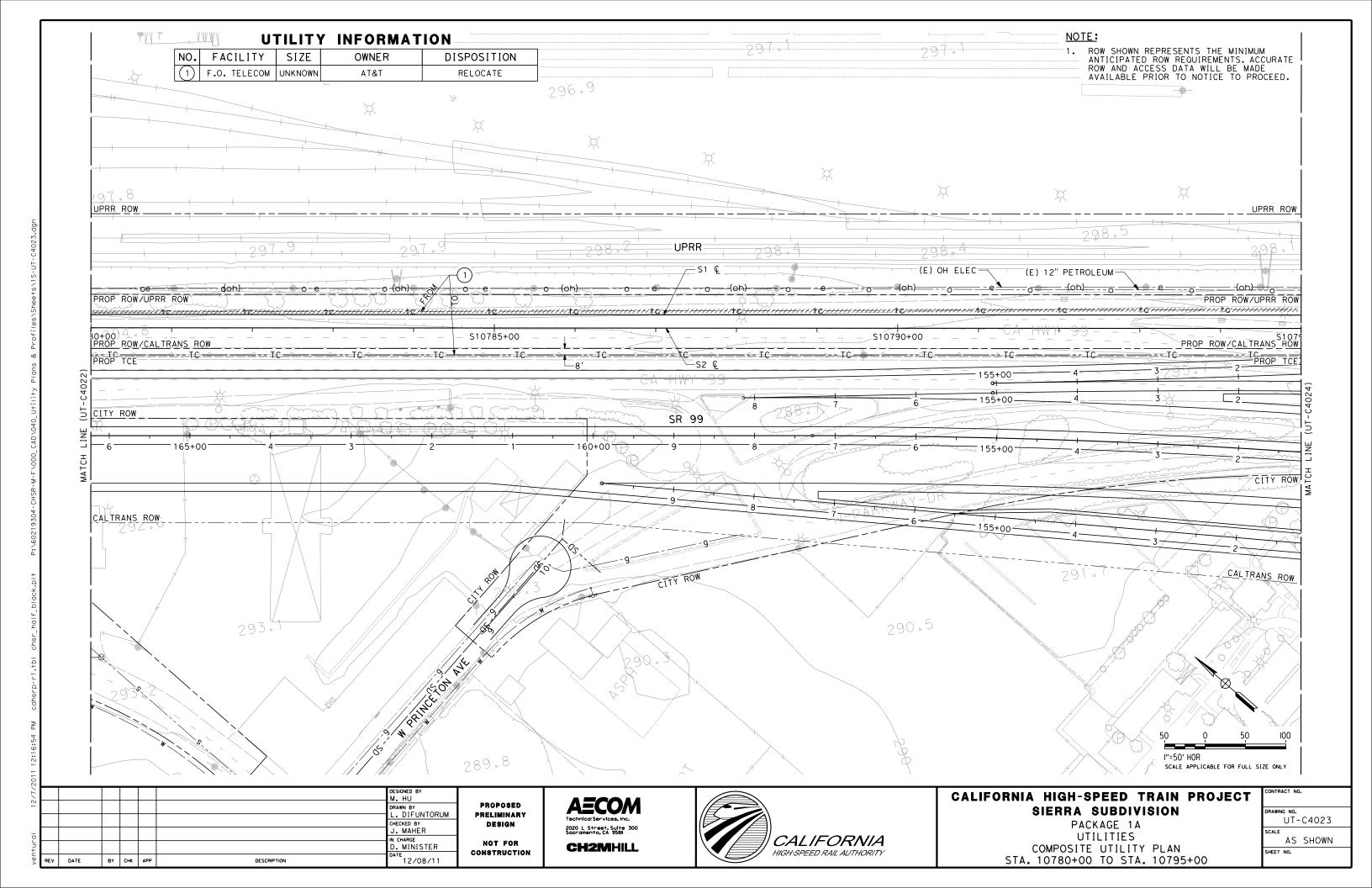


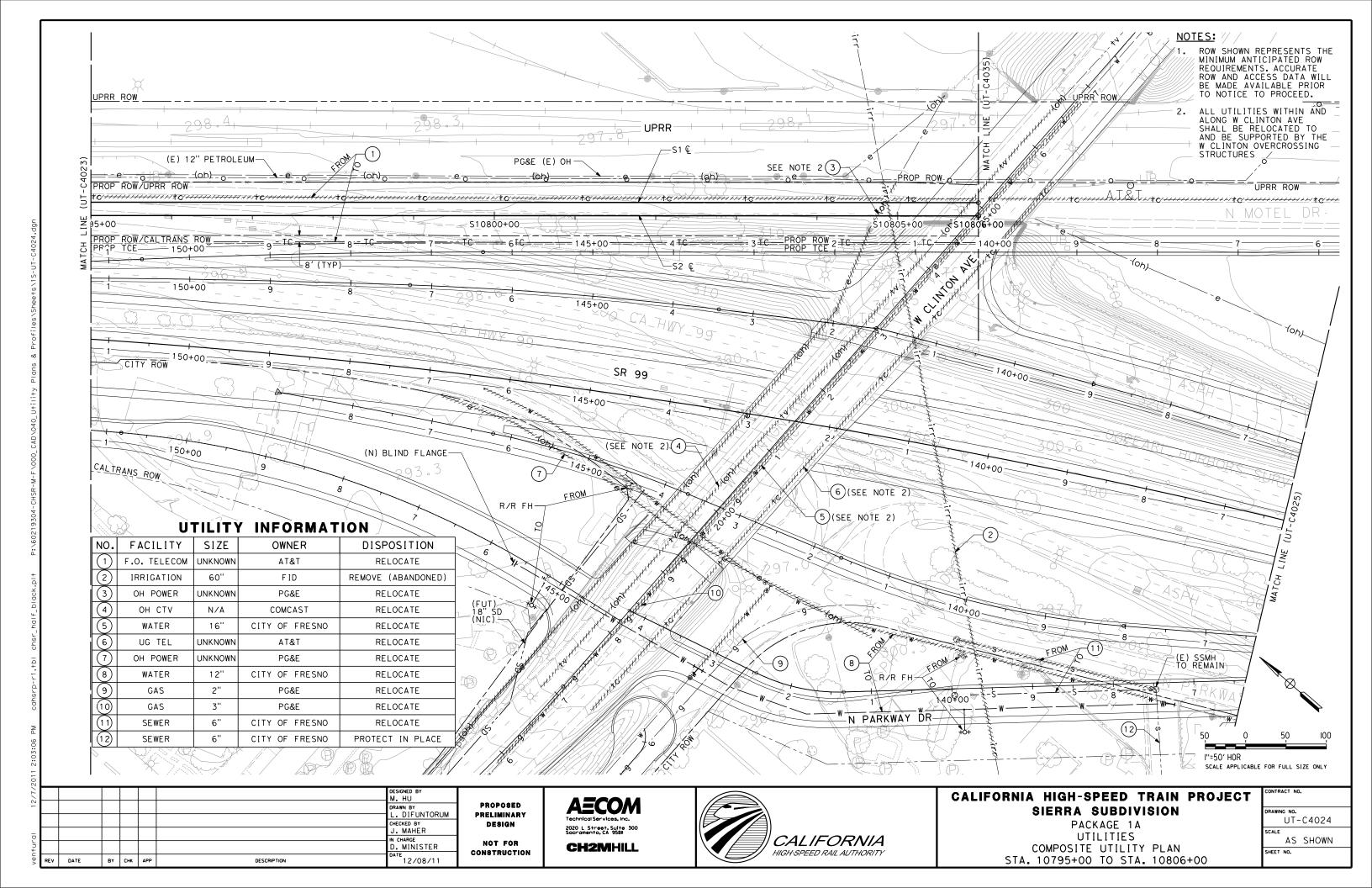


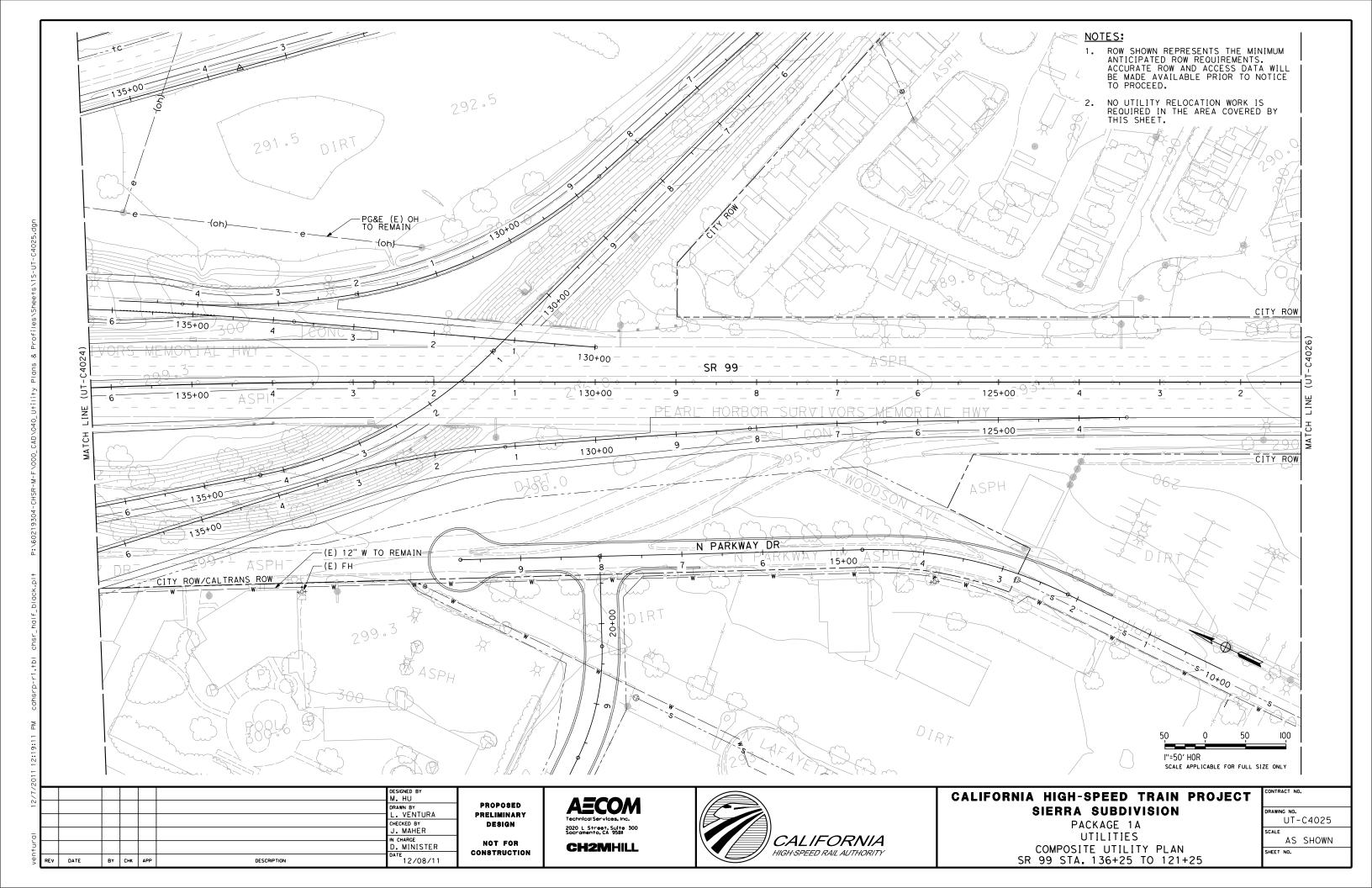


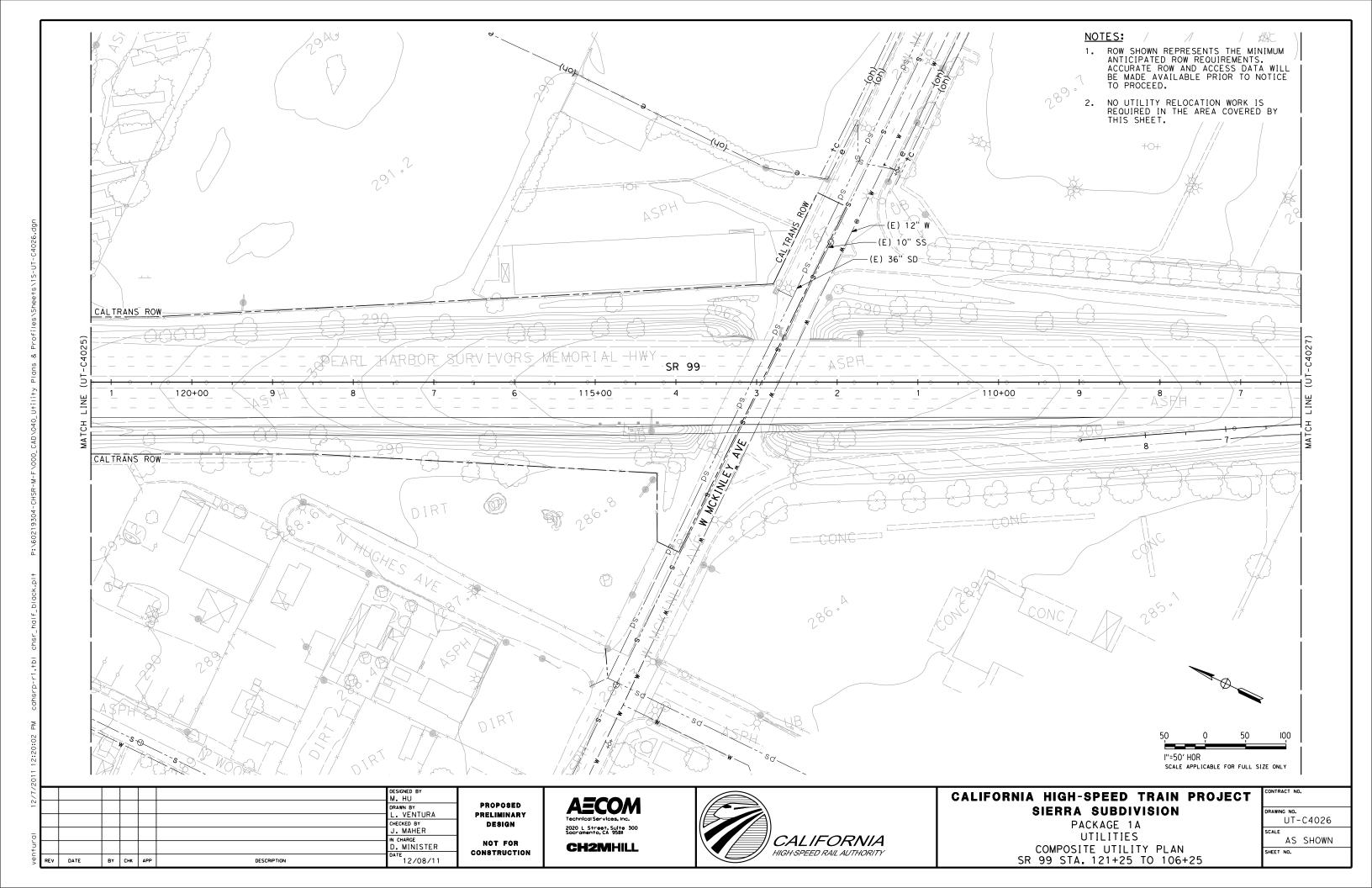


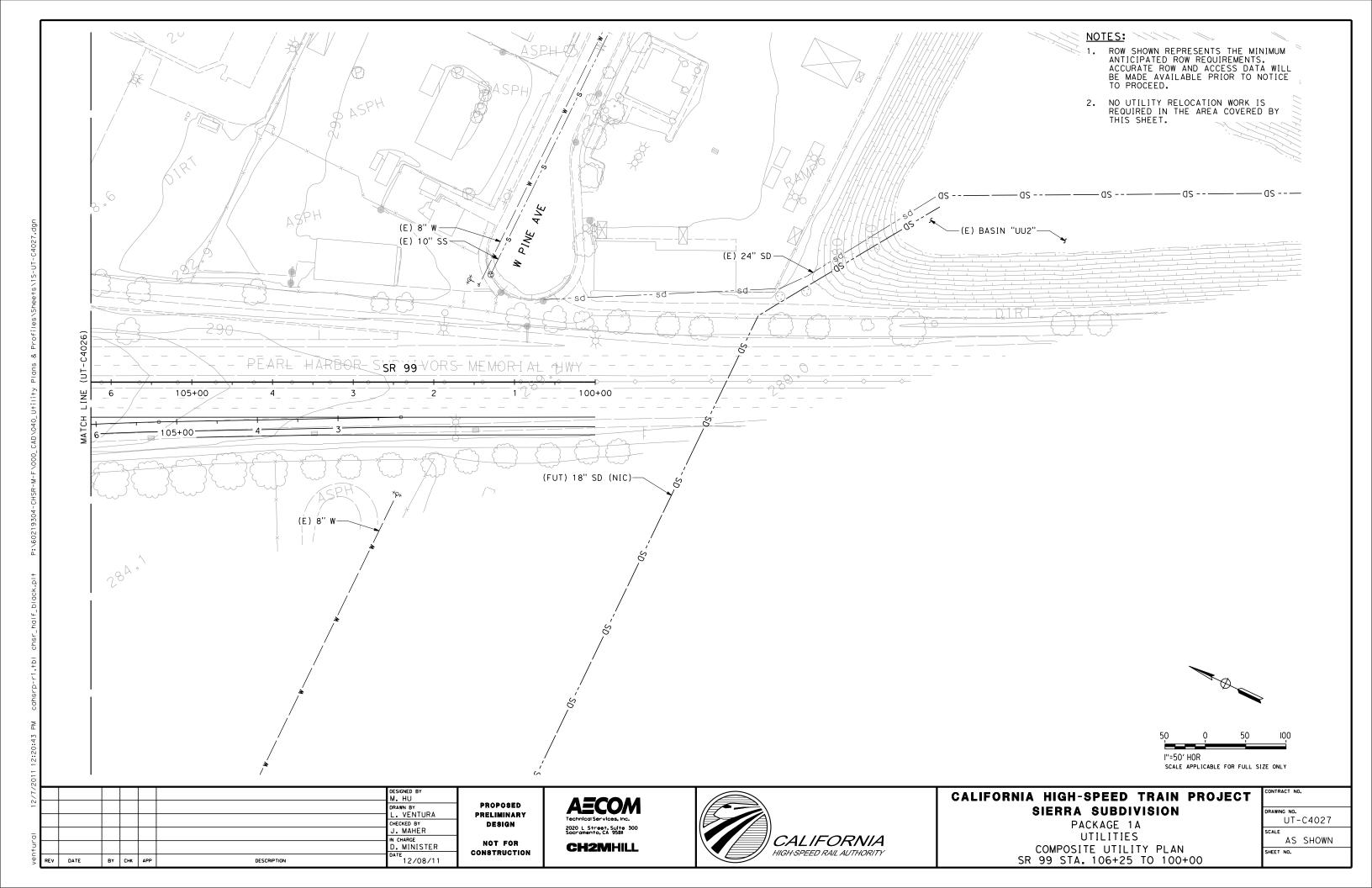


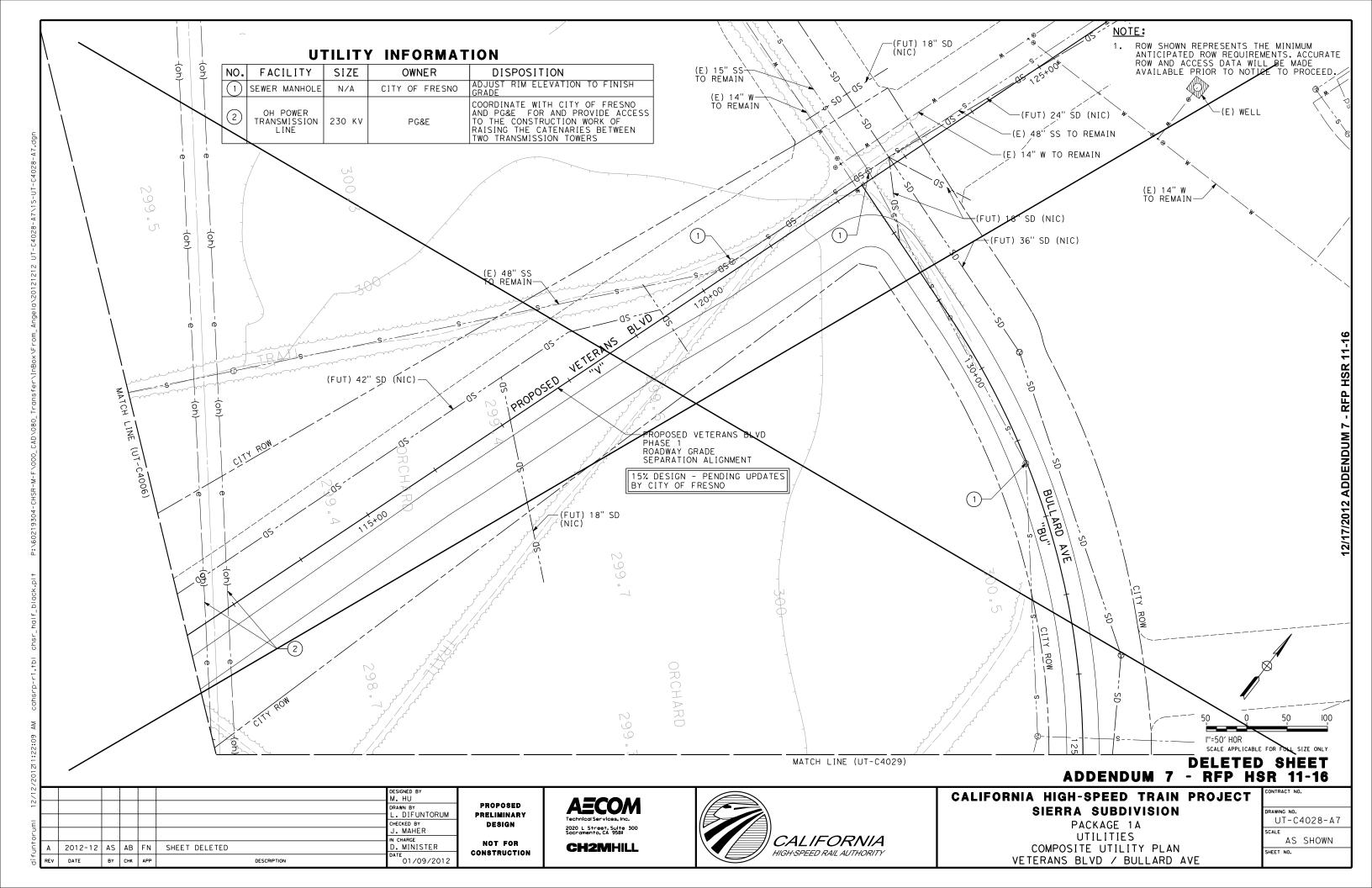


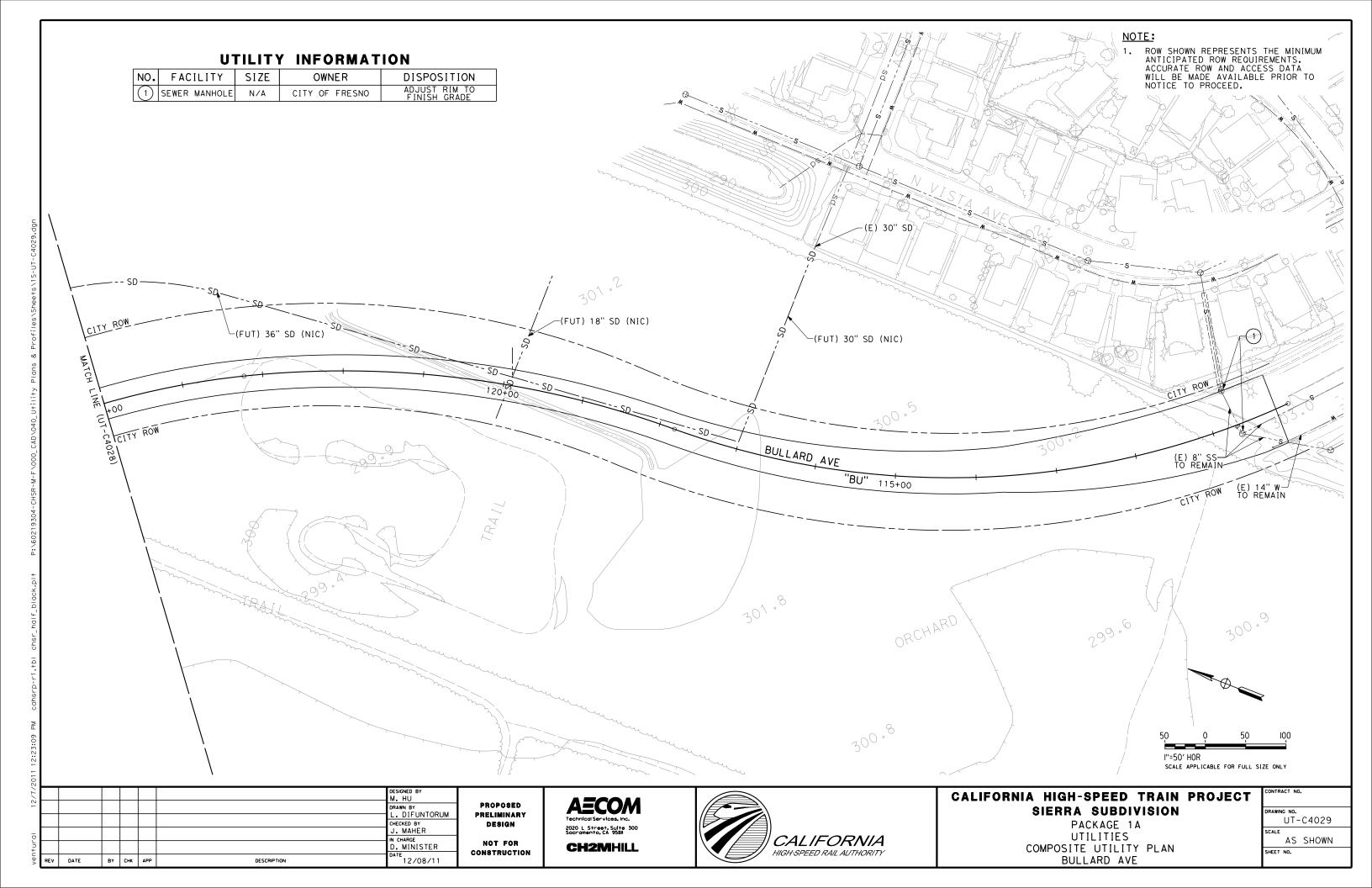


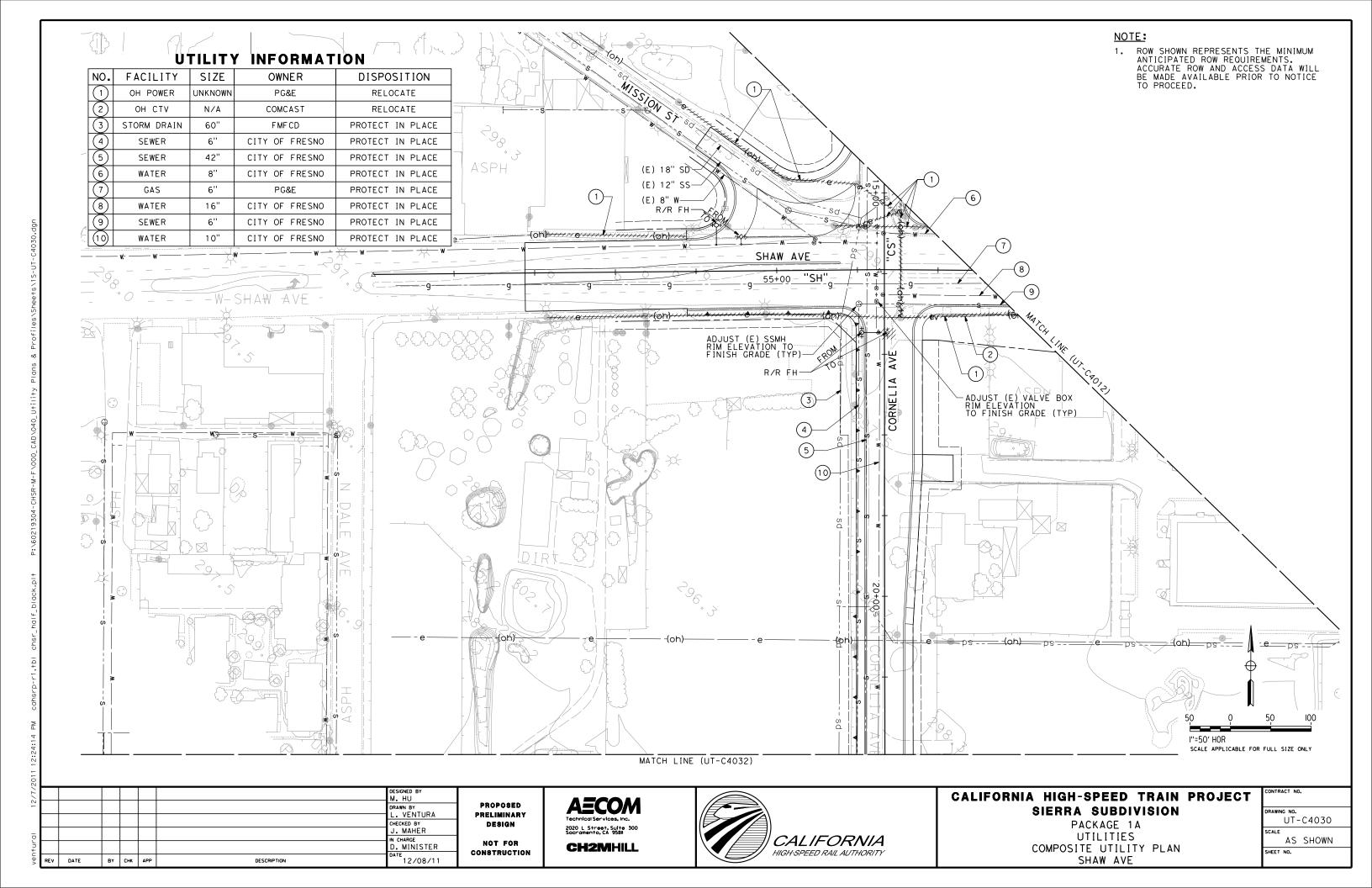


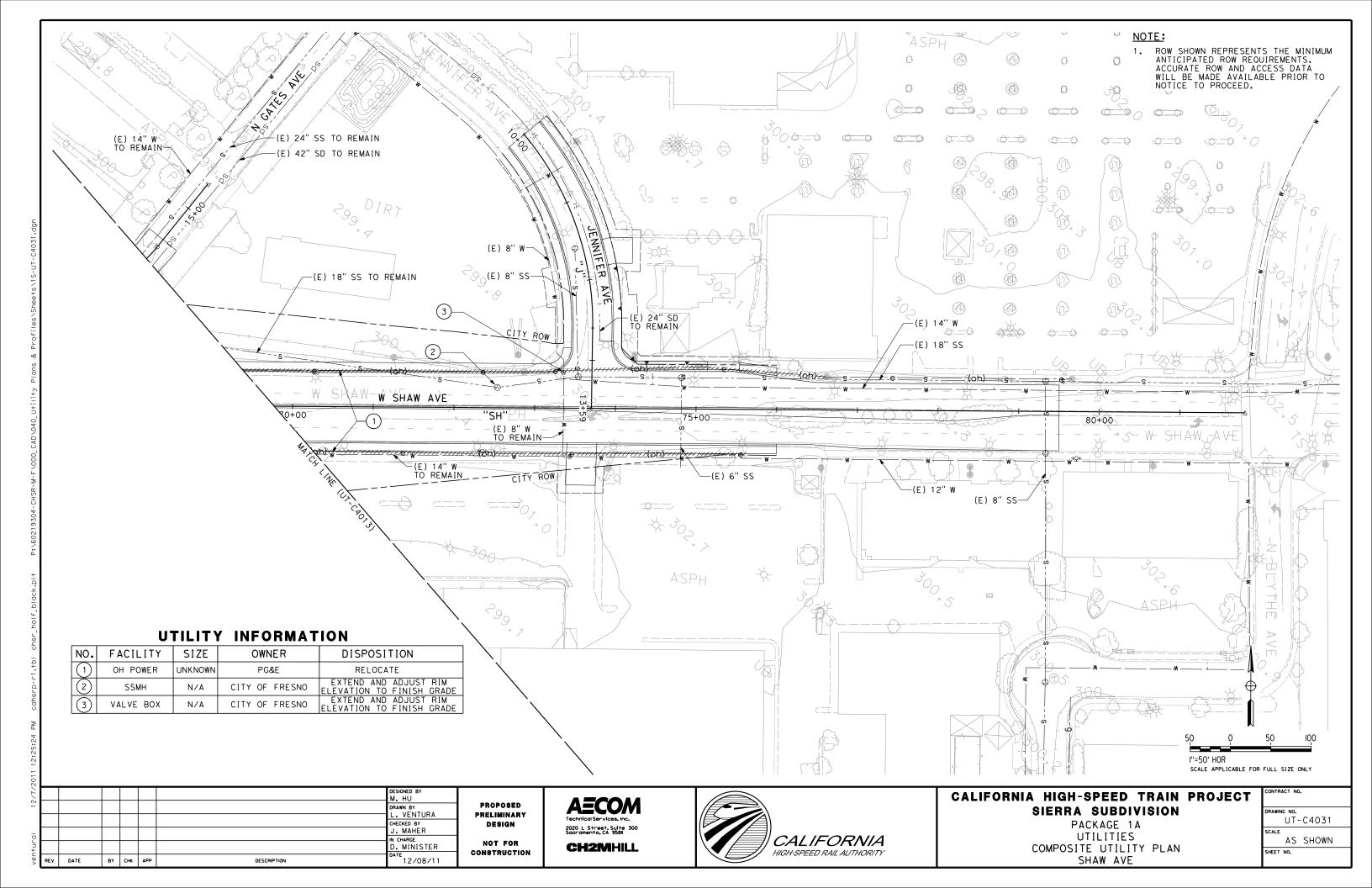


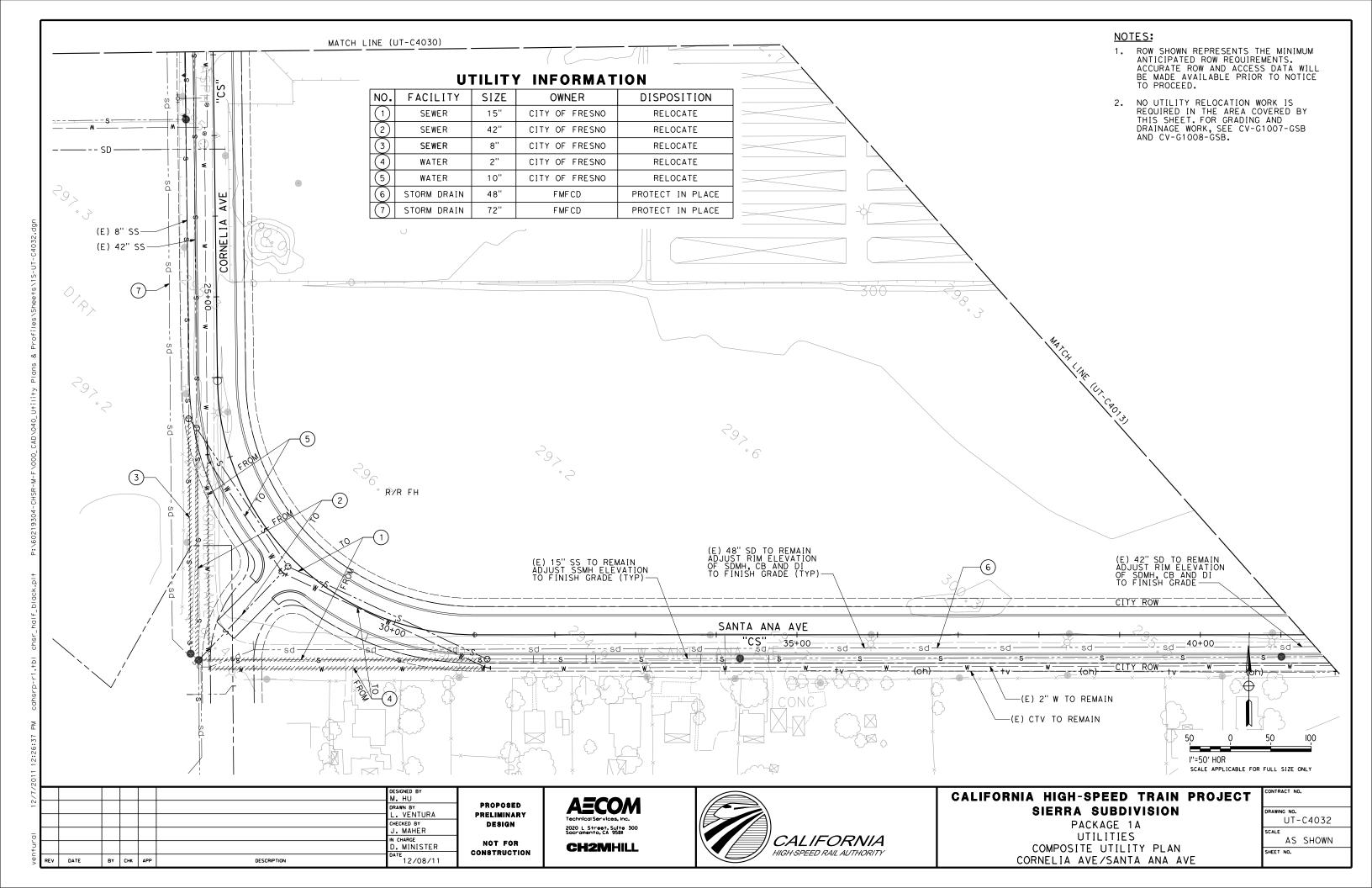


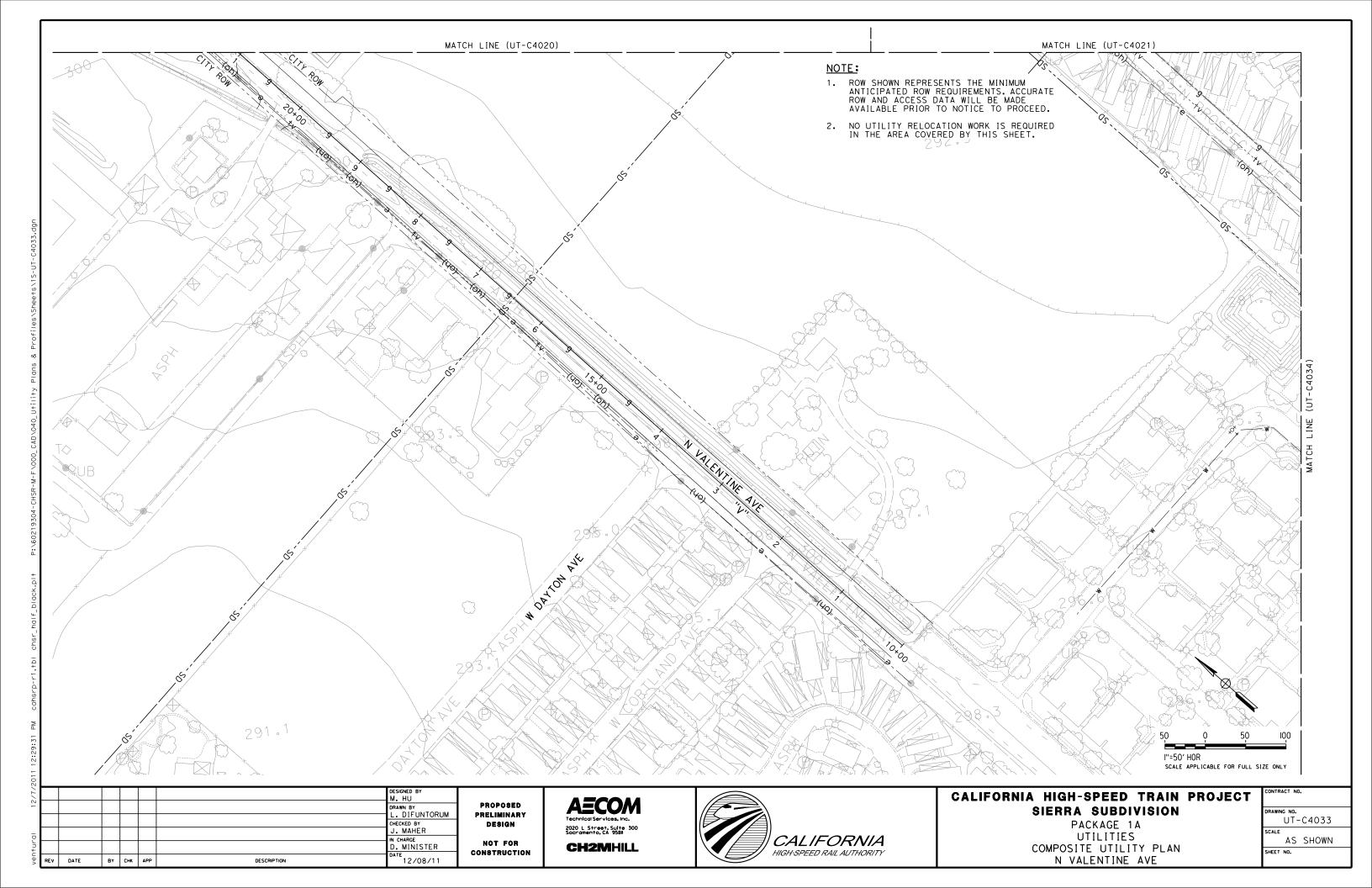


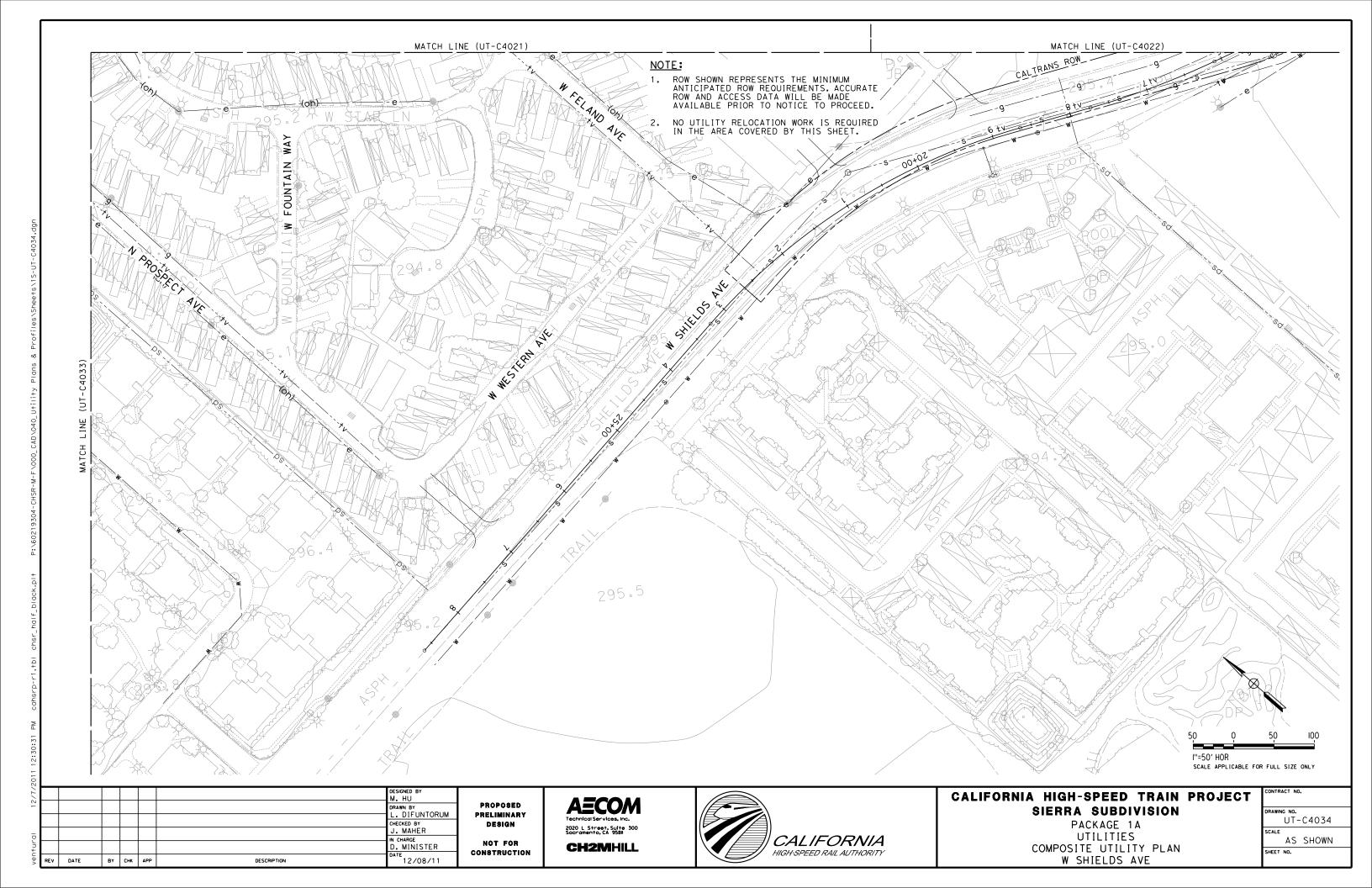


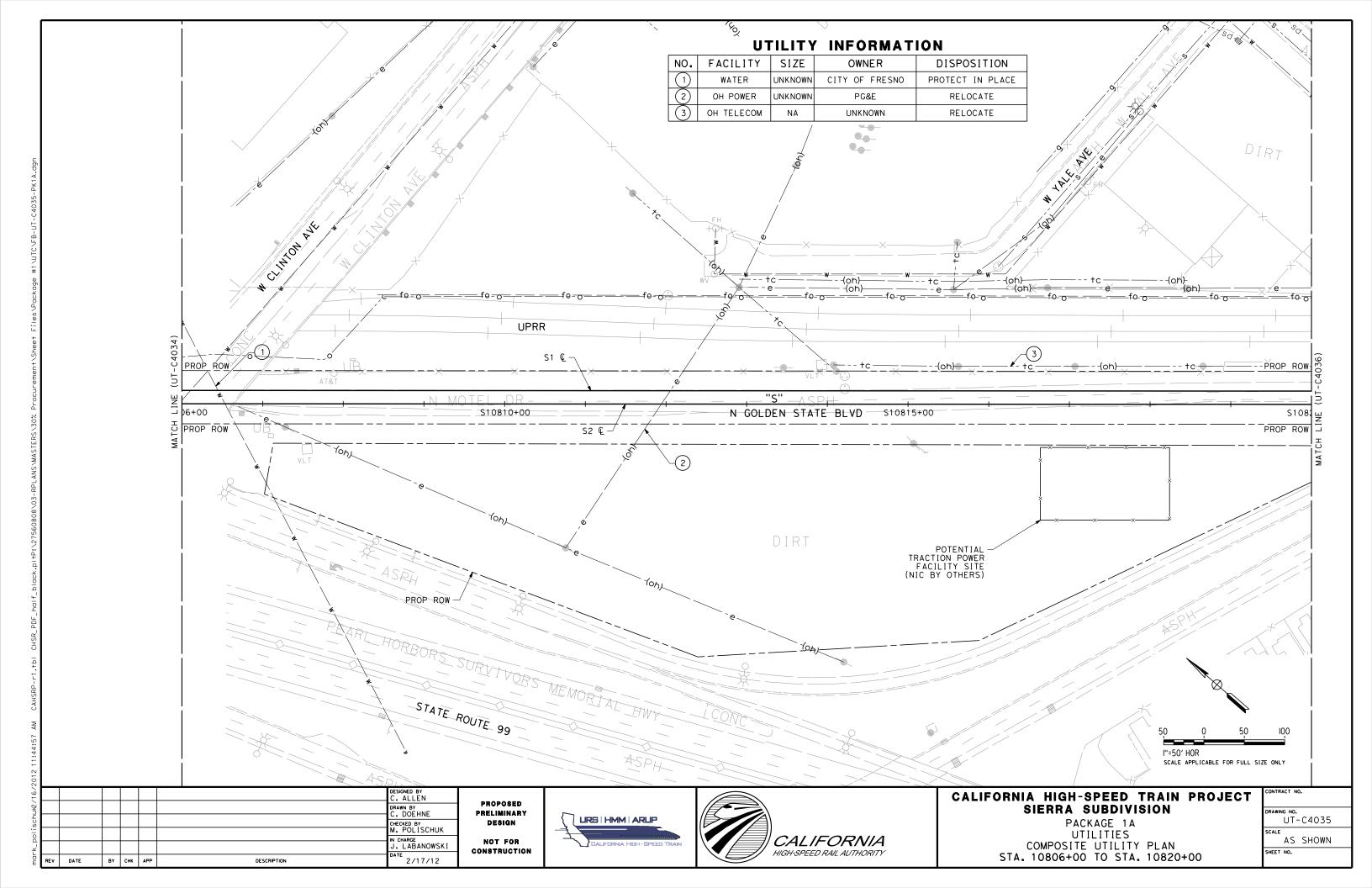


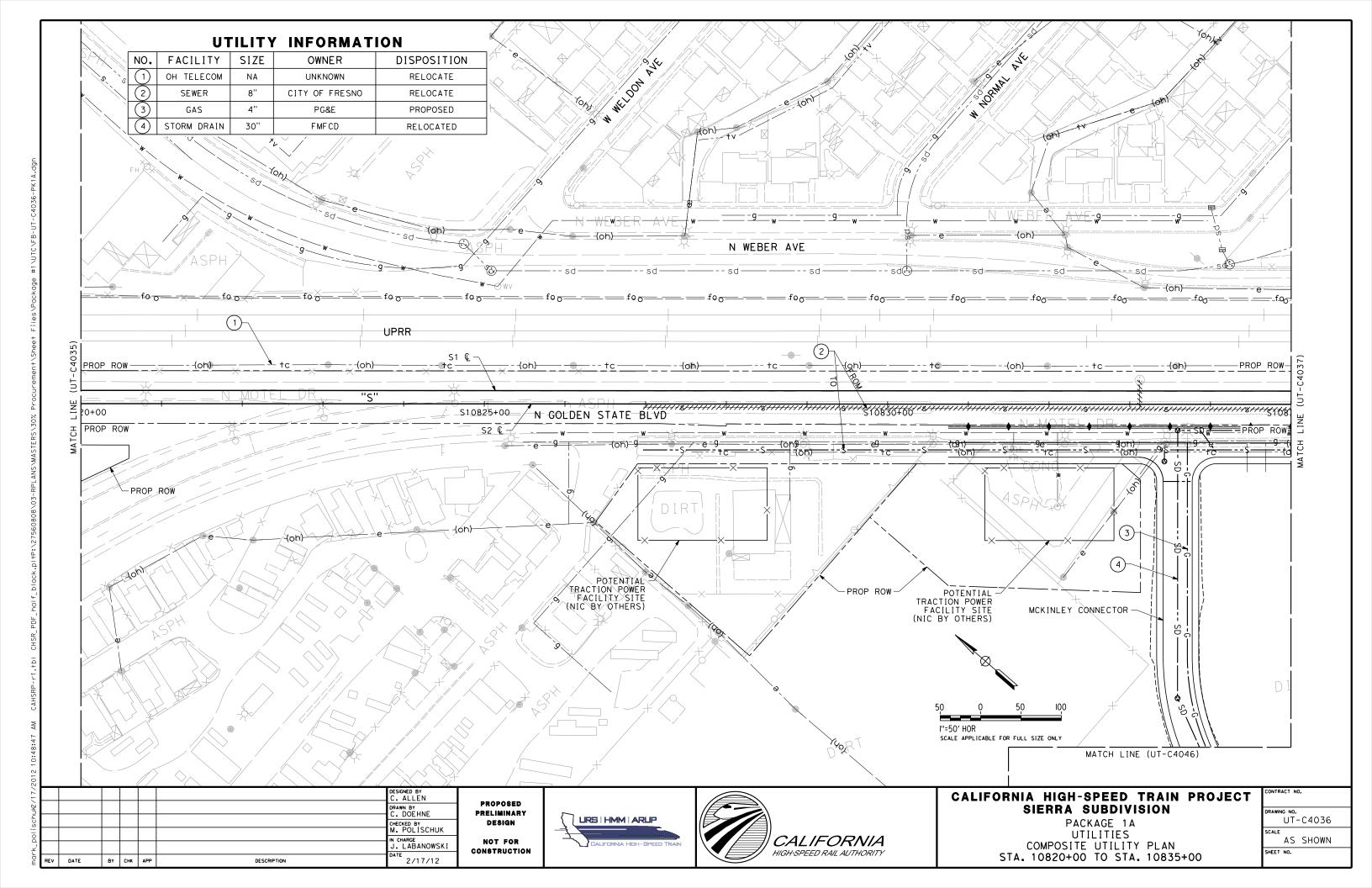


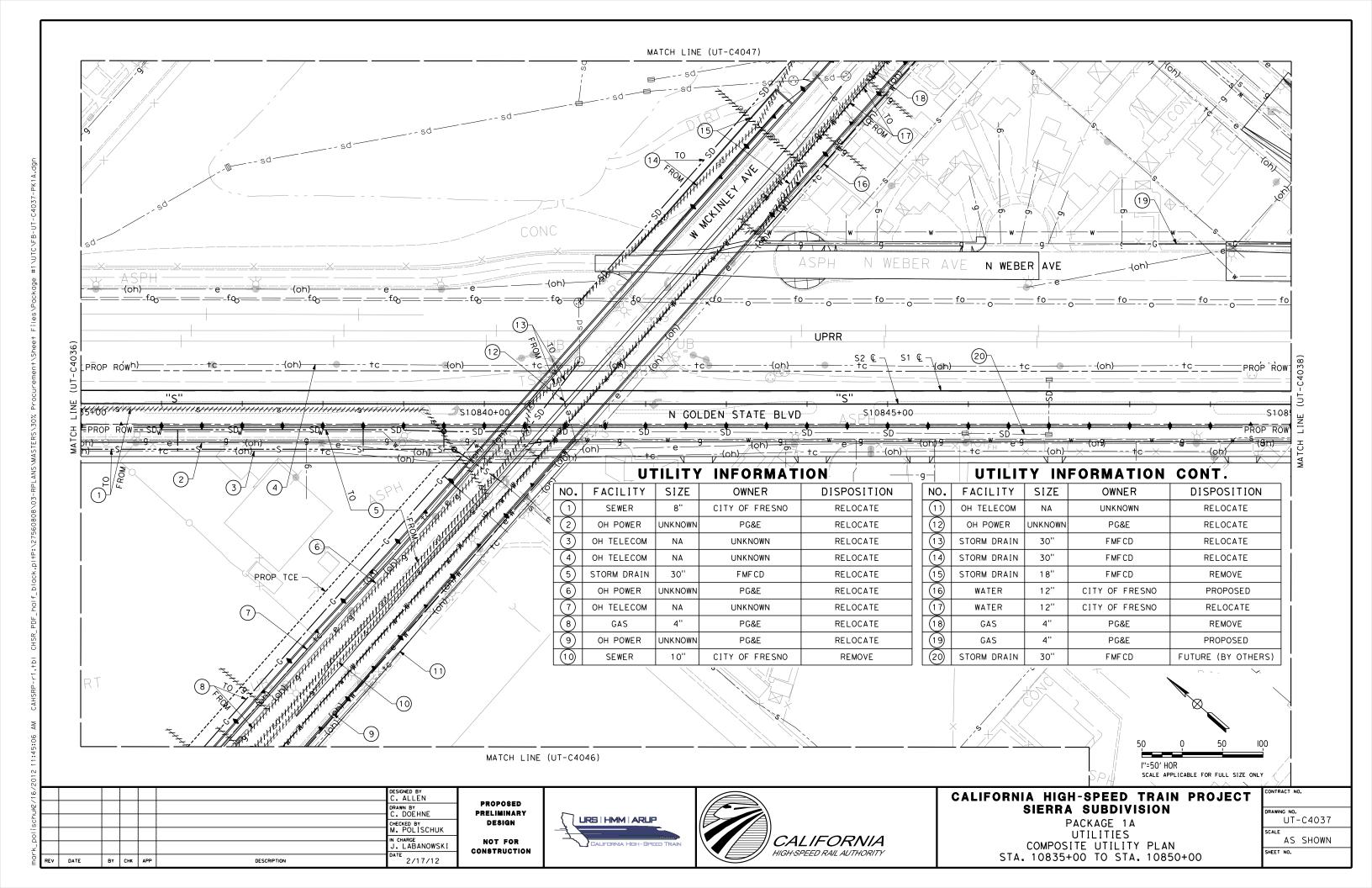


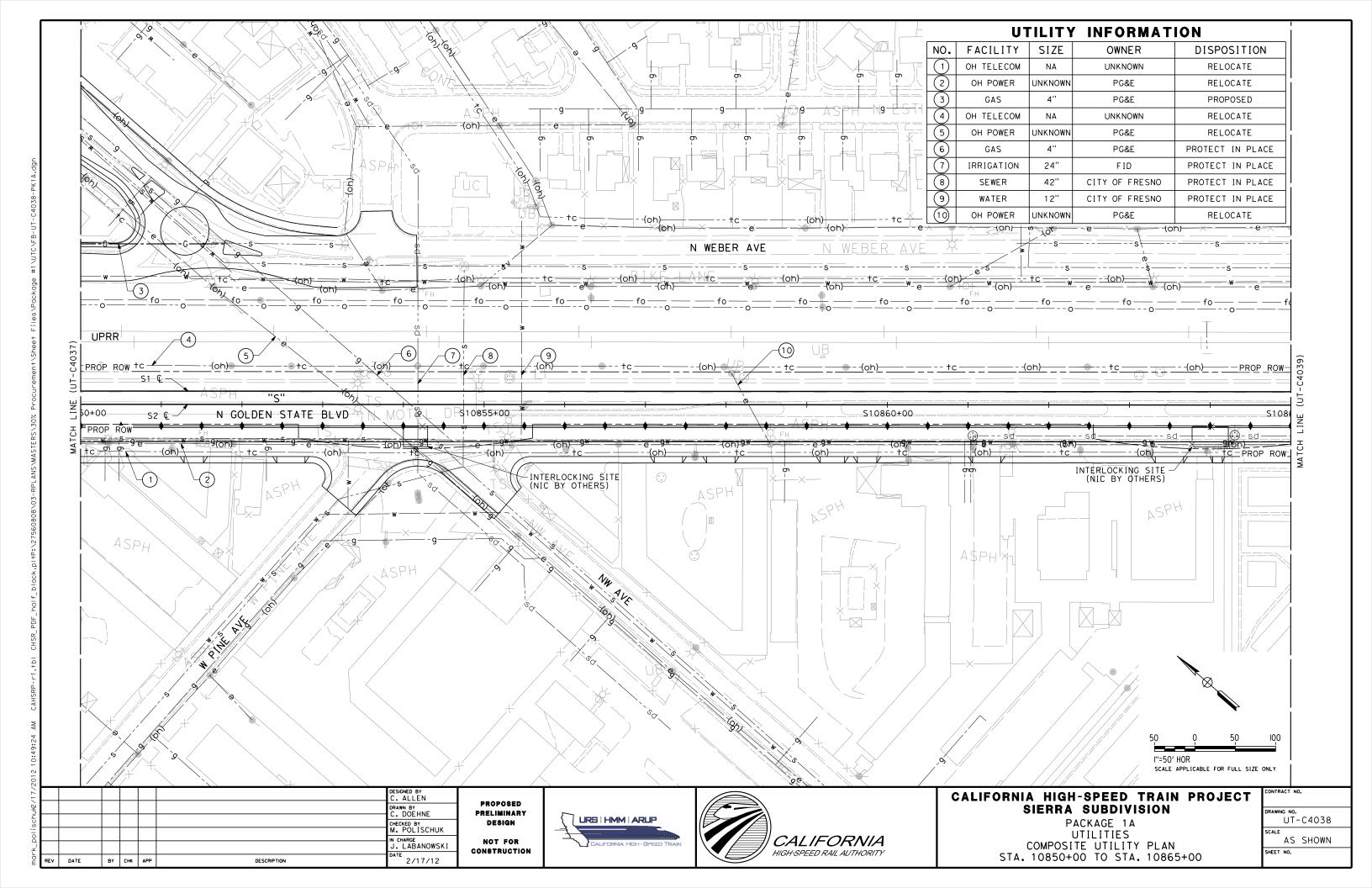


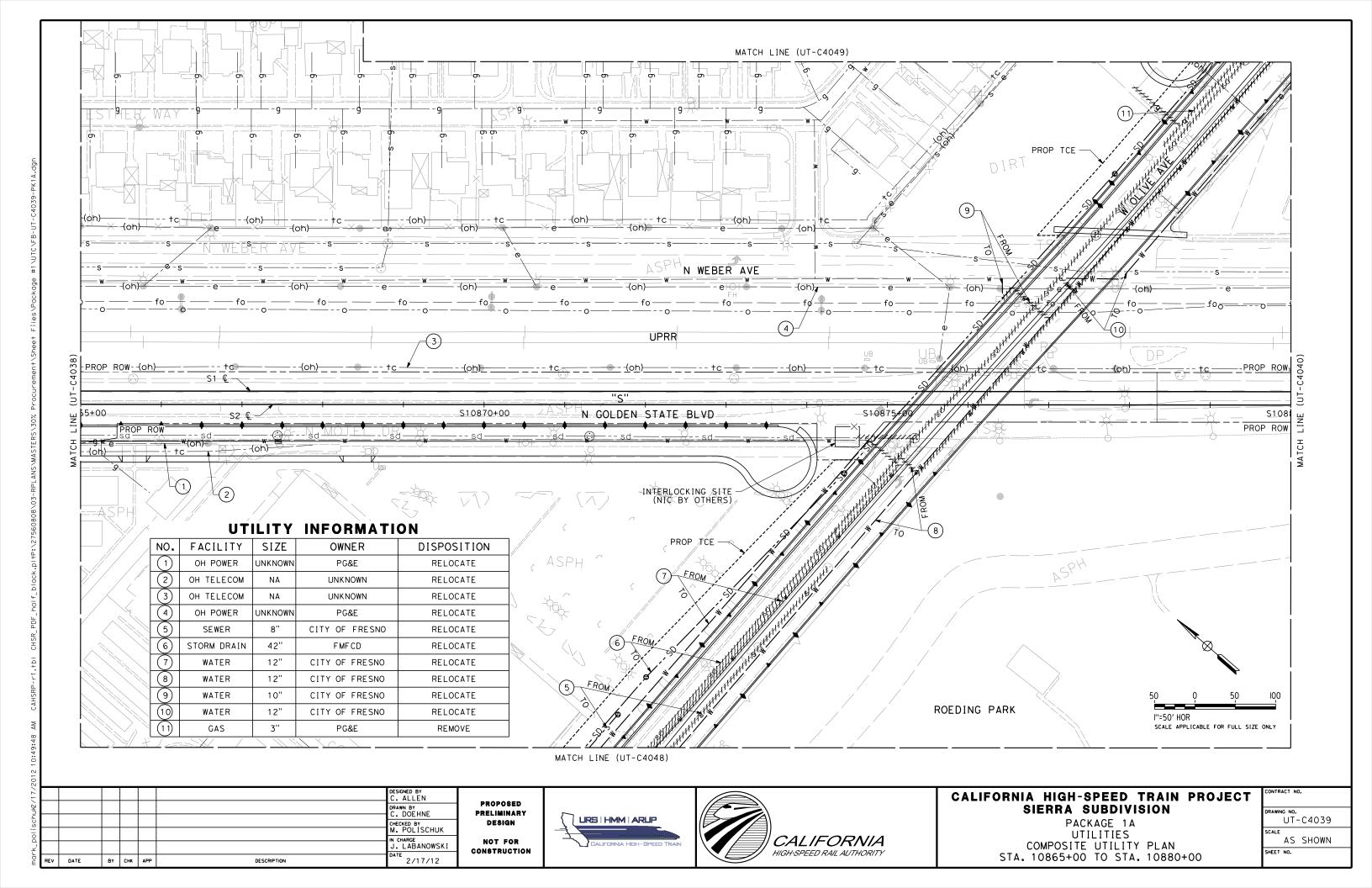


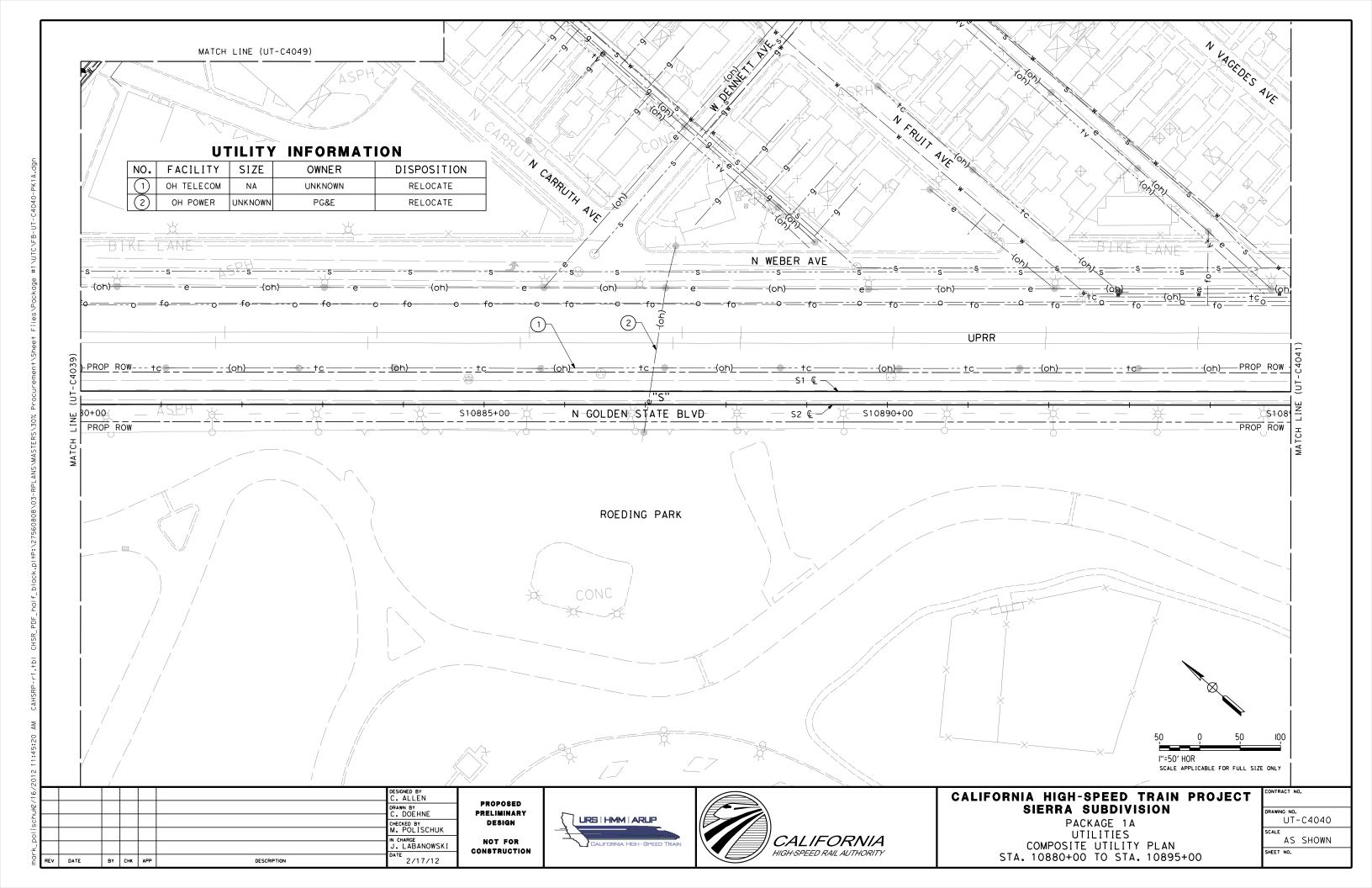


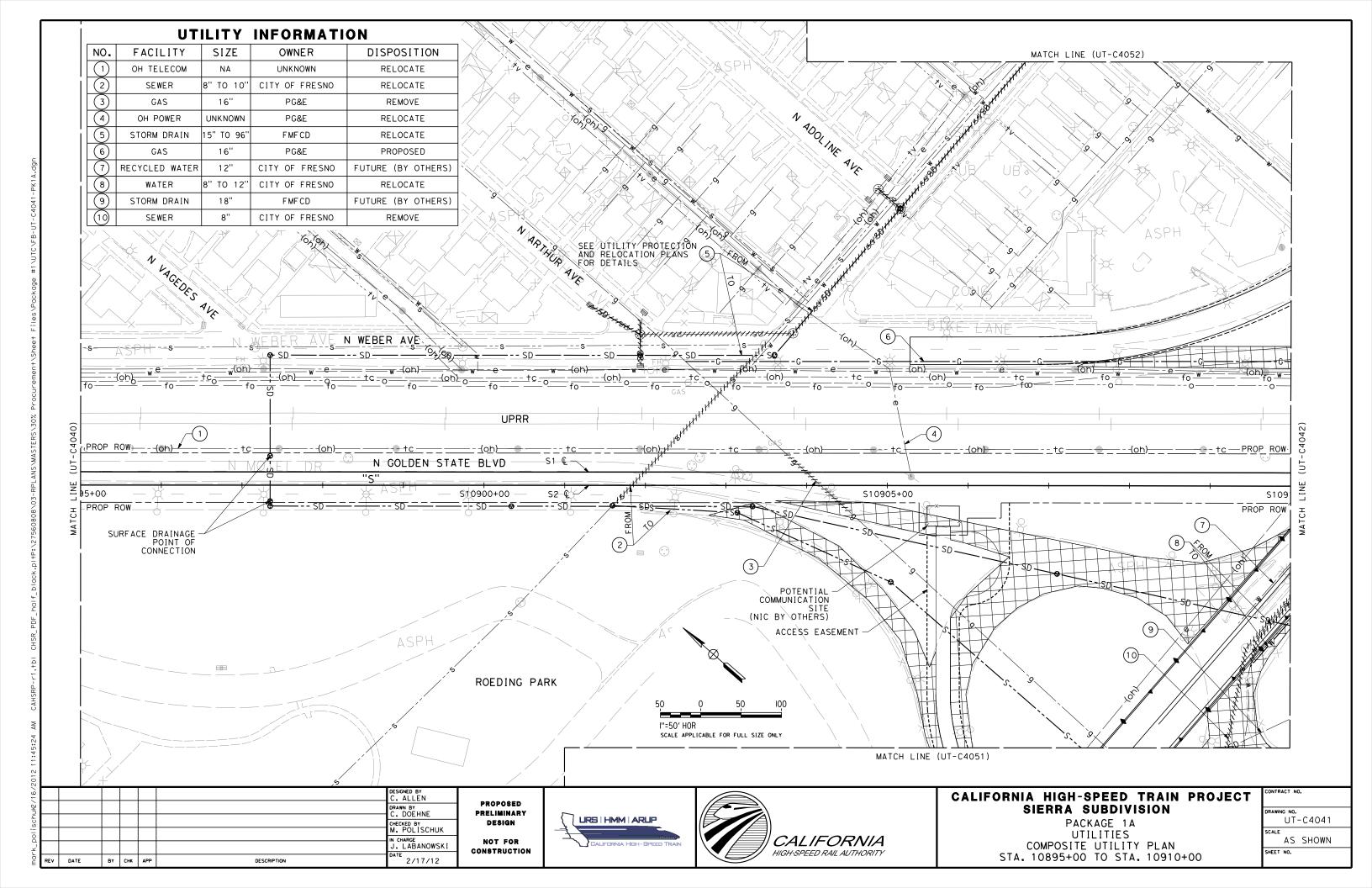


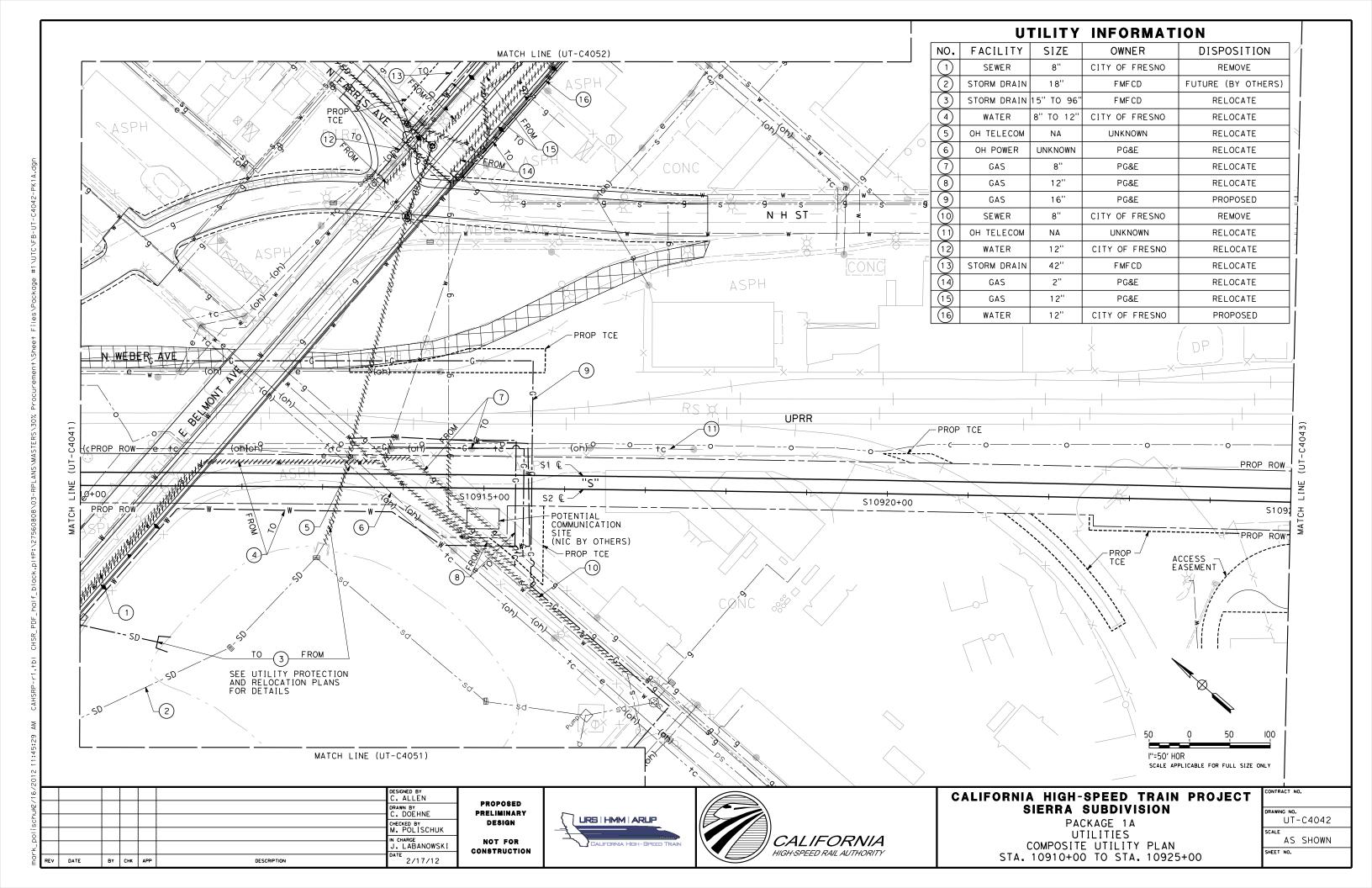


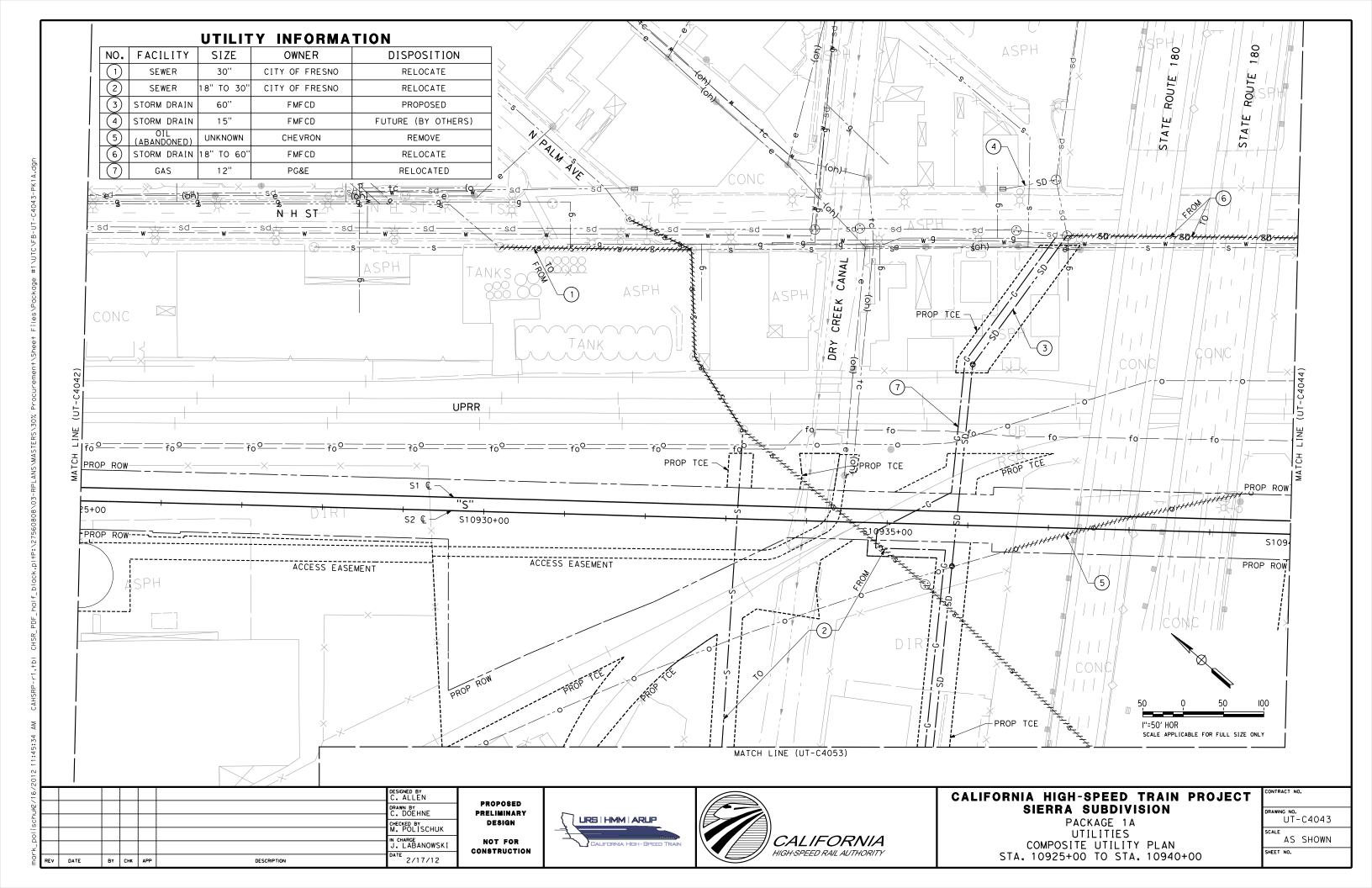


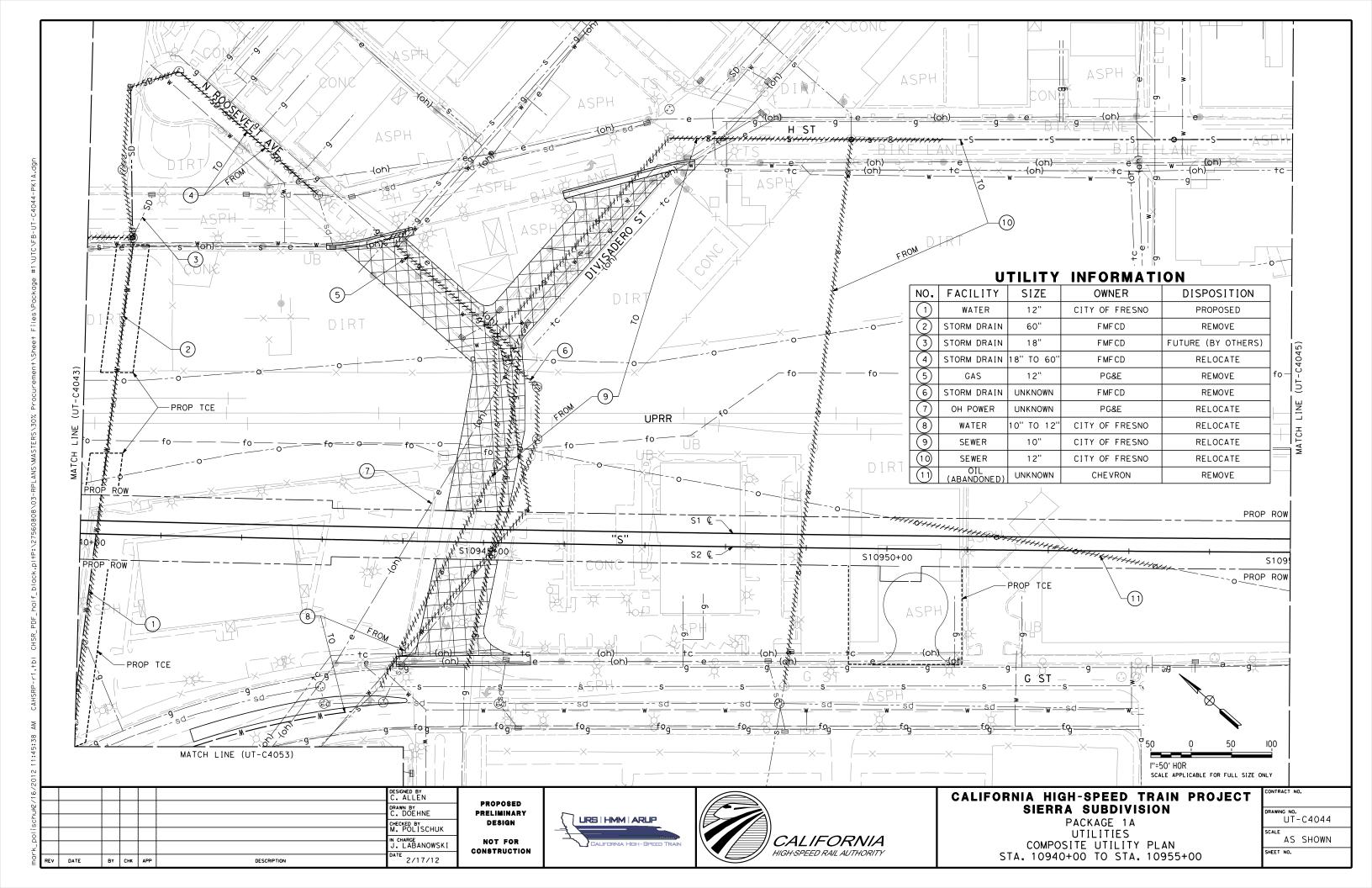


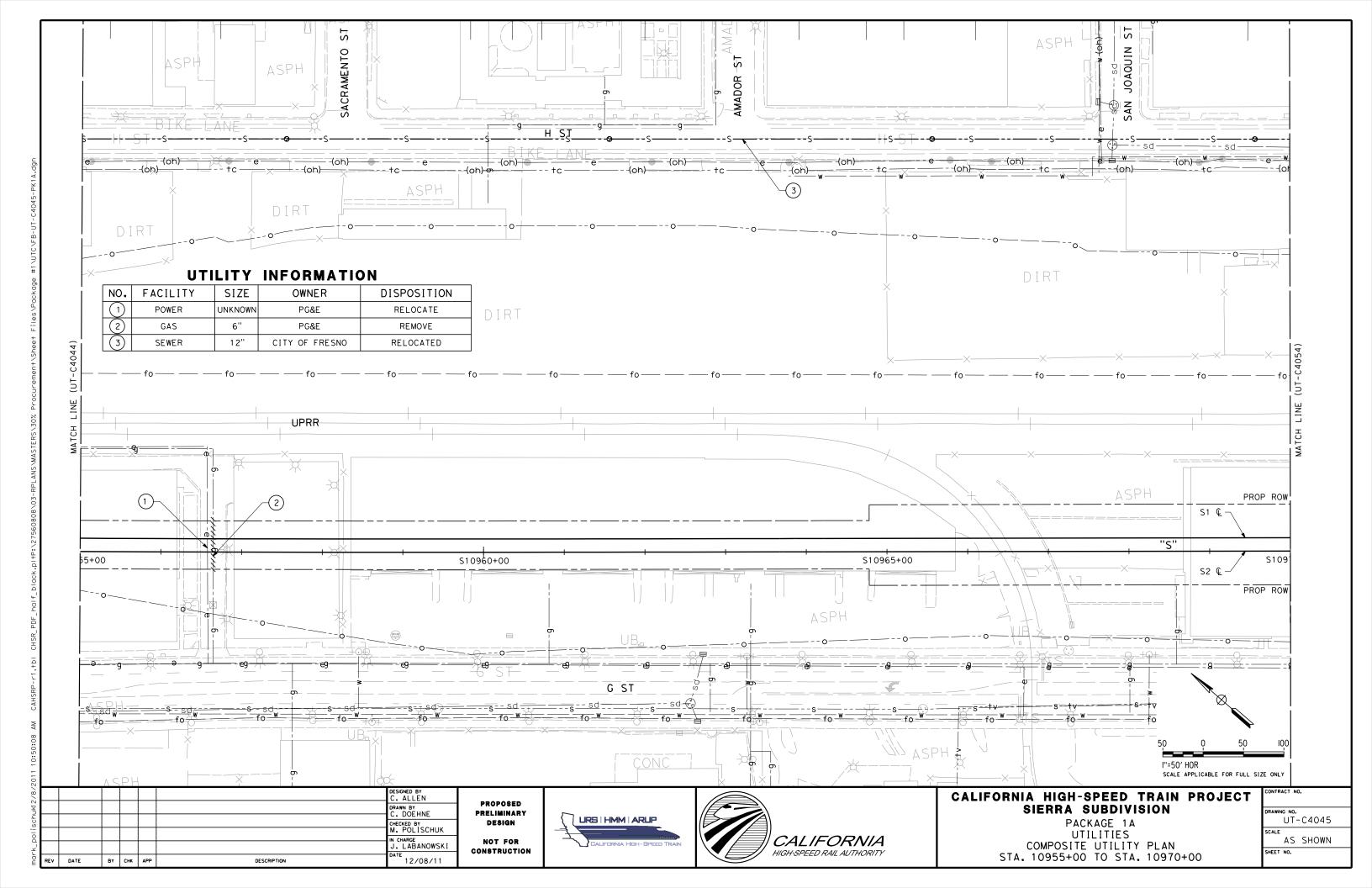


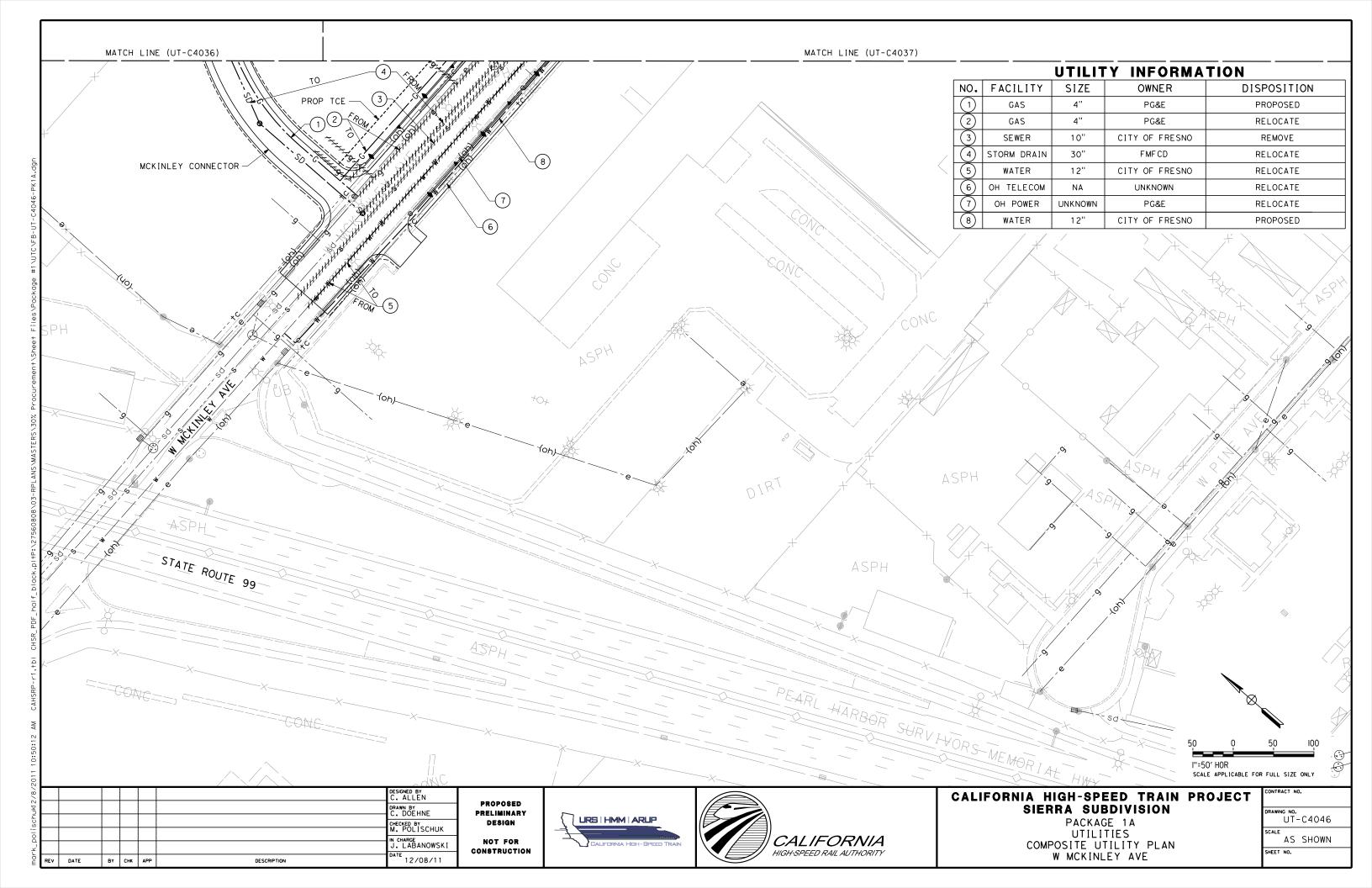


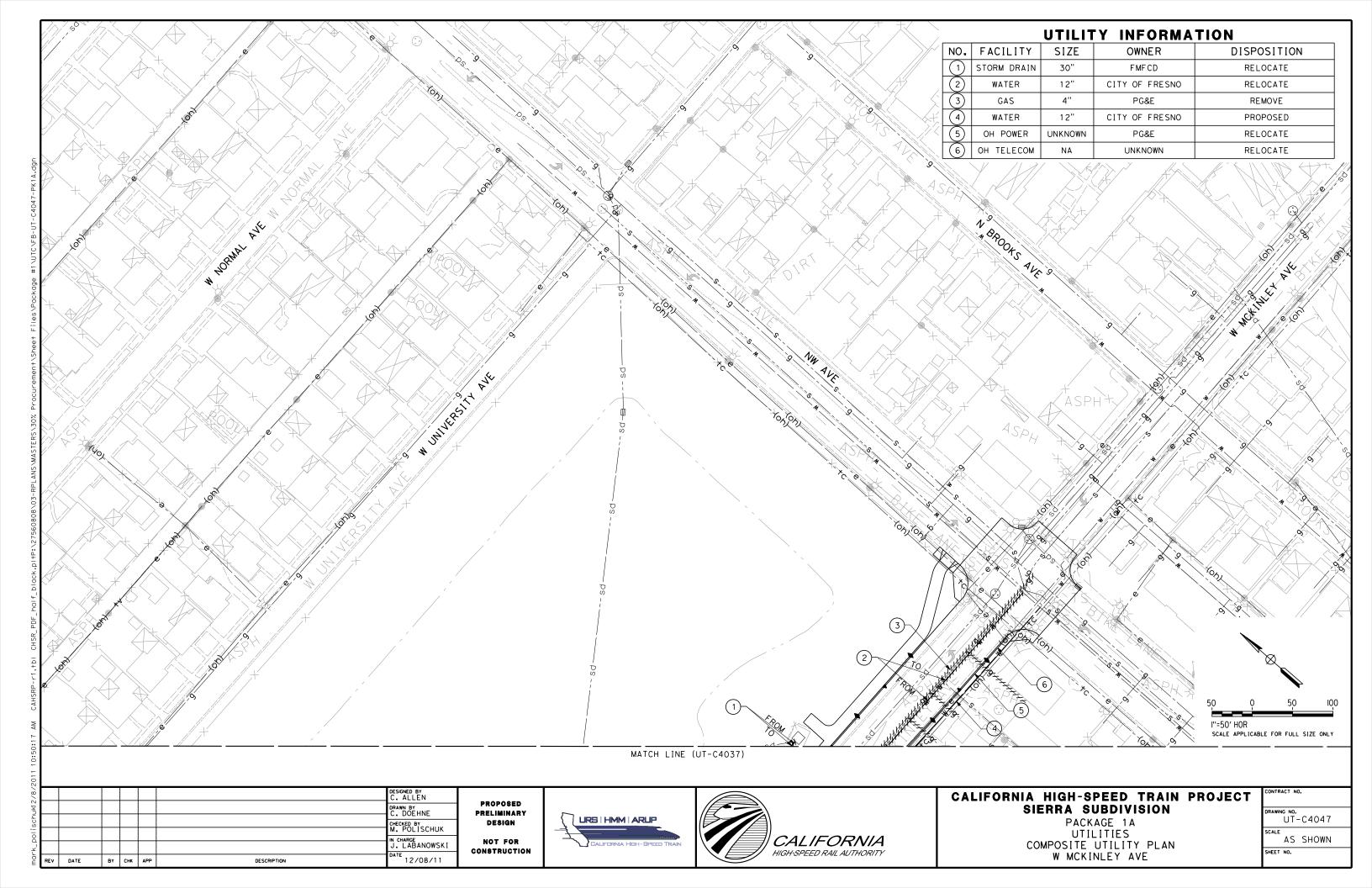


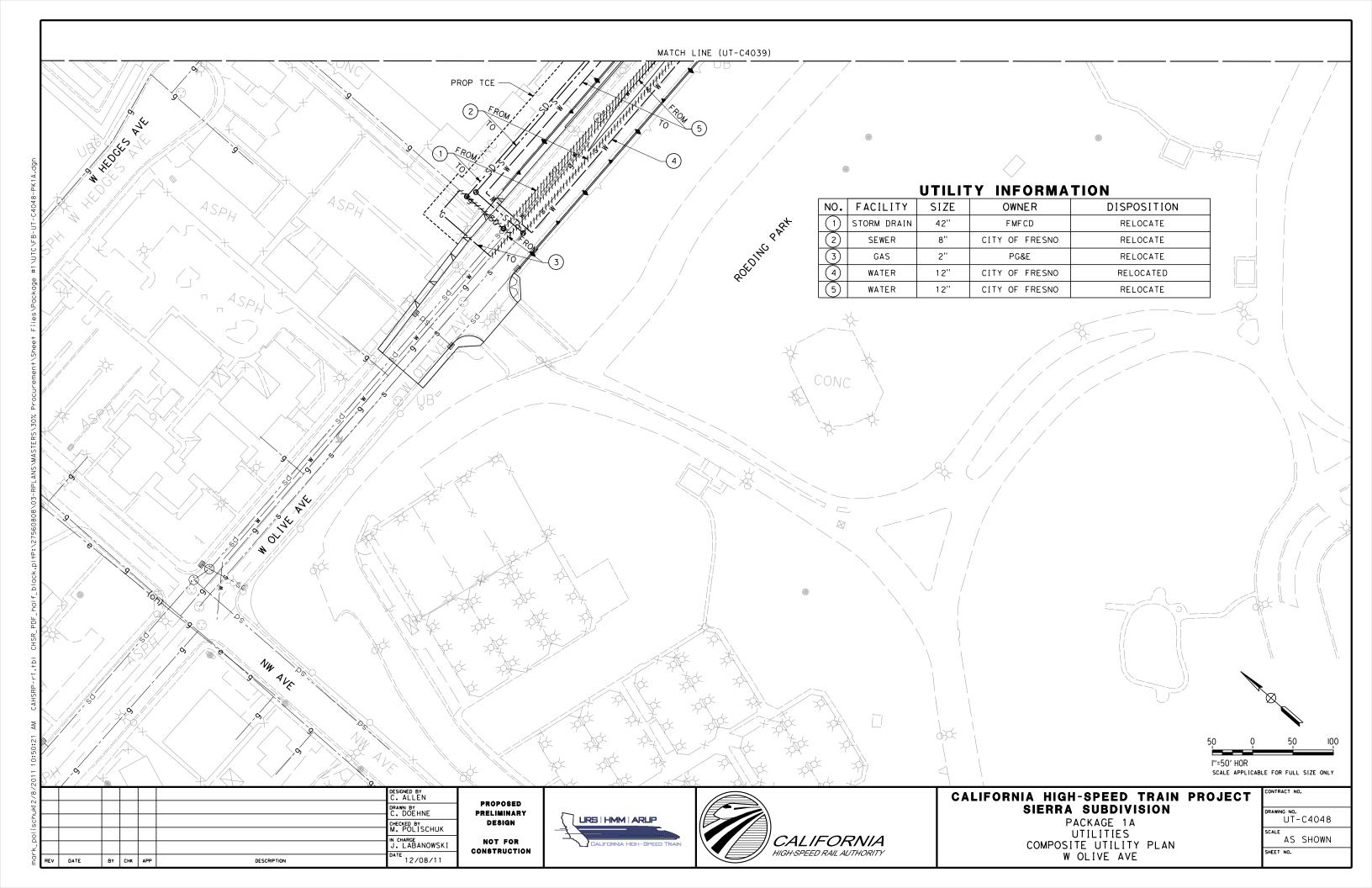


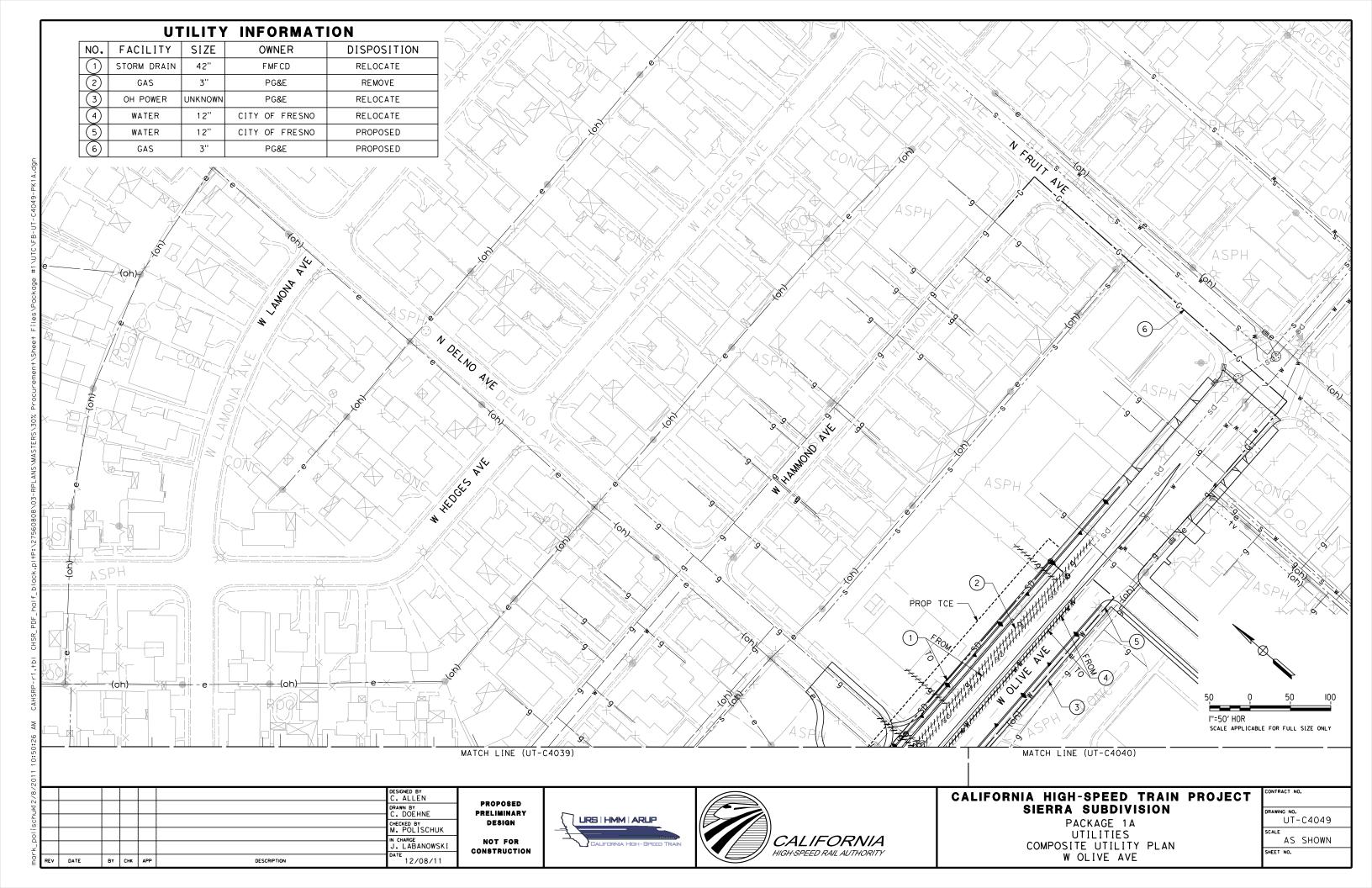


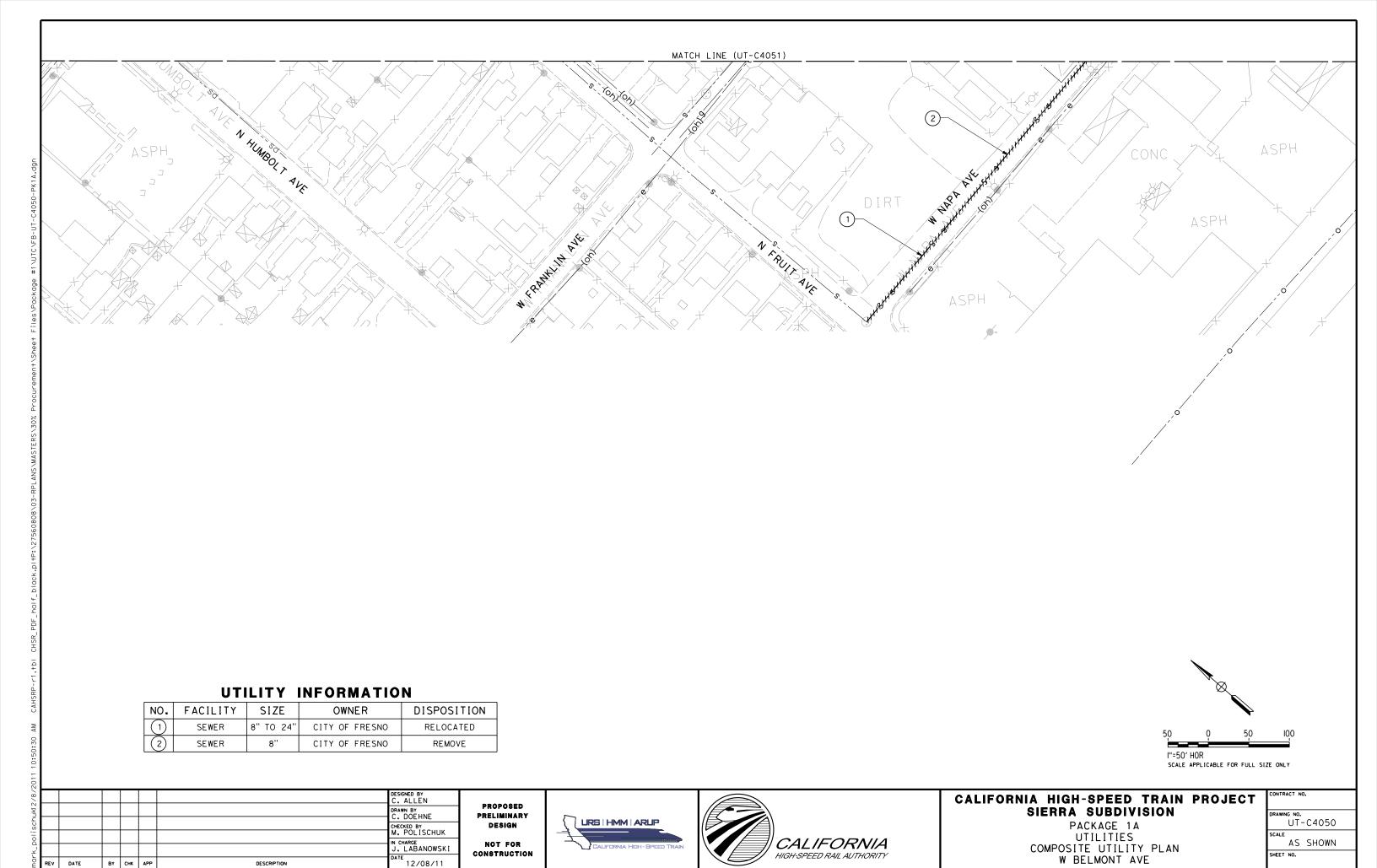












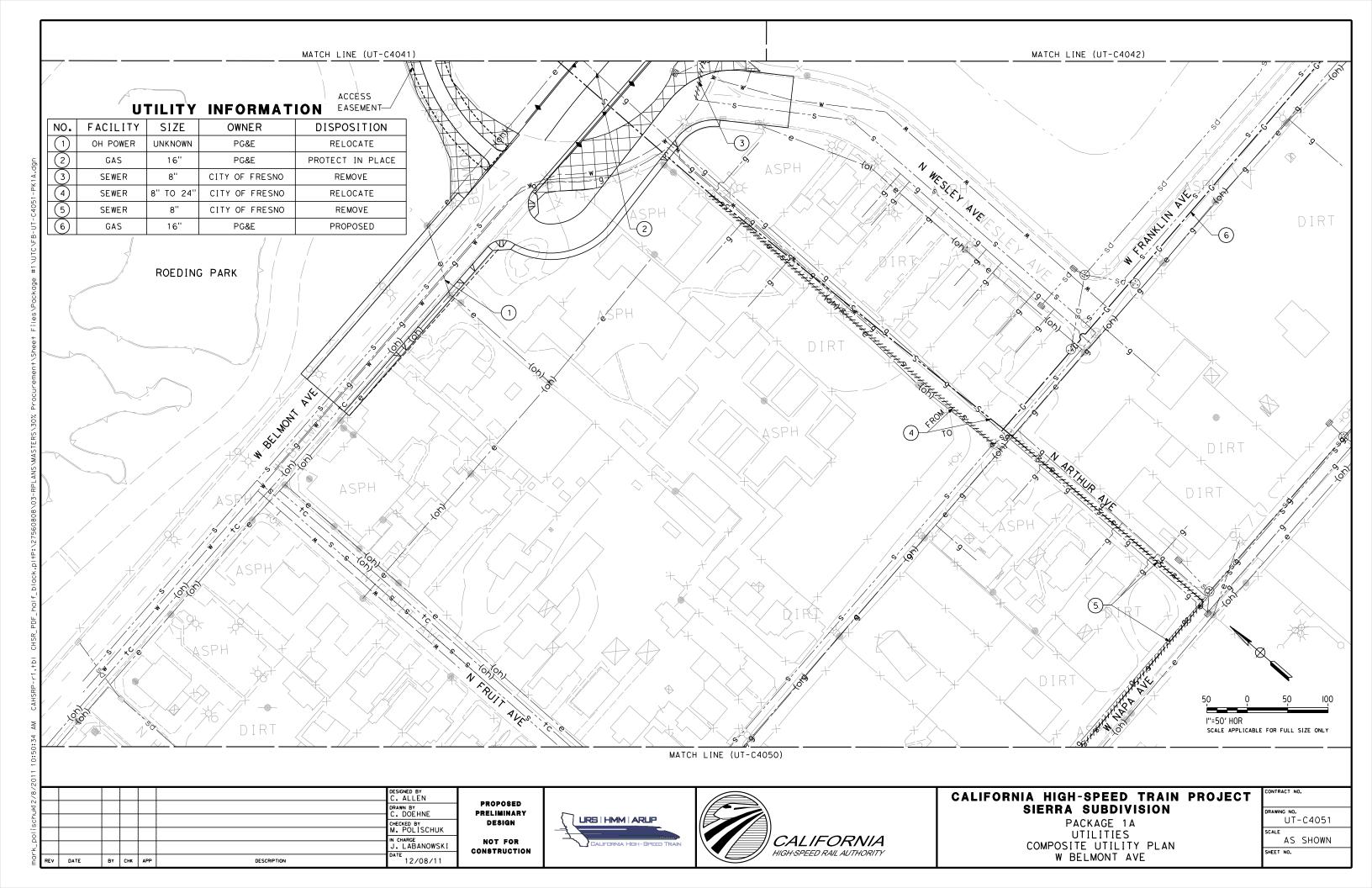
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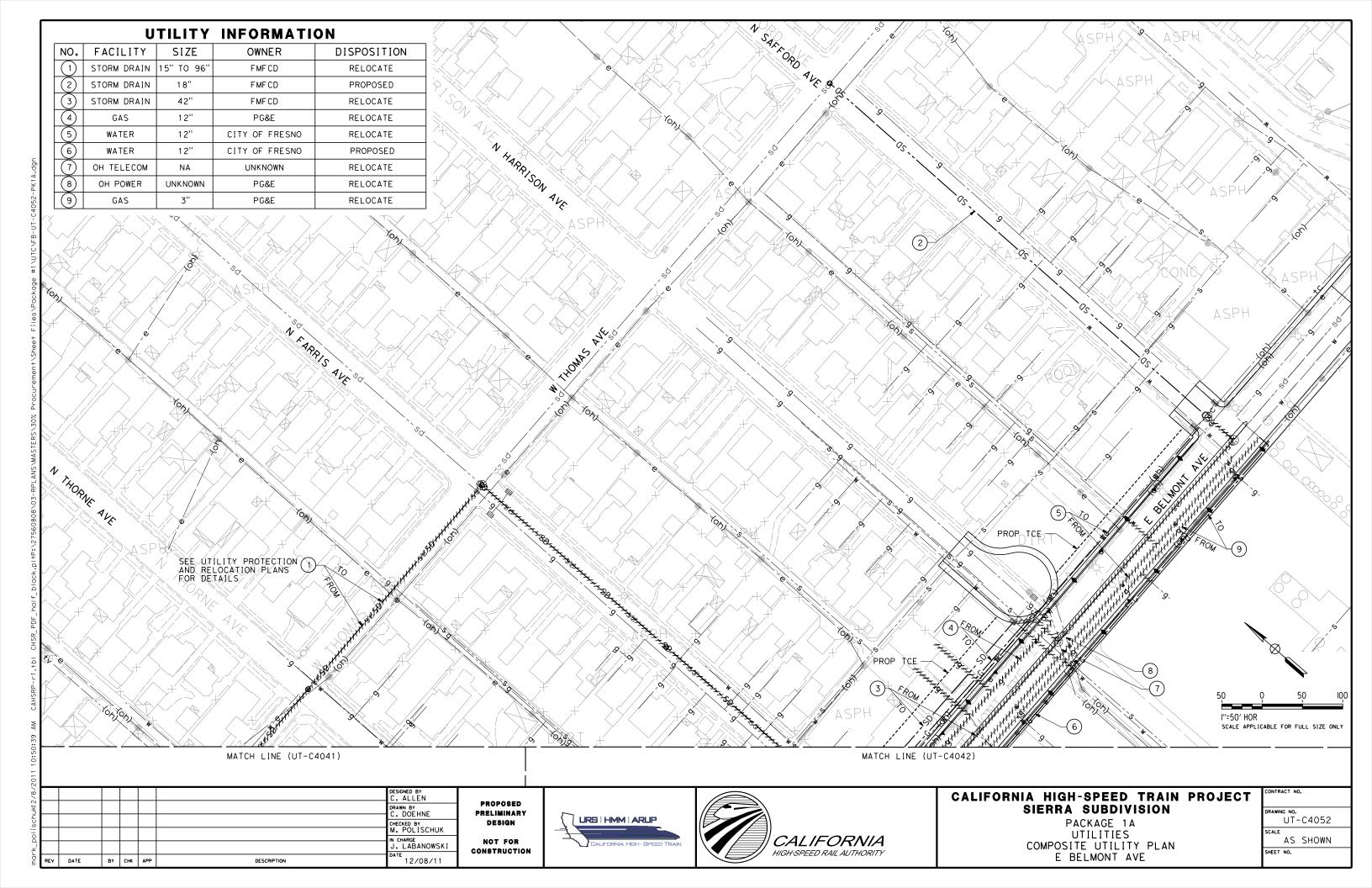
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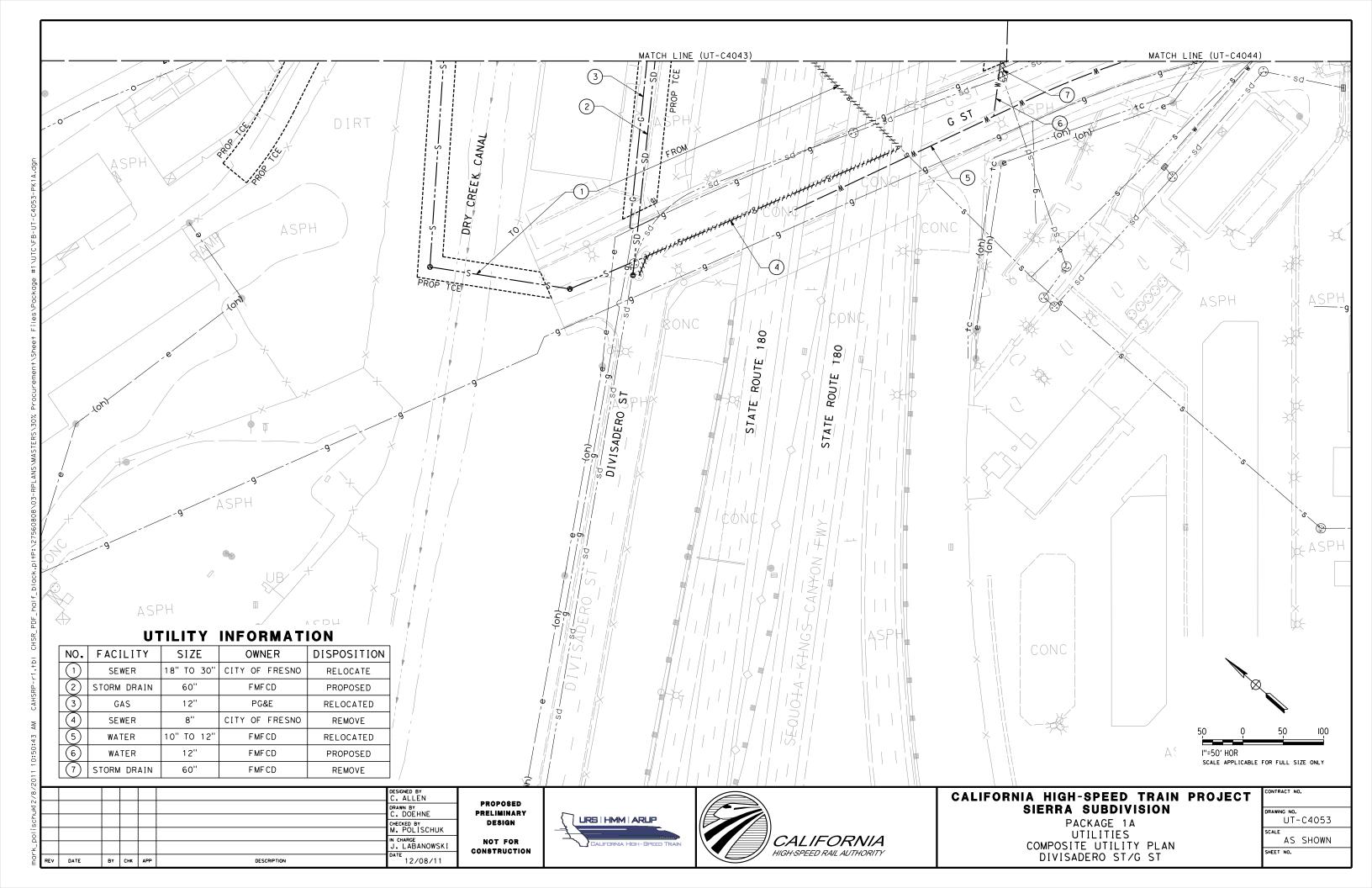
BY CHK APP

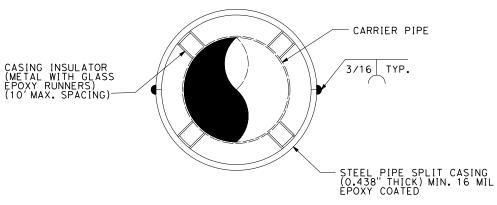
DESCRIPTION

HIGH-SPEED RAIL AUTHORITY









UTILITY TYPE	DIAMETER CARRIER PIPE	DIAMETER CASING PIPE
GAS	4''	8"
WATER	10"	16"
WATER	12"	20"
WATER	14"	24"
WATER	16"	30"

NOTES:

- 1. CASING PIPE SHALL END A MINIMUM OF 5' BEYOND THE HST R/W.
- 2. WATER PIPE INSIDE STEEL CASING SHALL BE DIP.
- 3. PROVIDE GATE VALVE ON EITHER END OF THE WATER LINE A MINIMUM OF 5' BEYOND THE END OF CASING.

DETAIL 1

SPLIT CASING DETAIL

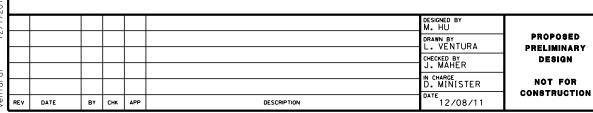
STORM DRAIN, SANITARY SEWER AND IRRIGATION CASING SCHEDULE			
DIAMETER CARRIER PIPE, RCP	DIAMETER CASING PIPE, RCP		
18"	48"		
30"	60"		
36"	66"		
39"	72"		
42"	78"		
45''	84"		
54''	90"		
60''	96"		
·	·		

NOTES:

- 1. ALL RCP SHALL MEET ASTM C76.
- 2. CARRIER PIPE CLASS AND WALL SHALL MEET CITY OF FRESNO, FMFCD AND FID STANDARDS.
- 3. CASING PIPE SHALL MEET ASATM C76, CLASS III, WALL C.

DETAIL 2

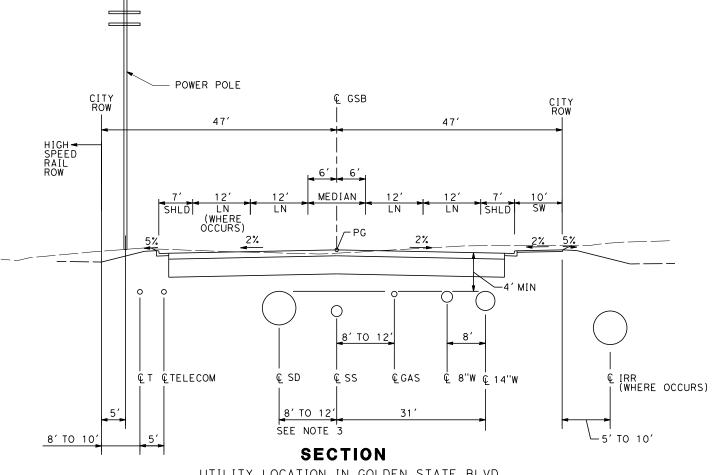
RCP CASING PIPE SCHEDULE



PRELIMINARY DESIGN NOT FOR

2020 L Street, Suite 300 Sacramento, CA 95811 **CH2MHILL**





UTILITY LOCATION IN GOLDEN STATE BLVD

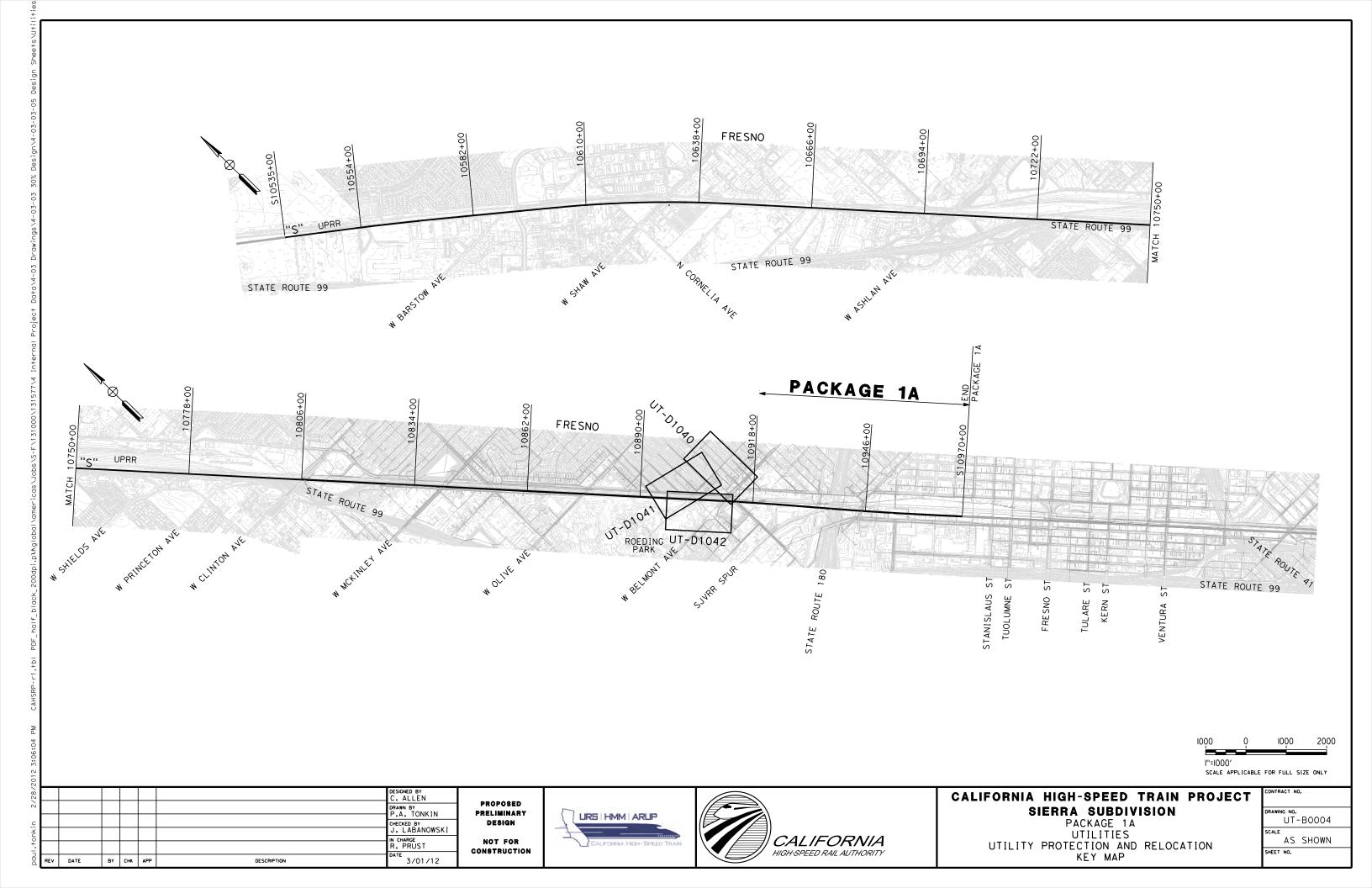
NOTES:

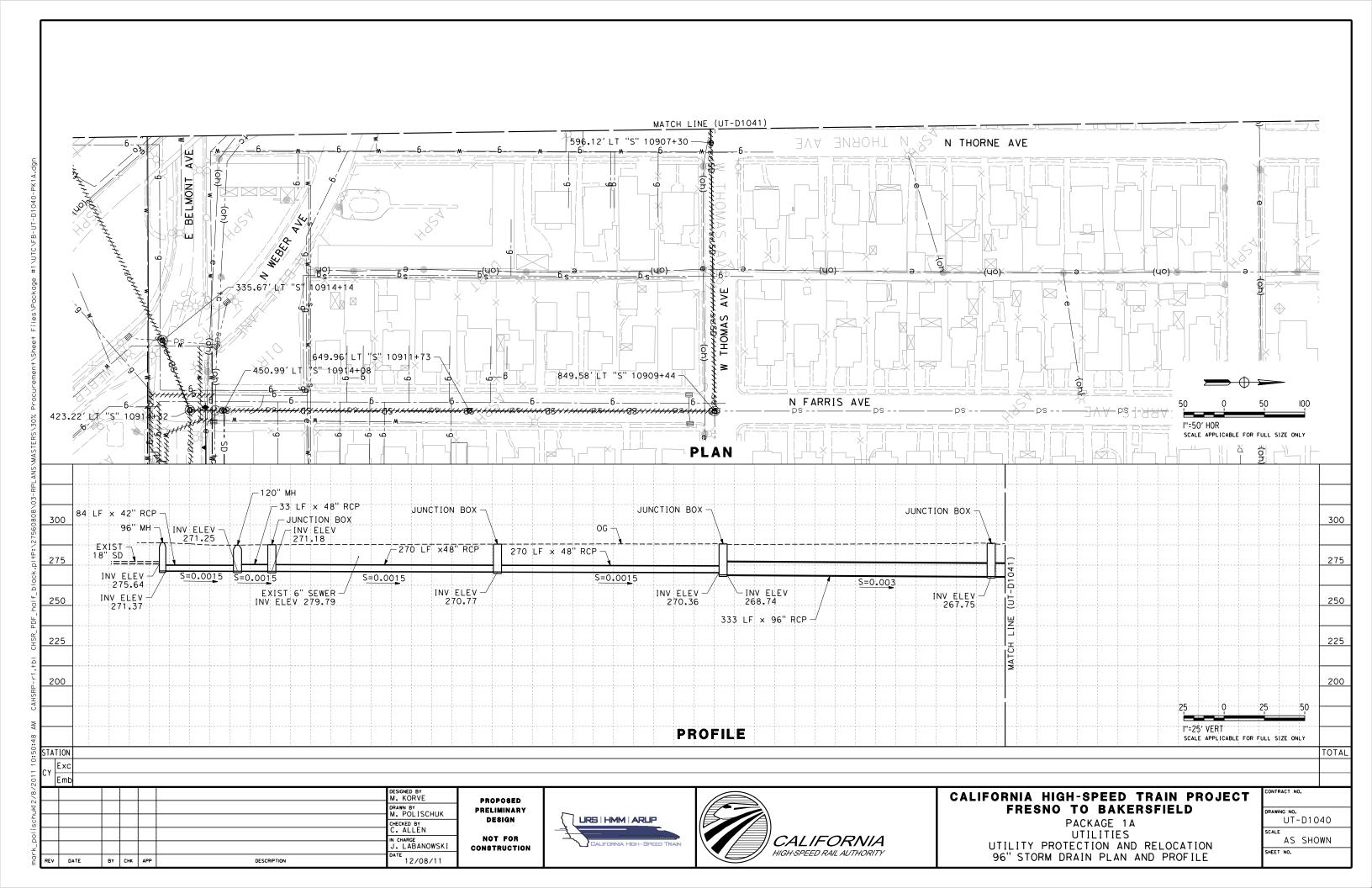
- 1. SECTION CUT IS LOOKING SOUTH.
- THE UTILITIES SECTION IS TYPICAL. LOCATION OF UTILITIES MAY BE ADJUSTED SUBJECT TO APPROVAL OF UTILITY AGENCIES OR UTILITY OWNER.
- 3. FOR STORM DRAIN LINES, SEE DWGS CV-G1001-GSB THROUGH CV-G1011-GSB.

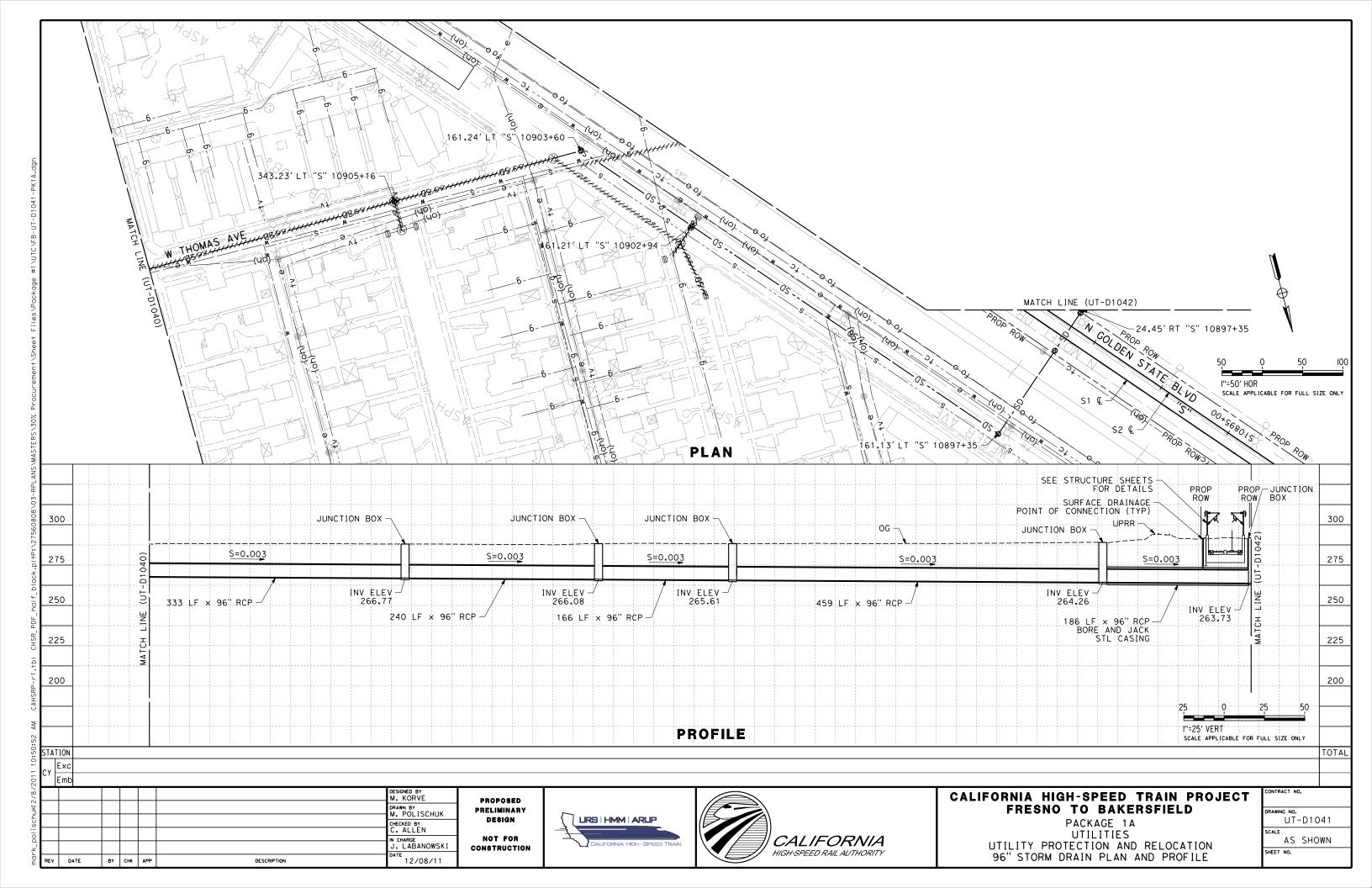
CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

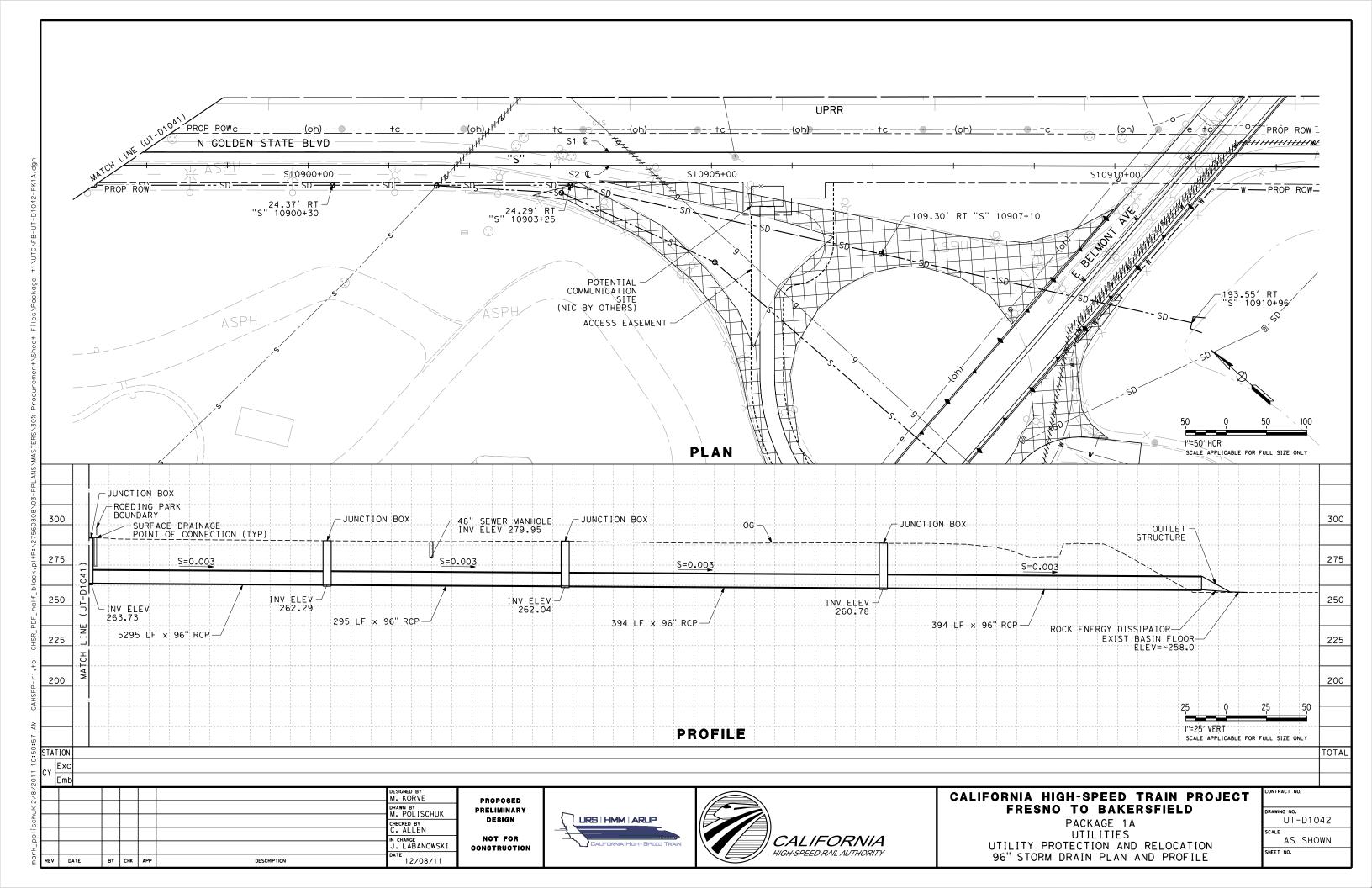
PACKAGE 1A UTILITIES COMPOSITE UTILITY PLAN SECTION AND DETAILS

CONTRACT	i NO.
DRAWING	
L	JT-C5011
SCALE	
	N.T.S
SHEET NO).









- 2. ALL CONSTRUCTION ACTIVITIES SHALL BE COORDINATED WITH LOCAL AND STATE JURISDICTION INSPECTOR(S). CONTRACTOR SHALL NOTIFY LOCAL AND STATE JURISDICTION INSPECTOR(S) _____ DAYS PRIOR TO THE START OF ANY CONSTRUCTION.
- 3. ALL DAMAGE CAUSED BY THE CONTRACTOR AND/OR SUB-CONTRACTOR SHALL BE RESTORED TO AN "AS GOOD OR BETTER" CONDITION.
- 4. UPON COMPLETION, THE CONTRACTOR SHALL LEAVE THE PROJECT AREA FREE OF DEBRIS AND UNUSED MATERIAL.
- 7. "ORIGINAL GROUND" SHOWN ON CROSS SECTIONS REFERS TO THE APPROXIMATE EXISTING GROUND LINE AT THE DESIGNATED CENTERLINE, BASELINE, LAYOUT LINE OR SECTION LINE.
- 8. MAINTAIN EXISTING SITE SURVEY CONTROL POINTS AND BENCH MARKS. SURVEY CONTROL POINTS OR BENCH MARKS DAMAGED OR REMOVED BY THE CONTRACTOR SHALL BE RESTORED BY A REGISTERED LAND SURVEYOR.
- 9. ALL WORK SHALL CONFORM TO ALL LOCAL, STATE AND FEDERAL CODES AND ORDINANCES IN EFFECT. CONTRACTOR SHALL PROVIDE AND MAINTAIN PROPER BARRICADES, RAILINGS, GUARDS, FLAGGING, LIGHTING, OR OTHER DEVICES NECESSÁRY FOR THE PROTECTION OF LIFE AND
- 10. VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING MATERIALS.
- 11. ALL HORIZONTAL AND VERTICAL DISTANCES ARE IN FEET AND /OR DECIMALS OF FOOT EXCEPT AS NOTED OTHERWISE.
- 12. ACCESS ROAD TYPE SHALL BE CONFIRMED ON A SITE-BY-SITE BASIS.

- DEMOLITION NOTES
 - 1. REVIEW ALL ENVIRONMENTAL STUDIES AND REQUIREMENTS PRIOR TO BIDDING.
 - 2. MAINTAIN TEMPORARY ACCESS DURING CONSTRUCTION. UTILITY OUTAGE AND ACCESS CLOSURES DURING CONSTRUCTION REQUIRE A MINIMUM OF _____ DAYS NOTICE TO OWNER OR SITE TENANTS.
 - 3. FOLLOW LOCAL JURISDICTION REQUIREMENTS FOR CLEARING AND GRUBBING.

- 1. DO NOT PERFORM ANY GRADING OPERATION SO AS TO CAUSE FALLING ROCKS, SOIL OR DEBRIS IN ANY FORM TO FALL, SLIDE OR FLOW ONTO ADJOINING PROPERTIES, STREETS OR NATURAL WATERCOURSES. SHOULD SUCH VIOLATION OCCUR THE CONTRACTOR MAY BE CITED AND THE CONTRACTOR SHALL IMMEDIATELY MAKE ALL REMEDIAL ACTIONS NECESSARY.
- 2. KEEP THE PROJECT AREA AND SURROUNDING AREA FREE FROM DUST NUISANCE.
- 3. PROVISIONS SHALL BE MADE TO PREVENT SURFACE WATERS FROM DAMAGING THE CUT FACE OF AN EXCAVATION OR THE SLOPED SURFACES OF A FILL. FURTHERMORE, PROVISIONS SHALL BE MADE TO PREVENT SEDIMENT-LADEN RUNOFF FROM LEAVING THE SITE.
- 4. ALL SLOPED AND EXPOSED AREAS SHALL BE SODDED OR PLANTED AS SOON AS FINAL GRADES HAVE BEEN ESTABLISHED. PLANTING SHALL NOT BE DELAYED UNTIL ALL GRADING WORK HAS BEEN COMPLETED. GRADING TO FINAL GRADE SHALL BE CONTINUOUS, AND ANY AREA WITHIN WHICH WORK HAS BEEN INTERRUPTED OR DELAYED SHALL BE
- 5. THE LIMITS OF THE AREA TO BE GRADED SHALL BE FLAGGED BEFORE THE COMMENCEMENT OF THE GRADING WORK.
- 6. ALL GRADING OPERATIONS SHALL BE PERFORMED IN CONFORMANCE WITH THE APPLICABLE PROVISIONS OF THE WATER POLLUTION CONTROL AND WATER QUALITY STANDARDS CONTAINED IN THE LATEST CALTRANS STORM WATER QUALITY HANDBOOKS.

- STOCKPILING NOTES
 - 1. IF THE STOCKPILING WORK INVOLVES CONTAMINATED SOIL, THEN ALL STOCKPILING WORK SHALL BE DONE IN CONFORMANCE WITH APPLICABLE STATE AND FEDERAL REQUIREMENTS.
- E. GRUBBING AND CLEARING NOTES
 - 1. WHERE APPLICABLE AND FEASIBLE THE MEASURES TO CONTROL EROSION AND OTHER POLLUTANTS SHALL BE IN PLACE BEFORE ANY GRUBBING WORK

ESIGNED BY DRAWN BY A. SHIELDS HECKED BY A. BOONE CHARGE A. BOONE DATE BY CHK APP DESCRIPTION 02/01/2012

PROPOSED PRELIMINARY DESIGN

NOT FOR CONSTRUCTION

2020 L Street, Suite 300 Sacramento, CA 95811 **CH2MHILL**

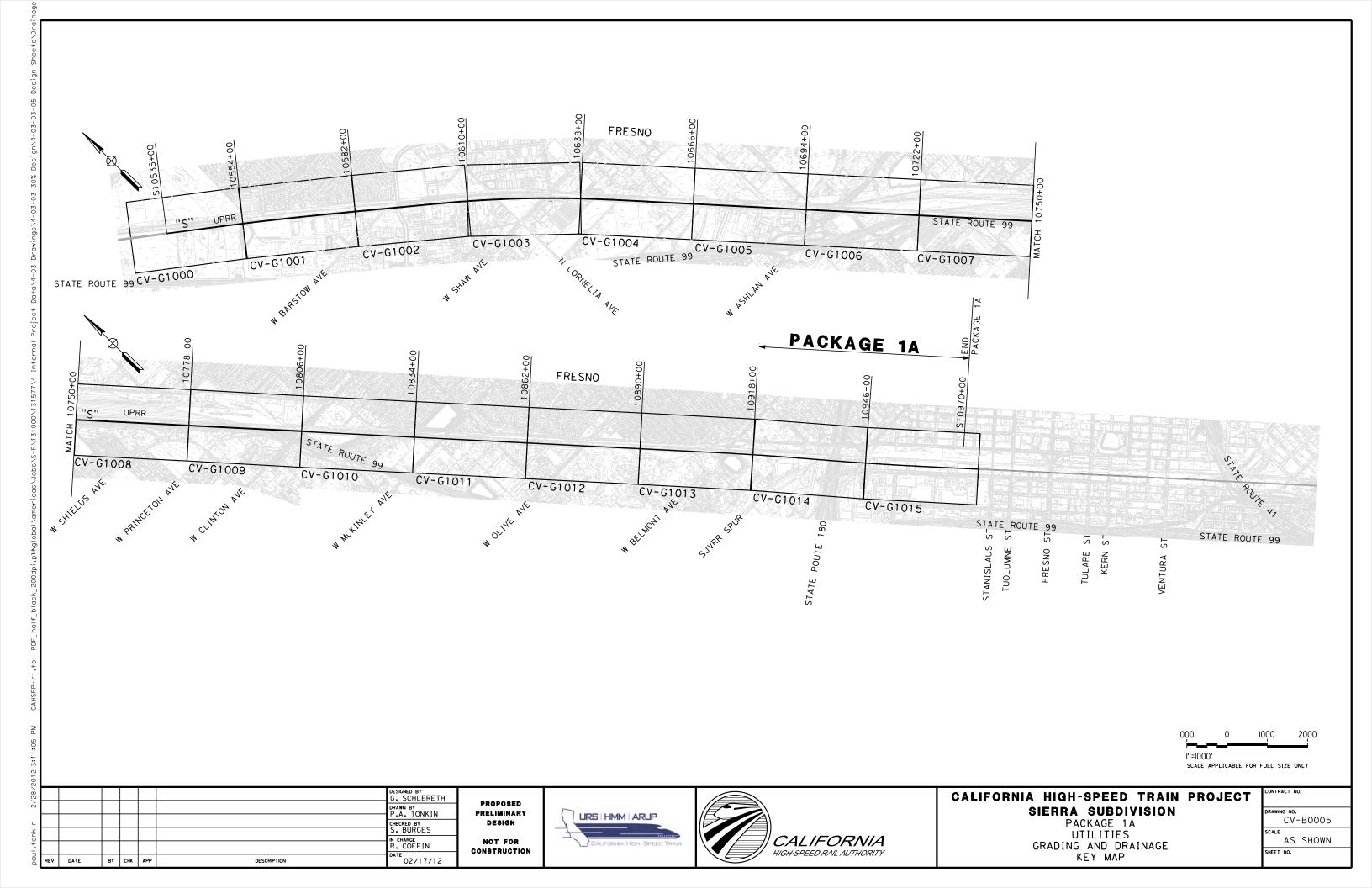


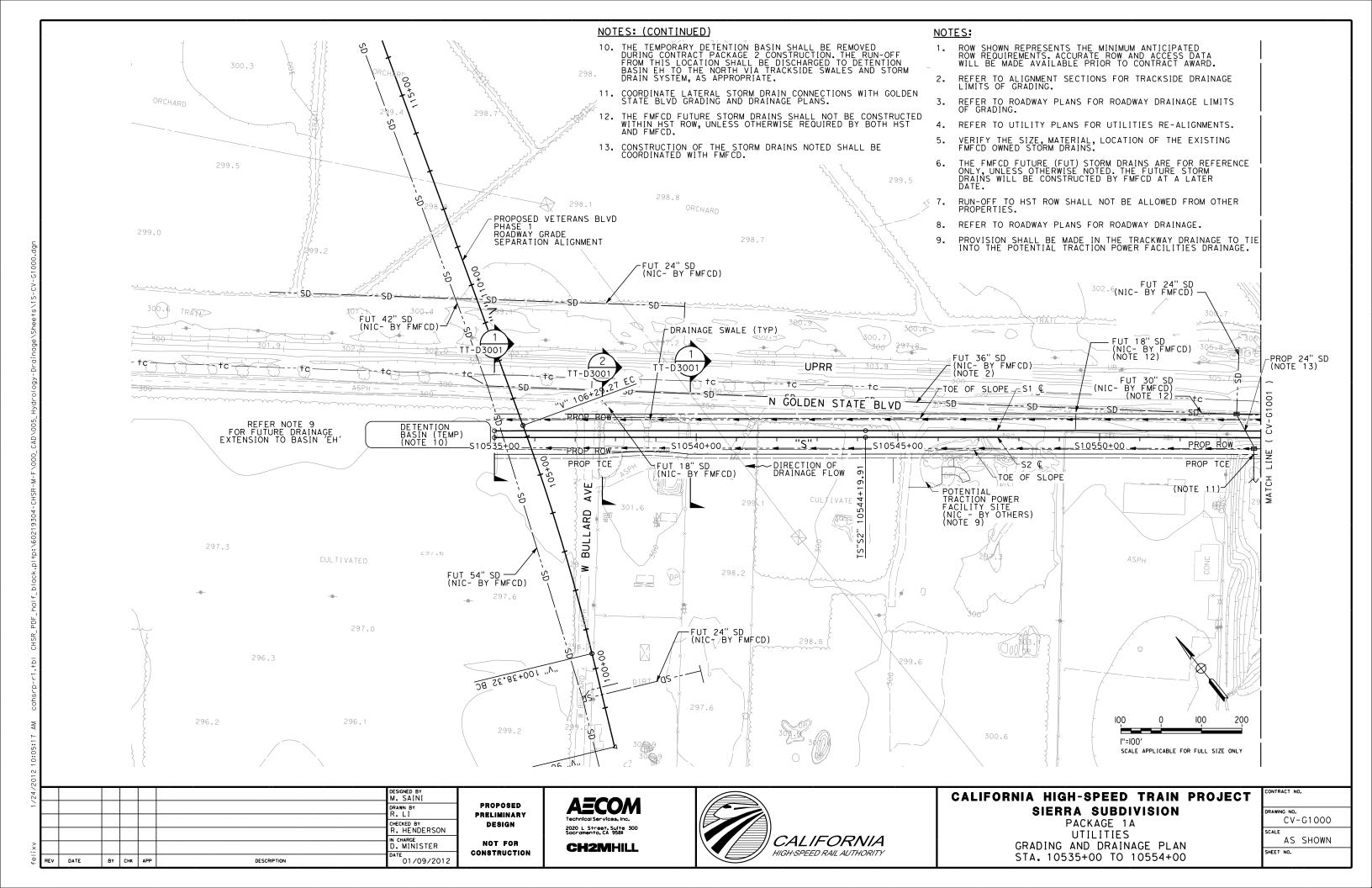
CALIFORNIA HIGH-SPEED TRAIN PROJECT SIERRA SUBDIVISION

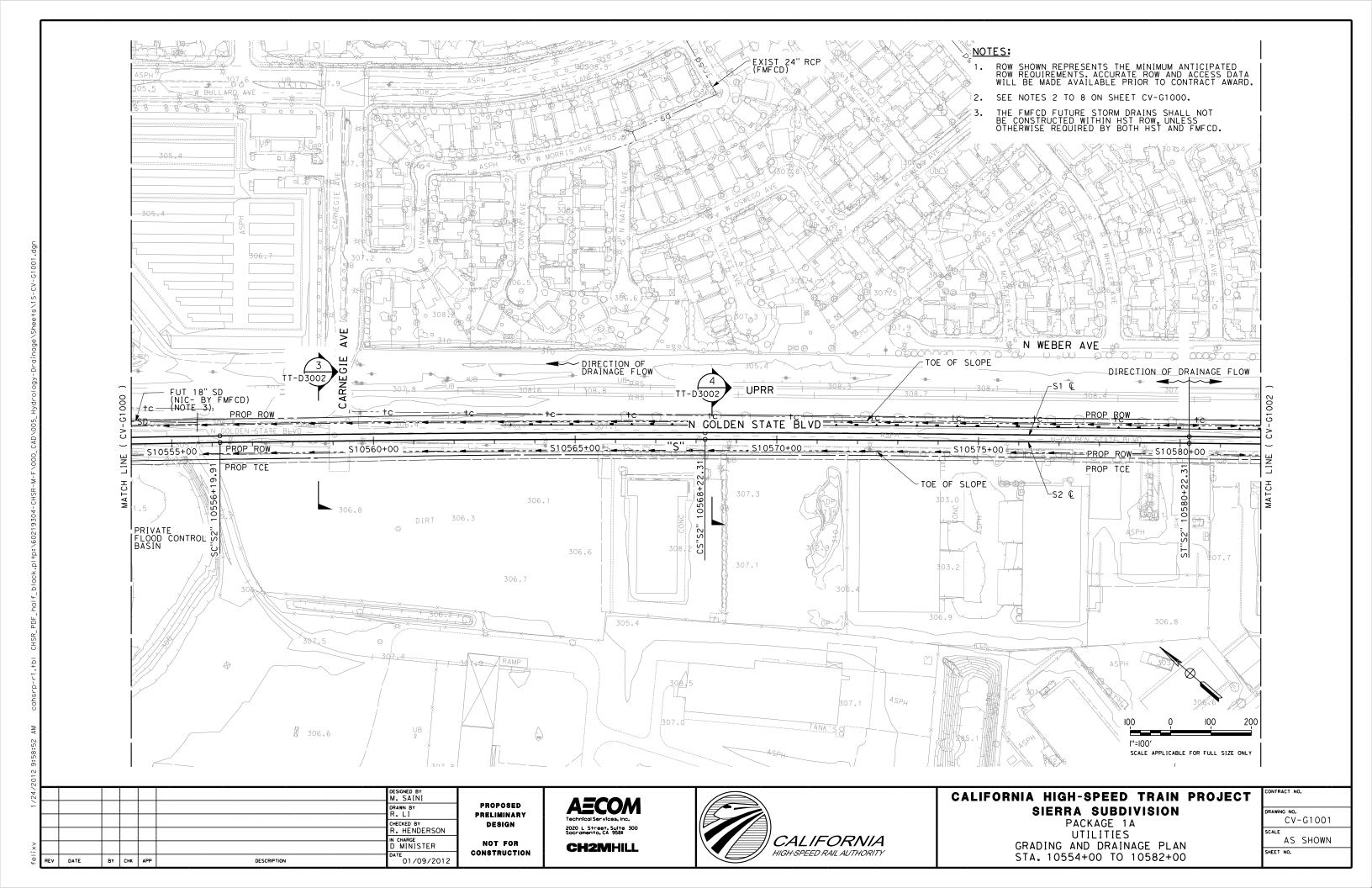
PACKAGE 1A GRADING AND DRAINAGE PLAN GENERAL NOTES

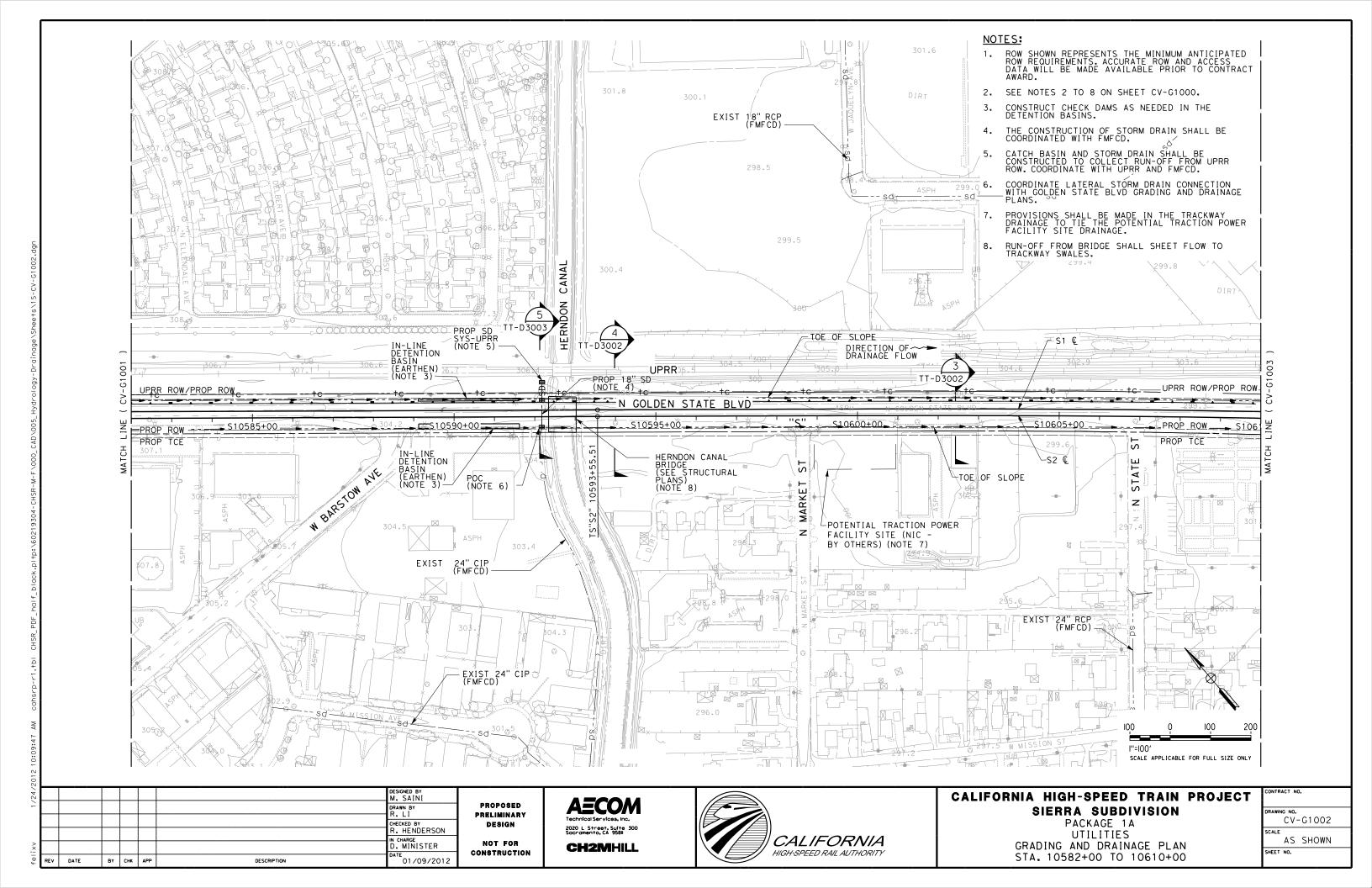
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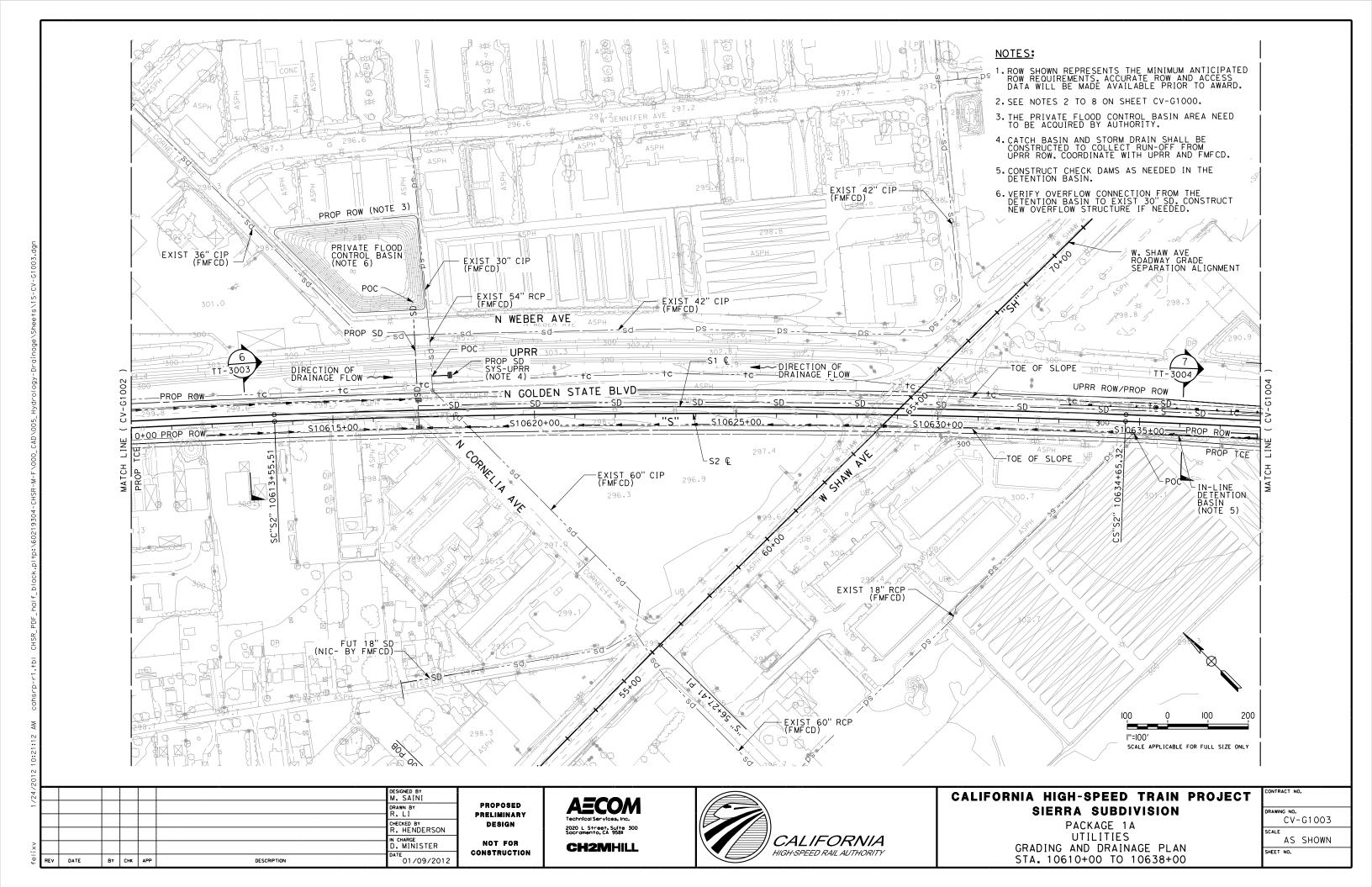
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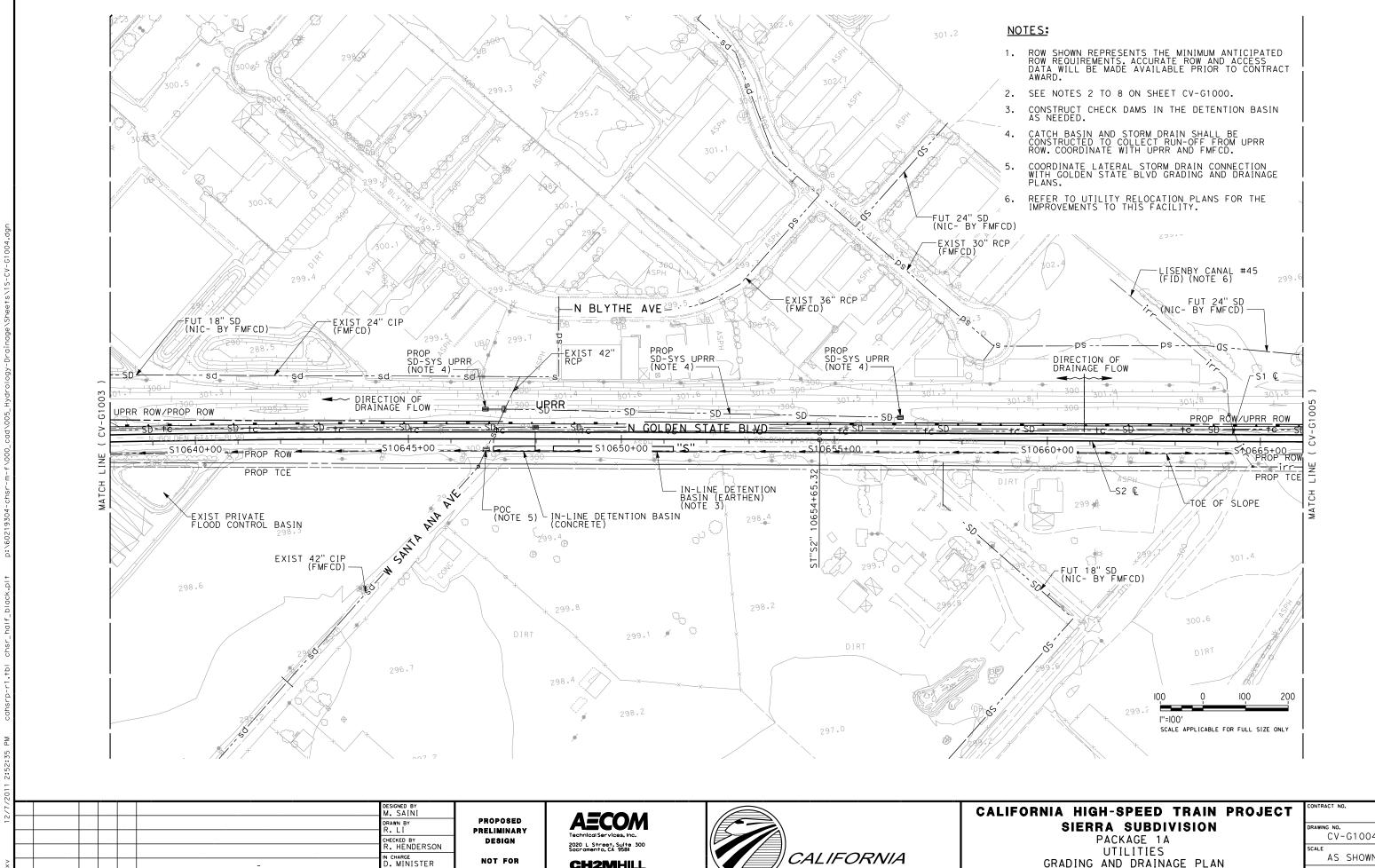












NOT FOR CONSTRUCTION

12/08/11

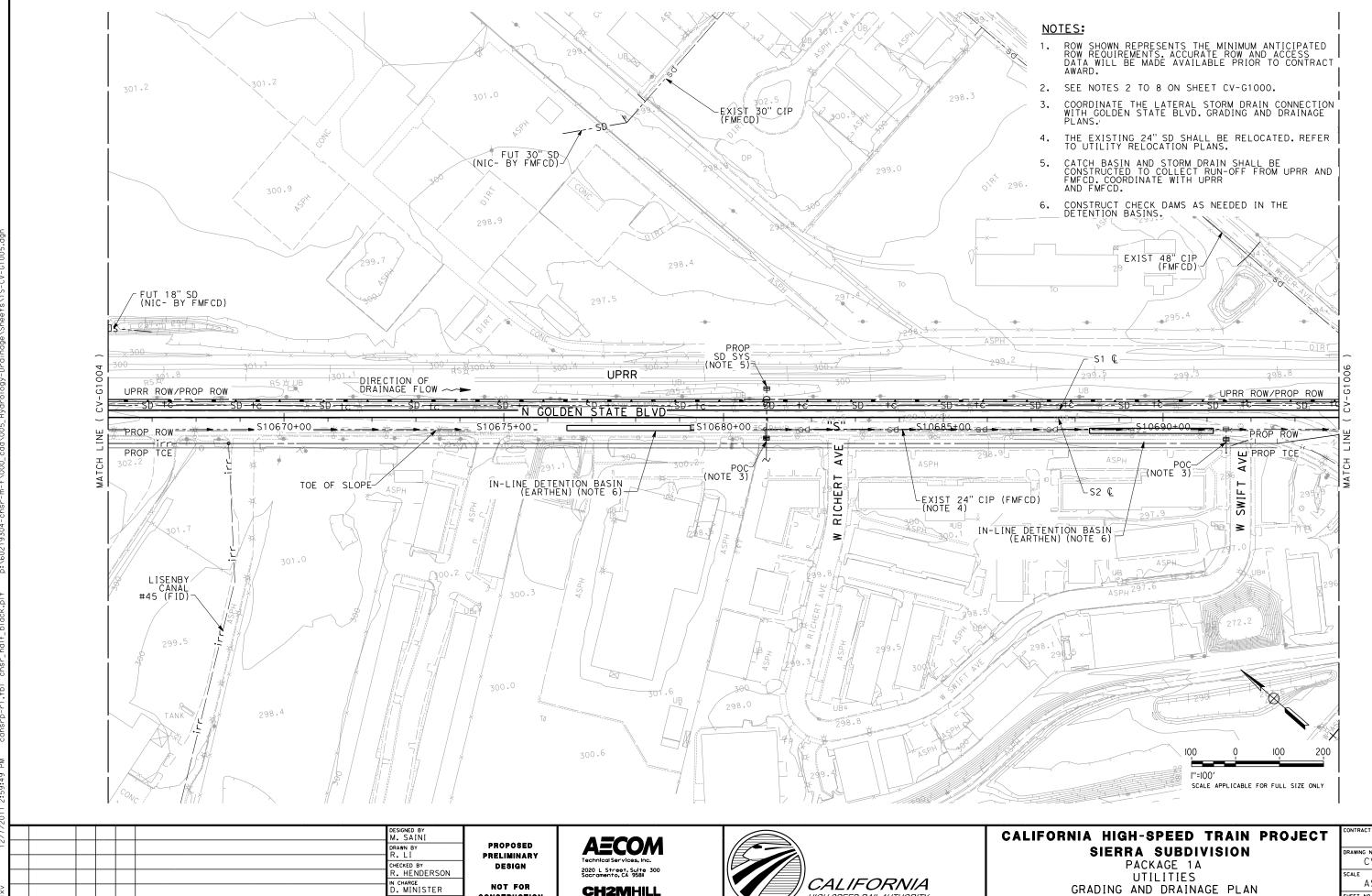
BY CHK APP

DESCRIPTION

CH2MHILL



GRADING AND DRAINAGE PLAN STA. 10638+00 TO 10666+00 CV-G1004 AS SHOWN



HIGH-SPEED RAIL AUTHORITY

CONSTRUCTION

12/08/11

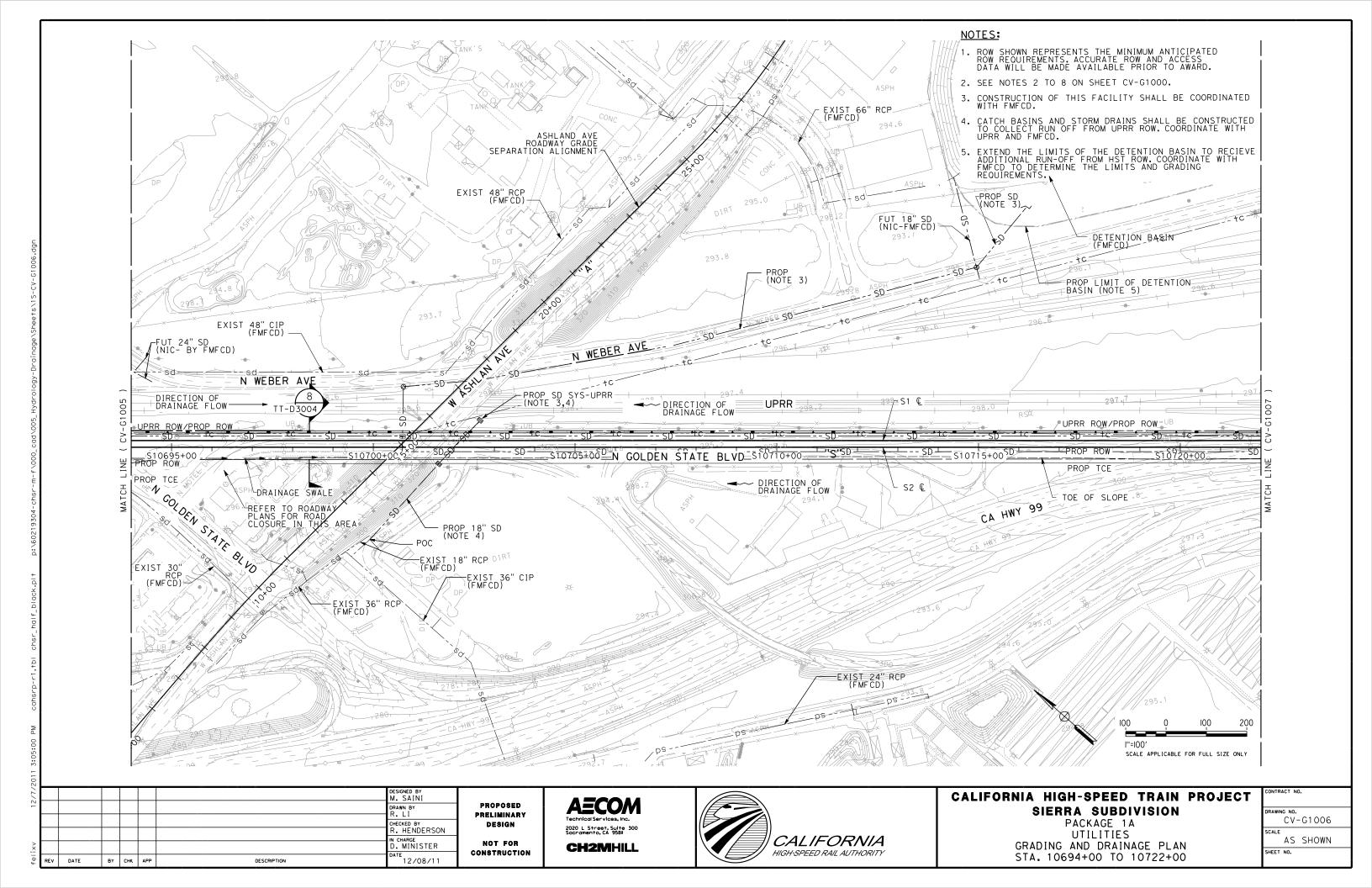
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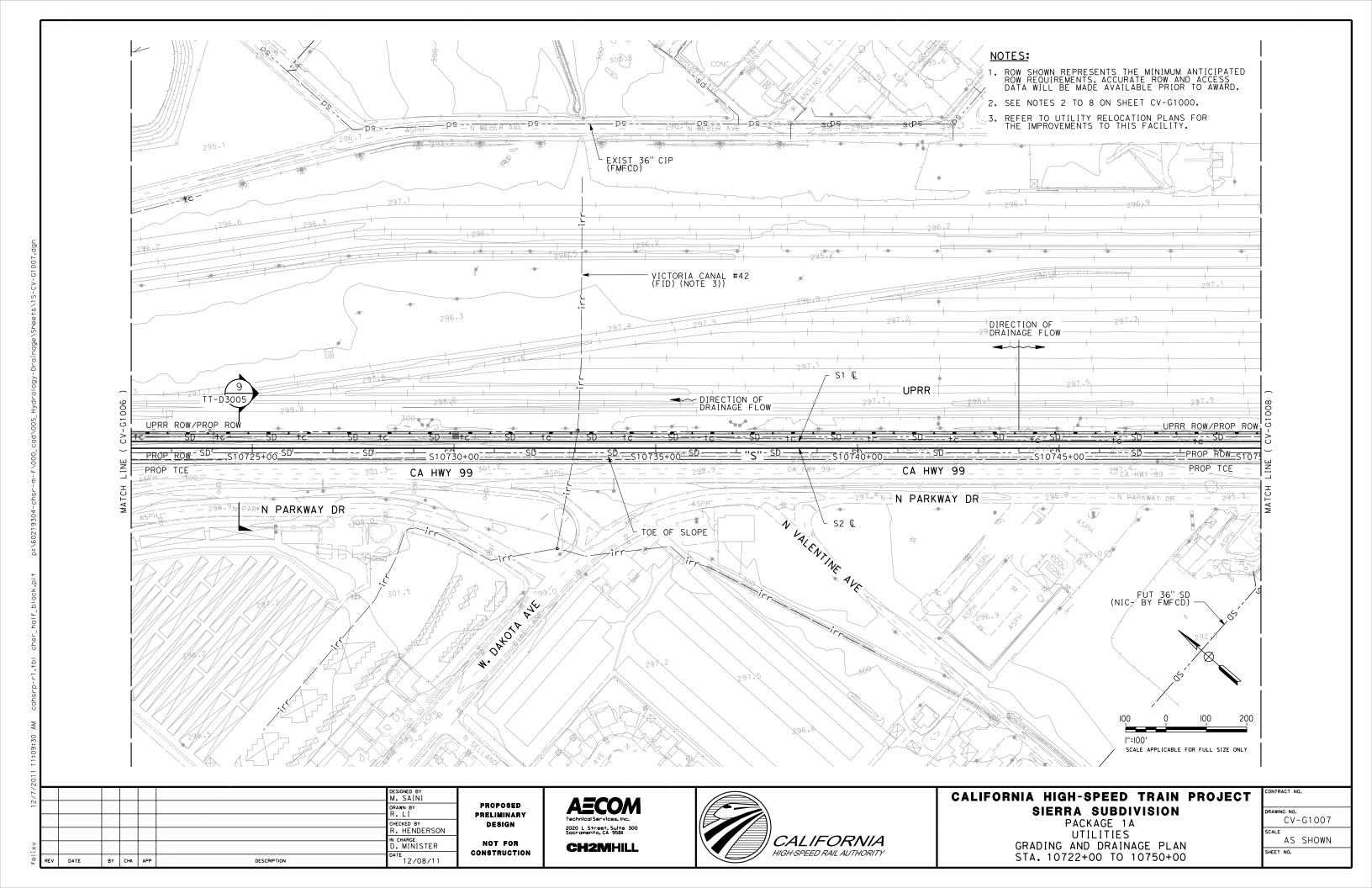
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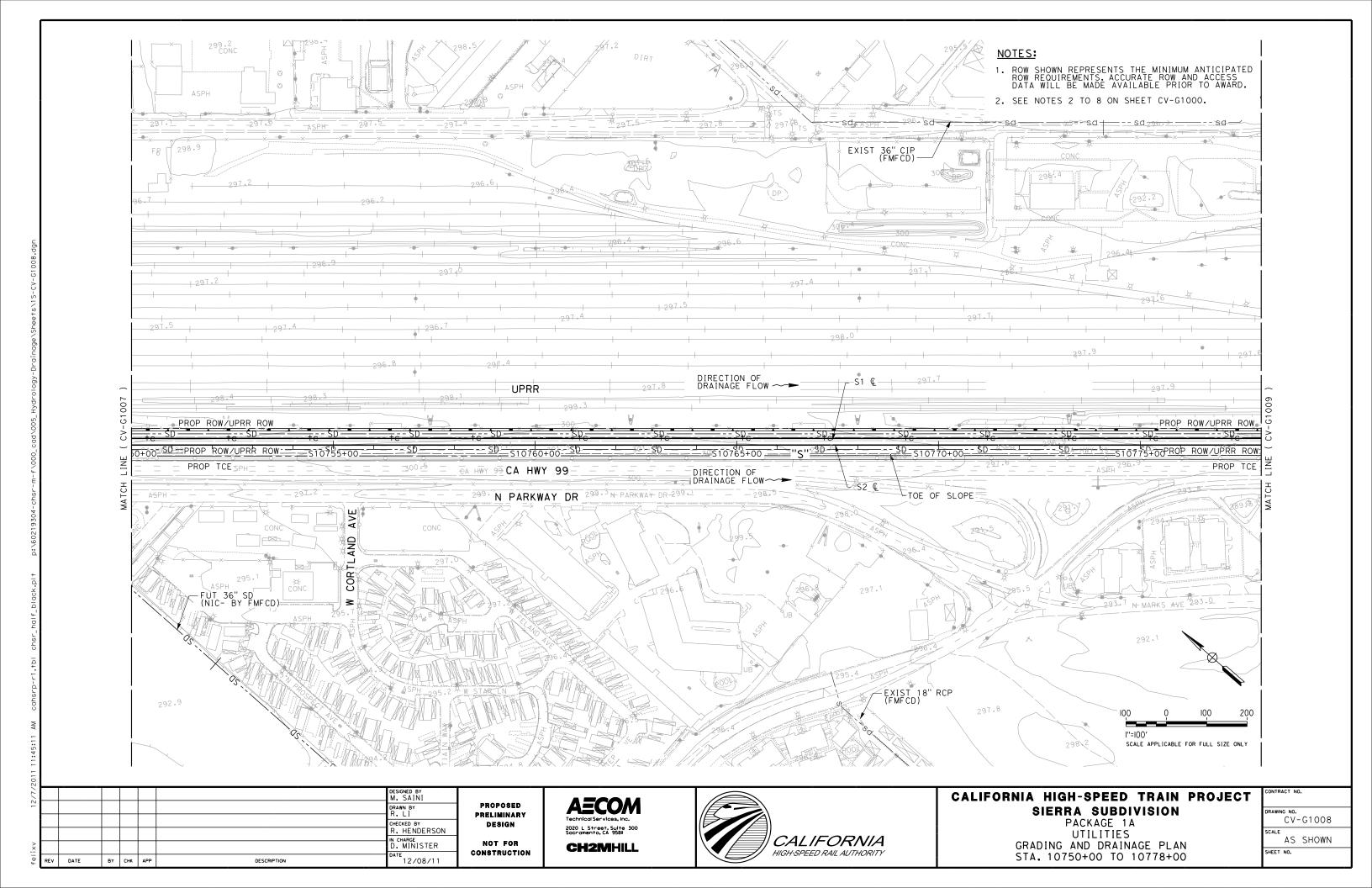
STA. 10666+00 TO 10694+00

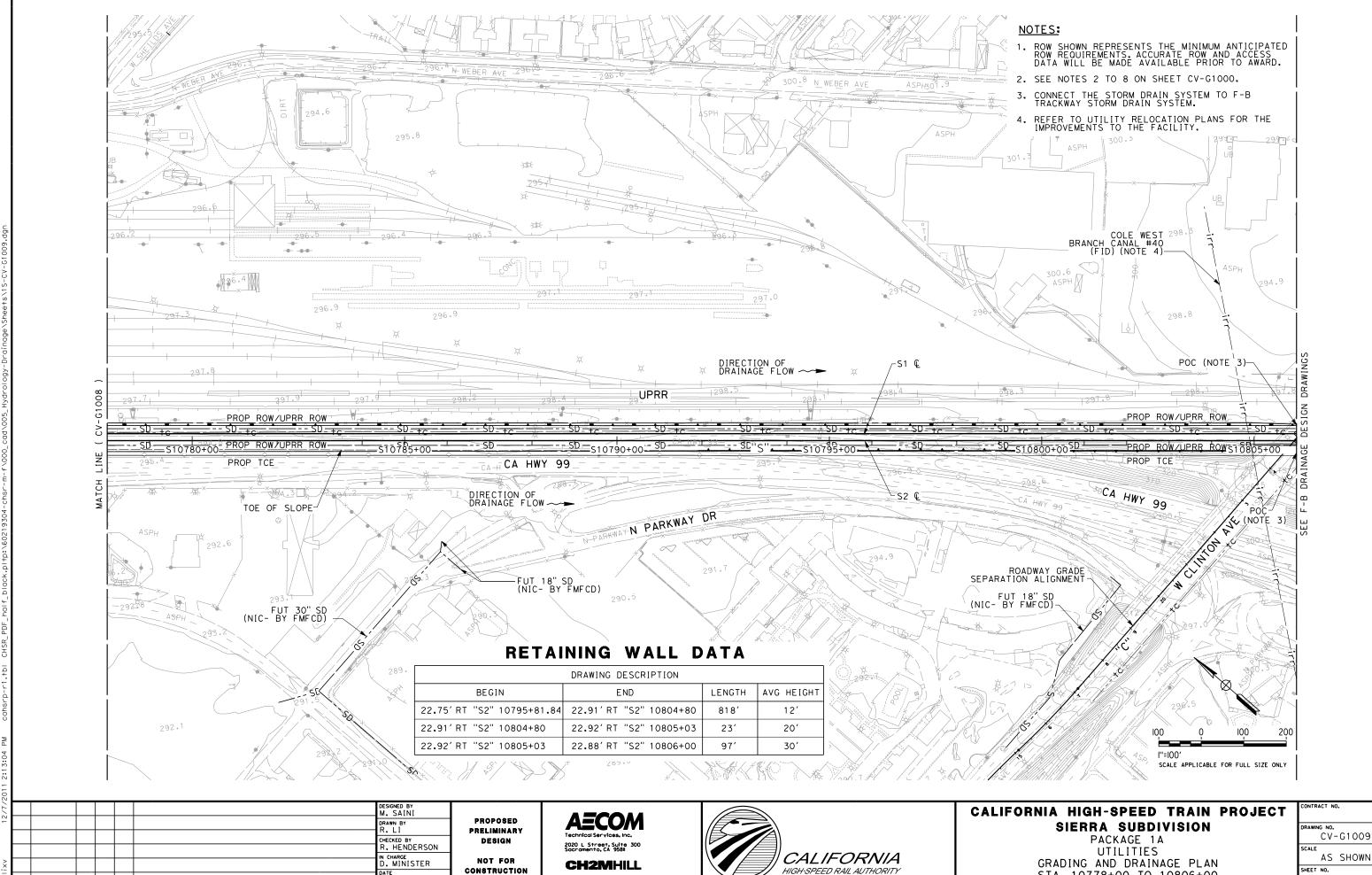
CV-G1005

AS SHOWN SHEET NO.







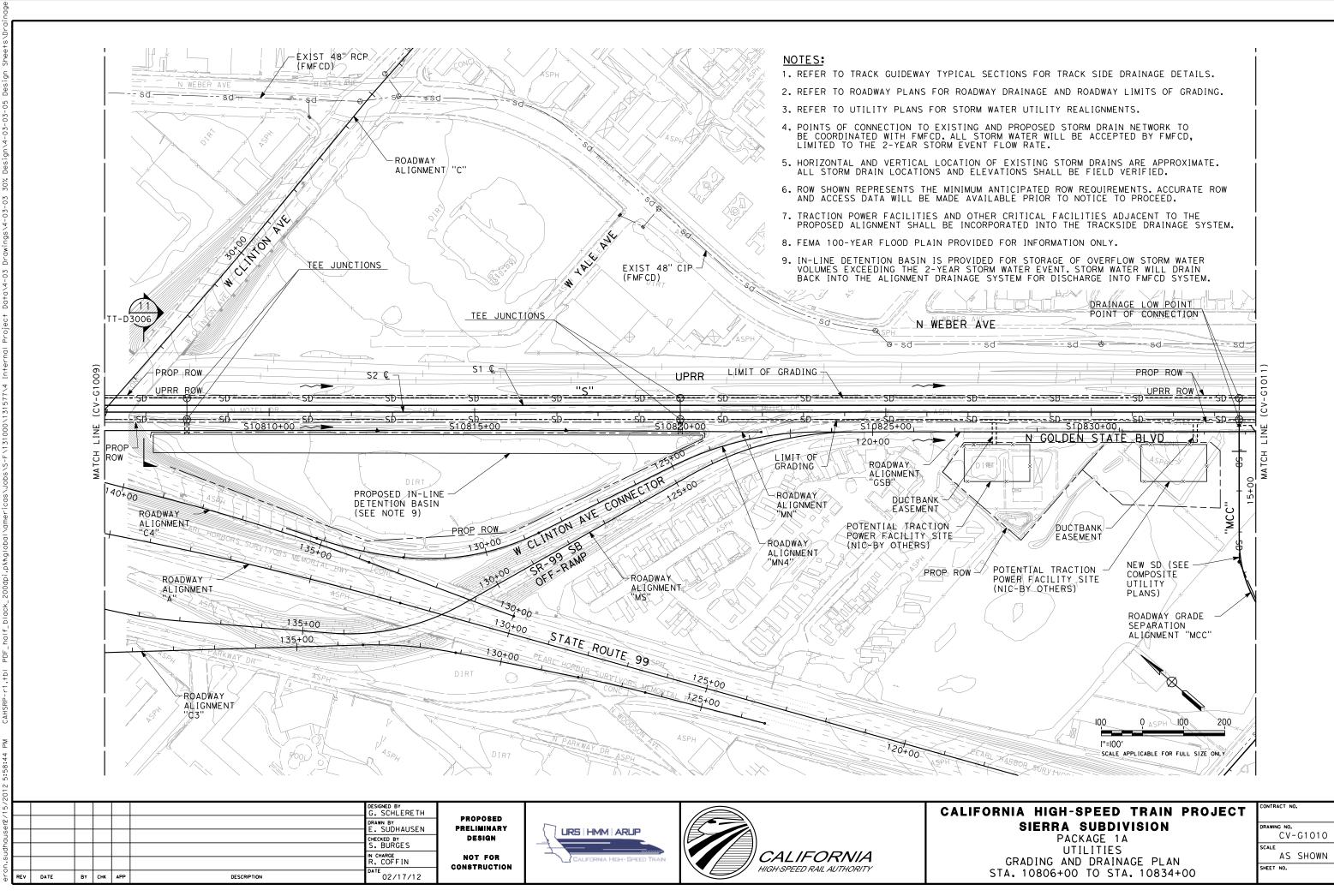


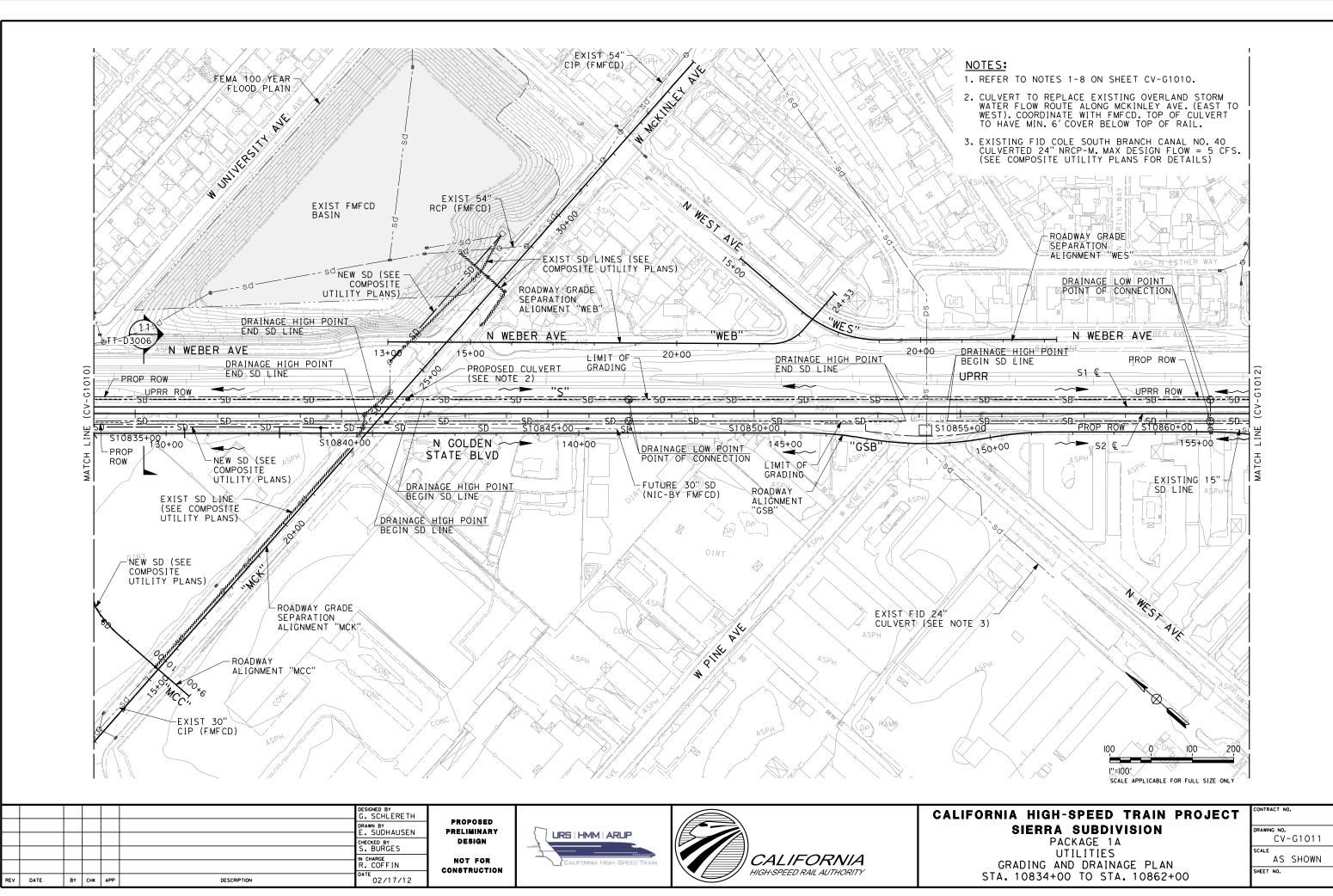
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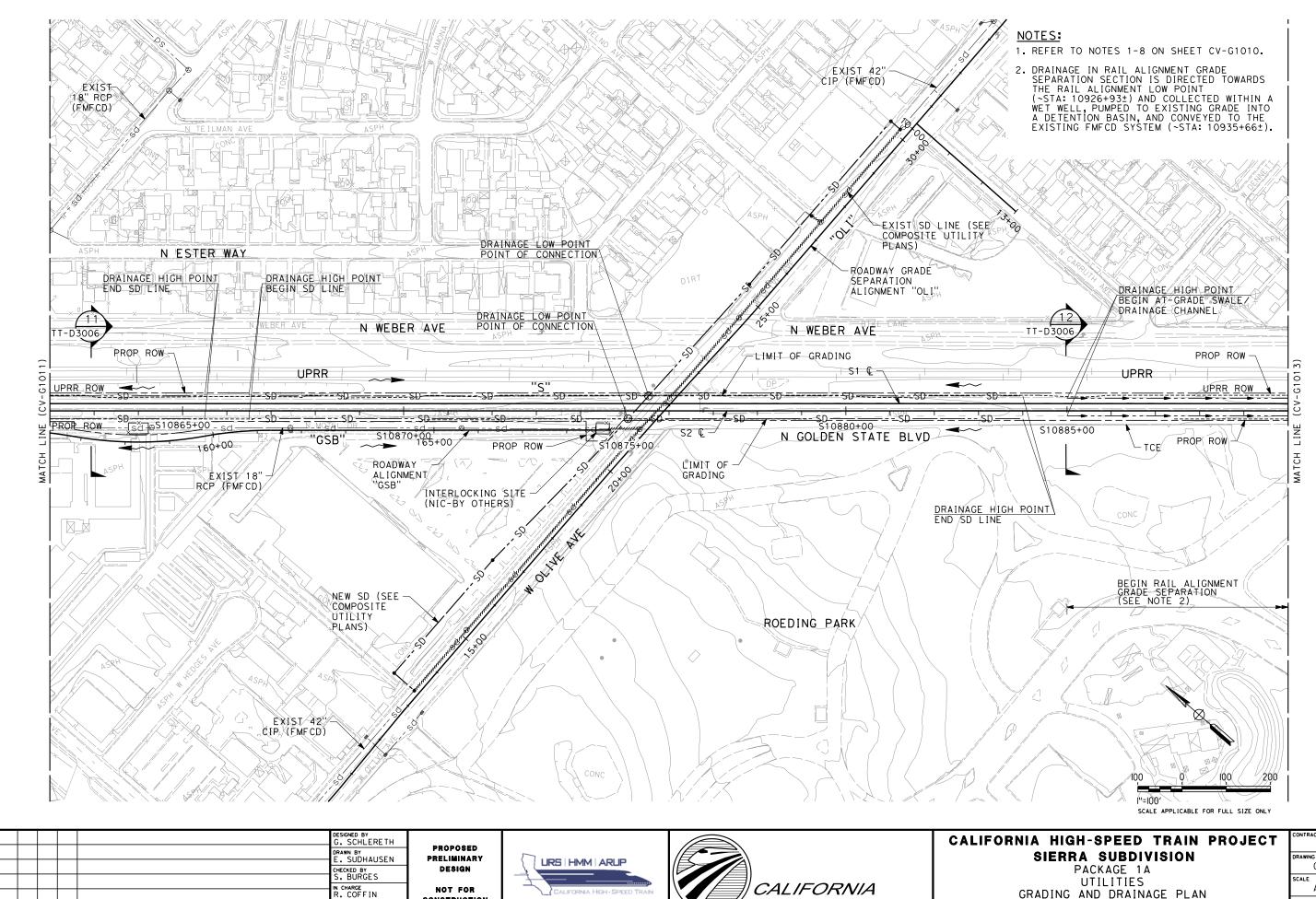
STA. 10778+00 TO 10806+00

BY CHK APP

DESCRIPTION







HIGH-SPEED RAIL AUTHORITY

CONSTRUCTION

02/17/12

BY CHK APP

DESCRIPTION

CV-G1012 AS SHOWN

GRADING AND DRAINAGE PLAN STA. 10862+00 TO STA. 10890+00

